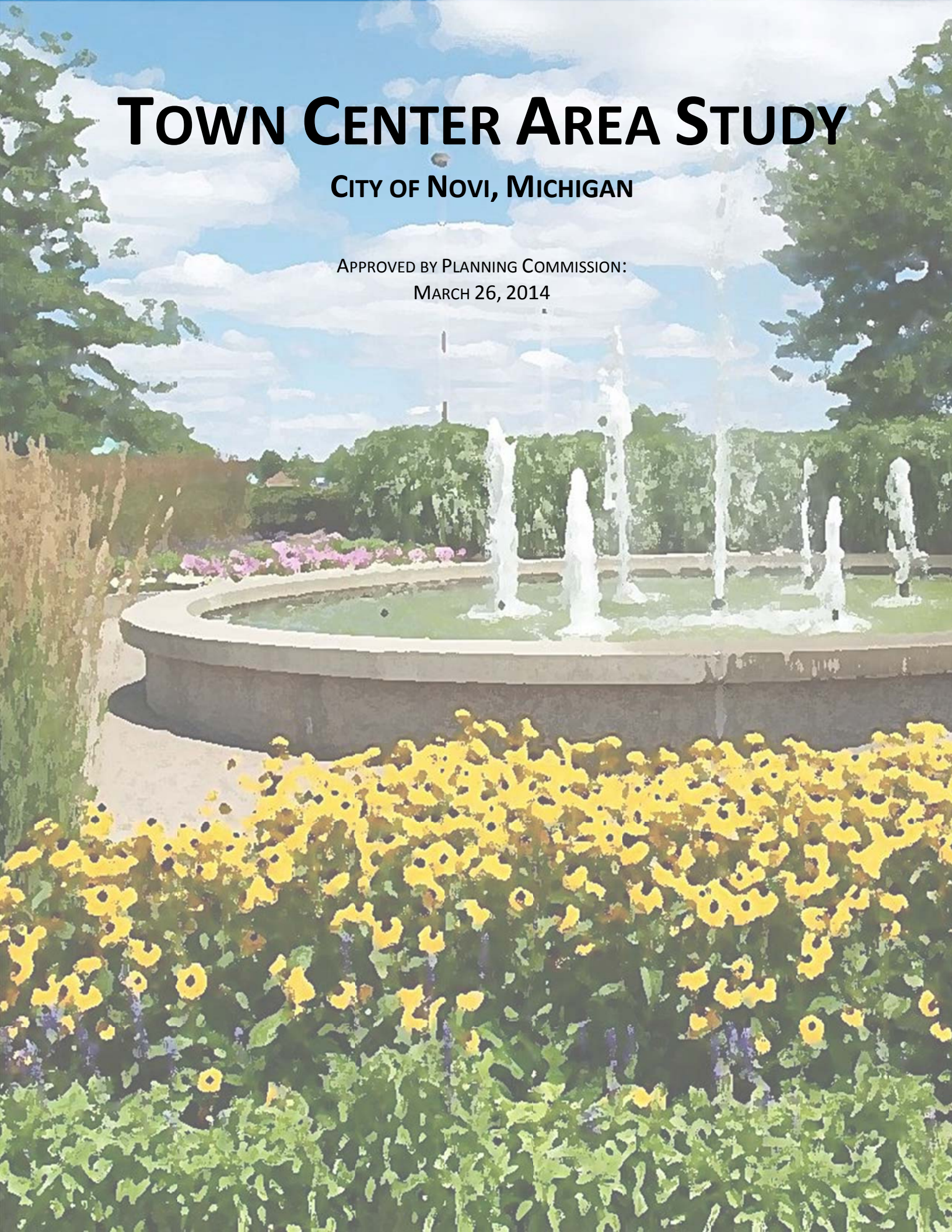


# TOWN CENTER AREA STUDY

CITY OF NOVI, MICHIGAN

APPROVED BY PLANNING COMMISSION:

MARCH 26, 2014





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# ACKNOWLEDGEMENTS

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# EXECUTIVE SUMMARY

Novi's Town Center area has been the focal point of a number of studies over the past twenty years. This most recent effort revisits the Town Center, evaluates its current condition, and makes recommendations for further changes and growth. These recommendations will be considered by the Planning Commission as it determines needed updates to the City's current Master Plan.

This study was commissioned in April, 2013, and is designed to evaluate and make recommendations on four main topics:

1. Land use
2. Zoning
3. Design guidelines
4. Wayfinding

## STUDY PROCESS

The study team began researching existing planning documents and other background data relating to the project area to understand current thinking on development in the Town Center area. To facilitate this process, the study area was broken down into ten subareas, as shown in the map on the following page.

This was followed by a public-input process which included: stakeholder interviews, a public drop-in open house, and an on-line survey to gather comments and opinions about the four main topics from property owners, managers, realtors, and residents. Next, the team evaluated the physical characteristics of the study area through a parcel-by-parcel evaluation and a field visit. Using this information, the team developed an overall vision for the Town Center area, guiding principles, and recommendations for the Planning Commission to consider regarding each topic.

## BACKGROUND

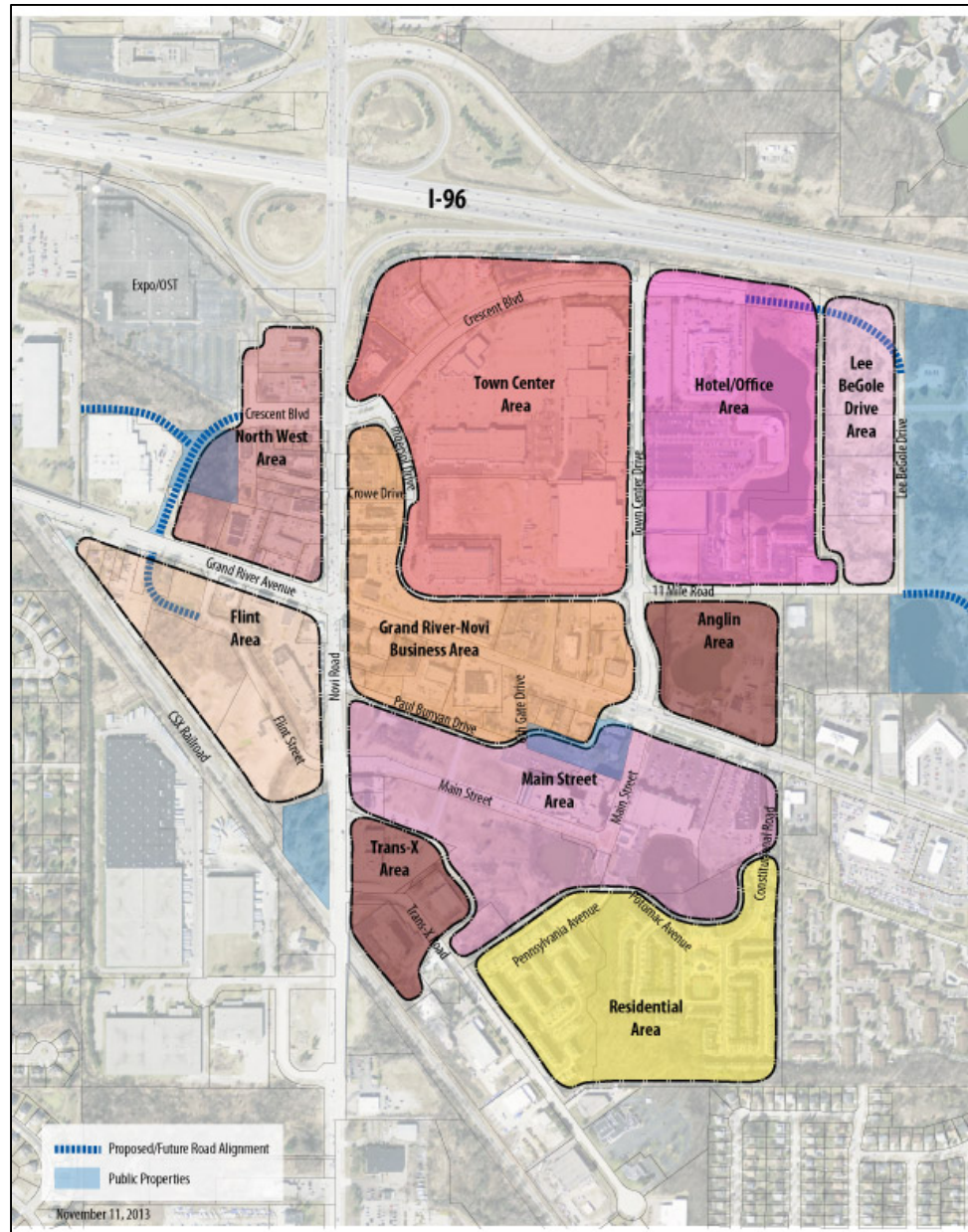
Background investigations of the Town Center area looked at existing and municipal land uses, economic trends of the area, zoning districts, vehicular circulation, and pedestrian amenities. Each subarea was assessed against these topics to present a clear picture of what has influenced development to this point. Some important facts were uncovered:

1. The study area contains a varied mix of existing land uses, including residential, municipal, office, commercial, industrial, and vacant parcels.
2. The study area is a regional shopping attraction and an important economic contributor to the City's tax revenues. It also offers a significant number of job opportunities.
3. Zoning districts in the study area are limited, but include the Town Center (TC) and Town Center One (TC-1) districts, which allow for a wide variety of commercial, office, residential and other land uses.

**Vision:**  
*“Development of the Town Center Study Area will create a dynamic, attractive city core that provides residents and visitors with unique opportunities to participate in active community life and meet their needs for goods, services, housing, and entertainment.”*

4. Vehicular circulation within the study area includes both arterial and local roadways. Important improvements to create a “ring road” are currently planned by the City, including the extension of Crescent Boulevard and the re-alignment of Flint Street.
5. Pedestrian amenities and features abound within the study area; however, not all are appropriately located or connected to the adjacent land uses, which limits their usefulness.

FIGURE 1. STUDY SUBAREAS





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## VISION AND GUIDING PRINCIPLES

To determine a vision and guiding principles for the study area, the project team involved property owners, property managers, realtors, and residents. These stakeholders had several opportunities to voice their opinions about land use, zoning, design, and wayfinding in the study area. Many ideas were generated, and summaries of each meeting are included in the appendix. Goals and objectives listed in the current and previous Master Plan documents were also referenced.

All of this information was synthesized to come up with an overall vision and guiding principles for the Town Center area:

### **Overall Vision:**

*Development of the Town Center Study Area will create a dynamic, attractive city core that provides residents and visitors with unique opportunities to participate in active community life and meet their needs for goods, services, housing, and entertainment.*

### **Guiding Principles: Land Use**

1. Land uses should be mixed and work together to establish a city center for Novi which provides an attractive, mixed-use hub of activity.
2. Residential land uses should encourage development of varied styles of residential development, including mid-rise, townhouse, loft-styled or others to attract young professionals, families and seniors, and supply patrons for nearby businesses
3. Private and public development should include community gathering spaces that provide venues for events that attract visitors, as well as green space for area residents.
4. Existing natural features (streams, wetlands, ponds), while limited, should be incorporated into private and public developments to create an attractive environment for pedestrian-scaled uses.

### **Guiding Principles: Zoning**

1. Zoning districts should allow mixed uses, but be flexible enough to accommodate uses that are economically feasible and market based.
2. Zoning should establish guiding design principles or form-based standards to ensure quality design.

- 
3. Zoning should continue to require development of pedestrian-scaled site amenities that are logically placed and provide functionality for the use.

### **Guiding Principles: Design Guidelines**

1. Design standards should provide guidance, but be flexible enough to allow for various aesthetic interpretations.
2. Site design standards should balance pedestrian and vehicular amenities.
3. Developments should be required to create and maintain high quality, attractive site environments, including decorative lighting, paving, walls, fences, pedestrian amenities, and diversified landscaping (trees, shrubs, perennials and groundcovers).
4. Street furnishings should coordinate across the study area by using consistent materials, colors, or other elements.

### **Guiding Principles: Vehicular Circulation**

1. Appropriately scaled gateway features should be installed at the boundaries of the “town center” to communicate to drivers that they are entering a special place.
2. Motorists visiting the area (vs. driving through) should be accommodated with alternative driving routes.
3. Staff should work with the Road Commission for Oakland County to evaluate appropriate speed limits and traffic management strategies along Grand River Avenue and Novi Road to accommodate the “town center” vision.
4. The City should continue its efforts to complete the ring road (Main Street, Town Center Drive, Lee BeGole Drive, Crescent Boulevard, and Flint Street).
5. Opportunities for transit should be accommodated in and around the Town Center study area.

### **Guiding Principles: Pedestrian Amenities**

1. Facilities should be provided throughout the study area to allow pedestrians safe and comfortable walking/biking environments, including crossing Grand River Avenue and Novi Road, and pedestrian movement to and from adjoining residential areas.
2. The City should continue to require streetscape improvements along Grand River Avenue and Novi Road and provide pedestrian amenities.

## LAND USE AND MASTER PLAN RECOMMENDED UPDATES

The overall intent of the Town Center area is to serve as a mixed-use, pedestrian-oriented focal point for the City of Novi. In general, the Town Center area should allow broad flexibility and have a variety of uses including retail, commercial, office, residential and institutional uses, and open space, with the Grand River/Novi Business Area and the Main Street Area serving as the “main hub.” The recommended land uses, and suggestions on form/design, pedestrian orientation and streetscape appearance were provided.

TABLE 1. RECOMMENDED LAND USE AND MASTER PLAN UPDATES

Subarea	Intent	Preferred Land Uses*	Form/Design
North West Area	<ul style="list-style-type: none"> <li>Transition to Grand River/Novi Road Business Area</li> <li>Gateway into Town Center</li> <li>Use Middle Rouge in site design</li> </ul>	<ul style="list-style-type: none"> <li>Restaurants, coffee shops, retail/office</li> </ul>	<ul style="list-style-type: none"> <li>Pedestrian-oriented with small front/side setbacks.</li> <li>Shared parking located at rear or side of building.</li> </ul>
Flint Street Area	<ul style="list-style-type: none"> <li>Provide mix of office and commercial</li> <li>Use Middle Rouge as site amenity</li> </ul>	<ul style="list-style-type: none"> <li>Office/research/technology abutting railroad</li> <li>Abutting Grand River/Novi:                             <ul style="list-style-type: none"> <li>- Restaurants</li> <li>- Outdoor cafes (near river)</li> <li>- Retail &amp; office uses</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>Maintain attractive architecture with reduced front/side setbacks.</li> <li>Shared parking located at rear or side of building.</li> <li>Integrate Middle Rouge as focal point with walkways or pedestrian plazas.</li> </ul>
Trans X Area	<ul style="list-style-type: none"> <li>Transitional to Main Street and Grand River/Novi Business areas</li> <li>Existing industrial uses transformed into office and retail</li> <li>Retail uses along Novi Road</li> </ul>	<ul style="list-style-type: none"> <li>Public market with specialty foods, produce and gift items</li> <li>Restaurants</li> </ul>	<ul style="list-style-type: none"> <li>Buildings fronting Novi Road with high-quality architecture and reduced setbacks.</li> </ul>
Main Street Area	<ul style="list-style-type: none"> <li>Mixed-use and City Center-style development</li> <li>Commercial and residential uses that include open spaces and pedestrian amenities</li> </ul>	<ul style="list-style-type: none"> <li>Residential uses on the south side of Main St.</li> <li>Mixed-use developments at corners (Main St./Novi Rd.; Main St./Grand River Ave.)</li> </ul>	<ul style="list-style-type: none"> <li>Dense, multi-family housing as loft-style condominiums, townhouses, or mixed-use commercial/office/residential buildings.</li> <li>Allow first floor residential</li> <li>Taller buildings (3-5 stories) at corners</li> <li>Outdoor events space</li> </ul>
Grand River/Novi Business Area	<ul style="list-style-type: none"> <li>Serves as “spine” of overall Town Center area</li> <li>Dense, walkable, retail/office/restaurant area</li> <li>Easily accessible from other subareas with ample pedestrian facilities</li> </ul>	<ul style="list-style-type: none"> <li>Retail uses</li> <li>Restaurants</li> <li>Outdoor cafes</li> </ul>	<ul style="list-style-type: none"> <li>Pedestrian-oriented with small front/side setbacks</li> <li>Landscaping used to screen parking lots</li> <li>Consistent lighting</li> <li>Pedestrian refuge islands at Grand River/Main St. intersection</li> <li>Reduced vehicle speeds</li> <li>Connections between existing screen walls and businesses.</li> </ul>

\*These preferred land uses are *in addition* to the current mix of land uses included in the TC Commercial, TC Gateway, and Office Commercial land use categories.





TABLE 1. RECOMMENDED LAND USE AND MASTER PLAN UPDATES (CONT.)

Subarea	Intent	Preferred Land Uses*	Form/Design
Town Center Area	<ul style="list-style-type: none"> <li>Regional commercial center</li> <li>Pedestrian accessibility via connections to the Grand River/Novi Business Area, Hotel/Office area and Anglin Area.</li> </ul>	<ul style="list-style-type: none"> <li>Large-format retail uses</li> <li>Shopping centers/general retail uses</li> <li>Restaurants/outdoor cafes</li> </ul>	<ul style="list-style-type: none"> <li>Maintain current form with unifying brick façade and attractive landscaping.</li> <li>Provide pedestrian connections and amenities within shopping centers and along roadways</li> </ul>
Anglin Area	<ul style="list-style-type: none"> <li>Serve as an eastern gateway into the core area</li> <li>Pedestrian-orientation</li> <li>Development should include the pond as a site amenity</li> </ul>	<ul style="list-style-type: none"> <li>Retail uses</li> <li>Professional offices</li> <li>Research and technology uses</li> </ul>	<ul style="list-style-type: none"> <li>Pedestrian-oriented with reduced front setbacks.</li> <li>Pedestrian paths connecting with core area</li> <li>Pond used as a focal point for new development.</li> </ul>
Hotel/Office Area	<ul style="list-style-type: none"> <li>Promote the Town Center area’s regional appeal via visibility from I-96</li> <li>Increase pedestrian connections to nearby uses</li> <li>Extend Lee BeGole Drive to complete ring road</li> </ul>	<ul style="list-style-type: none"> <li>Mid-rise residential</li> <li>Hotels/motels</li> <li>Extended stay hotels</li> </ul>	<ul style="list-style-type: none"> <li>Large-lot hotel, office research and technology developments</li> <li>Pedestrian-scaled amenities that connect to the commercial core</li> <li>Best practices around existing pond</li> </ul>
Lee BeGole Drive Area	<ul style="list-style-type: none"> <li>Opportunity for municipal services or office, research &amp; technology.</li> <li>Outside storage permitted</li> </ul>	<ul style="list-style-type: none"> <li>Municipal services</li> <li>Office, research and technology</li> </ul>	<ul style="list-style-type: none"> <li>Uses should connect to Town Center core</li> <li>Pond used as a focal point for new development</li> <li>Best practices around existing pond</li> </ul>

\*These preferred land uses are *in addition* to the current mix of land uses included in the TC Commercial, TC Gateway, and Office Commercial land use categories.

## RECOMMENDED ZONING UPDATES

Changes to the Zoning Ordinance include recommendations for the Town Center (TC) and Town Center 1 (TC-1) districts, as well as adding a “mid-rise” residential use to the Office Service Commercial (OSC) district.

### Recommendations for the TC and TC-1 Zoning Districts

The overall intent of the TC Districts to create mixed-use, “pedestrian accessible, commercial service district(s)” should be maintained. However, the revised TC Districts should allow more flexibility in terms of use and dimensions in order to realistically achieve such goals. The list below outlines the proposed changes:

**Intent:** Recommend that the intent of the TC and TC-1 Districts should be modified to clarify them, and de-emphasize discouraging development of separate, off-street parking facilities.

**Permitted Uses:** Recommend that the list of permitted uses be modified to clarify the definition of theater as a “performing arts” theater, add the following as permitted uses: pharmacies without drive-through services, extended-stay hotels, and green markets. Remove standard that prohibits

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residential uses on the first floor in buildings that front onto a local collector within the TC-1 District, and add regulation that prohibits *individual* garages on a front-facing façade.

**Special Uses:** Recommend adjusting regulation that prohibits clustering of drive-through restaurants within TC-1 District, and add incubator/prototype development workshops and pharmacies with drive-throughs as a special use in the TC-1 District. Lastly, add an outdoor farmers' market as a permitted or special use.

**Schedule of Regulations:** Recommend that the schedule of regulations be modified to increase maximum gross leasable floor area (GLA) within TC-1 from 7,500 s.f. to 13,000 s.f.; clarify clear vision areas on non-residential collectors and local streets; reduce all minimum residential setbacks from 30 feet to 15 feet; require 10-foot side setback for residential buildings that feature side windows; and clarify meanings of "interior" and "exterior" lot setbacks and adjust dimensions.

**Off-Street Parking:** Off-street parking regulations should be modified to add guidelines for implementing shared parking lots.

#### **Streetscape, Pedestrian Amenities and Alternative**

**Transportation:** Sections relating to architecture and pedestrian orientation should be modified to require larger non-residential establishments that exceed certain thresholds to provide public/pedestrian amenities; modify requirement for brick/decorative paving which to allow greater flexibility of pavement design; adjust regulation for surface parking screening to allow semi-transparent screening (brick pilaster with metal decorative fence); and add information regarding alternative transportation/transit.

**Design Guideline Amendments:** Section 2520 of the Zoning Ordinance should be modified allow greater flexibility in color of metal roofs, and review allowable building materials to increase flexibility while preserving the quality of the built environment.

**Zoning Map Amendments:** Amend the Zoning Map by rezoning the Anglin Area from OSC/OS-1 to TC, and rezoning the Trans-X Area from I-2 to TC-1.

#### **Recommendations for the OSC Zoning District**

Currently, the Office Service Commercial (OSC) zoning district does not allow any residential uses other than "transient" uses such as hotels or motels. The study recommends adding a residential use that is complimentary to the existing taller hotels and offices in this district. This change increases the



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flexibility for uses in the area, and provides a larger pool of potential customers to retail uses. This district allows five-story (maximum of 65 feet) buildings.

### **RECOMMENDED DESIGN GUIDELINES**

The study evaluated the existing design requirements for the TC and TC-1 districts, as well as other requirements found in the General Requirements section of the Zoning Ordinance. General standards were used to guide the design choices described in this chapter, and specific recommended changes were proposed for the following topics: pedestrian circulation, screening, lighting, site furnishings (benches, trash receptacles, bike racks, bollards and fences), planters, building facades, and plazas. Recommendations for each topic include design intent, scale and configuration, and color and materials specifications.

### **RECOMMENDED WAYFINDING FACILITIES**

The Town Center area has developed into a multi-faceted mix of destinations, including shopping centers, entertainment and restaurant venues, hotels and other attractions. To assist visitors in finding their destination easily, a wayfinding system of pedestrian- and vehicular-scaled signs was designed for the Grand River and Novi Road corridors. Because these roadways are within the jurisdiction of the Road Commission for Oakland County (RCOC), any sign installed within the right-of-way must meet their standards, and receive their approval. The report offers a recommended sign design as a first step in realizing a wayfinding system for the Town Center area.



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# INTRODUCTION

Novi's Town Center area has been the focal point of a number of studies over the past twenty years. In 1986, the City funded the Town Center Design and Development Study to guide the area's initial development into a unique, multi-use area that would create an identity for Novi. Over this same time period, the City's Master Plans have paid special attention to the Town Center area as it grew and changed into the heart of the City. Today, the Town Center is still evolving. This most recent effort revisits the Town Center, evaluates its current condition, and makes recommendations for further changes and growth.

## PURPOSE OF THE STUDY

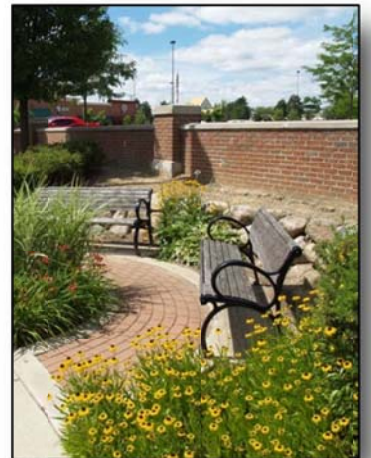
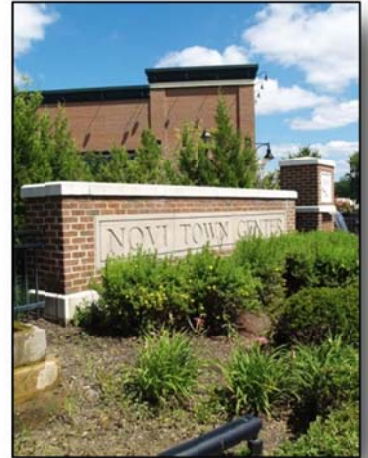
This study was commissioned in April, 2013, and is designed to evaluate and make recommendations on four main topics:

1. Land use
2. Zoning
3. Design guidelines
4. Wayfinding

The intent is to use the results of this effort to inform future Master Plans, which will communicate the City's vision and goals for development in the study area, and update land use options. The study results also offer modifications to the Zoning Ordinance, for the Town Center (TC), Town Center 1 (TC-1), and Office Service Commercial (OSC) districts, and landscaping provisions. Modifications to the current design guidelines are also offered. Lastly, a new wayfinding sign program is proposed to help visitors find destinations within the area.

## STUDY PROCESS

The study team began by researching background data on the project area. It first reviewed existing planning documents such as previous studies, the Master Plan and zoning ordinance, and design standards to understand current thinking on development in the Town Center. The next step was to conduct one-on-one interviews with development professionals and major property owners and managers within the Town Center area. Property owners and property management representatives were interviewed and asked general questions about the economic viability, design, and function of the study area. A public open house was held on September 11, 2013 and hosted by the City. Individual invitations were sent to each property and business owner in the Town Center area. They were asked to provide comments on land use, vehicular circulation, and pedestrian amenities. In addition, an on-line survey was posted to allow residents to participate, and provide comments about the future of the area.



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The team then consolidated the comments, and began evaluating the physical characteristics of the study area to determine whether the current land use designations were appropriate. The project area was divided into 10 subareas, and is shown in Figure 2 on the following page.

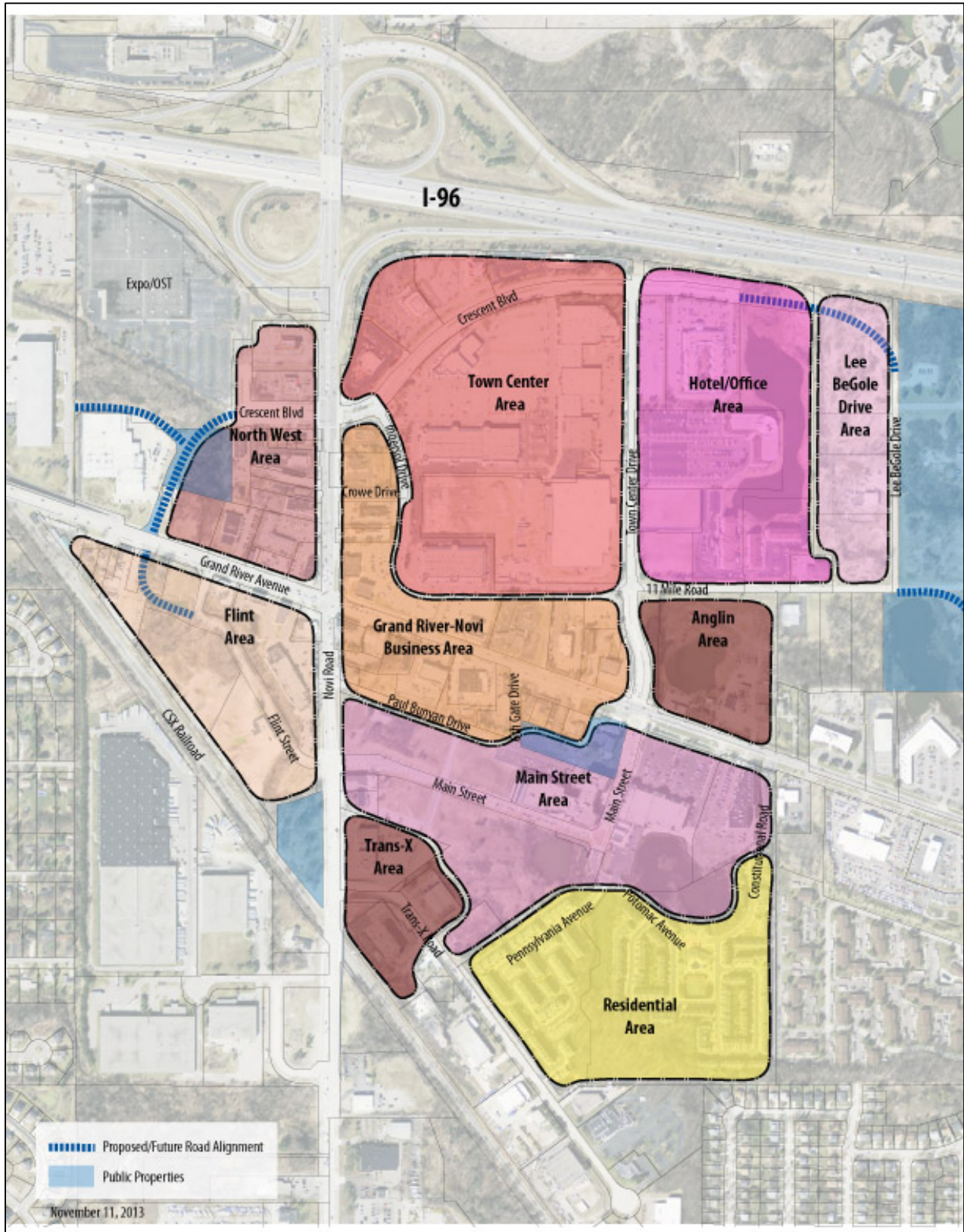
A number of vacant and under-developed properties were identified and analyzed for nine specific criteria. Based on the characteristics of these parcels, land use categories were refined and recommended for individual parcels.

At the same time, the design team walked the corridor evaluating attributes such as vehicular and pedestrian amenities, and various design elements of the streetscape. New design targets were developed to take advantage of the existing features that move the area toward the City's vision, and possible refinements to design elements that were inconsistent with the vision.

Concurrently, the team's sign professional evaluated the corridor to determine the need for, and potential locations of, wayfinding signs that would direct visitors to various destinations within the study area. A sign program was developed that reflects Novi's signage program currently used at municipal sites, and the Road Commission of Oakland County's sign standards.

# STUDY SUBAREAS

FIGURE 2. STUDY SUBAREAS







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# BACKGROUND

This chapter of the report describes the information that was used as a basis for determining the appropriate recommendations for each subarea. The topics below are organized to allow for easy comparison with the chapters that outline the recommended updates. A detailed matrix used in evaluating the land use of vacant and re-developable properties is provided in the Appendix.

## LAND USE

As described in the introduction, the current project area was divided into subareas, based on existing conditions and similar land use patterns. The existing land uses within each subarea are described below:

- 1. Flint Street Area:** These parcels are located in the southwest portion of the project area along an existing road called Flint Street. There are a mixture of uses in this subarea, comprised of light-industrial uses, a previous heavy-industrial use (concrete plant), one vacant parcel, and one residential parcel. Several retail uses are located at the intersection of Novi Road and Grand River. The closed-down concrete plant sits on approximately eight acres of land that is adjacent to an existing freight railroad line. The plant's recent closing offers a significant redevelopment opportunity. This subarea also contains a branch of the Rouge River, which could be used as an important amenity to redeveloping the area.
- 2. Trans-X Area:** The Trans-X Area has frontage along Novi Road, but also is adjacent to the railroad tracks. Properties within this subarea are currently being used for light-industrial purposes.
- 3. Main Street Area:** This subarea is comprised of the properties that are envisioned to become the core of Novi's "downtown." Many of the parcels are developed with multi-story buildings housing retail and office uses. However, some of the buildings are currently underutilized and need more tenants to draw a greater number of patrons to the subarea. It also contains several vacant parcels that were planned to contain residential uses, or additional retail/office uses. Attractions in this subarea include two wetlands on the south side of Main Street, and a large iconic structure, similar to a gatehouse, at the corner of Main Street and Grand River. These features have potential to become focal points of community gathering spaces and events.
- 4. Residential Area:** This subarea contains a residential development of up-scale rental properties that are popular with international business people. Buildings are multi-storied, and project a high-quality living environment. These units supply the Main Street and Town Center areas with customers and are the model for additional residential uses developing in the Main Street Area.



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5. **Anglin Area:** This property is approximately 10-acres in size, and previously contained a garden center and car wash. These businesses have been discontinued, and the property is for sale. While relatively large, it also contains a large pond, which limits the redevelopment potential, but also offers an attractive amenity for a prospective developer.
  6. **Lee BeGole Drive Area:** This subarea contains four parcels. One parcel contains the remains of a landscape contractor's yard, while the others are vacant. These parcels are sandwiched between the City's Department of Public Services (DPS) facility to the east, and hotel/office developments to the west. These parcels also contain an ephemeral stream that flows in the spring.
  7. **Hotel/Office Area:** The majority of this subarea is currently developed with two hotels and a large office campus, and associated parking areas. The two parcels at the intersection of Eleven Mile Road and Town Center Drive are vacant. They total approximately four acres in size.
  8. **Town Center Area:** This subarea contains the regional shopping center called Novi Town Center. It has been recently renovated to provide an attractive shopping experience with a mix of retail and restaurant uses. At this time, almost all of the commercial space is occupied.
  9. **Grand River/Novi Business Area.** As reflected in its name, this subarea covers both the north and south side of Grand River Avenue, as well as the east side of Novi Road at the intersection with Grand River. Both corridors have been developed over time, and therefore reflect several decades of development patterns.

The north side of Grand River and Novi Road north of the intersection contain properties that have been re-developed in the past ten years and either relate to the Novi Town Center mall, or are oriented toward Grand River / Novi Road.

The south side of Grand River and Novi Road south of the intersection contain buildings that are all oriented toward the road. However, the ages of these buildings cover a wide range. Some have been in place for decades (i.e. the historic building on the southeast corner of Grand River/Novi Road and a residential parcel most likely built in the 1940's), while other commercial properties have either been recently re-developed, or still contain commercial buildings built in the 1970s or 1980s. Most have parking facilities in the front or the side of the building.

10. **North West Area:** The parcels in this subarea are used for retail, restaurant, or office uses. The subarea contains one vacant parcel on the north side of Grand River. A portion of this vacant property contains a tributary of the Rouge River. Redevelopment of this property could use the river as an amenity or focal point.

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## ECONOMICS OF COMMERCIAL LAND USE

The Town Center area provides an important economic benefit to the City, in addition to providing jobs and goods and services. Several studies of the Town Center area were conducted in 2012 to evaluate the impact of the recent economic downturn, and to provide the Planning Department and Planning Commission with basic information about the spatial and social aspects of retail businesses and commercial centers in the City. Because the Town Center Area Study makes recommendations for land use and zoning changes, the economic impacts of these recommendations were considered using the following studies as a basis.

### Four Corners Study Area Report

Conducted in January, 2012, the Four Corners Study Area Report looked at the land uses abutting the intersection of Grand River Avenue and Novi Road. While the Four Corners Study area is slightly larger than the Town Center area considered in this report, it provides information about general trends regarding taxable value of this part of the City. The Four Corners Study Area Report calculates the taxable value of the commercial properties between 2006 and 2011, which decreased by an average of 16.60% during this time. This equates for a total decrease in taxable value of \$16.9 million. However, this area still represented almost \$89 million in taxable in 2011, with commercial uses covering approximately 263 acres.

### City of Novi Commercial Land Use Facts

In October of 2012, the Planning Department developed a white paper of facts regarding commercial land uses and commercial centers in the City of Novi. The Town Center area was not broken out individually, but the facts in this paper illustrate common themes that also apply to the Town Center area.

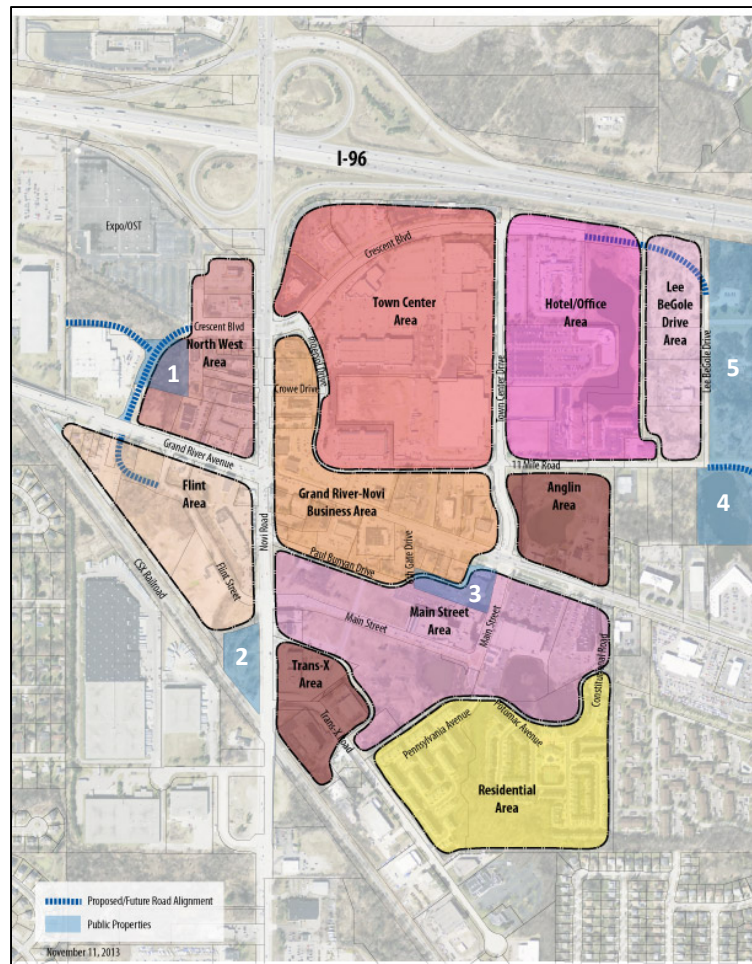
1. The City of Novi Assessor placed a taxable value of \$229 million on the total of all developed retail properties. This represents approximately 8.5% of the City's total \$2.7 billion taxable value for real property, and 7.8% of the \$2.9 billion total taxable value.
2. Based on the City's 2012 millage rate of 10.2, developed retail properties will generate about \$2.3 million dollars in revenue for the City.
3. SEMCOG's 2010 Employment Forecast by Industrial Class estimated that of the 37,928 persons employed in the City, 7,823 of those were employed in the "retail trade" sector. This represents 20.6% of the workforce. This compares to Oakland County and the SEMCOG region, both with 9.9% employed in the retail sector.

## MUNICIPAL FACILITIES

The study area contains several municipal facilities that serve the immediate area, or the entire City of Novi. The map below shows the location of these facilities, and descriptions of each follow:

1. **Ring Road Property.** This parcel is 2.85 acres in size, and is being set aside to expand Crescent Boulevard, creating the ring road system described in the “Vehicular Circulation” discussion later in this chapter.
2. **Historic Novi Cemetery.** The Novi Cemetery was established in 1844 by an early Novi settler, and contains veterans’ graves from as early as the Revolutionary War. This property is 2.4 acres in size.
3. **Fire Station #1.** This centrally-located Fire Station serves the northeast quadrant of the City of Novi. This parcel is unusually shaped and 2.4 acres in size.
4. **Drainage Course.** This 7.53-acre parcel contains a large pond, which assists in capturing stormwater runoff from adjacent properties.
5. **Novi Department of Public Services (DPS) Facility.** DPS serves the residents and property owners of Novi by maintaining all public utilities and roadways under its jurisdiction. This parcel is 32.8 acres in size.

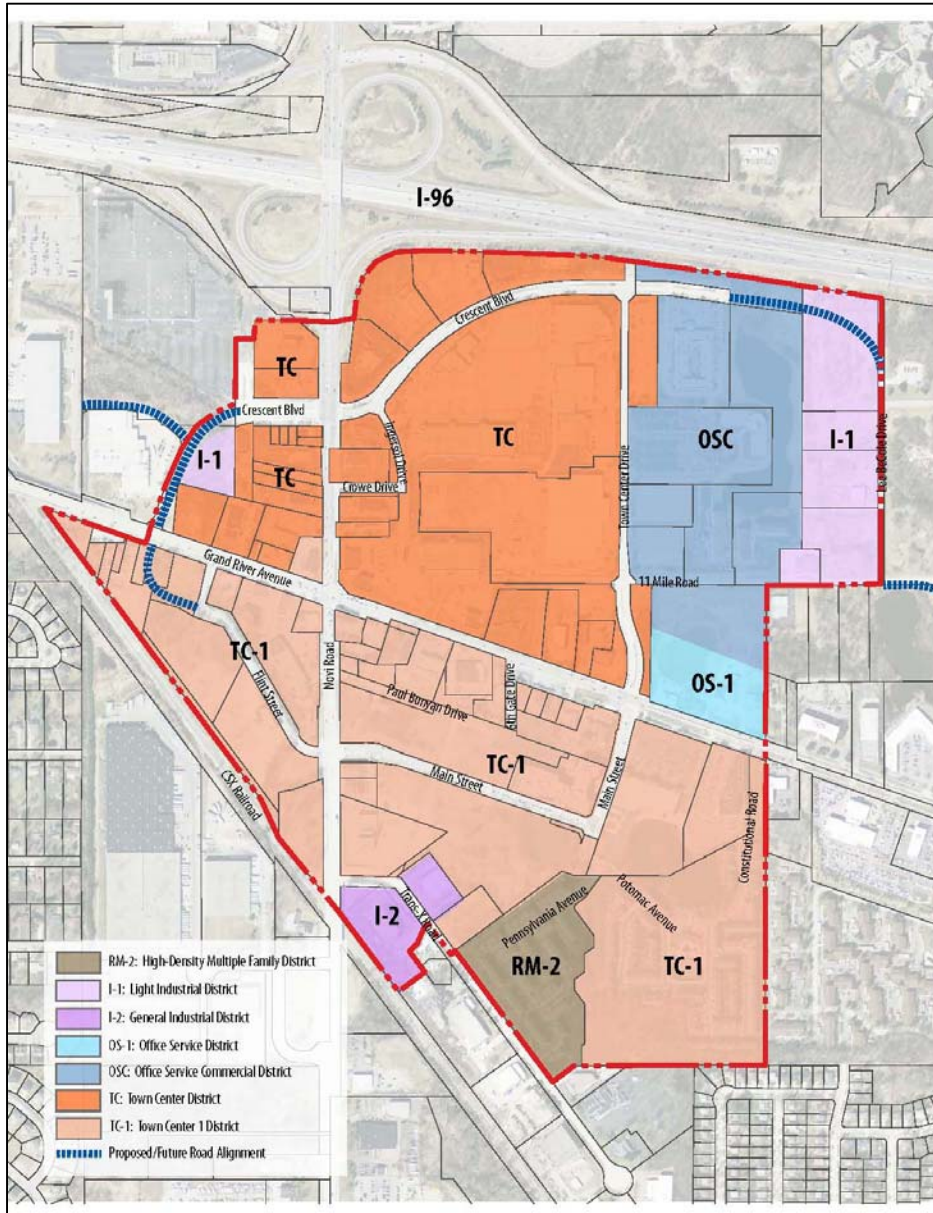
FIGURE 3. EXISTING MUNICIPAL FACILITIES



## ZONING DISTRICTS

The project area is zoned as a number of various zoning districts. Figure 4 below shows these districts, and the following text describes the intent of each district.

FIGURE 4. CURRENT ZONING DISTRICTS



- 1. High Density, Multiple-Family Residential (RM-2) District:** The High Density, Mid-Rise Multiple-Family Residential District provides for apartment-type residential accommodations. These developments have central services, including recreation, open space, off-street parking and other site amenities in a mid-rise configuration. This district provides high density living facilities in areas, or adjacent to areas, of intense commercial or office development. The Residential subarea is zoned RM-2.



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2. **Light Industrial (I-1) District:** This district is intended to primarily accommodate research, office and light industrial uses, including wholesale activities, warehouses, and industrial operations whose external, physical effects are not detrimental to surrounding districts. This district considers light industrial uses important to meeting the economic goals of the City, but also strives to define these uses so that they are free from posing a danger or nuisance to neighboring uses. The subareas zoned for light industrial uses include the Lee BeGole Drive Area and the North West Area.
  3. **General Industrial (I-2) District:** The General Industrial district is intended for manufacturing, processing and compounding of semi-finished or finished products from raw materials. Unlike the Light Industrial District, it is expected that the external physical effects of this use will be felt to some degree by surrounding districts. Subareas zoned for General Industrial uses include the parcels in the Trans-X Area.
  4. **Office Service (OS-1) District:** The OS-1 Office Service District is designed to accommodate uses such as professional and medical offices, banks and personal services. This district serves as a transitional area between residential and commercial districts and provides a transition between major thoroughfares and residential districts. The southern half of the Anglin Area (adjacent to Grand River Avenue) is zoned Office Service.
  5. **Office Service Commercial (OSC) District:** This district is intended to accommodate a large office building and/or a planned complex of office buildings with related retail and service establishments which may serve the area beyond the office complex itself.

The primary intent of this district is to provide limited areas for office buildings of greater height and more intense land use activity in an otherwise low-density community. Because of the greater building height, intensity of land use and associated higher volumes of vehicular and pedestrian traffic, this district is only located in proximity to areas of major commercial or civic development and have direct access to freeway or major thoroughfares.

The OSC District is designed to encourage the combining of mid-rise and low-rise office and office-related uses in planned developments and to encourage innovation and variety in type, design and arrangement of such uses. The Hotel/Office subarea is zoned OSC, as well as the northern half of the Anglin Area (adjacent to Eleven Mile Road).

6. **Town Center (TC) and Town Center 1 (TC-1) Districts:** These districts are intended to promote development of a pedestrian-accessible, commercial service district in which a variety of retail, commercial, office, civic and residential uses are permitted. These districts are further intended to discourage the development of separate off-street parking facilities for each individual use, and to encourage the development of off-street parking facilities that accommodate the needs of several individual uses.

Furthermore, the zoning ordinance acknowledges that uses which have as their principal function the sale or servicing of motor vehicles, and drive-in restaurants and restaurants with drive-through facilities, have a disruptive effect on the intended pedestrian orientation of the districts.

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The TC-1 District is specifically intended to encourage development of a "Main Street" with mixed land uses and shared parking. Flexible regulations regarding streetscape design, landscape design, provision of parking facilities, architectural and facade design, residential dwelling units, and setback standards are intended.

As shown on the map, TC and TC-1 districts are designated in the following subareas: Flint Street, Main Street, Town Center, and the Grand River/Novi Business Area.

## VEHICULAR CIRCULATION

The project area is served by two arterial roadways, Grand River Avenue and Novi Road. Grand River is a four-lane arterial road with a center turn lane. The speed limit on Grand River is 40 miles per hour (mph) within the project area, but increases to 45 mph east and west of the project area. Novi Road is also a four-lane arterial road. Recently, Novi Road was widened south of Grand River to create the four-lane plus center-turn-lane cross-section. This reflects the roadway design on the north side of Grand River. Novi Road has a speed limit of 45 mph.

A secondary road system also exists within the project area, made up of a "ring road" system encompassing Crescent Boulevard, Flint Street, Main Street and Lee BeGole Drive. Improvements to several of these roads are planned to complete the ring road:

1. Expansion of Crescent Boulevard to the west of Novi Road to intersect Grand River.
2. Slight re-alignment of Flint Street to intersect Grand River at the same point where Crescent Boulevard intersects with Grand River. The illustrations on the next page show the four alternatives considered. Also, see page 39 for a recommendation on the preferred alignment.
3. Pavement improvements to various roadways.
4. The City is studying the option of adding roundabouts to future roadway projects

The intent of the ring road system is to provide a secondary road system to Grand River/Novi Road, and help alleviate traffic congestion along these roads and at their intersection. The ring road will also accommodate people using non-motorized transportation methods (primarily walking and biking) in a more moderate, less vehicle-oriented environment.



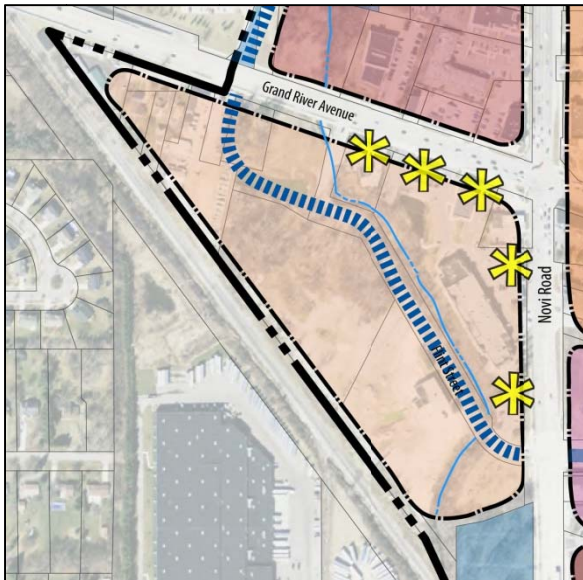




## PEDESTRIAN AMENITIES AND FEATURES

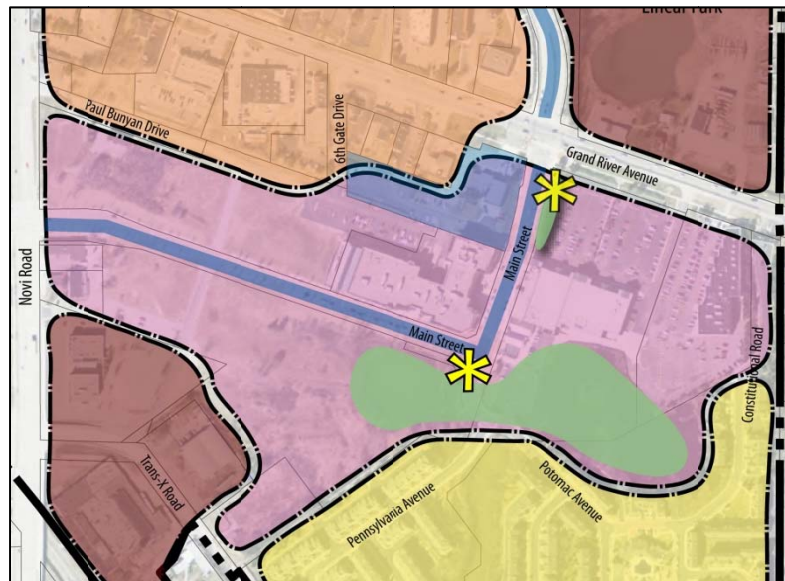
Pedestrian amenities and features, such as plazas, benches and trash receptacles, are located along Grand River Avenue, Novi Road, and Main Street. These amenities are intended to make it more comfortable and inviting for people to walk along these streets. Three of the subareas include these pedestrian amenities: the Flint Street area, the Main Street area, and the Grand River/Novi Business area.

1. **Flint Street Area:** The pedestrian amenities in this subarea include small, landscaped nodes next to the public sidewalk, and include benches and trash receptacles. The location of these nodes is shown by the yellow stars in the graphic below:



Existing pedestrian amenities along Grand River.

2. **Main Street Area:** The pedestrian amenities contained in the Main Street Area are built into the streetscape. This subarea was designed to accommodate pedestrians as much as vehicles. It contains wide sidewalks, plazas that are oriented toward restaurants, and numerous opportunities to sit and enjoy this dynamic environment. A major amenity that serves both pedestrians and vehicles is the large open structure, similar to a gatehouse, at the corner of Main Street and Grand River. It indicates to drivers that they are entering the Town Center area. It can also be used by pedestrians for refuge from the weather when waiting to cross Grand River. A second pedestrian area is provided at the point where Main Street takes a ninety-degree turn. It offers pedestrians a place to sit and watch the activities in the area. Both are illustrated by the yellow stars on the map below.

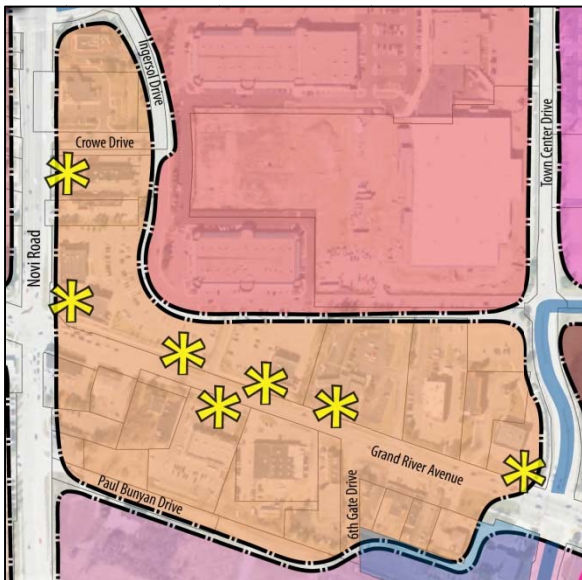


Existing gatehouse structure at Grand River and Main Street.



Existing plaza on Main Street.

- 3. Grand River/Novi Business Area.** This subarea contains a number of pedestrian plazas and nodes that either serve people walking along Grand River, or those visiting businesses located on this street. The large pedestrian plaza next to Potbelly's (northeast corner of Grand River and Novi Road) is buffered from traffic by its elevated position and extensive landscaping along the street. The remaining pedestrian nodes are smaller areas that either abut the sidewalk or the parking lot of the business it serves. The locations of the nodes within the study area are shown on the map with yellow stars.







Existing pedestrian plaza in Grand River/Novi Business Area (Northeast corner of Grand River and Novi Road).



Existing pedestrian node in Grand River/Novi Business Area (on Grand River).

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# VISION AND GUIDING PRINCIPLES

## INTRODUCTION

To determine the future direction for the study area, the project team involved property owners, managers, realtors and residents. These stakeholders had several opportunities to voice their opinions about their vision and goals for future development and change. Town Center area property owners and managers were interviewed, and a public input meeting was conducted. The goals and policies outlined in previous plans were also evaluated in light of current conditions and economic realities. This chapter describes the process and results of gathering public input, and the recommendations for a future vision and guiding principles for the Town Center.

## PUBLIC INPUT

The public input process consisted of three strategies to gather insights about the future of the study area: 1. Host one-on-one interviews with large property owners; 2. Host a public input meeting, and invite the remaining property owners as well as residents at large; 3. Provide an on-line vehicle for sharing ideas and responding to specific questions. The following describes each effort, and summarizes the results collected. The full results are included in the appendix of this document.

## Stakeholder Interviews

Interviews were scheduled with 11 of the largest private property owners, managers and realtors within the Town Center study area. A list of questions was developed to ensure that the interviews addressed similar topics and provided a consistent structure across meetings. Additionally, each person was provided the opportunity to discuss other ideas and topics they thought important.

The most common opinions that emerged from the interviews are as follows:

- Strongest land use market today is for residential and office uses
- General perception that retail is overbuilt
- Residential uses are preferred for vacant parcels along Main Street
- Requirements for first floor retail is unrealistic
- Design standards are good but need to allow for flexibility

Stakeholder correspondence received after the interviews is also provided in the appendix.



## Public Input Meeting – Drop-in Open House

An open house style meeting was scheduled for the morning of September 11, 2013 at the Novi Civic Center building. Over 100 invitations were distributed to property owners and managers within the study area. Also, an invitation was posted on the City's website that opened the meeting to all residents. The open house was scheduled before the typical work-day began, hoping to capture people before they became involved in that day's activities.

The open house targeted three topics: 1. Land Use; 2. Vehicular Circulation; and 3. Pedestrian Amenities. Large aerial photographs were posted around the room, one for each topic. Questions relating to each topic were then hung next to the aerial photograph. Visitors were given post-it-notes, and were asked to write their idea on the note, and then stick it on the aerial photograph in the relevant location.



Aerial photo with open-house attendees' ideas recorded on post-it-notes.

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While only a dozen or so people attended the meeting, they had many ideas for the project area. The following summarizes the results of the Drop-In Open House:

**Land Use:**

- Redevelop older properties along Grand River, including the Fire Station.
- Attract young professionals to Main Street through multi-storied (4 to 5 stories) loft-style residential buildings with balconies and greenery.
- Provide community gathering space near wetland areas along Main Street; schedule events here, such as the farmer's market.
- Provide City-based administration of Main Street (such as a DDA) to oversee maintenance and assist with economic development.

**Vehicular Circulation:**

- Install traffic calming techniques along Grand River to slow motorists in this area; review speed limit on Grand River.
- Install monuments/gateway/vertical signage to Town Center area (Novi Road and Main Street); banners on light poles ("Free Parking").
- Create alternative travel routes to Grand River/Novi Road. Extend Lee BeGole Drive over I-96 to provide alternate route.
- Pass ordinance to require parking at side/rear of buildings that face public streets.

**Pedestrian Amenities:**

- Walkable concept still viable, especially with apartments.
- Landscaping expanded internally at Town Center; include welcoming color/flowers; enforcement/maintenance should be stronger; Main Street planters too large.
- Ask for professional traffic engineer recommendations for both pedestrian and vehicular circulation on Town Center Drive.
- Wood benches need maintenance; consider other type of bench that is more durable; consider advertising on benches.

## **On-Line Survey Results**

A survey was posted on Novi's website to enable folks who could not attend a meeting to weigh in on the project. The survey consisted of 27 questions, and were a mix of yes/no, multiple-choice, and open-ended questions. The on-line survey questions mimicked the Drop-in Open House, as they focused on land use, vehicular circulation, and pedestrian amenities. Thirty-two responses were received, and are summarized on the next page.

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### **Land Use:**

- About two-thirds of the respondents think that the Town Center area needs more independent stores and up-scale, family-friendly restaurants.
- Most respondents think drive-through businesses are not desirable in the Town Center area; but of those that like the idea, they would limit their number to 1-3 drive-through businesses.
- Only about half of the respondents thought additional residential was needed in the study area. Of those agreeing with this, they preferred loft-style condominiums in multi-story buildings.
- A majority of respondents think there is enough office space in the study area. Those that want to add more think that medium-sized, multi-tenant buildings are missing.
- Development along Flint Street should be commercial or office uses; development along Lee BeGole Drive should be commercial or research/technology uses.

### **Vehicular Circulation:**

- Approximately 60% of respondents report that there are traffic problems within the study area along both Grand River and Novi Road during the afternoon rush hour.
- Most thought expanding Crescent Boulevard west of Novi Road will improve traffic problems.
- Regarding the re-alignment of Flint Street, respondents chose the three options equally, so no preference was determined

### **Pedestrian Amenities:**

- Most don't think the current pedestrian nodes are well placed. Respondents would move them closer to building entrances and away from the roadways.
- A little more than half of the respondents thought there were enough sidewalks. But those that didn't thought more walks should be added that connected the entire study area, and made it all "walkable."
- Almost all of the respondents thought landscaping improved the pedestrian experience, but wanted to see more of it.

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## VISION AND GOALS IN PREVIOUS PLANNING DOCUMENTS

The City of Novi has been planning the development of the Town Center area since the mid-1980s, when development within the City and the surrounding area was expanding dramatically. The following documents were developed to plan for the Town Center area, as well as the city as a whole, and still provide guidance for future development/re-development of the study area.

### 1986 Town Center Design and Development Study

The Town Center Design and Development Study was initiated to provide recommendations that form a set of design principles and guidelines for improvements related to the public and private street environment. The goals of the study were to:

1. Reflect the City of Novi's long-term commitment to establishing a quality Town Center District.
2. Create an image for the district.
3. Provide for multi-access from Novi Road and Grand River Avenue.
4. Enhance the environment by encouraging multi-seasonal and day/night use and activities; and development of parks, open green space and plazas throughout the District.
5. Promote continuity and compatibility among public and private development.

The study provides a summary of the design and development guidelines included in the report. Many of the ideas expressed in this study are still relevant today. These include the following:

1. Town Center should not be all retail, rather it should be multi-use.
2. Quality shopping facilities would be desirable. Small, interesting shops, eating places, dinner theater, a performing arts center, and an open air farmer's market should be encouraged.
3. Image, ambiance, and area amenities and activities are needed to attract visitors.
4. Design a "sense of place," a "sense of scale," and "sense of identity" so that building masses and parking facilities do not dwarf the pedestrian. There should be planned separation of vehicular from pedestrian traffic.
5. Alternate forms of off-street parking facilities (e.g., parking deck or ramp) should be considered to reduce expanses of surface parking lots.
6. A park or "Town Square" with possible ice skating, band shell, gazebo, and walkways would be desirable.
7. There should be recognition of environmental assets in and near the Town Center area.

***Vision for Town Center in 1986***  
*"The overall feeling for the Town Center is one of diversity of activity yet unity in overall character. It is a downtown area that is alive both for those who live outside the City of Novi and, more importantly, an identity area for the community of Novi as a whole."*

*Town Center Design & Development Study, 1986*

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## 2007 Master Plan

The 2007 Master Plan describes the area as the Ring Road Area, which is defined by the following secondary roads: Crescent Blvd., Flint Street, Main Street, and Town Center Drive. The Master Plan designates several goals for this area:

1. **Town Center Commercial:** Continue to designate the Novi Road area as “Town Center” which is intended for a variety of uses including retail, commercial, office, residential, civic, cultural, and open space with a pedestrian-oriented, city-center character. Continuing this designation will encourage new development and redevelopment that complements and strengthens the value of the entire Town Center area.
2. **Office:** Plan for large-scale office use within the study area immediately adjacent to the I-96/Novi Road interchange. Providing for nearby office development will strengthen retail and restaurant uses along Novi Road by bringing additional potential customers to the area. Office development is appropriate along the future ring road since it is not as dependent as retail is to having frontage on Novi Road.
3. **Senior Housing 2:** Plan for a medium intensity senior housing complex within the study area.
4. Complete the proposed ring road from Fonda Drive to Grand River Avenue.
5. Encourage development of the ring road subarea in a manner that adds value to the City’s Town Center and Main Street areas.

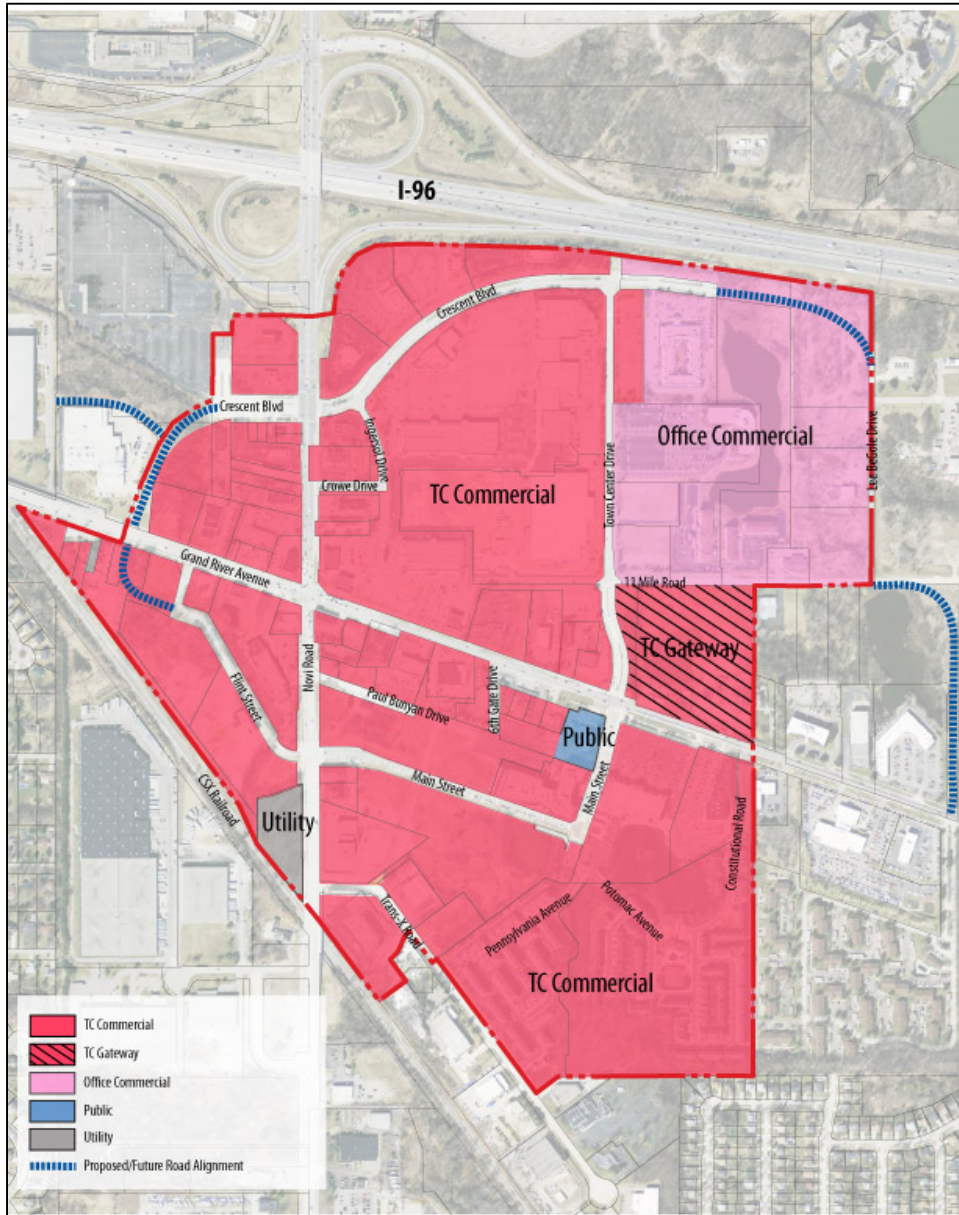
## 2010 Master Plan

Building on the previous work and meeting the requirements for a Master Plan update, the 2007 Master Plan was re-visited in 2010, and updated. One section specifically addresses the Town Center area. The 2010 Master Plan designates the project area as either Town Center Commercial, Town Center Gateway, or Office Commercial. These designations are identified as follows:

1. **Town Center (TC) Commercial:** This land use is designated for pedestrian oriented, community focal point area with a variety of uses including retail, commercial, office, residential, civic uses, and op spaces with a city center character.
2. **Town Center (TC) Gateway:** This land use is designated for mixed-use development that provides an appropriate transition and a sense of arrival into the Town Center Commercial area. These uses include a mixture of moderate density residential, commercial, and office uses.
3. **Office Commercial:** This land use is designated for a variety of medium- and large-scale general and medical office buildings or complexes . The map on the next page illustrates the location of these Master Plan designations.



FIGURE 6. FUTURE LAND USE PLAN 2010





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## TOWN CENTER'S FUTURE DIRECTION

The previous studies and input from property owners provide guidance for the desired direction of future development in the Town Center Study Area, and the detailed recommendations found in the following chapters. To ensure consistency in application of these recommendations, the following vision and guiding principles describe the desired future character and functioning of the study area.

### Overall Vision

Development of the Town Center Study Area will create a dynamic, attractive city core that provides residents and visitors with unique opportunities to participate in active community life and meet their needs for goods, services, housing and entertainment.

### Guiding Principles

To achieve the vision above, the following guiding principles have been developed:

#### Land Use:

1. Land uses should be mixed and work together to establish a city center for Novi which provides an attractive, mixed-use hub of activity.
2. Residential land uses should encourage development of varied styles of residential development, including mid-rise, townhouse, loft-styled or others to attract young professionals, families and seniors, and supply patrons for nearby businesses.
3. Private and public development should include community gathering spaces that provide venues for events that attract visitors, as well as green space for area residents.
4. Existing natural features (streams, wetlands, ponds), while limited, should be incorporated into private and public developments to create an attractive environment for pedestrian-scaled uses.

#### Zoning:

1. Zoning districts should allow mixed uses, but be flexible enough to accommodate uses that are economically feasible and market based.
2. Zoning should establish guiding design principles or form-based standards to ensure quality design.
3. Zoning should continue to require development of pedestrian-scaled site amenities that are logically placed and provide functionality for the use.

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**Design Guidelines:**

1. Design standards should provide guidance, but be flexible enough to allow for various aesthetic interpretations.
2. Site design standards should balance pedestrian and vehicular amenities.
3. Developments should be required to create and maintain high quality, attractive site environments, including decorative lighting, paving, walls, fences, pedestrian amenities, and diversified landscaping (trees, shrubs, perennials and groundcovers).
4. Street furnishings should coordinate across the study area by using consistent materials, colors, or other elements.

**Vehicular Circulation:**

1. Appropriately scaled gateway features should be installed at the boundaries of the “town center” to communicate to drivers that they are entering a special place.
2. Motorists visiting the area (vs. driving through) should be accommodated with alternative driving routes.
3. Staff should work with the Road Commission for Oakland County to evaluate appropriate speed limits and traffic management strategies along Grand River Avenue and Novi Road to accommodate the “town center” vision.
4. The City should continue its efforts to complete the ring road (Main Street, Town Center Drive, Lee BeGole Drive, Crescent Boulevard, and Flint Street).
5. Opportunities for transit should be accommodated in and around the Town Center study area.

**Pedestrian Amenities:**

1. Facilities should be provided throughout the study area to allow pedestrians safe and comfortable walking/biking environments, including crossing Grand River Avenue and Novi Road, and pedestrian movement to and from adjoining residential areas.
2. The City should continue to require streetscape improvements along Grand River Avenue and Novi Road and provide pedestrian amenities.

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## OVERALL FRAMEWORK PLAN

In light of the vision and guiding principles, the next task the study took on was to develop an overall framework plan. This plan, illustrated on the following page, identifies key locations of the study area where changes would enhance the Town Center and meet the desired vision.



FIGURE 7. OVERALL FRAMEWORK PLAN

**Structure**

- Preserve the strong axial structure between Grand River and Novi Road, which facilitates primary vehicular movement.
- Strengthen the secondary vehicular relationships by completing the ring road. This can become the “neighborhood” route.
- Create strong pedestrian connections between Town Center and residential. Allow people to move easily through the site.
- Enhance points of interest between residential and Town Center to draw people in and through the space.
- Attempt to slow traffic along Grand River between Novi Road and Main Street.

**West Gateway**

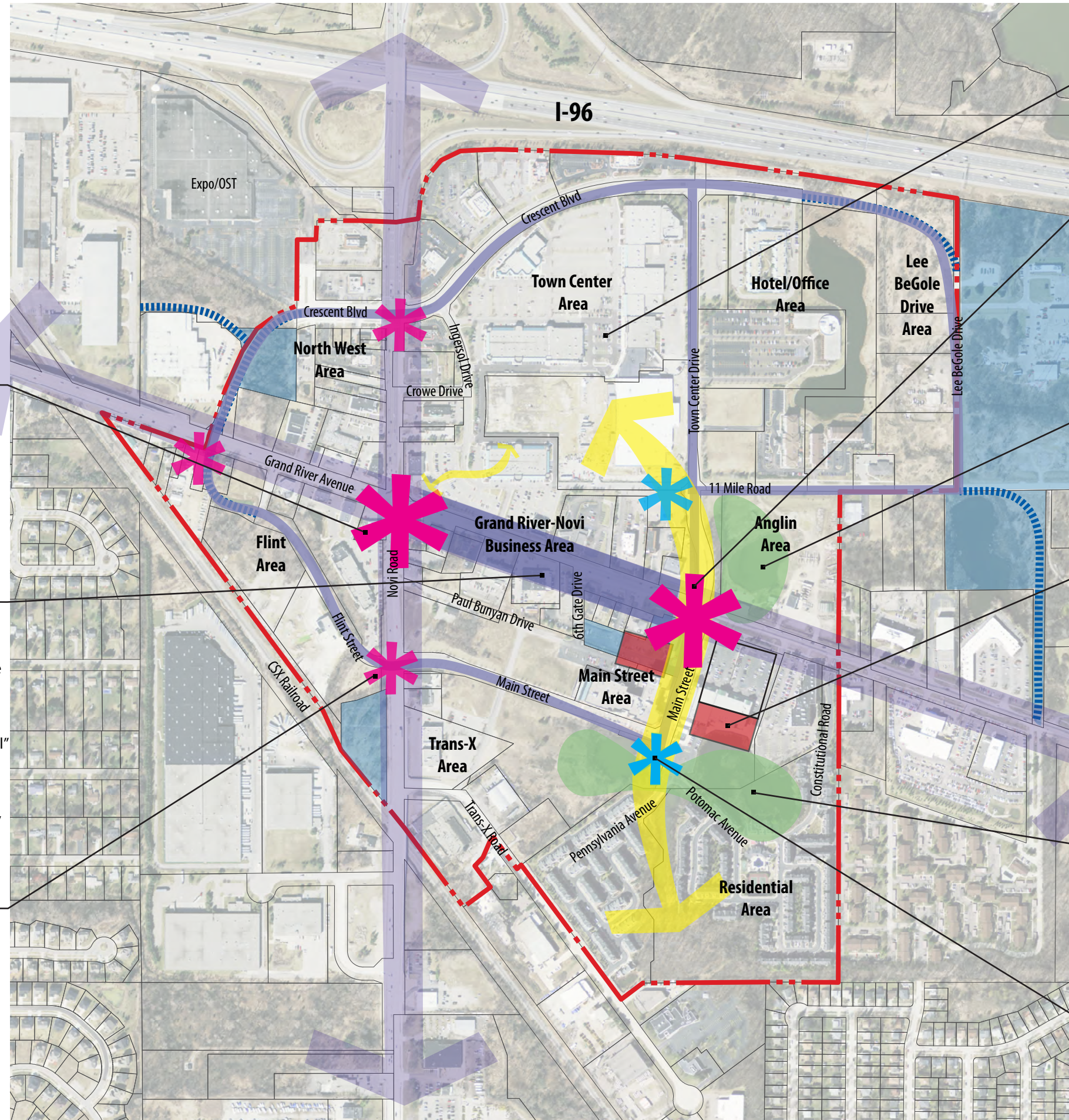
- This forms the west bookend to the east gateway.
- Enhance the plaza on the NE corner. The height difference of the space makes it comfortable even when right next to traffic.
- Program seasonal events in plaza on NE corner.
- Create stronger and more visual connection between corner and interior of Town Center.

**Grand River**

- Crossing Grand River on foot is difficult, both mentally and physically.
- Create opportunities for pedestrians to pause as they cross by shortening the distance they have to walk. Use pedestrian refuge islands in the center or bumpouts at the sides.
- Consider other traffic calming techniques along Grand River to slow traffic, clearly indicate that this is a “special” place, and better facilitate non-motorized movement.
- Create stronger, meaningful associations between businesses and Grand River, such as restaurant patios, new construction sited at lot line, or amenities carefully placed.

**Secondary Crossings**

- Enhance intersections where ring road crosses primary roads.
- Make more pedestrian friendly so people can move through site easily with or without cars.
- Focus should be on ease of mobility and accessibility - not a big visual impact.



**Town Center**

- Enhance as a destination.
- Improve the pedestrian connection from Town Center to residential south of Grand River.

**East Gateway**

- Enhance and program existing structure at corner of Grand River and Main St. It is a prime place for programming and gateway opportunities.
- Visually and programmatically extend the gateway to north side of Grand River with Farmers’ Market or small cafe.
- Fire Station offers opportunities for redevelopment.

**Green Space**

- Enhancing this green space creates another draw for residents through the Main St. corridor to Town Center. The small plaza at the NW corner now has meaning as a resting spot and segue into Town Center.

**Specialty Market**

- Encourage thematic connection between store and programming at corner.
- Consider programming Farmers’ Market or ethnic food market at corner.

**The Pond**

- Untapped opportunity for design and programming at corner.
- The pond, with very little effort, could provide great views and with the addition of a boardwalk would provide strolling opportunities and a reason for people to come out.

**Respites**

- These spaces are respites along the way between residential and Town Center. They are thematically linked and help tie the area together.





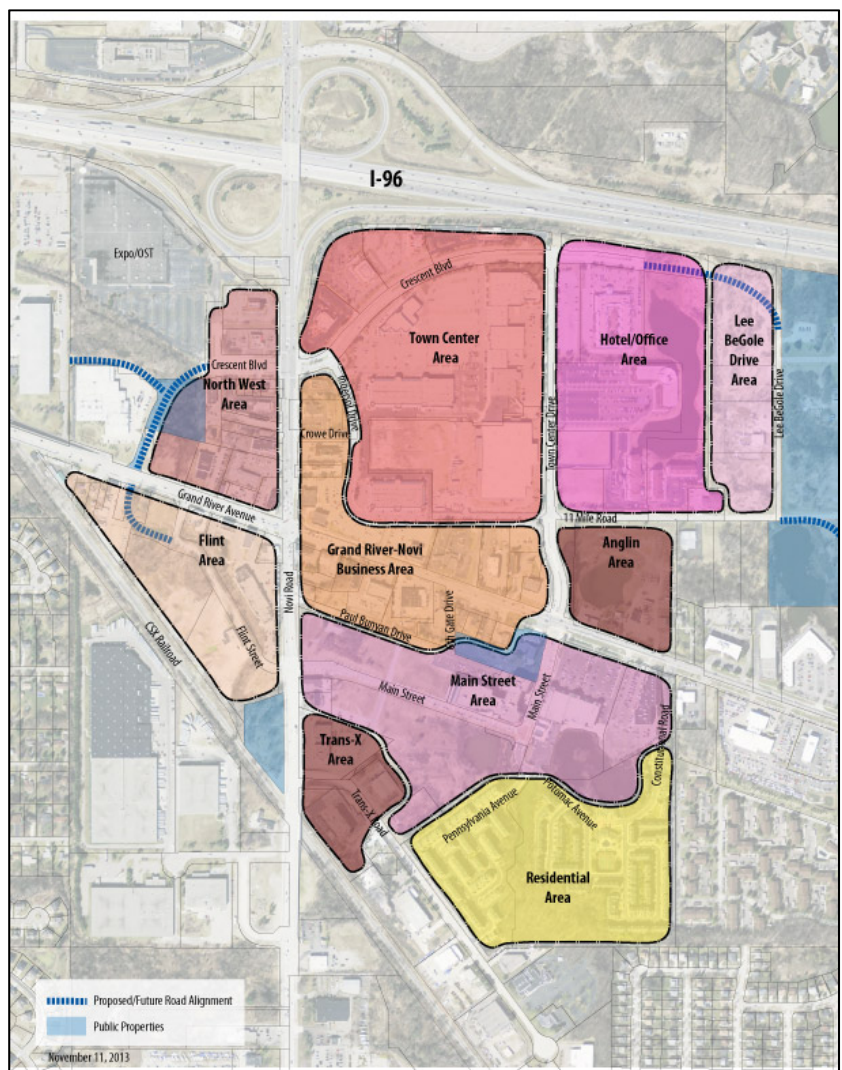
# RECOMMENDED LAND USE AND MASTER PLAN UPDATES

## SUBAREA INTENT, POTENTIAL PERMITTED USES AND FORM/DESIGN

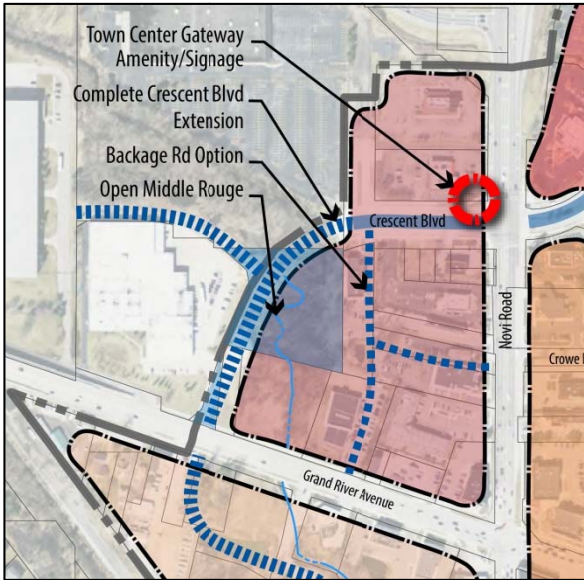
To facilitate study of the project area, The Town Center area was divided into ten subareas, as shown in the figure below. Each sub-area has unique characteristics and are part of the Town Center area proper or are influenced by the prime location of the corner of Grand River Avenue and Novi Road. This document analyzes the intent, desirable land uses and form/design for each sub-area and makes recommendations for future adjustments to the City's Future Land Use Plan.

While each sub-area has district attributes in regards to appropriate land use and form, the overall intent of the Town Center Area is to serve as a mixed-use/pedestrian-oriented focal point for the City of Novi. In general, the Town Center area should allow broad flexibility and have a variety of uses including retail, commercial, office, residential, institutional uses, and open space, with the Grand River/Novi Road Business Area and Main Street Area serving as the Town Center area's main "hub." Each subarea is described in the following pages along with additional recommended land uses. (Note that the existing land uses in the current categories (Town Center Commercial, Town Center Gateway, and Office Service Commercial) remain in effect. Suggestions on form/design, pedestrian orientation and streetscape appearance are also provided.

FIGURE 8. STUDY SUBAREAS







New development that expands restaurant choices in the North West Area.



Existing pedestrian-oriented commercial development in the North West Area.

## NORTH WEST AREA

### *Intent*

Provide a logical transition into the west part of the Grand-River Avenue/Novi Road Business Area via a flexible mix of commercial and retail uses. The area will provide a gateway to the future Adell Tower development and will also benefit from the extension of Crescent Boulevard. Vehicular circulation could further be improved, as called for in the 2010 Master Plan, by adding a backage road between the ring road and Grand River, interconnect parking areas, and share access points onto Novi Road to eliminate individual driveways. This option would require purchase of right-of-way, and could potentially disrupt existing parking areas. The North West Area is a prime location for additional retail, office and restaurant use. There is also an opportunity to provide professional office space in an attractive park setting along the Middle Rouge Creek.

### *Preferred Land Uses*

- Restaurants, coffee shops and retail/office uses

### *Other Land Uses to be Considered*

- Market, specialty food & beverage
- Furniture
- Personal service establishments (i.e., barbershops, photographic studios)
- Health and veterinary services
- Banks
- Indoor recreational facilities (i.e., health clubs, etc.)

### *Form/Design*

Buildings along Novi Road and Grand River Avenue should be pedestrian-oriented at appropriate scale with small front/side setbacks. Shared parking areas should be located in the rear or side setback.

### *Economic Impact*

The recommendations above will build on the existing commercial land uses in the area, and expand the mix of retail, restaurant, and service offerings.

## FLINT STREET AREA

### **Intent**

Provide a mix of uses similar to the North West Area with a flexible mix of commercial and office uses. City-center residential on upper-stories or live/work units could be integrated as well. It is anticipated that Flint St. will be redeveloped and improved to connect with Crescent Boulevard and complete the Town Center ring road. The Middle Rouge River should be exposed and park like amenities should be created with adjoining pathways. Office, professional and research technology uses are proposed for the area abutting the railroad while a mix of commercial uses, restaurants, and residential uses will front Grand River Avenue and Novi Road.

### **Preferred Land Uses**

- Office, research and technology uses (abutting the railroad)
- Areas abutting Grand River Avenue or Novi Road
  - Restaurants or restaurants with drive-thru
  - Outdoor cafes abutting Middle Rouge Creek
  - Retail and office uses

### **Other Land Uses to be Considered**

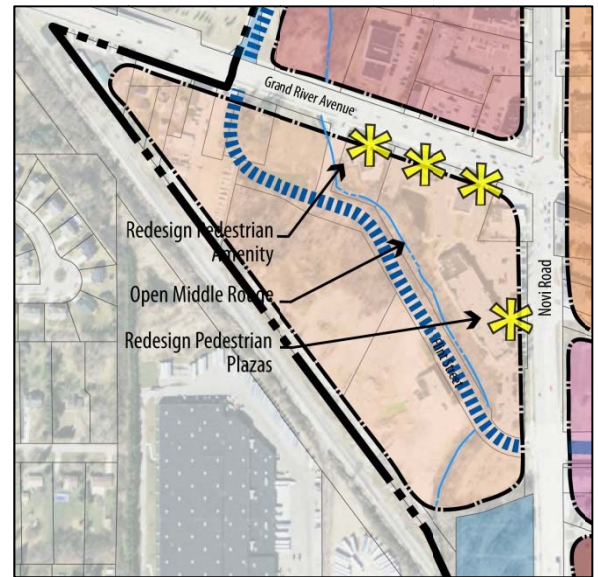
- Municipal services
- Personal service establishments
- Banks
- Upper story residential or live/work units

### **Form/Design**

Buildings along Novi Road and Grand River Avenue should maintain attractive architecture with reduced front and side setbacks. Shared parking areas in side/rear are encouraged along the railroad tracks. The Middle Rouge Creek should be utilized as a pedestrian focal point with walkways or pedestrian plazas. Flint Street should be re-aligned using the option that follows the street's current location, and meets Grand River directly across from the proposed intersection with Crescent Boulevard. Pedestrian amenities can be integrated into the street project to take advantage of the Middle Rouge Creek. Select areas could be covered.

### **Economic Impact**

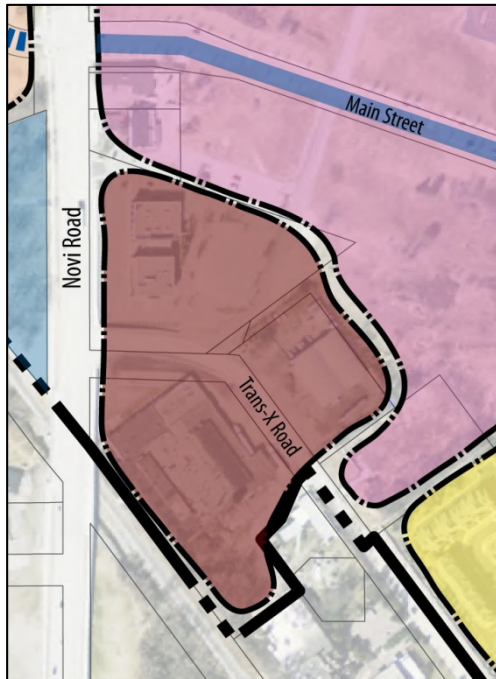
These recommendations will expand the commercial offerings of this area, and provide space for innovative research and technology uses and job opportunities.



Desirable existing commercial development in the Flint Street Area.



Existing pedestrian amenities on Grand River.



## TRANS-X AREA

### ***Intent***

The Trans-X sub-area is a transitional area that announces the Main Street and Grand River/Novi Road Business Areas. Existing industrial uses should be transformed to office and retail uses. It is a possible location for a public market place with individual leased spaces for specialty foods, produce and gift items. The frontage of Novi Road will continue to be redeveloped for commercial uses including restaurants and retail uses. Permitted Office, Research and Development uses will provide additional employment opportunities.

### ***Preferred Land Uses***

- Public market with specialty foods, produce and gift items
- Restaurants

### ***Other Land Uses to be Considered***

- Retail businesses
- Personal service establishments
- Banks
- Office, research and technology uses

### ***Form/Design***

Buildings fronting Novi Road should be high quality architecture with reduced setbacks.

### ***Economic Impact***

The recommendations above will expand the commercial offerings of this area, as well as provide space for innovative research and technology uses and job opportunities.



## MAIN STREET AREA

### **Intent**

The Main Street Area should provide opportunities for mixed-use and City Center-style development involving both residential and commercial uses. New residential uses, parks, and pedestrian amenities along Main Street are encouraged to provide street vitality. Wetland areas and ponds should be developed as parks. Additionally, the current fire station parcel (southwest corner of Main and Grand River) presents an opportunity for future redevelopment. The existing gateway structure on the opposite corner presents programming opportunities for community events.

### **Preferred Land Uses**

- Residential uses for Main Street area
- Mixed-use developments at the corners of Main Street (Novi Road/Grand River Avenue)

### **Other Land Uses to be Considered**

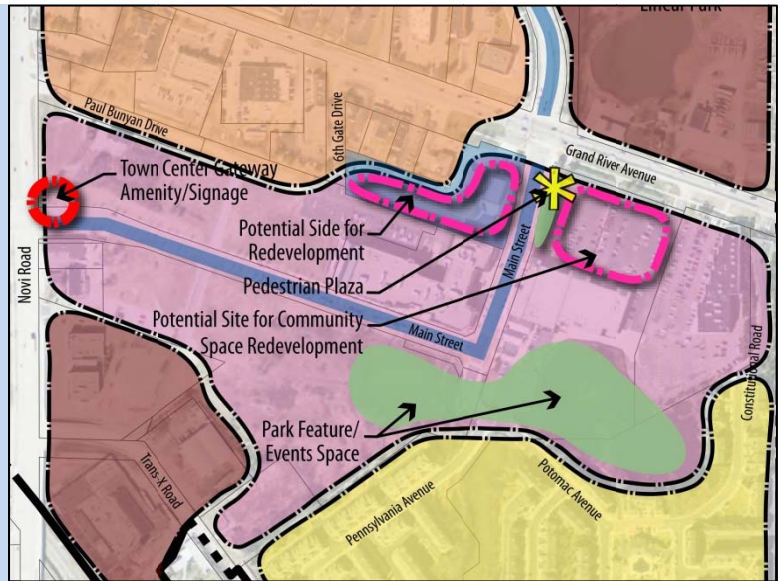
- Restaurants, bistros, outdoor cafes
- Entertainment areas, performing arts theaters (near restaurants)
- Community events such as a farmers market, ethnic food market, art market, car show, etc.
- Banks, Offices
- Live-work buildings

### **Form/Design**

Dense multi-family housing should take forms such as loft-style condominiums, townhouses, or mixed-use commercial/office/residential buildings. First floor residential uses and shared on-street/off-street parking should be allowed. Attractive streetscape amenities should provide space for special events and pedestrian connectivity to adjacent subareas. Select pedestrian areas could be covered. Taller buildings (3-5 stories) at the corner of Main and Grand River would provide greater sidewalk vitality and a visual landmark.

### **Economic Impact**

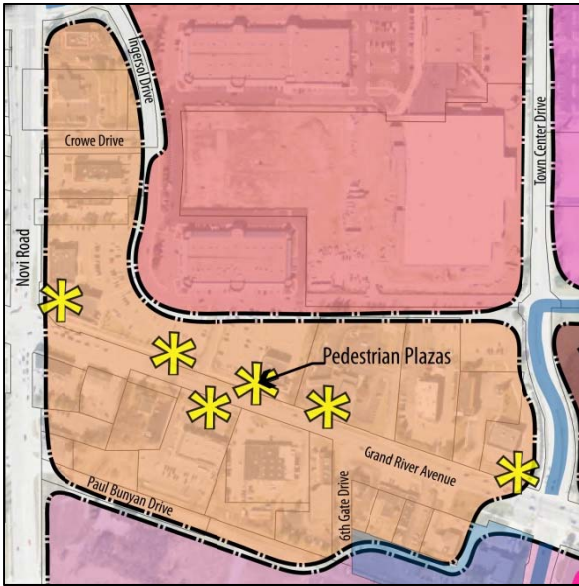
The recommendations above will expand the commercial offerings of this area.



Existing entry structure provides opportunity for pedestrian connections to redevelopment.



Pedestrian features add vitality to the streetscape.



## GRAND RIVER/NOVI ROAD BUSINESS AREA

### **Intent**

The Grand River/Novi Road Business Area is intended to provide a dense, walkable, retail, office and restaurant area and serve as the spine of the overall Town Center area. This subarea should be easily accessible from the other subareas via ample vehicular and pedestrian facilities, including connections, crosswalks and signage.

### **Preferred Land Uses**

- Retail uses
- Restaurants
- Outdoor cafes

### **Other Land Uses to be Considered**

- Office uses
- Personal service establishments
- Banks

### **Form/Design**

Buildings along Novi Road and Grand River Ave should be pedestrian-oriented with reduced front and side setbacks. Attractive landscaping should be used to screen parking lots from the road and create a more comfortable pedestrian experience. Consistent lighting should be used to identify this corridor as a “unique place”. Pedestrian refuge islands should be considered at the Grand River/Main Street intersection to assist pedestrians crossing Grand River and to communicate to motorists that they are entering the Town Center. The City should work with the Road Commission to reduce speeds in this area. Connections should be established through existing screen walls for pedestrian access to existing pedestrian nodes along Grand River. Select pedestrian areas could be covered to protect users from the elements.

### **Economic Impact**

The recommendations above create a more comfortable environment for pedestrian access, increasing the number of visitors and business patrons, and drawing people in from the adjoining residential areas.



Reduced front setbacks provide pedestrian scale.



Pedestrian plazas connected to businesses offer easy access and comfortable retreat from traffic.



## TOWN CENTER AREA

### *Intent*

Anchored by Wal-Mart and several smaller national retail chains, the Town Center Area is a regional commercial center. The current mix of retail, office and restaurant uses are attractively sited within a well-landscaped and managed development. While the land-uses and form are not likely to change in the near future, the Town Center Area should provide pedestrian accessibility via connections to the Grand River/Novi Road Business Area, Hotel/Office Area, and Anglin Area. Additionally, the Town Center's location next to I-96 provides regional access and visibility to the area.

### *Preferred Land Uses*

- Large format retail uses
- Shopping centers/general retail uses
- Restaurants/outdoor cafes

### *Other Land Uses to be Considered*

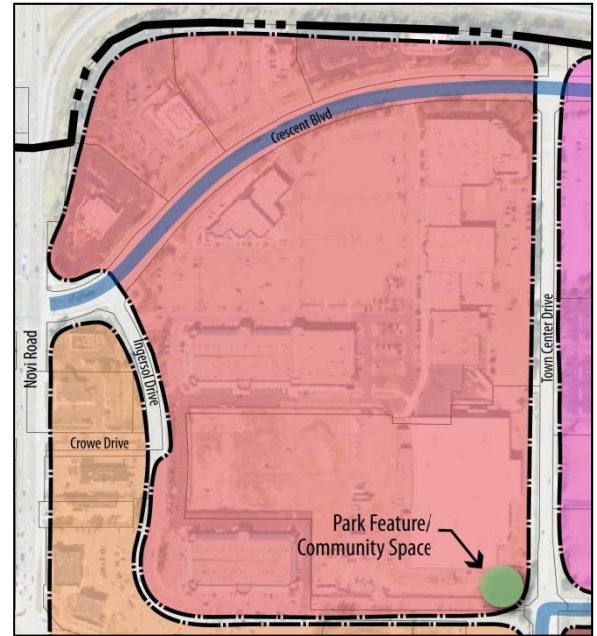
- Office uses
- Banks
- Personal service establishments
- Indoor recreational facilities (i.e., health clubs, etc.)
- Events in re-developed park feature/community space such as a farmers market, ethnic food market, art market, etc.

### *Form/Design*

Anchor and outlot development should maintain the current form with a unifying brick façade and attractive landscaping. Pedestrian connections and pedestrian amenities should be provided both internally and along Town Center Drive and Crescent Blvd. Select pedestrian areas could be covered to protect users from the elements.

### *Economic Impact*

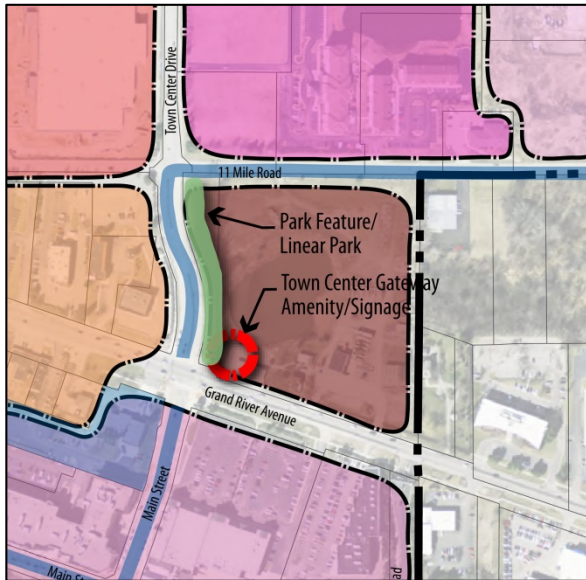
This shopping mall offers significant employment opportunities and tax revenue. Therefore, the recommendations above build on the successful pattern already established at the Novi Town Center to further develop an attractive and inviting regional shopping and entertainment environment.



Signage throughout Novi Town Center provides a consistent image for the area.



Pedestrian features create an attractive and inviting environment.



## ANGLIN AREA

### ***Intent***

The Anglin Area is intended to serve as the eastern “gateway” into the Grand River/Novi Road Business and Main Street Areas. A wide variety of permitted uses and pedestrian-oriented form will activate the area and provide a logical entranceway. Future development should utilize the existing pond as a site amenity.

### ***Preferred Land Uses***

- Retail uses
- Professional offices
- Research and technology uses

### ***Other Land Uses to be Considered***

- Personal service establishments
- Municipal services
- Restaurants (including drive-thru)
- Park feature to access community events and pond.

### ***Form/Design***

Buildings along Grand River should be pedestrian-oriented with reduced front setbacks. Pedestrian paths should connect to the Town Center, Grand-River/Novi Road Business, Hotel/Office and Main Street Areas. The pond and wetland area should be used as a focal point for the new commercial or office space. This green space could also be used to host community events, and the pond used as an outdoor ice rink.

### ***Economic Impact***

Redevelopment of this parcel will expand the available commercial and/or office space for new businesses, as well as provide new job opportunities.

## HOTEL/OFFICE AREA

### *Intent*

With visibility from I-96, the Hotel/Office Area is intended to promote the Town Center area's regional appeal. Well-known hotel brands and office complexes will tie into the district via ample pedestrian connections. A future road connection should extend to Lee BeGole Drive to complete the ring road concept.

### *Preferred Land Uses*

- Hotels/motels
- Extended stay hotels
- Mid-rise residential

### *Other Land Uses to be Considered*

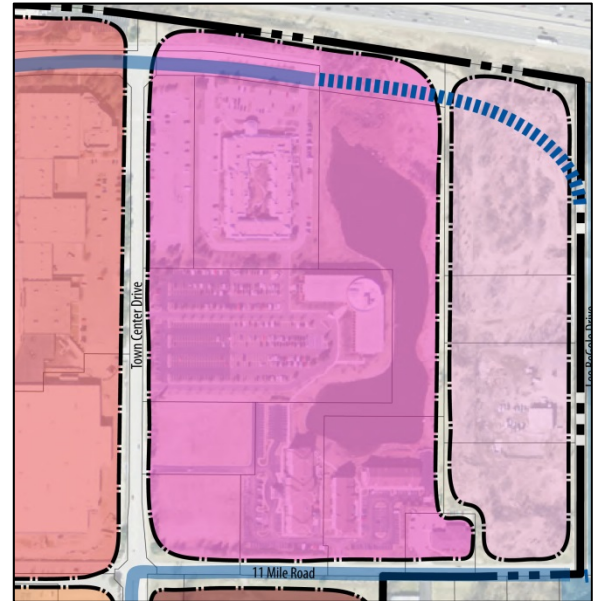
- Office, research and technology uses

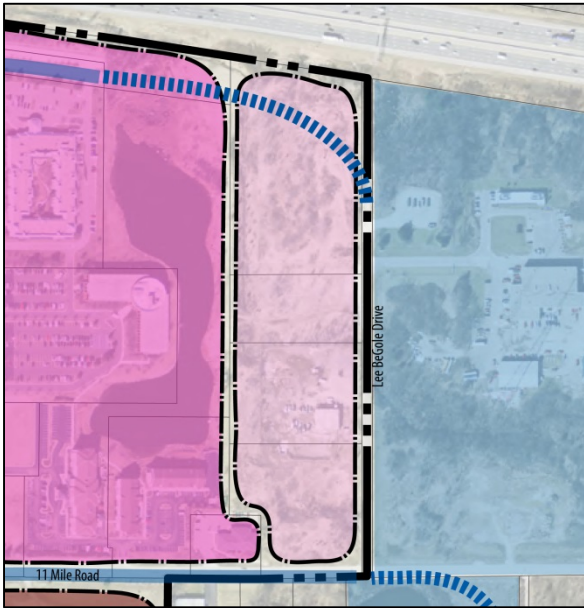
### *Form/Design*

Large-lot hotel and office, research and technology developments, with pedestrian-scaled amenities that connect to the district's retail and commercial core. Select areas could be covered. Best-practices in parking-lot design, landscaping and storm water management should be utilized around the existing pond area.

### *Economic Impact*

The recommendations above build on the successful hotel/office developments already in place. Development on the northeast corner of Eleven Mile and Town Center Drive provides space for a mixed-use project, offering new commercial, office, hotel/motel, or technology/research uses, all of which will offer new job opportunities.





## LEE BEGOLE DRIVE AREA

### ***Intent***

The Lee BeGole Drive Area provides an opportunity for municipal services as well as office, research and technology. Due to its location on the edge of the Town Center area, more intensive municipal services and outside storage is permitted.

### ***Preferred Land Uses***

- Municipal services
- Office, research and technology uses

### ***Other Land Uses to be Considered***

- Screened outdoor storage with municipal services
- Demonstration area for City-owned renewable energy generation facility for City use (i.e. wind turbines or solar panels)
- Public transit hub to serve public transit system

### ***Form/Design***

Research and technology developments, or municipal services which connect to the district's retail and commercial core. Best-practices in parking-lot design, landscaping and storm water management should be utilized. The pond and wetland area should be used as a focal point for the redeveloped office/research/technology or municipal service areas.

### ***Economic Impact***

The recommendations above offer opportunities for new development in a number of fields. This will expand the job opportunities in the area.

# SUBAREA LAND USE MATRIX

Table 2. Subarea Land Use Matrix

USE	SUBAREAS								
	North West	Flint	Trans-X	Main Street	Grand River/ Novi	Town Center	Anglin	Hotel/Office	Lee BeGole Drive
<input type="checkbox"/> =Desirable Land Use 									
<b>Residential</b>									
Dwelling, Mid-rise Condo/Apartments									
Dwelling, Townhouse/Rowhouse									
Dwelling, Loft-Style Condominium									
Dwelling, Upper Stories in Mixed-Use Building									
Live-Work Unit									
<b>General Retail Commercial Use</b>									
Retail Businesses									
Market, Farmers or Green									
Market, Grocery or Supermarket									
Market, Specialty Food and Beverage									
<b>Food and Drink</b>									
Restaurant, Sit-Down									
Restaurant, Drive-Thru									
Restaurant, Take-Out/Delivery									
Bar									
Entertainment with Food Service									
<b>Entertainment and Recreation</b>									
Athletic/Health Club									
Bowling Alley									
Theater									
<b>Health and Veterinary Services</b>									
Animal Clinic or Grooming									
Animal Hospital									
Animal Kennel									
Emergency Medical Care									
Assisted Living									
<b>General Service</b>									
Hotel/Motel									
Personal Care									





USES	SUBAREAS								
	North West	Flint	Trans-X	Main Street	Grand River/ Novi	Town Center	Anglin	Hotel/Office	Lee BeGole Drive
General Service (Contd.)									
Equipment Rental (Indoor)									
Funeral and Internment Services									
Maintenance and Repair: Machinery and Small Engine									
Maintenance and Repair: Personal and Household Items									
Mini-Storage (Indoor)									
Outdoor Storage									
Offices and Financial Services									
Banks									
Offices, Administrative, Business and Professional									
Offices, Medical and Dental									
Research and Development									
Incubator/Prototype Development									
Laboratory									
Municipal/Quasi-Municipal									
Parks									
Government Services, Safety									
Essential Public Services, Indoor									
Essential Public Services, Outdoor									

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# RECOMMENDED ZONING UPDATES

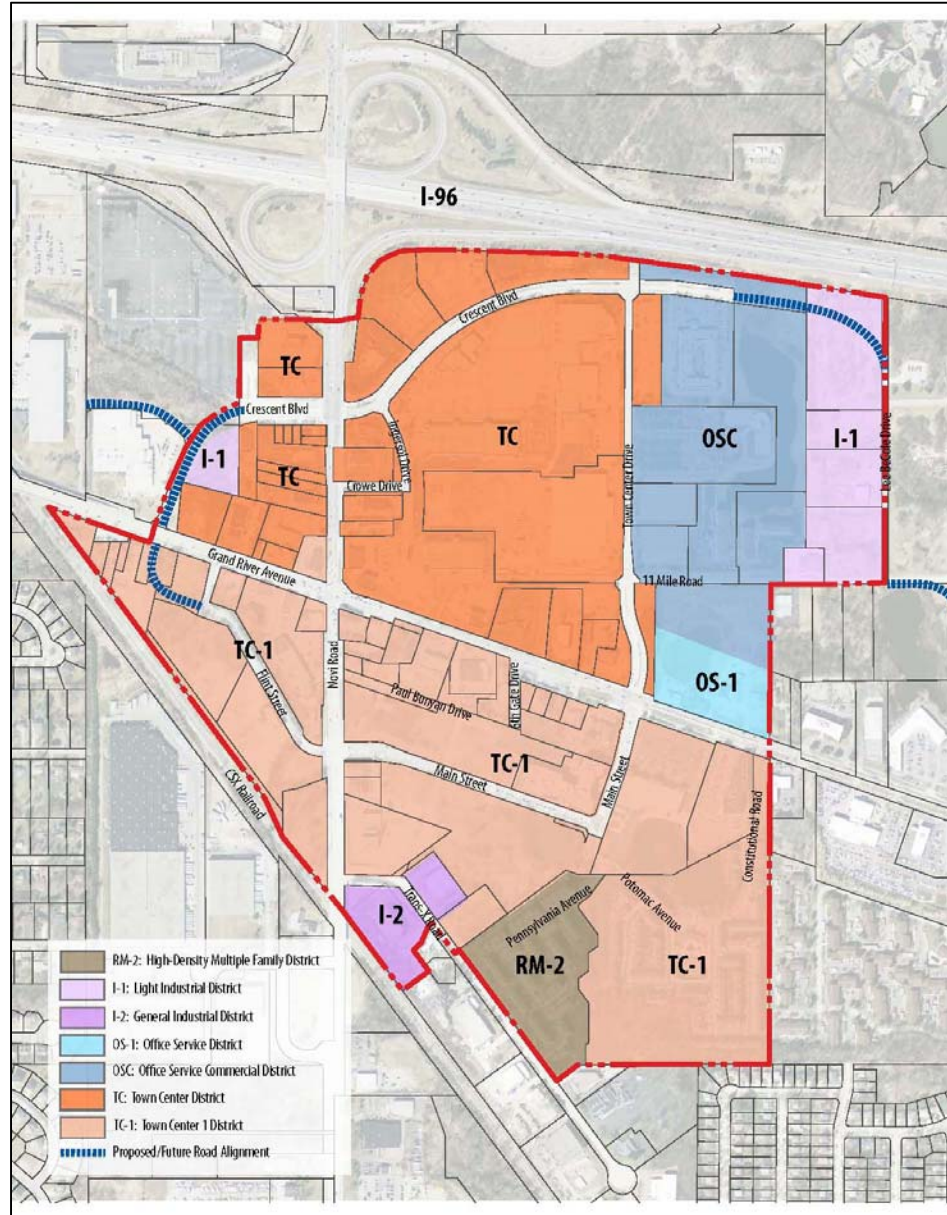
## EXISTING TOWN CENTER ZONING DISTRICTS

The current TC and TC-1 Town Center Districts are designed to promote the development of a pedestrian accessible, commercial service district in which a variety of uses are permitted. The TC-1 District, established in the late 1990s, was developed to reflect the City's goals for a mixed-use "City Center" along Main Street. Current TC-1 regulations prohibit residential uses on the first floor for buildings fronting a public street and establish strict dimensional and design standards, such as maximum leasable floor area and brick paver requirements. These restrictions are intended to create an active streetscape featuring multiple storefronts. Due to changing market conditions and demands, however, a large portion of the Main Street area currently sits undeveloped. In addition, the TC and TC-1 Districts have failed to develop into a cohesive, interconnected and pedestrian-friendly city center, with most viable development having a strictly automobile-oriented design. The map on the next page illustrates the locations of these zoning districts. Table 2 that follows outlines the existing intent, uses, and dimensional regulations laid out in the TC and TC-1 Districts and Figure 9 denotes the existing TC and TC-1 District boundaries.

To facilitate the development of existing and vacant parcels into a viable and active "town center" area and coordinate growth with the surrounding subareas, it is recommended that the TC and TC-1 District regulations be amended and simplified. The amended ordinance should continue to address design features vital to walkability and community vibrancy, such as building form, streetscape transparency, parking-lot design and landscaping, but should be more flexible with regards to permitted uses, floor area dimensions, and ornamental design features. The proposed changes, introduced below, intend to maintain the City's goals for a lively city center, but are shaped to accommodate the ever-changing cycles of the land-use market.

The Office Service Commercial (OSC) also contains significant vacant property that has not developed into additional hotel and/or office uses. To address this area, the study recommends that mid-rise residential uses be allowed in this district as either a permitted use or special land use.

FIGURE 9. EXISTING ZONING BOUNDARIES



## EXISTING TC AND TC-1 ZONING STANDARDS

TABLE 3. TC AND TC-1 ZONING STANDARDS

CURRENT ZONING STANDARDS	<b>Intent</b>		
	<ul style="list-style-type: none"> <li>Promote the development of a pedestrian accessible, commercial service district in which a variety of retail, commercial, civic and residential uses are permitted.</li> <li>Discourage development of separate off-street parking facilities for each individual use.</li> <li>TC-1 District is designed to encourage “Main Street” with mixed land uses and shared parking.</li> </ul>		
	<b>Permitted Uses</b>		
	<ul style="list-style-type: none"> <li>Generally recognized retail</li> <li>Professional offices</li> <li>Hotels</li> <li>Banks (No Drive-Through)</li> <li>Restaurants (No Drive-Through)</li> <li>Single-Family and Multi-Family Residential, given that buildings fronting onto a publicly-dedicated roadway shall have <b>non-residential uses on the first floor</b>.</li> <li>Other uses as listed</li> </ul>		
	<b>Special Uses</b>		
	<ul style="list-style-type: none"> <li>Open air businesses/outdoor sales when incidental to a similar principal use indoors</li> <li>Microbreweries/Brewpubs</li> <li>Fast-food drive-through in TC-1 only, and may not be located closer than 150 ft. from any other parcel with a drive-through.</li> <li>Other uses as listed</li> </ul>		
	<b>Off-Street Parking</b>		
	<ul style="list-style-type: none"> <li>Off-street parking areas set back a minimum of 20 ft.</li> <li>Screened from public right-of-way and internal roads by ornamental brick-on-brick wall or landscaped berm</li> <li>In TC-1 District: No front yard or side yard parking on any non-residential collector. Access to rear-yard parking provided by alleyways.</li> </ul>		
	<b>Architecture/Pedestrian Orientation</b>		
	<ul style="list-style-type: none"> <li>TC-1 District: Buildings may not exceed 125 ft. in width, unless pedestrian entranceways are provided at least every 125 ft. of frontage.</li> </ul>		
	<b>Schedule of Regulations</b>	<b>TC</b>	<b>TC-1</b>
	Height:	65 ft./ 5 stories	78 ft./5 stories*
	Retail Space Floor Area:	No restrictions	7,500 s.f. max. gross leasable floor area for retail bldgs. May be exceeded under certain conditions.
	<b>Building Setbacks:</b>		
Front (Interior/Exterior)	**15 ft./50 ft.	15 ft./***	
Side (Interior/Exterior)	10 ft./50 ft.	10 ft./50 ft.	
Rear (Interior/Exterior)	10 ft./50 ft.	10 ft./50 ft.	
<b>Non-Residential Collector and Local Streets:</b>			
Front (Min./Max.)	--	0 ft. / 10 ft.	
Side (Min./Max.)	--	0 ft. / None	
Rear (Min./Max.)	--	0 ft. /None	

\*Height bonus for mixed-use buildings with retail/office on the first floor.

\*\*For setbacks between separate buildings on same lot.

\*\*\*No less than 80 ft., no greater than 137 ft. as measured from centerline of arterial.





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## PROPOSED ZONING CHANGES

While the amended TC and TC-1 Districts should reflect the land use goals outlined previously within this report, they should maintain the original intent of the TC Districts to create mixed-use “pedestrian accessible, commercial service district(s).” However, the revised TC Districts should allow more flexibility in terms of use and dimensions in order to realistically achieve such goals. The following changes could be made to the TC and TC-1 Districts to implement the main goals and intent of the Town Center area as a whole. In addition, the proposed mid-rise residential use added to the Office Service Commercial (OSC) district will provide greater flexibility of uses in this area.

### TC and TC-1 Intent

The intent of the TC and TC-1 districts should be modified as follows:

- **Clarify intents of TC and TC-1 Districts.** The overall intent is to serve as a mixed-use/pedestrian-oriented focal point for the City of Novi. The intent should promote a mix of uses including retail, commercial, office, residential, institutional uses and open space. Further, the intent should be clarified to mention that the separate districts are not intended to regulate for different uses, but rather regulate the *intensity* of such uses and dimensions of form/design. While the TC and TC-1 Districts are intended to function as a cohesive area, the TC District is intended to accommodate regional commercial uses/form while the TC-1 District is intended to accommodate neighborhood-scaled development.
- **De-emphasize intent to “discourage development of separate off-street parking facilities.”** While shared parking and efficient distribution of peak parking demand are sustainable and worthwhile long-term goals, the lack of a central parking agency or special assessment district within the Town Center area makes it difficult to achieve such goals.

### TC and TC-1 Permitted Uses

The list of permitted uses in the TC and TC-1 districts should be modified as follows:

- **Clarify definition of “theater” to indicate performing arts theater.** Multi-plex theaters are appropriate for TC but may not be for TC-1 (§1601.2a(4)). However, single or dual screen theaters and performing arts theaters may add character to the neighborhood-scaled TC-1 areas.
- **Add pharmacies as permitted use in the TC District, provided that such uses do not have drive-through service as the principle use (§1601).** Pharmacies with drive-through services should be regulated by building

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form, but the presence of such service does not seriously impact the demand for the pharmacy.

- **Add extended-stay hotels as a permitted use (§1601).**
- **Add green market as a permitted use (§1601).** An indoor market with booths could serve as a catalyst for development within the TC and TC-1 Districts. An outdoor farmers' market could be added as a special use. Supplemental definitions and standards could be added to regulate location, pedestrian features, hours of operation, etc.
- **Remove standard that prohibits residential uses on the first floor in buildings that front onto a local collector within the TC-1 District, but add regulation that prohibits *individual* garages on a front-facing façade (§1601.10d).** Entrances to parking garages are acceptable on front-facing facades. Attached-style residential uses such as townhomes are appropriate for Main Street, given that form, building placement, and certain design features are regulated within the Ordinance.

### TC and TC-1 Special Uses

The list of special uses should be modified as follows:

- **Review regulation that prohibits clustering of drive-through restaurants within TC-1 District (§1602.4g).**
- **Add incubator/prototype development workshops as a special use (§1603.2).** Incubator workshops have the potential to promote adjacent development. Additional regulations could be included to regulate for building form, hours of operation, and performance-based criteria.
- **Add pharmacies as special use in the TC-1 District, provided that such use does not have drive-through service as a principal use.**
- **Add outdoor farmers' market as permitted use (§1601).** A visible outdoor market could serve as a catalyst for development within the TC and TC-1 Districts. Supplemental definitions and standards could be added to regulate location, pedestrian features, hours of operation, etc.

### OSC Permitted and/or Special Use

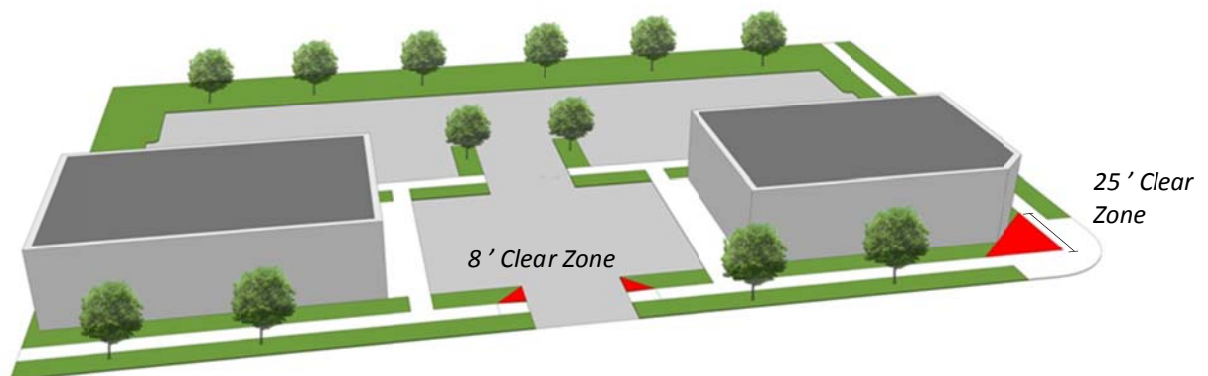
The list of permitted and/or special uses should be modified to include mid-rise residential condominium or apartment buildings.

## Schedule of Regulations

The schedule of regulations should be modified as follows:

- **Increase maximum gross leasable floor area (GLA) within TC-1 from 7,500 s.f. to 13,000 s.f. (§1603.3).** The existing 7,500 s.f. regulation prohibits the development of many types of uses. Additionally, existing standards within the TC and TC-1 Districts regulate for maximum building frontage, varied façade design treatments, and maximum spacing between building entrances, rendering the 7,500 s.f. standard excessive.
- **Add more information regarding clear vision areas on non-residential collectors and local streets (§1603.4).** Cross reference Section 2513 – Corner Clearance and add a graphic, such as Figure 10, for reference.
- **Reduce minimum residential setback from 30 feet to 15 feet from all property lines, except when interfering with corner clearance areas (§1601.10b).** The reduced setback is intended to promote the construction of attractive townhouse-style apartments along Main Street; a continuous street-wall activates the streetscape.
- **Add note that requires 10-foot side setback for residential buildings that feature side windows (§1601.10b).**
- **Clarify meanings of “interior” and “exterior” lot setbacks and adjust dimensions (§1603.4).** Section 201, Definitions, establishes a definition of “interior lot/yard”; however, the “exterior lot/yard” definition is ambiguous. The existing front setback requirements for properties fronting arterials within the TC and TC-1 Districts are excessive, and should be reduced to a minimum of 15 feet. The reduced setback allows for more pedestrian-oriented building placement and appropriately reflects the front setbacks of recently-constructed development.

FIGURE 10. EXAMPLE CLEAR VISION AREA GRAPHIC



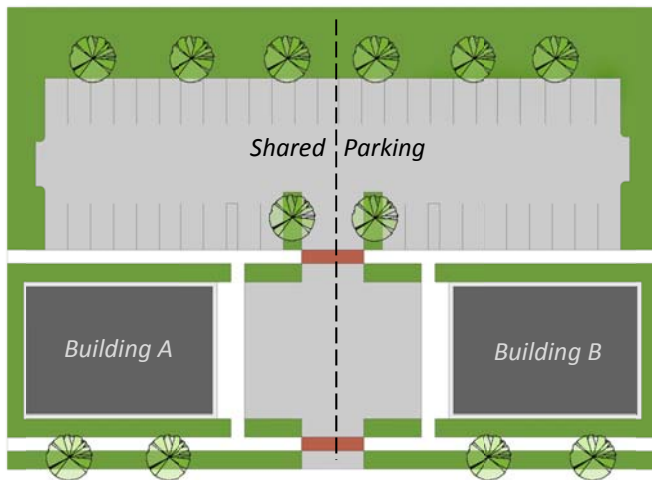
Source: Carlisle/Wortman Associates, Inc.

## Off-Street Parking

Off-street parking regulations should be modified as follows:

- **Add additional guidelines for implementing shared parking lots (i.e. information on obtaining cross-access and maintenance agreements (§1603.10)).** The use of shared parking lots limits the number of curb-cuts, improves efficiency in terms of parking demand, and enhances the pedestrian experience. Figure 11 illustrates a collective parking lot layout that could be included in the Ordinance.
- **Cross reference Section 2505.8 – Dual functions of off-street parking.** Said reduction may be considered by the Planning Commission only after the submittal by the applicant of a Shared Parking Study, prepared by an individual or firm with demonstrated experience in parking analysis. The methodology used in the report shall generally follow the guidelines set forth in the following documents: *Shared Parking – Potential for Application with the Town Center Districts* (December, 1993), prepared for the City of Novi, and *Shared Parking* (4<sup>th</sup> Printing 1990), prepared by the Urban Land Institute.

FIGURE 11. EXAMPLE SHARED PARKING GRAPHIC



Source: Carlisle/Wortman Associates, Inc.



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## Streetscape, Pedestrian Amenities and Alternative Transportation

Sections relating to architecture and pedestrian orientation should be modified as follows:

- **Require larger non-residential establishments that exceed certain street frontage, setback, or land area thresholds to provide public/pedestrian amenities including, but not limited to, patio/seating area, pedestrian plaza with benches, arcade, sculpture, public art or water features.** These features serve the purpose of engaging, accommodating and enhancing the pedestrian experience within the TC and TC-1 Districts. Feature details should be left to the discretion of the Planning Commission, and may be waived or modified. Consider offering a “Cash in Lieu of Amenity Construction” option (§1603.8) and discuss options for money management.
- **Modify requirement for brick/decorative paving which will allow a greater flexibility of pavement design and reduce maintenance costs (§1603.7).** Attractive brick pavers can be an effective accent in a streetscape setting, but if they are not properly installed and maintained, they can quickly become a safety, maintenance and visual problem. A safe, tidy and well-landscaped plaza or sidewalk can achieve the same effect of decorative paving at a lower cost.
- **Adjust regulation for surface parking screening to allow semi-transparent screening (brick pilaster with metal decorative fence (§1603.5)).** While parking areas should be somewhat screened to maintain attractive streetscapes, unadorned, solid brick screening fences sometime detract from the streetscape and do not accommodate transparency between parcels/uses.
- **Cross reference Section 2526 – Bicycle Parking Facility Requirements.** Bicycle facilities should be required to reduce the Town Center’s automobile orientation.
- **Internal shuttle bus system.** The Town Center area may be too large for some pedestrians to walk and access all the amenities offered. These people could be served by an internal shuttle bus system, possibly originating at one of the hotels, that circulates through the Town Center area. The bus could serve the various hubs within the Town Center area and provide safe travel across Grand River Avenue and Novi Road.
- **Alternative public transportation/transit.** Development should promote alternative public transportation concepts including connections to park-and-ride facilities, lanes for public bus standing, linkages to safety paths, facilities for bicycle storage, and possible connections to public bus, taxi, rideshare or public senior transit. A public transit hub could be developed to serve the buses in the Lee BeGole Area.

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## Design Guideline Amendments

Section 2520 of the Zoning Ordinance should be modified:

- **Metal Roofs.** Note #3 in this section should be modified to allow greater flexibility for standing seam metal roofs. The current standard requires 100% copper or copper-bearing paint in the TC and TC-1 districts. It should be modified to include colors and/or materials which are complimentary to a copper finish.
- **Allowable Building Materials.** Notes #13 and #14 should be modified to allow greater flexibility in building materials while preserving the quality of the built environment.

## Zoning Map Amendments

The zoning map should be modified as follows:

- **Rezone the Anglin Area from OSC/OS-1 to TC.** Since this property is directly across Grand River from the large gatehouse structure on Main Street, it then becomes the “bookend” to the core Town Center area eastern gateway. Therefore, the land uses and form on this parcel should follow the patterns established in the area. The proposed zoning districts will provide greater variety of permitted uses and pedestrian-oriented form. This will activate and promote the area as a gateway into the core Town Center area.

These uses and proposed character will work with, and not compete with, the current businesses in the area, creating a critical density of like uses to make the district economically viable.

- **Rezone Trans-X Area from I-2 to TC-1.** The Trans-X Area is also at a logical “gateway” to the core Town Center area. Its strategic location demands that the uses be modified to reflect the commercial and mixed-use character of the Town Center area. Therefore, rezoning these parcels to TC-1 will accommodate a wider variety of uses while exhibiting a pedestrian-oriented form. It will also allow the parcels to act as the southern gateway to the core Town Center area.

Transitioning these parcels from industrial to mixed-use will benefit nearby businesses. The TC-1 uses will compliment and support surrounding businesses and residential areas rather than potentially generating noise, odors, or other negative impacts to the commercial/office/residential uses that are developing around them.

The proposed zoning changes are represented in Figures 12 and 13 on the following pages.

FIGURE 12. CURRENT TC/TC-1 ZONING DISTRICTS MAP

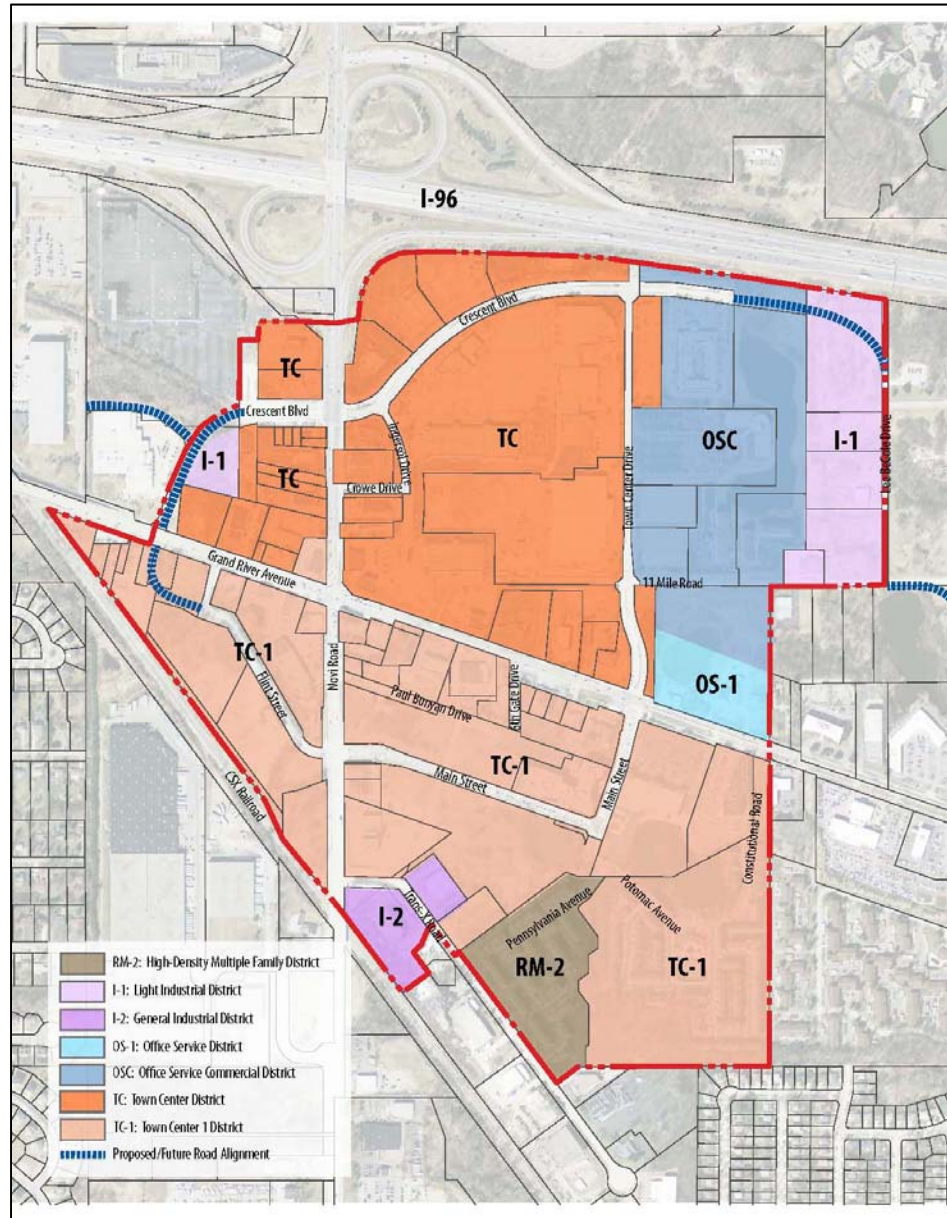
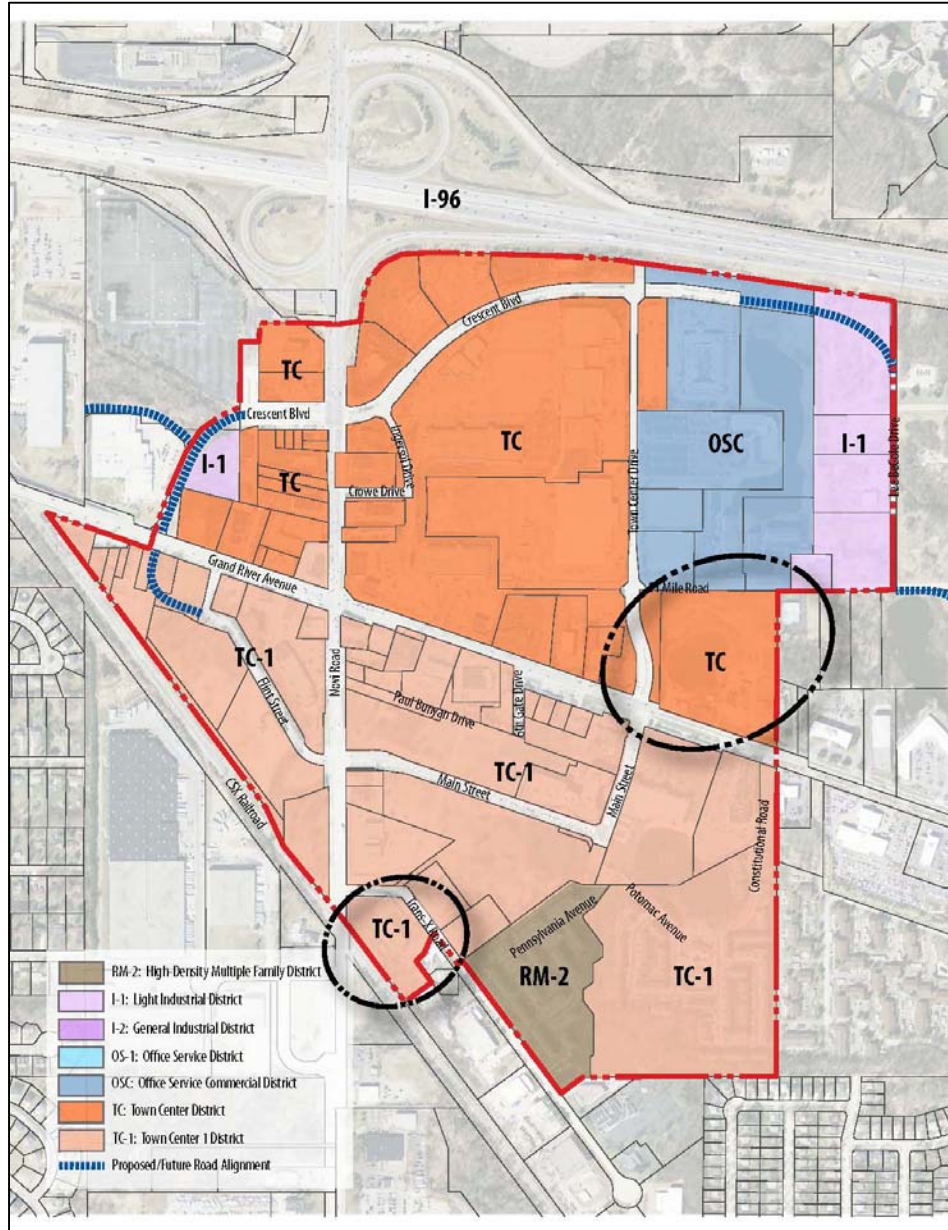


FIGURE 13. PROPOSED TC/TC-1 ZONING DISTRICTS MAP

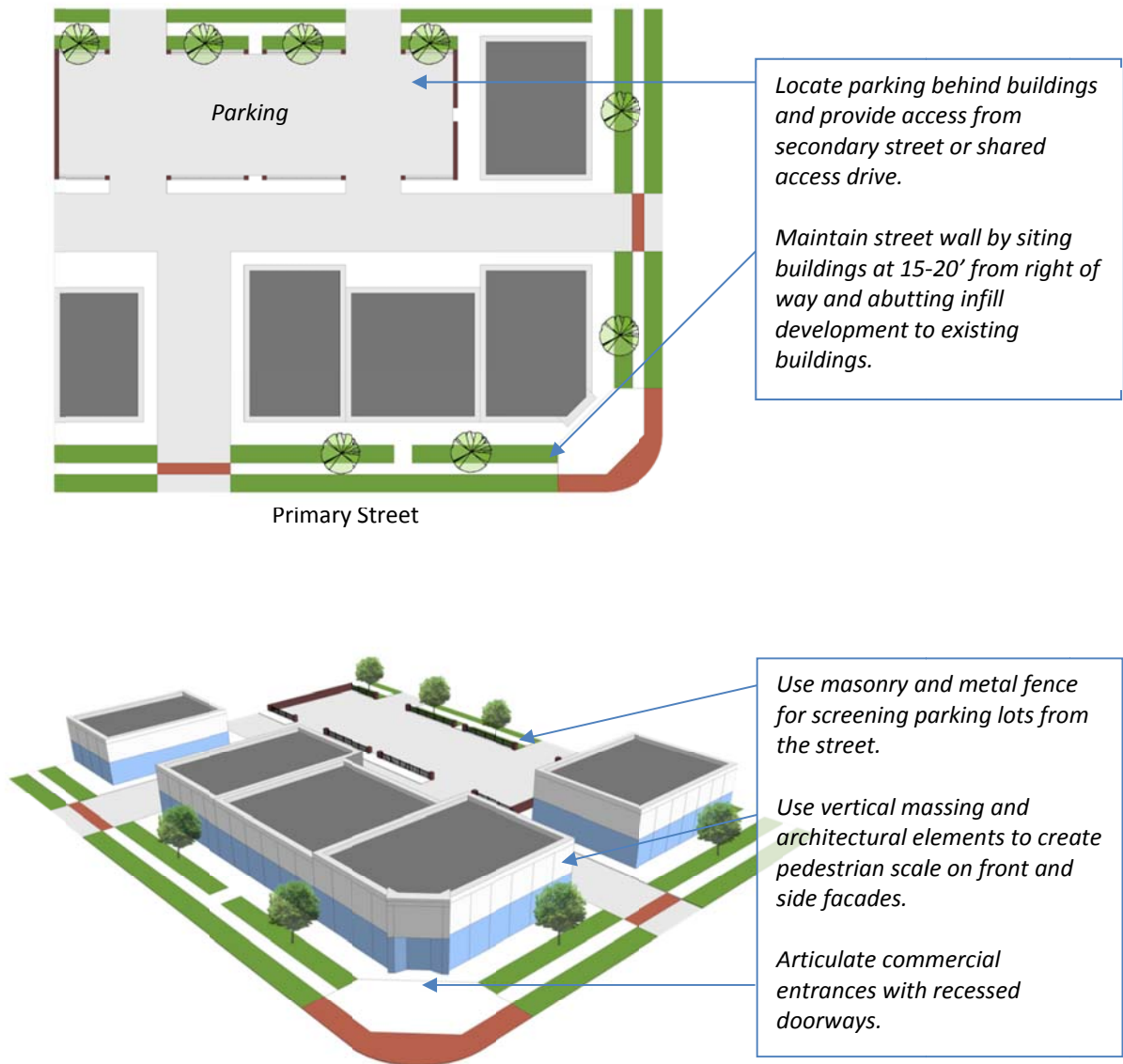




## FORM-BASED GRAPHICS AND ELEMENTS

In addition to the proposed revisions mentioned above, the amended TC and TC-1 District regulations should include clear graphics that represent the desired form(s)/design(s) within the Districts. These graphics may denote elements such as building and parking lot placement, transparent façade designs, pedestrian-oriented open space design and building height. Figure 14 below displays several example graphics that depict requirements for rear-yard parking areas and zero-setback building lines. These graphics can efficiently supplement the general zoning standards, are easy to insert within the ordinance, and offer a means of envisioning the zoning ordinance on the ground.

FIGURE 14. EXAMPLE FORM-BASED GRAPHICS



Source: Carlisle/Wortman Associates, Inc.

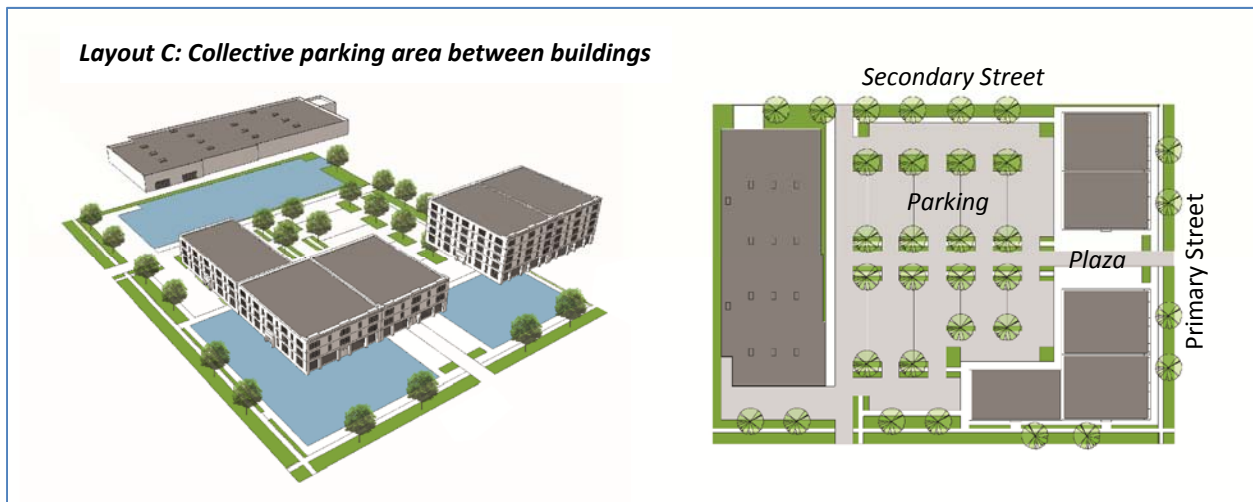
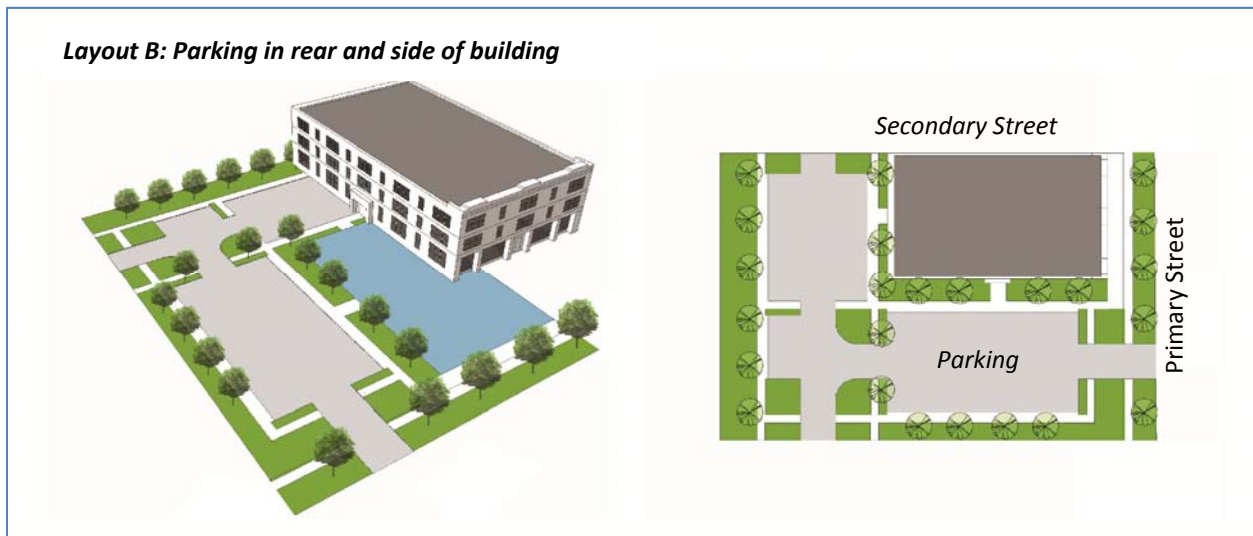
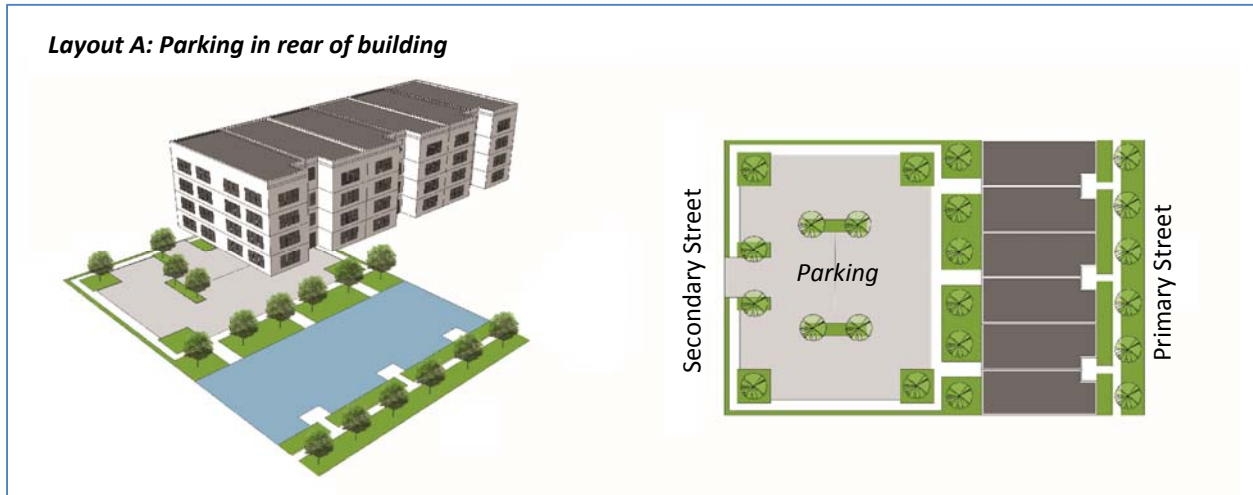
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In the long run, it may be beneficial to establish new hybrid zoning districts or form-based overlay districts for several of the subareas previously discussed in this report. **Form-based** districts put an emphasis on regulating form and building design rather than only land use and dimensions, and offer a long-term regulating plan. Both hybrid and overlay form districts introduce innovative form regulations while maintaining the administrative structure and procedures for the traditional zoning ordinance. A **hybrid district** combines the graphic orientation and street frontage/site configuration provisions from form-based models with the use provisions and design guidelines of the traditional ordinance into a stand-alone district. Conversely, an **overlay district** maintains the general intent and use standards of the base districts (TC and TC-1) while adding overlying form-based districts that include supplemental site configuration and design regulations. Figures 15a and 15b display several examples of site configuration and design graphics that could be included within a hybrid or form-based overlay.

Hybrid or form-based overlay districts could potentially follow the boundaries of the selected subareas. Each district could require unique building design and placement standards, reflecting the different characteristics and intent of each subarea. For example, the Main Street Area and Grand River/Novi Business Area, while similar in intent, may require different building form and placement due to the nature of the streets that run through them; the distinct hybrid/form-based districts for each subarea could reflect such differences by requiring varying minimum front setbacks, site configuration and façade design. It should be noted that hybrid/form-based overlay districts need not be created for every subarea. Due to their location away from main corridors, the Hotel/Office, Lee BeGole Drive and Flint Street areas would see limited benefit from supplemental form-based regulations.

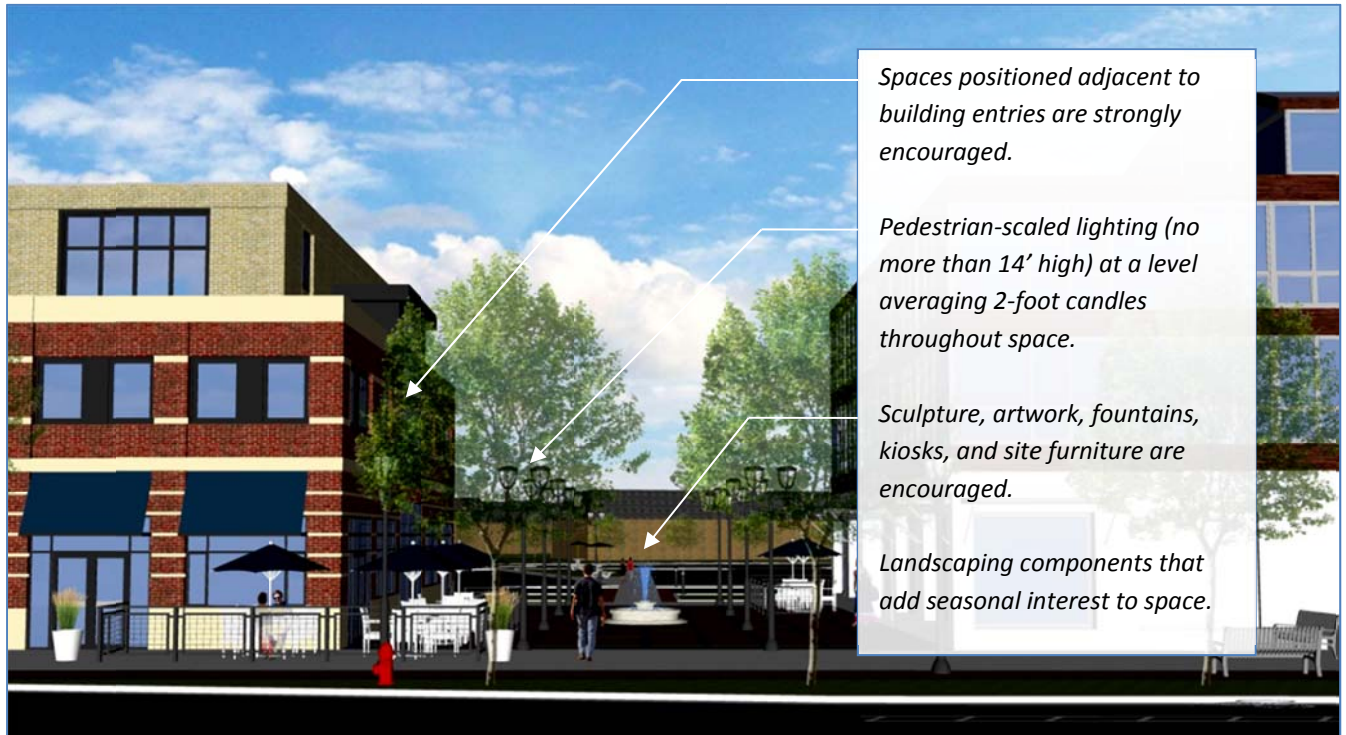
Additional regulations that could be addressed by hybrid or form-based overlay districts include façade transparency, parking lot frontage and screening, building materials, streetlights, street furniture, and pedestrian plazas. Form-based districts offer an opportunity to incorporate design guidelines into the Zoning Ordinance at a subarea level without interrupting the intent and general land use standards of the TC and TC-1 districts and the administrative structure of the Zoning Ordinance as a whole. Design guidelines will be discussed in more detail in the following chapter.

FIGURE 15A. FORM-BASED ELEMENTS: SITE CONFIGURATION GRAPHICS



Source: Carlisle/Wortman Associates, Inc.

FIGURE 15B. FORM-BASED ELEMENTS: PEDESTRIAN-ORIENTED OPEN SPACE



Source: Carlisle/Wortman Associates, Inc.





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# RECOMMENDED DESIGN GUIDELINES

Novi's Zoning Ordinance provides requirements for a number of site and building features within the TC and TC-1 districts. This chapter provides both general and specific recommendations for creating requirements that are more flexible, but still achieve the desired result of high-quality architectural design and functional and attractive site amenities.

*Section 1603, Schedule of Regulations/Required Conditions* of the TC and TC-1 districts specifically outlines architectural and pedestrian requirements for this district. Also, *Section 2520, Exterior Building Wall Façade Materials*, describes the required materials and application of these materials to building facades. Both sections were evaluated to create the recommendations that follow.



Main Street illustrates many desirable features, such as pedestrian circulation facilities and street amenities, including masonry screen walls, planters and benches.

*These guidelines recommend incorporating a complementary mix of materials and colors, maintaining lines and proportions, and responding to the rhythm and pattern of existing buildings in a way that enhances the Town Center character and contributes to the creation of place.*

## GENERAL STANDARDS

General standards that were used to guide the design choices described in this chapter include the following:

- The Town Center development should promote pedestrian circulation and encourage physical and visual connections between adjacent uses.
- Pedestrian amenities such as streetscape features, site furnishings, lighting, pavers, and landscaping should be required and meet the standards provided in this chapter or deemed compatible by the City of Novi.
- All projects should demonstrate quality architecture and landscaping consistent with these guidelines.
- Any changes to the built environment should complement rather than compete with or duplicate existing open spaces and site furnishings.
- Provisions for circulation between developments on adjacent parcels should be required for larger sites through joint drives and cross-access connections.
- Design choices and site furnishing should be simple and timeless. Generally, these guidelines recommend incorporating a complementary mix of materials and colors, maintaining lines and proportions, and responding to the rhythm and pattern of existing buildings in a way that enhances the Town Center's character and contributes to the creation of place.

## PEDESTRIAN CIRCULATION

### *Design Intent*

Transforming the Town Center from a destination dominated by vehicles to a more pedestrian-orientated place will require design decisions that allow people on foot to move comfortably and safely through the area. The pedestrian circulation network should be given a greater sense of comfort and safety through the use of street trees, landscaping, pedestrian-scaled lighting, complementary materials, and other site amenities. These features clearly indicate that the space is designed to accommodate pedestrian movements.

### *Scale and Configuration*

Sidewalks should be 6 to 10 feet wide, made of concrete, and incorporate barrier-free sidewalk ramps. Sidewalks should be located along all roadways and provide internal connections to buildings and parking lot areas. Brick pavers can be used as an accent, or to signify a change in the pedestrian environment, such as at a pedestrian plaza.

As development occurs in the Town Center, larger developments should provide more and larger-scaled amenities, as compared to small projects. Examples include pedestrian aisles across parking areas, functional and aesthetically pleasing pedestrian-oriented entryways, and public open spaces for individuals and groups to gather.

### *Color and Materials*

Pavement for sidewalks and plazas should be predominantly concrete, and may be scored using a modern, rectangular pattern, or approved equal. Brick should be used judiciously as an accent. Paver color should coordinate with the colors and tones of existing buildings.



Example of a clear pedestrian way, framed by street trees, lighting, and other amenities along Novi's Main Street.



Example of a pedestrian connection within a parking lot.

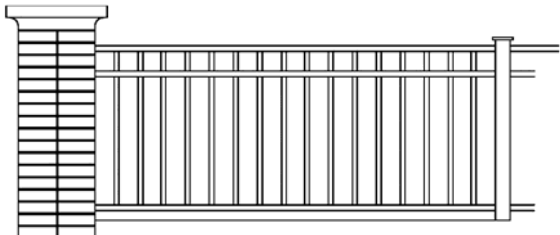


Large expanses of brick paving should be avoided due to maintenance issues. Instead, brick should be used as an accent to a concrete walk or plaza.





Encourage use of a low, masonry wall and column with metal fence combination, as shown in Main Street area.



Example of a metal fence combined with masonry column.



Encourage the use of combined masonry and fence to provide transparency in screening structures.

## SCREENING

### *Design Intent*

The primary screening objective is to reduce the visual impact of parking lots and encourage pedestrian circulation through these different zones. Rather than a continuous low brick wall which creates a physical barrier to pedestrians, a combination of masonry, metal fencing, and plant materials should be used to screen parking areas. This mix of materials will provide transparency, depth, and movement, as well as seasonal interest.

### *Scale and Configuration*

Fences and walls should be setback a minimum of 5 feet from the back of sidewalk to allow for a planting strip. Pedestrian circulation pathways take priority in terms of site design and arrangement; screening material should accommodate open space connections between adjacent uses. The screening wall may be configured to accommodate larger landscaped areas and street trees, or incorporate seating elements and signage.

Maximum height for fences adjacent to the public right of way should be 36 inches and 6 feet for any internal privacy fence used for screening utilities, dumpsters, or service area.

### *Color and Materials*

The screening of parking lots along roadways should be accomplished with a stone or masonry pillar and fence detail depicted here. The design should be coordinated with the materials, colors, quality, scale and detail of the adjacent building. All other fence materials, including chain link and vinyl-clad chain link should be prohibited.

## LIGHTING

### *Design Intent*

A well-lit physical environment improves safety and furthers the perception of a more inviting and active place after dark. To meet the lighting needs of Town Center area users, and also maintain a high aesthetic quality, parking lot and pedestrian light fixtures should be consistent with other fixtures in the same lighting tier. Accent light fixtures shall be timeless and unobtrusive, and complementary in terms of material, scale, and style.

### *Scale and Configuration*

Tier 1 fixtures are located along Grand River and Novi Road. Fixtures are more decorative in nature, and will be in accordance with DTE standard. Tier 2 fixtures are placed along Town Center Drive, Crescent Boulevard, and Crowe Drive. These cobra style fixtures are in accordance with DTE standard roadway fixtures. Tier 3 lighting includes a cobra head, parking lot fixture mounted at 25 feet or less, as well as a 14-foot pedestrian light fixture to be used along pedestrian sidewalk connections, refuge islands, plazas, and nodes.

### *Color and Materials*

Tier 1 light is a teardrop style LED luminary with a standard black DTE pole accented with a decorative black clamshell base. Use Holophane ESL 150 4K AS X 4 LED Teardrop or approved equivalent.

Tier 2 (and Tier 3 parking) light is black with a square, thin LED fixture placed on a straight vertical pole. Use American Electric Lighting ATB2 SERIES 135W LED 700MA or approved equivalent.

Tier 3 pedestrian-scaled fixture is an LED, full cut-off fixture, with acorn-shaped glass globe and decorative metal band around the globe, placed on a contemporary bottle neck pole. Use Lumec Serenade DSX Series S56C1-SFX-BSC or approved equivalent.



Tier 1:  
Major  
Roads

Tier 2:  
Internal  
Roads

Tier 3:  
Pedestrian Ways  
Parking Areas



Tier 1 fixture shall be specified as a Holophane ESL 150 4K AS X 4 LED Teardrop or approved equivalent.



Tier 2 and 3 fixtures shall be specified as an American Electric Lighting ATB2 Series 135W LED 700 MA or approved equivalent, mounted at the appropriate height determined by location.



Tier 3 fixtures shall be specified as a Lumec Serenade DSX Series 356C1-SFX-BSC or approved equivalent.



Recommended metal and wood composite Plainwell bench, similar to existing benches in Town Center area.



Example of recommended alternative Plainwell bench in all aluminum.



Example of recommended Plainwell trash receptacle.

## SITE FURNISHINGS

### *Design Intent*

Site furnishings fulfill two roles in the Town Center area: they allow pedestrians to comfortably and functionally use a space, and they help to define the pedestrian realm.

Seating and landscape elements improve the user experience and encourage people to linger in a place, while trash receptacles, bike racks, information kiosks, and bollards convey the Town Center's commitment to cleanliness, choice, and safety.

## BENCHES

### *Scale and Configuration*

Benches should provide at least 6 lineal feet of seating area. Benches should be accessible and functional, placed on a level surface near other furnishing such as lighting, trash receptacles, shade trees, and public art. Wide steps, ledges, and planters can provide flexible, informal seating areas around plazas and retail areas.

### *Colors and Materials*

Benches shall be metal and wood composite, with simple lines and a classic look. Use Landscape Forms Plainwell aluminum end frames with black powder coat finish with wood bench or an all-aluminum bench, or approved equal.

## TRASH RECEPTACLES

### *Scale and Configuration*

Trash receptacles should be placed at appropriate intervals within the site. Place in close proximity to pedestrian waiting areas and building entrances without being a nuisance or obstacle.

### *Colors and Materials*

Trash receptacles should have 35 gallon capacity with top opening. Receptacle shall have a black powder coat aluminum frame with the option of a wood or aluminum side panel. Use Landscape Forms Plainwell or approved equal.



## BIKE RACKS

### *Scale and Configuration*

Bike racks should be placed near building entrances and preferably under an overhang. Bike racks should not impede the flow of pedestrian traffic or interfere with a seating area when in use. The inverted “U” rack design should be used, as this design provides the most efficient bicycle parking. Bicycle parking facilities should follow the standards in *Section 2526 Bicycle parking facility* requirements in the Zoning Ordinance.



Inverted “U” bike rack design.

## BOLLARDS

### *Scale and Configuration*

Bollards are used in pedestrian areas to provide separation from vehicular areas, as well as to provide low-level light on walking surfaces. Bollards should be placed at 6-foot intervals along large pedestrian areas, particularly those near T-intersections that do not have other prominent elements such as raised planters.

### *Color and Materials*

Bollards should be black, metal with a straight post and a louvered light fixture on top. Use Holophane H102 large or approved equivalent.



Recommended bollard.

## FENCES

### *Scale and Configuration*

Restaurant café seating areas should be separated from the sidewalk with a 2 to 3 foot tall fence.

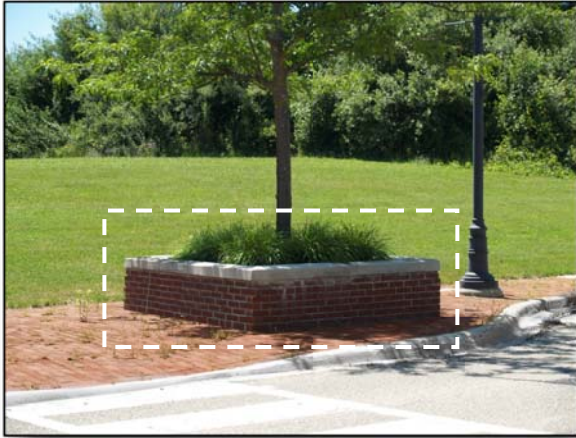
### *Materials and Finish*

Fences should be metal, black, painted or plastic coated. Fences may incorporate tall, rectangular planters intermittently.



Example of a metal fence enclosing café outdoor seating area to be encouraged.





Example of a masonry planter with limestone cap located on Main Street.



Example of a sample planting area defined by an edge which is attractive and effective.



Example of a tree grate found on Main Street.

## PLANTERS

### *Scale and Configuration*

Raised planters help to separate the street edge from the pedestrian realm. Planters 1 to 2 feet high and 8 inches to 1 foot wide can provide added seating, shade, and add a buffer between pedestrian and vehicular traffic. Planters should be used on larger sites which have adequate room for pedestrian passage around the planter.

### *Color and Materials*

Temporary planters should be compatible with the aesthetics of the private development. Classic and/or contemporary planters are preferred. Planters in the public right-of-way may be scored concrete or masonry; masonry planters should have a limestone cap. Planters should respond to adjacent site furnishings and building façade in terms of colors and materials.

## TREE GRATES

### *Scale and Configuration*

Street trees surrounded by hardscape should have a metal grate to allow water to reach the root system. Refer to the *City of Novi Landscape Design Manual* for street tree and landscaping requirements. Tree grates should be used where trees are desired within pedestrian ways and where there is adequate room for pedestrian passage around the trees.

### *Materials*

Grates should be metal with a simple pattern.

## LANDSCAPING MAINTENANCE

All landscaping beds should be irrigated per the City of Novi's Landscape Design Manual.

## BUILDING FACADE

### *Design Intent*

A distinctive Town Center character will be achieved through consistent lighting, coordinated site furnishings, a continuous pedestrian network, and a commitment to high-quality design, rather than a prescribed color palette or specific building appearance.

### *Scale and Configuration*

New development should respond to the building material, fenestration, vertical and horizontal lines, overhangs, and cornice of adjacent structures, while maintaining a unique architectural style. Windows that provide such interest should be transparent to allow views of activity within the building interior.

### *Materials*

Any building façade wall greater than 100 feet long may be eligible to receive consideration for alternative quality materials to provide additional interest. Also, consideration should be made for limiting use of opaque material to cover windows. Refer to *Section 2520 Exterior Building Wall Façade Materials* and *Article 16: TC and TC-1 Town Center Districts, Section 1603.9* of the Zoning Ordinance for more information.

## PUBLIC ART

### *Design Intent*

Public art celebrates a community's heritage and identity. Taking note of Pascal Knapp's international public art exhibit CowParade, Novi can embrace the Novi Special as the symbol of the Town Center area by installing fiberglass replicas at different pedestrian-oriented sites, with the support of local companies and organizations. Public art, or other amenities, such as patio seating, plazas, benches, or water features should be required for larger site developments which exceed a certain threshold for street frontage or land area.

Public art may also include sculpture, fountains, gateway arches, or plaza designs that incorporate interesting elements such as a sun dials, labyrinths, checkerboard pattern, or water features.



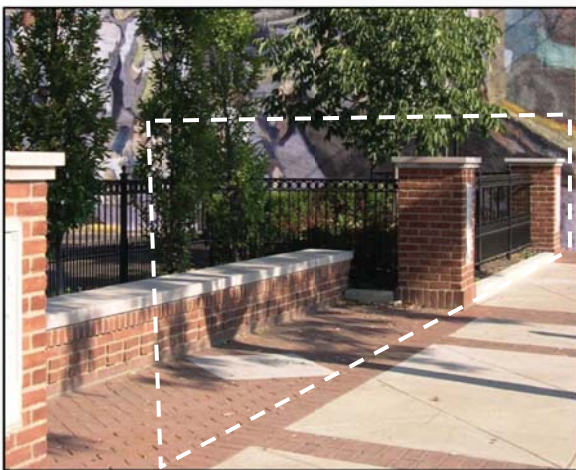




Example of an inviting, well designed plaza with strong pedestrian circulation.



Encourage fountains/water features, similar to the current fountain/water feature found in Novi Town Center.



Integrate multiple site amenities in plaza designs.

## PLAZAS

### *Design Intent*

The Town Center should focus on creating larger plaza areas in close proximity to retail uses, restaurant patios, natural features, and other amenities. People seek public spaces with life and activity; small plazas often leave users feeling isolated or over exposed. Plazas should incorporate feature elements such as landscaping or public art. Seating arrangements should provide flexibility and accommodate a range of users, activities, and group sizes.

### *Scale and Configuration*

A good plaza creates the sense of entering into a space, rather than just stepping off the path. They should be easily accessed by a range of transit modes including walking, biking, and driving. Larger plazas should be designed to accommodate specific functions, such as outdoor performances, vendor stalls, or outdoor education.

### *Color and Materials*

Plazas should incorporate a high concentration of site furnishings such as benches, screening, public art, tier 3 pedestrian lighting, and trash receptacles.

# RECOMMENDED WAYFINDING SYSTEM

The Town Center area has developed into a multi-faceted mix of destinations, including shopping centers, entertainment and restaurant venues, hotels, and municipal services, such as a recycling center. To help visitors easily find their destination, a system of strategically placed signs (called a “wayfinding system”) is proposed that builds on the existing image established by the City of Novi’s sign program. The intent of the proposed system is to have at least eight signs within the Town Center area. More signs can be added at other intersections over time as needed.

## ROAD COMMISSION FOR OAKLAND COUNTY REQUIREMENTS

Since both Grand River Avenue and Novi Road are under the jurisdiction of the Road Commission for Oakland County (RCOC), any signage is required to follow the Michigan Manual of Uniform Traffic Control Devices (MMUTCD). This manual identifies wayfinding signs as “Community Wayfinding Signs,” and rules regarding these types of signs are found in *Section 2D.50 Community Wayfinding Signs* of the manual. This section has requirements for every aspect of Community Wayfinding Signs, including sign size and height, location, spacing, minimum text sizes and spacing, number of destinations per sign, and other requirements.

Community Wayfinding Signs are possible in two configurations: one size that serves vehicles and one size that serves pedestrians. The vehicular signs are generally much larger, and require larger fonts and fewer messages to enable a person to drive and safely read the sign at the same time. The pedestrian signs are generally smaller and allow more messages, as reading a sign does not pose a safety hazard.

The next page shows the proposed sign designs for the Town Center area. The sign designs will need to be evaluated and approved by the RCOC, who also has the ability to grant variances from the MMUTCD requirements.



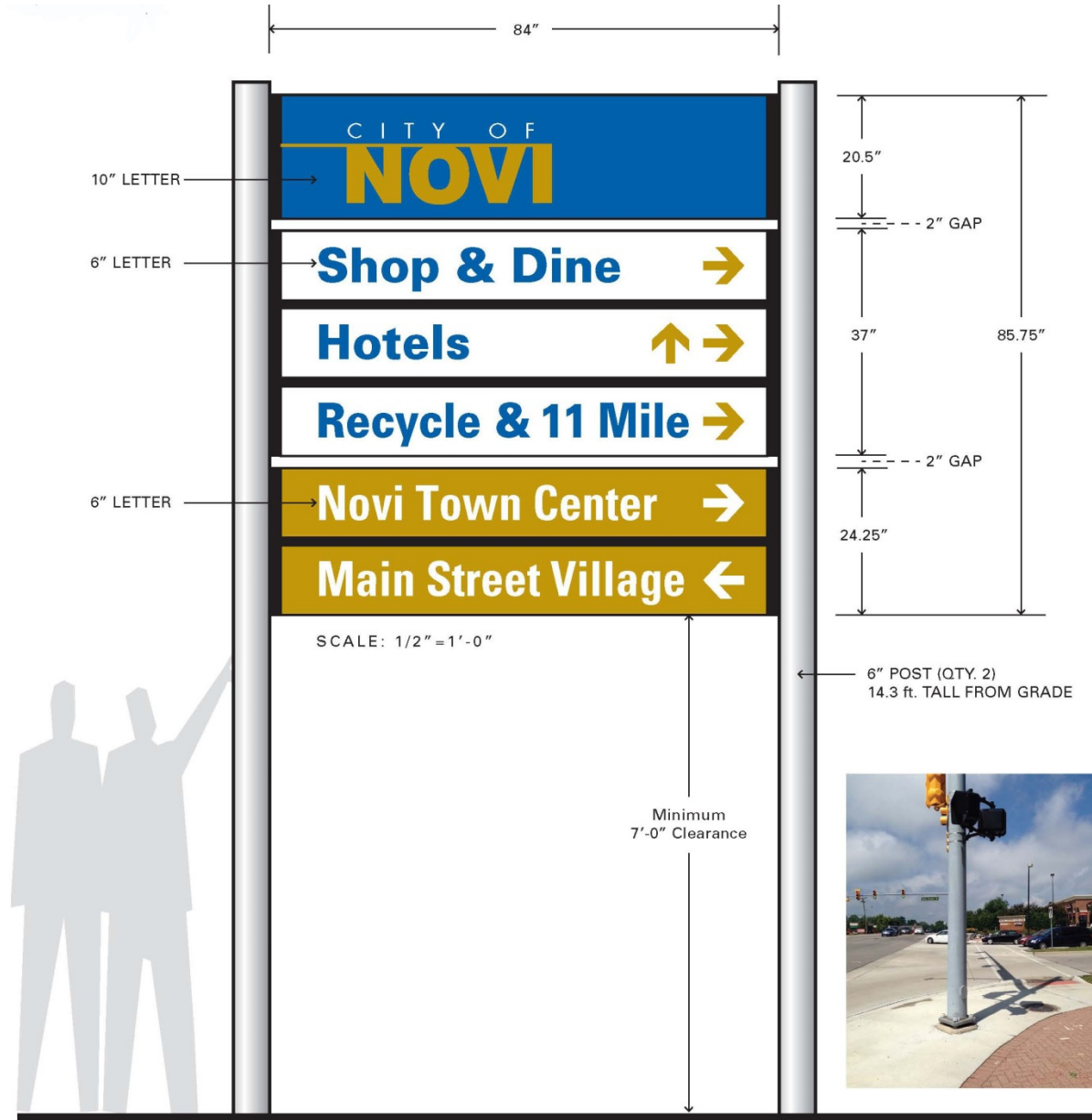


FIGURE 16. PEDESTRIAN-SCALED WAYFINDING SIGN DESIGN



This option is a pole-mounted sign. If used in the right-of-way, it would not be in compliance with the MMUTCD requirements for vehicular users unless a variance was granted, or the RCOC deemed it a pedestrian information sign. If it were considered a pedestrian sign, these types of signs also have specific setback requirements.

FIGURE 17. VEHICLE-SCALED WAYFINDING SIGN DESIGN



This option is a double-pole ground sign. Its size was determined by using the required letter sizes that comply with the MMUTCD requirements for vehicular users. The size also will impact the locations of the sign, as the rules dictate how far a Community Wayfinding Sign can be from traffic control signs and signals, which take precedence.



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## RCOC REQUIREMENT DETAILS

As mentioned above, the RCOC will need to review and approve any signage within the right-of-way of Grand River Avenue or Novi Road. In designing the wayfinding system for the Town Center area, several requirements have posed specific challenges:

- *Sub-section 05: The requirements prohibit using Community Wayfinding Signs to provide direction to primary destinations or highway routes or streets.* This requirement limits the ability to identify important destinations in the Town Center area, such as 11 Mile Road, Novi Town Center shopping center, or the Main Street shopping area.
- *Sub-section 22: Community Wayfinding Signs are limited to three destinations per sign.* The proposed sign design includes separate panels to overcome this limitation.
- *Sub-sections 27 – 30: The requirements have specific letter size and spacing requirements.* The proposed vehicle-scaled sign design meets these requirements except for the words “City of.”
- *Sub-section 31: The requirements include specific font styles.* The proposed font style is similar in stroke/weight to the Standard Alphabets in the “Standard Highway Signs and Markings” document. However, the RCOC will need to approve the style.
- *Sub-section 33: The requirements include specific arrow styles.* The arrow style proposed is a contemporary style that complements the clean approach intended for consistent branding of the overall Novi sign plan. However, the RCOC will need to approve the style.

## ESTIMATED LOCATIONS AND COSTS

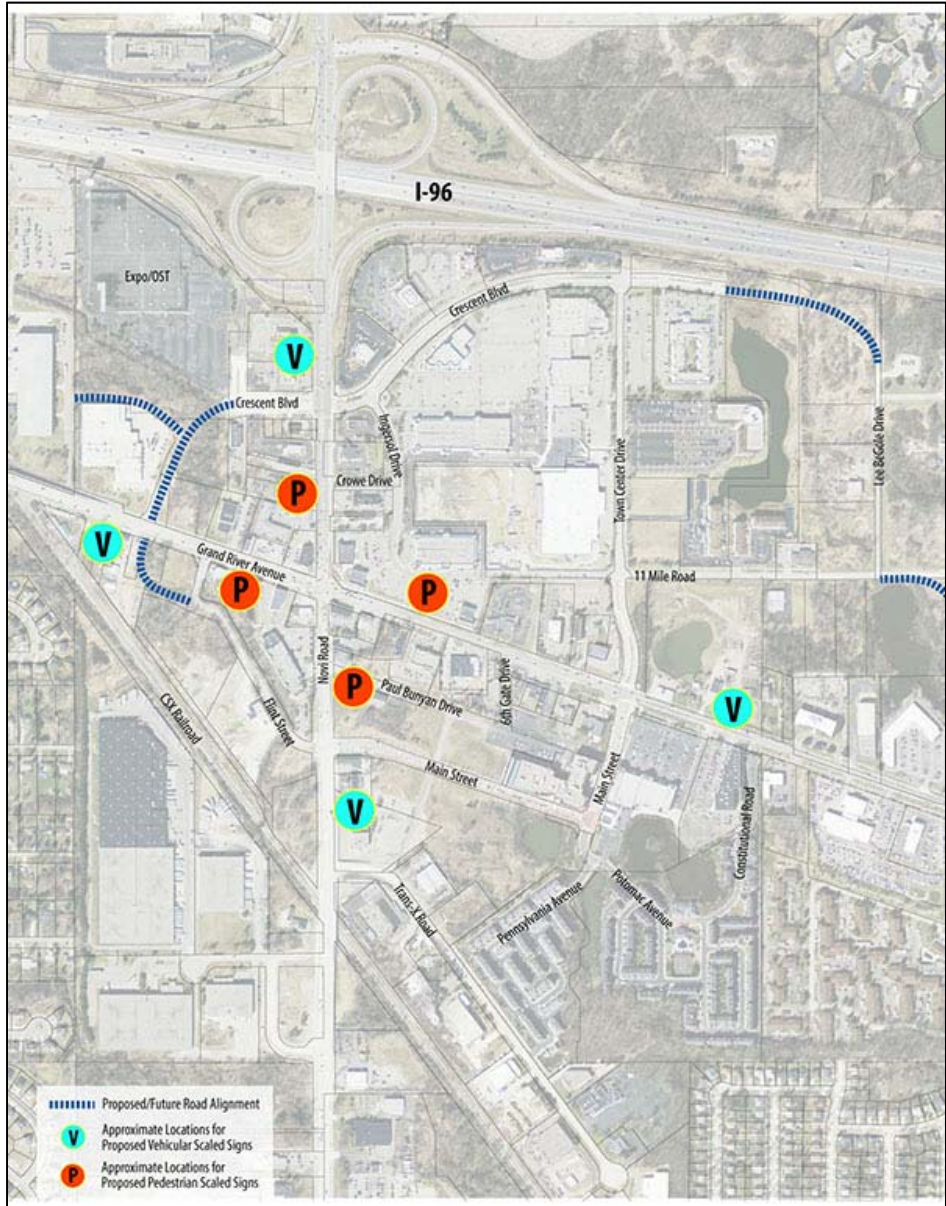
The map on the following page provides preliminary locations for four pedestrian-scaled signs, and four vehicular-scaled signs. The exact locations will need to be field verified as part of the RCOC permitting process.

Preliminary estimated costs for fabricating the signs have been calculated. These costs are based on four to six units being fabricated at one time. However, they do not include final build and location drawings, mandatory municipal/MDOT/RCOC permit application/approvals, or assembly and installation. The estimate for the pedestrian-scaled sign is \$4,000 - \$6,000 per sign; the estimate for the vehicle-scaled sign is \$8,000 - \$10,000 per sign.

## NEXT STEPS

The wayfinding sign system presented in this report is a first step in developing a workable solution for directional signs in the Town Center area. The next steps in realizing a wayfinding system will be to work with the staff at the RCOC to determine a sign design that will meet the needs of visitors, as well as provide signage that is safe for motorists.

FIGURE 18. APPROXIMATE LOCATIONS OF WAYFINDING SIGNS







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# APPENDIX





October 18, 2013

Land Use Summary Matrix for Vacant & Re-developable Properties

Map No.	Property I.D.	Acres	State Equalized Value	Ownership	Existing Land Uses	Adjoining Existing Land Uses	2010 Master Plan Designation	Proximity to Pedestrian Facilities	Proximity to Centralized Parking	Existing Natural Resources / Recreation Opportunities/ Constraints	Potential Land Use Implications			Comments
											Potential Land Uses	Economically Feasible?	Potential Traffic Impact	
Northwest Area														
1	50-22-15-476-031	1.61 ac.	SEV: \$168,750 SEV/ACRE: \$104,813	E & R Novi LLC	Vacant	W: Industrial E: Office N: Vacant / Stream corridor S: Grand River	TC Commercial	Sidewalk along Grand River	None	Ingersol Creek on E. side	TC Commercial	Yes	Minimal	TC Commercial is appropriate, as it allows a variety of uses, including open spaces with urban character. This could incorporate the creek into the site design as an amenity.
Flint Street Area														
4	50-22-15-477-011	1.29 ac.	SEV: \$79,500 SEV/ACRE: \$61,628	George Keros	Vacant	N: Grand River S: Vacant E: SF Residential & Industrial W: Flint St. & Commercial	TC Commercial	Sidewalk along Grand River	None	Ingersol Creek on NE corner; constructed pond at south	Industrial, Research, Development & Technology (IRDT) or Office, Research, Development & Technology (ORDT)	Possibly	Minimal	The two potential land uses are similar; but one has an "Industrial" emphasis while the other has an "Office Emphasis." If industrial emphasis is preferred, combining these parcels could create a manufacturing incubator area of small research/ manufacturing businesses. If office emphasis is preferred, this district could extend the existing ORDT district on the north side of Grand River.
5	50-22-22-226-005	4.26 ac.	SEV: \$464,650 SEV/ACRE: \$109,073	Hunter Development Co.	Vacant with some previous industrial uses	N: Vacant & Flint St. S: Industrial E: Flint Street W: Railroad Tracks & Industrial	TC Commercial	None	None	None			Moderate	
	50-22-22-226-003	4.46 ac.	SEV: \$575,250 SEV/ACRE: \$128,980		Previous industrial use - concrete plant	N: Vacant & previous industrial S: Cemetery E: Flint St. W: Railroad Tracks & industrial								



Map No.	Property I.D.	Acres	State Equalized Value	Ownership	Existing Land Uses	Adjoining Existing Land Uses	2010 Master Plan Designation	Proximity to Pedestrian Facilities	Proximity to Centralized Parking	Existing Natural Resources / Recreation Opportunities/ Constraints	Potential Land Use Implications			Comments
											Potential Land Uses	Economically Feasible?	Potential Traffic Impact	

Flint Street Area (Continued)

3	50-22-15-477-012	.59 ac.	City Assessment: \$81,200 Assessed Value/Acre: \$127,627	Lee BeGole	Residential	N.A.	Location for Re-aligned Flint Street intersection w/Grand River	Sidewalk along Grand River	None	None	Road ROW	N.A.	N.A.	
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2	50-22-15-477-006	0.34 ac.	SEV: \$37,500 SEV/ACRE: \$110,294	Country Building Supplies (Patrice LLC)	Commercial / Office	N: Grand River S: Railroad Tracks E: Commercial / Office W: Industrial	TC Commercial	Sidewalk along Grand River	All properties have own parking	None	Industrial, Research, Development and Technology	Possibly	Minimal	Given the existing and adjacent land uses and proposed realignment of Flint Street, proposed land use would coordinate with same designation on other side of Railroad Tracks & Grand River. It would also provide different employment opportunities than retail/commercial. Flint street would act as boundary.
	50-22-15-477-005	1.46 ac.	SEV: \$198,500 SEV/ACRE: \$135,959		Industrial									
	50-22-15-477-002	0.31 ac.	SEV: \$57,100 SEV/ACRE: \$184,194	GAP Property Management, LLC	Industrial	N: Grand River S: Railroad Tracks E: Industrial W: Industrial	TC Commercial	Sidewalk along Grand River	All properties have own parking	None	Industrial, Research, Development and Technology	Possibly	Minimal	
	50-22-15-477-003	0.32 ac.	SEV: \$63,850 SEV/ACRE: \$199,531											
	50-22-15-477-004	0.17 ac.	SEV: \$30,150 SEV/ACRE: \$177,353											
50-22-15-477-001	0.66 ac.	SEV: \$93,300 SEV/ACRE: \$141,364	CSX Transportation	Industrial	N: Grand River S: Railroad Tracks E: Industrial W: Railroad Tracks / SF Residential	TC Commercial	Sidewalk along Grand River	Has own parking	None	Industrial, Research, Development and Technology	Possibly	Minimal		

Map No.	Property I.D.	Acres	State Equalized Value	Ownership	Existing Land Uses	Adjoining Existing Land Uses	2010 Master Plan Designation	Proximity to Pedestrian Facilities	Proximity to Centralized Parking	Existing Natural Resources / Recreation Opportunities/ Constraints	Potential Land Use Implications			Comments
											Potential Land Uses	Economically Feasible?	Potential Traffic Impact	

Trans-X Area

11	50-22-23-151-027	1.48 ac.	SEV: \$98,250 SEV/ACRE: \$66,385	Frank Stevens Trust	Industrial	N.: Vacant & Commercial/Office S: Trans-X & Industrial E: Vacant & Industrial W: Commercial/Office	TC Commercial	None	Property has own parking	None	Industrial, Research Development and Technology	Possibly	Minimal	This use would accommodate the existing uses, and could encourage redevelopment of existing quonset hut to some type of indoor recreation use. Indoor recreation would be more compatible if the parcel to the north were developed as residential. Potential zoning change to I-1.
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12	50-22-23-151-029	3.64 ac.	SEV: \$511,250 SEV/ACRE: \$140,453	aven Investments, LL	Industrial	N: Industrial S: Railroad Tracks E: Grand River W: Trans-X	TC Commercial	Sidewalk along Grand River (But along overpass)	Property has own parking	None	Industrial, Research, Development & Technology	Possibly	Minimal	This use could accommodate existing industrial use, but would encourage the redevelopment of the parcel to better coordinate with property to the north. Potential zoning change to I-1.
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Map No.	Property I.D.	Acres	State Equalized Value	Ownership	Existing Land Uses	Adjoining Existing Land Uses	2010 Master Plan Designation	Proximity to Pedestrian Facilities	Proximity to Centralized Parking	Existing Natural Resources / Recreation Opportunities/ Constraints	Potential Land Use Implications			Comments
											Potential Land Uses	Economically Feasible?	Potential Traffic Impact	

Main St. Area

10	50-22-23-151-039	10.69 ac.	City Assessment: \$773,643 Assessed Value/Acre: \$72,371	TCF Bank	Vacant	N: Main St. & Commercial/Office S: Commercial/Office & Industrial E: Multi-Family & Commercial/Office W: Commercial/Office & Industrial	TC Commercial	Sidewalks on Main St.	Parking at Atrium Building across Main St.	One-acre pond at northeast corner of site	Maintain TC Commercial designation with emphasis on Multi-family residential with open space surrounding pond and/or	Yes	Potentially moderate based on density	Allow first-floor residential under zoning.
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9	50-22-23-176-032	7.09 ac.	City Assessment: \$374,400 Assessed Value/Acre: \$52,807	Victor Cassis	Vacant	N: Commercial/Office & Vacant S: Vacant E: Commercial/Office W: Vacant	TC Commercial	Sidewalks on Main St.	Parking at Atrium Building east of property	None	Maintain TC Commercial designation with emphasis on continuing existing land use patterns on north side of	Yes	Potentially moderate based on extent of development	
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	50-22-23-106-723	0.88 ac.	No Information Found	Detroit Metropolitan Credit Union	Vacant (but soon to be developed into Credit Union)	N: Vacant S: Commercial/Office E: Novi Rd. & Commercial/Office W: Vacant	TC Commercial	Sidewalks on Main St. and Novi Rd.	Parking at Atrium Building east of property	None	Maintain TC Commercial designation to accommodate proposed	Yes	Minimal	
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8	50-22-23-176-019	2.39 ac.	SEV: \$0	City of Novi	Public/ Institutional (Fire Station)	N: Grand River S: Commercial/Office E: Commercial/Office W: Commercial/Office	Public	Sidewalk along Grand River & Large pedestrian gazebo just to the east at the intersection	Parking at Atrium building south of property	None	TC Commercial	Yes	Minimal (Due to small lot size)	Given its prominent position at Grand River & Main St., continuation of TC Commercial at this location is appropriate.
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Map No.	Property I.D.	Acres	State Equalized Value	Ownership	Existing Land Uses	Adjoining Existing Land Uses	2010 Master Plan Designation	Proximity to Pedestrian Facilities	Proximity to Centralized Parking	Existing Natural Resources / Recreation Opportunities/ Constraints	Potential Land Use Implications			Comments
											Potential Land Uses	Economically Feasible?	Potential Traffic Impact	

Grand River - Novi Business Area

6	50-22-23-102-021	0.72 ac.	SEV: \$158,050 SEV/ACRE: \$219,514	43269 Grand River LLC	Commercial/Office (Office in existing house)	N: Grand River S: Vacant & Commercial/Office E: Commercial/Office W: Commercial/Office	TC Commercial	None	Property has its own parking	Trees	TC Commercial	Yes	Minimal (Due to small lot size)	Given its prominent position along Grand River, continuation of TC Commercial at this location is appropriate.
	50-22-23-102-013	0.45 ac.	SEV: \$49,000 SEV/ACRE: \$108,889	Victor Cassis	Vacant	N: Commercial/Office S: Commercial/Office E: Commercial/Office W: Commercial/Office	TC Commercial	None	Parking at Atrium building east of property	Trees / None	TC Commercial	Not in its present form	N.A.	Given this parcel's location, it would more likely be developed if it were combined with the parcel along Grand River.

Anglin Area

15	50-22-23-126-006	10.2 ac.	SEV: \$508,150 SEV/ACRE: \$49,819	Joy Rose LLC	Commercial/Office	N: Commercial/Office & Vacant S: Grand River & Commercial/Office E: Main St. & Commercial/Office W: Main St. & Commercial/Office	TC Gateway	None	None	1.67 ac. pond	TC Gateway	Yes	Moderate to large	TC Gateway is appropriate for this parcel given is existing development pattern, size and potential for using the existing pond as an amenity.
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Hotel/Office Area

14	50-22-14-376-017	2.17 ac.	SEV: \$212,700 SEV/ACRE: \$98,018	Singh IV LP	Vacant	N: Commercial/Office (Office) S: Grand River E: Commercial/Office (Hotel) W: Commercial/Office (Retail)	Office/Commercial	Sidewalk along Town Center Rd. (west of property)	Parking at AutoDesk lot to the north of the properties	None	Office/Commercial	Yes	Moderate to large	Office/commercial is appropriate given the surrounding existing land uses (particularly if Anglin property is used for office)
	50-22-14-376-018	1.86 ac.	SEV: \$183,300 SEV/ACRE: \$98,548											



Map No.	Property I.D.	Acres	State Equalized Value	Ownership	Existing Land Uses	Adjoining Existing Land Uses	2010 Master Plan Designation	Proximity to Pedestrian Facilities	Proximity to Centralized Parking	Existing Natural Resources / Recreation Opportunities/ Constraints	Potential Land Use Implications			Comments
											Potential Land Uses	Economically Feasible?	Potential Traffic Impact	




Lee BeGole Area

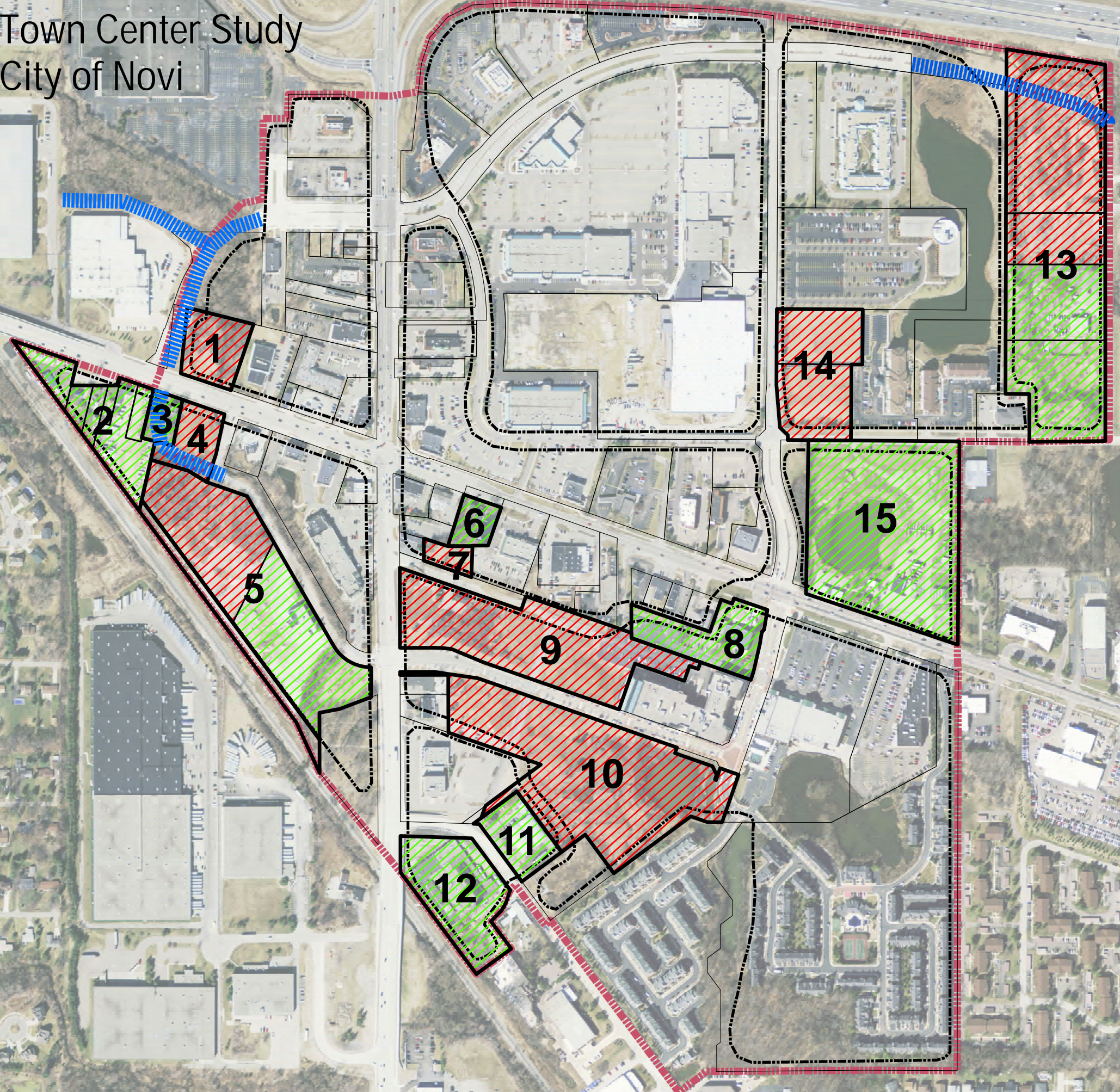
13	50-22-14-451-013	6.13 ac.	SEV: \$150,350 SEV/ACRE: \$24,527	Crystalbrooke II LLC & Emeraldbrooke II	Vacant	N: I-96 S: Eleven Mile Rd. E: Public (DPW Yard) W: Commercial/Office (Hotel & offices)	Office/Commercial	None	None	Stream along western boundary	Industrial, Research Development and Technology	Yes	Moderate (Due to parcel size)	Potential for these parcels to be combined and developed as one. Adjacency to DPW yard limits desirability for commercial uses or doctor's offices. No outside storage allowed under zoning.
	50-22-14-451-015	2.03 ac.	SEV: \$64,700 SEV/ACRE: \$31,872											
	50-22-14-451-017	2.99 ac.	SEV: \$133,150 SEV/ACRE: \$44,532											
	50-22-14-451-012	3.55 ac.	SEV: \$116,000 SEV/ACRE: \$32,676											



# Town Center Study City of Novi

## Land Use Summary for Vacant & Re-developable Properties

-  Potential Redevelopment
-  Vacant Land
-  Proposed Future Road Alignment





## **Town Center Area Master Plan Recommendations**

*City of Novi*

July 15, 2013

### Interview Questions:

1. What developments/land uses do you see as the next step for the evolution of the Town Center area? The Main Street area?
2. Does the mix of commercial/office provide enough space, and the right kind of space with associated amenities to attract desirable businesses?
3. Are there industrial or office uses that are appropriate for selective areas?
4. Do the building material standards illustrated in these areas reflect quality? Should these standards be broadened to include more/varied building materials?
5. For the Main Street area: should the building frontages continue to be set at the edge of the sidewalk, and connected to the adjacent building (vs. stand-alone buildings)?
6. What residential types would be appropriate for the Town Center and/or Main Street areas? Townhouses? Ranch condominiums? Rental apartments? Single-family homes?
7. Do existing pedestrian nodes make sense? Are they in the right locations? Are they designed for maximum appeal and ease of use? Should they remain, or be moved/eliminated?
8. Do you think the pedestrian walkways are perceived as safe and comfortable to use? Should more walkways and non-motorized transportation amenities be added to either area? If yes, what types of amenities, and where?
9. Are there road improvements or traffic safety changes necessary for the area? Is completion of the ring road (Crescent Boulevard) important?
10. Is there sufficient open space to make these areas attractive and a destination? If yes, which spaces serve this purpose effectively? If no, what would you add?
11. Do you feel safe/secure at night in the Town Center and/or Main Street areas?

## City of Novi

### Town Center – Common Themes from Stakeholders

- Strongest land use market today is for residential and office uses
- General perception that retail is overbuilt
- Residential uses are preferred for vacant parcels along Main St.
- Requirements for first floor retail is unrealistic
- Design standards are good but allow flexibility



## Town Center Study

City of Novi

### Stakeholder Interview Notes: Larry Czekaj, TCF Bank (August 5, 2013)

- What does TCF have in their business plan for this area?
  - Achieve disposition of property **ASAP** at **highest price**
  - Need to find the right developer and identify any impediments to developer getting the property through the development process.
  - They see proposed best use as residential use
  - Thanks Fountain Walk is a failure
- Doesn't know what Main St. can do
- Doesn't think commercial on the first floor/residential on the second floor is going to work
- More retail on this property won't work. Requiring retail by zoning is viewed as an impediment.
- TCF owns everything on south side of Main St. Nothing on the north side of Main St. Also has mortgage on Main St. bldg. (Main St. #1 or Atrium Building)
- This Main St. area doesn't have frontage/traffic/visibility as the properties do on Novi/Grand River. Doesn't think retail will work on S. side because it hasn't worked on the N. side of Main St. Doesn't think office will work here either
- Environmental issue on TCF's vacant parcel (spoils pile)-think it's almost resolved. Tagged by DEQ for arsenic levels. Could be due to spoils pile (was a contractor's yard)
- Had a potential development (townhouse(Pulte)) but fell through due to environmental issues
- Main St. concept just won't work at this location.
- Went through the condominium termination process to assist in the development of Main St.
- Residential use = multi-family (approx. 100 units)
  - Stacked townhouse product (owned vs. rental). Extend similar house style/design as Singh development
  - Concerned about Quonset hut built across Trans-X
- Property @ corner of Novi & Main St....thinking this should be commercial (1.5 acre parcel).
- Building materials/design/image isn't an issue for the bank except that it affects buyers' ability to pay the bank's price (due to other expenses for improvements). Most developers understand the Main St. concept (up to the sidewalk). Like no setbacks as it gives ability to max out property with development.
- Pulte -- trying to do something similar to Liberty Park with garages in back. Had combination of ranches & stacked units.
- Thinks this area is safer @ night. May have too much of a police presence might reduce business a little.
- No traffic/road issues.

## Town Center Study

City of Novi

### Stakeholder Interview Notes: Mark Szerlag (August 5, 2013)

- Mark represents TCF Bank for residential property
  - Also Wonderland Plaza
  - Also Ex. Main St. building leasing for office space
  - Sold site under construction for auto place.
  - Country Epicure Plaza -- leased it & sold it
  - Wasabi Restaurant building
  - Ace Cutting Tools (behind Main St.) may be interested S. E corner of Novi Rd & Main St.
- 1.5 acre piece on S. Side of Main St. May have a buyer for small office building.
- Thinks mixed use land uses are good. Going to get push back for retail below & residential above. Developers don't make money on these.
- National companies prefer parking at front of business. Businesses that lease buildings with parking at rear are not national firms & don't pay rents that justify new construction.
- Mixed use vision is cool, but not economically available.
- Anglin property-needs to sell & develop, possibly with a building like Lakepointe Office Building. Sees "L" shaped office building facing existing pond. Possibly rezone this parcel to TC1? "Spite strip" exists along the west side of this parcel (Town Center Rd.).
- Marketing Novi Expo Center will help Main St. office space rentals
- Flint St. -- Not appropriate for new industrial use. Barb thinks office (?) so does Mark. Potential townhomes.
- Quonset Hut -- Demolish this
- Wants to see continued clean-up of industrial uses bet. I-96 and Main St. Sees office replacing industrial.
- Design Standards: Thinks these should continue to be used/promoted. Illustrates quality development. Barrier of entry can be challenging, but once you're in, you're in + everyone else needs to follow the standards as well. Variances are available. Highly desirable parcels in highly desirable community -- keep it nice.
- Possibly allow new materials -- have open mind to them.
- Building height: Novi thinking of allowing taller buildings along I-96 and possibly on Anglin parcel. "Allowing" would be ok, but not mandating it.
- Zero setback requirement: Works best when developer can use on a large parcel. Not easy making neighbors play nice when they are set up this way. Not seeing any demand for this.
- Owner-occupied buildings are more desirable than landlord occupied. Hard to get financing for speculative building projects.
- Pulte project: Wanted to do a project similar to "Liberty Park" that they did on 12 Mile and replicate it here on the south side of Main St. (TCF Bank parcel)
- Pedestrian amenities are important -- amenities worth pursuing

- Traffic improvements (besides Crescent Blvd. + Flint St. area). Grand River + Novi Rd. improvements are great. Go with this. Likes continuation of Crescent Blvd.
- Security along Main St. area: No issues.
- Flint. St. – Preferences would be smaller parcels to accommodate single, owner-occupied buildings. (3,000-7,000 sq. ft. owner-occupied buildings). Dr. offices, law firm, etc.
- TC1 building size limitation. Doesn't think this benefits anyone – large national retailers wouldn't want the TC1 location, so the limitation isn't protecting this area from large retailers. Not enough population close enough.
- Drive thru restriction – typically drive-through restaurants want to be next to each other. Not sure about this restriction.
- Vacant parcel of Grand River next to Belle Tire...Small office building here. Only 100' wide + Belle Tire driveway on this parcel.
- Retail is the slowest to recover. Demand for small retail space is low.
- Two strongest land uses today are office and multi-family (Owner-occupied easy to finance but rentals also popular, just hard to finance) in Town Center area.

## Town Center Study

City of Novi

### Stakeholder Interview Notes: Jim Clear, Matt Quinn, Chuck Fosse (August 7, 2013)

- Lots of interest from national retailers for Novi Town Center.
  - Lots of people wanting to start-up local mercantile, but prefer national retailers who can weather ups/downs of economy
  - Considering Hobby Lobby (~ 55,000 sq. ft.) Ballys would be raised + Borders building expanded 18,000 sq. ft.
  - Occupancy ~ 90% +/- once Hobby Lobby comes in (without Hobby Lobby, occupancy ~ 70%).
  - WalMart – doing very well. WalMart 24-hour store.
  - Novi Town Center manages all common areas. Also has its own security team.
- Chuck: Project architect for 15 years. Seen thru various redevelopment phases. Helped coordinate WalMart
  - Wants more flexibility in zoning requirements
  - 1986 plan maybe went too far in controlling aesthetics
  - Have quality standards without being so specific (use of materials/colors). Thinks variety would create more downtown style vs. campus, homogenous style. Variety creates more vibrant area.
  - Façade waivers go thru Planning Commission.
- Pedestrian nodes
  - Good if they have an application, but not just to have a pedestrian space where it's not needed.
  - Not good if just put them in to break up parking, i.e. one on Grand River near Sacos – doesn't work.
  - No one walking/biking to get to Town Center now. People do walk from the Hotel to Town Center.
  - Residential + office element is missing
  - Town Center would like to see more office (office use for old Expo Center)
- Form Based Code for Town Center – Doesn't make any sense to change this now since so much is already built.
  - May work more for undeveloped areas.
- Out parcels
  - One by fountain (Ingersol + Crescent). Marketing now. 5-6,000 sq. ft. Could be multi-tenant. Looking for higher use. Premier position.
- Design standards – Main St.
  - Almost can't keep landscaping standards, i.e. 4' landscaping along urban building just doesn't work within a downtown district. Have to reduce costs.



- Requiring a brick wall along Crescent Drive – why? No reason because it's green space between the road and parcels. Not screening parking.
- Main St. area – Matt thinks more residential...loft design? Urban, upscale.
- Traffic – seems to work around Town Center.
  - Grand River/Novi intersection – lots of traffic.
- Wayfinding signs – Town Center signs work (have 6 pedestrian and 6 traffic wayfinding signs). The signs complement the center.
  - Wayfinding as part of this project – along major thoroughfares to find hotels, DPN offices
  - Can't hurt and will only help
  - Barb says they'll be asking for input.
- Lighting – new lighting good. On Crescent, Town Center, Novi Rd. and Grand River.
- Flint St.: Think there is enough retail and restaurants.
  - Market will determine what should go here.
- When Beaumont Building went away from Main St., that really negatively impacted this area.
- For Main St. – City has to get into this some way – constructing a parking garage? Needs help since there's no DDA to promote it. Also no municipality-controlled parking.
- Still a need for more drive thru restaurants.
  - Limited basis would be fine
- Town Center would consider a drive thru Starbucks + one other restaurant
- No drug store in this area. –Anglin parcel? This type of use also needs a drive thru.

# Town Center Study

City of Novi

## Stakeholder Interview Notes: Asher Husain (August 7, 2013)

- Main Street – lack of critical mass has made it hard to attract/retain tenants.
  - No traffic for retail tenants (both auto + foot traffic)
- Main St. – Parking also an issue...tenants want easy parking. Parking is too complicated/confusing for retail/restaurants. Office tenants get it + like the garage pkg.
  - There is enough parking
  - (Perception issue)
- Urban design concept of Main St. hasn't been well received in Michigan.
- Mixed use concept where parking is far away lowers true value of a building. (Opinion of TCF Bank appraiser)
- Marrying attractive urban concept while making parking easier.
- Wants to be able to put more prominent signs that the building has garage parking: two little signs not sufficient. Limited by zoning codes.
- Signage
  - Could tenants of building do a marquee sign on Grand River? Thinks this isn't allowed by zoning. Maybe a common sign for Main St. East.
- Disappointed that Farmers Market moved. Thought it was because signage was too small (due to zoning?)...moved because not enough people know about it or could find it.
- Doing more events would be helpful. Make area more inviting for events. Thinks a park or some public space-develop wetland into an attractive pond. Amphitheater for summer concerts. Central town square.
- Residential on S. Side of Main St. would be a good idea.
- Would it be possible to convert part of the Main St. 1 building (Atrium Bldg.) to residential? Do a PUD project?
- Don't see tall buildings as a problem.
- Sees railroad as an asset for future commuter rail. But making this a reality very hypothetical.
- Pedestrian walkways – likes bicycle lane on Novi Road.
  - Planters in middle of sidewalk make it difficult for bikes (walk too narrow)
  - Bike racks should be required w/new developments
  - Make this area more bike friendly.
- When condo association was dissolved, landlords were given the responsibility of the street/landscape maintenance. Going forward, this needs to be revisited and City needs to take the lead. With different landlords, maintenance is not happening consistently.
- Having separate buildings may allow for more convenient parking...but keep the downtown feel. Possibly have alleys between buildings to allow vehicular/pedestrian access (w/flowers/landscaping)

- Safety: Too many kids have discovered Main St. as an unsupervised hang out space. Issues = vandalism; kids getting into vacant offices + having parties. Part of this due to vacancies.
  - Thinks it safe out on the street. There is a party crowd + they have had some fights. But Atrium Bldg. bar area has good security and police respond.
  - Office tenants thinks it is safe
- Refocus City's efforts to realize the vision of Main Street
  - More events
  - Central town center features – parks
  - Ameliorate perceived problem of parking

# Town Center Study

City of Novi

## Stakeholder Interview Notes: Victor Cassis (August 20, 2013)

- Thoughts on future of this area?
  - Town Center – it's developed. Eleven Mile Road, in front of Bone Fish Grill and the Coney Island is a dangerous road. Suggest widening this road (using parking spaces to the south); place road further away from the buildings.
  - Town Center is alive again...doesn't see any other changes needed at this time.
- Mix of retail + office space...thoughts on this? Too much retail? Do we need more office?
  - Likes separation of residential, commercial and industrial in the community.
  - The mall attracts visitors and supports businesses at the periphery.
  - The private money or free market will decide about the mix of businesses/ office/residential. Novi should be open to and welcoming of change.
  - Still a residential community.
  - TC-1 allows a mix of uses.
    - Office uses: overwhelming/excess of office at this time
- Main Street: Wants to bring life back to this area.
  - Too many bars...couldn't make it
- Traffic study: Many done for Grand River (first done in 1988)
  - Make Grand River boulevard w/lanes to allow pedestrians a comfortable place to walk and cross.
  - Doesn't matter how many benches you put along Grand River...people just aren't using them. Grand River needs to be a different type of road...it's a freeway now.
  - Slow speeds down on Grand River at Novi Road (for 2-block area – E + W of Novi Road)
- Main Street:
  - Thinks residential uses would be wonderful. The more intense, the better. Provide a warm and friendly appearance (not contemporary) to the residential buildings. Two or three stories, or taller (high-rise) – loft-type units
  - Still thinks mixed-use with retail below is a good idea.
- Main Street – Cassis parcel: Wants to keep the zoning of his parcel the same as currently designated (needs the height allowed)
  - The City of Novi should be taking care of the street & the amenities (not the business owners).
    - Can't make it more expensive for businesses wanting to come into the area.
- Pedestrian Areas:
  - Main Street – w/further development (esp. intense residential). There will be more people. His developments will bring in more people too.
  - Main St. must have a more comfortable walking environment
    - Planters are too large – make them smaller



- Don't need planters and benches (since planters could also be used for seating)
  - Need more intimacy
  - Use tree grates and small trees so they don't overwhelm the street. Flowers too.
  - Style of lights. Glad to hear about new lighting program.
    - Wants lights that are easier to maintain
  - Make Main Street more pedestrian friendly
  - Continue pathways on Grand River and Novi Rd. – use alternate pavement types to minimize the extent of concrete
- Doesn't see traffic as an issue. Can't really accommodate more traffic signals
  - Encourages developing the ring road (Crescent Blvd., Flint St., Main St.).
  - Incorporate small green areas throughout the study area to accommodate some outdoor activities/events. Small nodes throughout the study area (i.e. one in Town Center, Main St., etc.).
  - Outdoor areas should be kid-friendly. Be deliberate in locating outdoor areas.
- Security – is it a problem? No.
- Credit Union will be located on the Northeast corner of Novi Road and Main St.

LAW OFFICES

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January 7, 2014

Barb McBeth, Deputy Director  
Community Development  
City of Novi  
45175 West Ten Mile Road  
Novi, MI 48375

Re: Novi Town Center Study Subareas

Dear Barb:

I desire to pass along to you two requests from Jim Clear and the management/ownership team of the Novi Town Center Development.

We have been following the Novi Town Center study for the various subareas. Of importance to us is the Town Center subarea and the Hotel/Office subarea.

In reviewing the current land ownership situation and other areas for possible re-development in the future, it appears to be important that both of the aforementioned subareas have as a lawful permitted use a restaurant (fast food drive-through) which is: "a business establishment in which a patron purchases food or beverages, that 1) are served in disposable containers or wrappers, 2) may have been previously prepared and 3) are principally ordered and received from a motor vehicle and consumed off-premises."

This modification would allow such businesses as a Tim Horton's Restaurant with a drive-through, something like a Panera with a drive-through or a business like a Starbucks with a drive-through. It appears to us that the Town Center subarea and the Hotel/Office subarea would be ideal possibilities for businesses of this type. We would ask that they be included as permitted uses within these areas.

The second request is to reconsider signage within both the Town Center subarea and the Hotel Office subarea. Please consider the sign area of 130 square feet for any building owner/tenant of over 25,000 square feet. The national owner/tenant groups find it difficult to come into the Town Center area given the limited square footage allowed for signage. By limiting the larger signs to larger buildings this market base may be expanded. An example of the larger owner/tenants for which the increased signage would be allowed would be such businesses as Golfsmith, TJ Maxx, Walmart and the hopefully future Hobby Lobby stores. Please consider this as a very important consideration.

If you would like to meet with Jim Clear and myself to discuss these matters further, please give me a call. I look forward to the revisions appearing in the Carlisle Wortman Land Use and Master Plan Update for this Town Center study area.

Thanking you in advance for your consideration, I remain

Very truly yours,

SEGLUND GABE QUINN GATTI & PAWLAK



Matthew C. Quinn

MQC/ckj

cc: Mayor Robert Gatt  
Jim Clear  
Carlisle Wortman

# Questions

<b>LAND USE</b>	
1. Does the Town Center area need more commercial development? If yes, any particular type (independent stores, shopping centers, fast-food or upscale restaurants)?	
2. Do you think drive-throughs are desirable in the Town Center area? If yes, would you limit the number, and if so, to how many?	
3. Would the Town Center area benefit from adding residential land uses, such as town houses, condominiums and/or rental properties? If yes, what type of residential development would be best?	
4. Is there enough office space in the Town Center area? If no, what's missing (small office buildings, large multi-tenant buildings, etc.)?	
5. How should the vacant parcels along Flint Street and Lee BeGole Street be developed/re-developed (which land use types)?	
<b>VEHICULAR CIRCULATION</b>	
1. Do you experience traffic problems on any roadways in the Town Center area? If yes, where and what time?	
2. Will the extension of Crescent Blvd. help relieve traffic congestion in the Town Center area?	
3. How should Flint Street be re-aligned? Similar to where it is now? Along the rail-road tracks? Down the middle of the vacant parcels?	
4. Does existing signage help motorists find various destinations in the Town Center area? Are there any destinations that need better directional signage?	
<b>PEDESTRIAN AMENITIES</b>	
1. Do you think the pedestrian nodes with benches are well placed in the Town Center area? If not, where would you put them?	
2. Are more sidewalks needed? If yes, where? Are they wide enough? If not, how wide should they be?	
3. Does the landscaping along the roadways improve the pedestrian experience? If not, how would you change it?	



# Town Center Study

City of Novi

## Drop-In Open House Comments (9-11-13)

### Land Use

- Consider redevelopment of “older” parcels along Grand River (adjacent to Paul Bunyan Drive) into 4-5 story residential condominiums.
- Redevelop the fire station
- Move the fire station to a less central location (possibly Flint Street area?)
- Attract young professionals to the Main Street area with the type of housing stock they want. 4-5 stories. Affordable.
- Loft residential on south side of Main Street.
- Tall residential and mixed use needed on Main Street. Balconies/greenery.
- Loft apartments on south side of Main Street with balconies overlooking wetland.
- Develop the wetland area and nearby uplands on south side of Main Street into a City park. Example: Bond Park in Birmingham.
- Main Street wetland area: Aesthetics; have a place to play; ice skating?; City participation; City park needed.
- Congregation place/open space needed on Main Street.
- Farmers Market should be located in Town Center, not out on Taft.
- Ethnically focused, international shopping areas/destinations.
- DDA covering entire area (Town Center, Main Street, Twelve Oaks Mall, Fountain Walk)
- Property owners should not be solely responsible for Main Street maintenance & fixtures.
- Help Main Street businesses with economic development assistance.

### Vehicular Circulation

- Install traffic calming techniques along Grand River to help motorists slow down and make pedestrians more comfortable using the sidewalks.
- Adjust drive by Coney Island for safety.
- Ask for professional traffic engineer recommendations for both pedestrian and vehicle circulation on Town Center Drive. *(Also placed in “Pedestrian Amenities” category)*
- Review speed limits on Grand River (?)
- Need to focus on main streets.
- Install monuments at Main Street and Novi Road to announce Main Street.
- Gateway/vertical signage into Town Center area (on Novi Road)
- Better signage for businesses on Main Street from Novi Road.
- Create alternative routes of travel other than Grand River and Novi Road.



- Extend Lee BeGole Drive over I-96 to provide alternative route.
- Property owner (Pat Rice) does not want the Flint Street re-alignment to go through the Country Building Supply property.
- Pass ordinance to require parking to be placed at side and/or rear of buildings that face public roads.
- Existing fire station parking needs to be better maintained (City property).
- Banners on light poles (free parking)

### **Pedestrian Amenities**

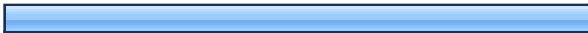




- Walkable concept still viable, especially with apartments.
- Landscape enforcement/maintenance ordinance should be stronger.
- Landscaping: mixed annuals & perennials for color. Welcoming color.
- In Town Center mall, more internal landscaping and at building entrances.
- Ask for professional traffic engineer recommendations for both pedestrian and vehicle circulation on Town Center Drive. (*Also placed in "Vehicular Circulation" category*)
- Wood benches need maintenance. Other types of benches – more durable. Aesthetic.
- Advertising on benches. Something to consider.
- Main Street planters are too large. Issue for maintenance and pedestrian access.



**1. Does the Town Center area need more commercial development?**

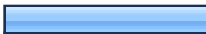

		Response Percent	Response Count
Yes		75.8%	25
No		24.2%	8
<b>answered question</b>			<b>33</b>
<b>skipped question</b>			<b>1</b>

**2. If yes, any particular type?**




		Response Percent	Response Count
Independent stores		88.0%	22
National chain stores		28.0%	7
Shopping centers		12.0%	3
Fast-foot restaurants		12.0%	3
Upscale restaurants		68.0%	17
Other (please specify)			6
<b>answered question</b>			<b>25</b>
<b>skipped question</b>			<b>9</b>





**3. Do you think drive-through facilities such as restaurants, banks, or pharmacies are desirable in the Town Center area?**

		Response Percent	Response Count
Yes		30.3%	10
No		69.7%	23
answered question			33
skipped question			1




**4. If yes, would you limit the number in the Town Center area, and if so, to how many?**

		Response Percent	Response Count
1 - 3 drive-through businesses		71.4%	10
4 - 6 drive-through businesses		7.1%	1
7 - 10 drive-through businesses		0.0%	0
Wouldn't limit the number of drive-through businesses		21.4%	3
answered question			14
skipped question			20



**5. Would the Town Center area benefit from adding more residential land uses?**

		Response Percent	Response Count
Yes		46.9%	15
No		53.1%	17
answered question			32
skipped question			2




## 6. If yes, what type of residential development would be best?

		Response Percent	Response Count
Two-family attached homes		0.0%	0
Townhouses		43.8%	7
<b>Loft-style condominiums in multi-story buildings</b>		<b>75.0%</b>	<b>12</b>
Apartments in multi-story buildings		25.0%	4
	Other (please specify)		1
<b>answered question</b>			<b>16</b>
<b>skipped question</b>			<b>18</b>


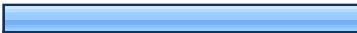


## 7. Is there enough office space in the Town Center area?

		Response Percent	Response Count
Yes		58.1%	18
No		41.9%	13
<b>answered question</b>			<b>31</b>
<b>skipped question</b>			<b>3</b>





## 8. If no, what's missing?

		Response Percent	Response Count
Small, individual office buildings		38.5%	5
<b>Medium-sized, multi-tenant buildings</b>		<b>84.6%</b>	<b>11</b>
Large multi-tenant buildings		15.4%	2
	Other (please specify)		0
<b>answered question</b>			<b>13</b>
<b>skipped question</b>			<b>21</b>



## 9. How should vacant parcels along Flint Street be developed/re-developed?

		Response Percent	Response Count
Residential uses		23.3%	7
<b>Commercial uses</b>		<b>53.3%</b>	<b>16</b>
Office uses		40.0%	12
Research/technology uses		33.3%	10
	Other (please specify)		2
<b>answered question</b>			<b>30</b>
<b>skipped question</b>			<b>4</b>

**10. How should the vacant parcels along Lee BeGole Street be developed/re-developed?**






		Response Percent	Response Count
Residential uses		17.9%	5
<b>Commercial uses</b>		<b>53.6%</b>	<b>15</b>
Office uses		32.1%	9
Research/technology uses		42.9%	12
	Other (please specify)		1
<b>answered question</b>			<b>28</b>
<b>skipped question</b>			<b>6</b>

**11. Do you experience traffic problems on any roadways in the Town Center area?**



		Response Percent	Response Count
Yes		60.6%	20
No		39.4%	13
<b>answered question</b>			<b>33</b>
<b>skipped question</b>			<b>1</b>



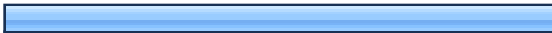

## 12. If yes, which roadways?

		Response Percent	Response Count
Grand River		50.0%	11
<b>Novi Road</b>		<b>81.8%</b>	<b>18</b>
Crescent Boulevard		27.3%	6
11 Mile		13.6%	3
Main Street		4.5%	1
	Other (please specify)		0
<b>answered question</b>			<b>22</b>
<b>skipped question</b>			<b>12</b>




## 13. If yes, what time of day?

		Response Percent	Response Count
A.M. rush hours (6-9 a.m.)		30.0%	6
<b>P.M. rush hours (4-7 p.m.)</b>		<b>100.0%</b>	<b>20</b>
	Other (please specify)		3
<b>answered question</b>			<b>20</b>
<b>skipped question</b>			<b>14</b>

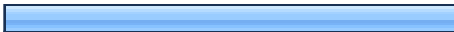

**14. Will the extension of Crescent Boulevard west of Novi Road with connection to Grand River help relieve traffic congestion in the Town Center area?**

		Response Percent	Response Count
Yes		82.8%	24
No		17.2%	5
answered question			29
skipped question			5



**15. How should Flint Street be re-aligned?**

		Response Percent	Response Count
Similar to where it is now		34.8%	8
Along the rail-road tracks		34.8%	8
Down the middle of the vacant parcels		30.4%	7
Other (please specify)			3
answered question			23
skipped question			11

**16. Does existing signage help motorists find various destinations in the Town Center area?**

		Response Percent	Response Count
Yes		67.9%	19
No		32.1%	9
answered question			28
skipped question			6



**17. Are there any destinations in the Town Center area that need better signage?**

		Response Percent	Response Count
Yes		25.0%	6
No		75.0%	18
answered question			24
skipped question			10

**18. If yes, which destinations?**

	Response Count
	6
answered question	6
skipped question	28

**19. Do you think the pedestrian nodes with benches and landscaped plazas are well placed in the Town Center area?**

		Response Percent	Response Count
Yes		35.7%	10
No		64.3%	18
answered question			28
skipped question			6

## 20. If not, where would you put them?

		Response Percent	Response Count
Closer to building entrances		52.9%	9
Away from roadways		70.6%	12
At road intersections		5.9%	1
	Other (please specify)		1
answered question			17
skipped question			17



## 21. Are more sidewalks needed?

		Response Percent	Response Count
Yes		42.3%	11
No		57.7%	15
answered question			26
skipped question			8



## 22. If yes, where?

	Response Count
	7
answered question	7
skipped question	27



### 23. Are existing sidewalks wide enough?

		Response Percent	Response Count
Yes		88.0%	22
No		12.0%	3
answered question			25
skipped question			9

### 24. If not, how wide should they be?

		Response Percent	Response Count
Five-foot wide		33.3%	1
Six-foot wide		0.0%	0
<b>Eight-foot wide</b>		66.7%	2
Ten-foot wide		0.0%	0
Other (please specify)			0
answered question			3
skipped question			31

### 25. Does the landscaping along the roadways improve the pedestrian experience?

		Response Percent	Response Count
Yes		81.5%	22
No		18.5%	5
answered question			27
skipped question			7



**26. If not, how would you change it?**

	<b>Response Count</b>
	4
<b>answered question</b>	<b>4</b>
<b>skipped question</b>	<b>30</b>

**27. Other comments, thoughts and observations are welcome. Thank you for your input.**

	<b>Response Count</b>
	10
<b>answered question</b>	<b>10</b>
<b>skipped question</b>	<b>24</b>

**Page 2, Q2. If yes, any particular type?**

1	Unique pubs / ethnic food	Oct 1, 2013 7:33 PM
2	There are empty store fronts available. especially downtown Novi	Sep 29, 2013 10:02 AM
3	Walking shopping areas similar to a downtown safe from cars.	Sep 27, 2013 12:50 PM
4	We need to get a Chic Fil A up here in the North	Sep 26, 2013 9:09 AM
5	Services	Sep 25, 2013 12:00 PM
6	Please bring in more high end, family friendly options for dining; J Alexanders, Bravo, Red Coat Tavern,...	Sep 25, 2013 11:24 AM

**Page 2, Q6. If yes, what type of residential development would be best?**

1	Housing that would attract young, employed professionals	Sep 25, 2013 11:24 AM
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**Page 2, Q9. How should vacant parcels along Flint Street be developed/re-developed?**

1	Mix of uses.	Oct 11, 2013 11:08 AM
2	where the heck is Flint Street?	Sep 25, 2013 12:00 PM

**Page 2, Q10. How should the vacant parcels along Lee BeGole Street be developed/re-developed?**

1	Connect Lee BeGole to the Bulivard in the TC	Sep 26, 2013 9:09 AM
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**Page 3, Q13. If yes, what time of day?**

1	dinner time	Nov 21, 2013 7:03 AM
2	hours especially when Suburban Showcase is active	Oct 14, 2013 9:04 AM
3	ALL TIMES OF DAY	Sep 25, 2013 9:38 AM

**Page 3, Q15. How should Flint Street be re-aligned?**

1	Alignment so that it the full ring road (crescent bld) does not require turns	Nov 21, 2013 7:03 AM
2	to allow for downtown store fronts on either side with residential above	Sep 27, 2013 12:53 PM
3	So that it is directly across from the Crescent Boulevard Ext. to Grand River	Sep 25, 2013 9:34 AM

**Page 3, Q18. If yes, which destinations?**

1	The interior area along Main Street	Nov 21, 2013 7:03 AM
2	Diamond Jim Bradys	Sep 29, 2013 10:46 AM
3	Non area residents can't tell what is available in the town center	Sep 29, 2013 10:05 AM
4	Those on the back side.	Sep 25, 2013 4:19 PM
5	Terrible out-of-towners are forever cutting across several lanes to turn in from Novi Road. Not sure where one would PUT the signage, but clearly, these people don't know where stores are located!	Sep 25, 2013 2:15 PM
6	the individual stores. don't care where walmart is	Sep 25, 2013 9:40 AM

**Page 4, Q20. If not, where would you put them?**

1	These need to be placed so that they function with the commercial businesses. For example the open space near Bigby Potbelly and Bagger Daves should be accessible for all restaraunts to have outdoor seating. Fire Station along Grand River should be moved so that the land can be used for a gathering space similiar to that in Farmington. One capable of a farmers market, a band shell, and a focal point for an art fair.	Nov 21, 2013 7:09 AM
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**Page 4, Q22. If yes, where?**

1	Complete Main Street and sidewalks should connect to new open spaces, such as, area near or at Fire Station location.	Nov 21, 2013 7:09 AM
2	Crescent, Flint	Nov 8, 2013 9:36 AM
3	grand river	Oct 14, 2013 9:08 AM
4	The entire area should be walkable.	Oct 11, 2013 11:22 AM
5	To connect the different areas of strip malls. It's very disconnected.	Sep 27, 2013 1:06 PM
6	there needs to be a walkable form to the entire area. Meaning a way to walk from place to place	Sep 26, 2013 9:12 AM
7	Over I-96	Sep 25, 2013 9:12 AM

**Page 4, Q26. If not, how would you change it?**

1	More of it	Nov 8, 2013 9:36 AM
2	more in central part of complex	Sep 25, 2013 4:55 PM
3	more landscaping - make an impression	Sep 25, 2013 9:44 AM
4	Need more greenery, street trees, shrubs, etc along the roadways	Sep 24, 2013 8:12 AM





**Page 4, Q27. Other comments, thoughts and observations are welcome. Thank you for your input.**

1	Increase residential in the area. Residential can be located near 11 Mile Road and Main Street. Create a large usable gathering location that links Town Center and Main Street. This location could be the Fire Station site. The Fire Station building is out dated. Move it further down Grand River or over to Eleven Mile near the Police and DPW facilities.	Nov 21, 2013 7:09 AM
2	timing of lights particularly on Novi road/grand river is terrible. Also, there should be a right turn only lane on southbound Novi road at grand river to alleviate backups past 696 due to timing of lights and people using the right lane to go through southbound on Novi Rd at Grand River.	Oct 14, 2013 9:08 AM
3	* Allow and encourage more dense development in the area. There's a lot of parking that doesn't ever gets used. * Each quadrant should have a publicly accessible open space. * Allow apartments/condos in and around area to bring in more people within walking distance to support businesses and restaurants. * No uniformity among businesses with landscaping, signs, lights, and so forth. Standards should have some continuity.	Oct 11, 2013 11:22 AM
4	Consider building parking garages instead of big parking lots to build more shopping/retail/restuarant space and create a more intimate experience. Current layout is very disconnected and does not create community, very car based. Because the shopping center is so large, cars go too fast and create a dangerous environment for pedestrians due to blind corners and lack of traffic control in parking lots. Feels more like a collection of strip malls than a downtown shopping area. Great hopes for Flint St to accomplish this intimate downtown feel.	Sep 27, 2013 1:06 PM
5	i would love to see Lee BeGole connected to the Bullivard so you could complete the circle and go to 11 mile, it would be nice if it actually conected to Meadowbrook but hey . . . it would be nice if the bullivard connected somewhere from Novi Road.	Sep 26, 2013 9:12 AM
6	WALLMART HAS RUINED THE CENTER AND NOVI !!!!!	Sep 25, 2013 4:24 PM
7	A store like Walmart was a bad idea for the Town Center. It really brings down the clientele in that area. The store is poorly managed by always being dirty and disorganized. Not what Novi stores represent.	Sep 25, 2013 4:00 PM
8	Novi should model a Rochester or Plymouth which cater to a higher paying customer base. It appears to many that Novi is bringing in too many establishments which attract a younger, rougher, undesirable crowd (Tilted Kilt, 80's disco, ...) There is a significant amount of education and wealth in the area and it appears we are all dining and shopping in the Plymouth's, Northville's and Milford's of the area. A WHole Foods, or a Vics/Westborn Market would be value added as well.	Sep 25, 2013 11:29 AM
9	get rid of walmart	Sep 25, 2013 9:41 AM
10	I think that their doesn't need to be any new construction, just place new tenants into the empty buildings all ready there.	Sep 25, 2013 9:35 AM