



CITY of NOVI CITY COUNCIL

Agenda Item 3
July 23, 2018

SUBJECT: Consideration of the request of DTN Management/Tricap Holdings for approval of the Preliminary Site Plan, Phasing Plan, Woodlands Permit, and Storm Water Management Plan, JSP 18-10, and the request to rename Flint Street to Bond Street. The property is zoned TC-1 (Town Center One) and is approximately 7.74 acres. It is located on the southwest side of Flint Street south of Grand River Avenue and west of Novi Road. The applicant is proposing a mixed-use development called The Bond, with two four-story multi-family residential buildings with a total of 253 apartments and a 5,578 square foot single-story commercial building.

SUBMITTING DEPARTMENT: Community Development Department - Planning *Pa...*

CITY MANAGER APPROVAL: 

BACKGROUND INFORMATION:

The subject property is approximately 7.74 acres and is located on the southwest side of Flint Street. Flint Street is located near the southwest quadrant of Grand River Avenue and Novi Road (Section 22). The proposed development is a permitted use in the existing TC-1, Town Center District, and the request does not require rezoning the property. The applicant is proposing to redevelop the former Fendt Transit Mix Concrete Plant into a mixed use development with two four-story multi-family residential buildings with a total of 253 apartments and a single-story commercial building (5,578 SF). The site improvements include a two level parking structure, surface parking, site amenities such as a swimming pool, landscaped courtyards and related landscape improvements. The building's orientation is primarily toward Flint Street, with only a few of the building's windows opening onto the rear property line adjacent to the railroad tracks.

The development is proposed to be constructed in three phases. Each of the two residential buildings will be constructed in a separate phase. A temporary six-space gravel parking area for cemetery visitors will be constructed along with phase 1. The commercial building will be constructed in the third phase along with paving the parking that is offered for convenient access to the cemetery.

Parking for Cemetery Visitors

The applicant is proposing to dedicate six parking spaces in the proposed commercial parking lot fronting on Novi Road to provide convenient access to visitors to the Novi Public Cemetery.

Mixed Use Eligibility

The site plan qualifies for a mixed-use development and the higher densities the TC-1, Town Center district offers since the applicant is proposing 10 percent of total development as a non-residential use. Because the applicant is proposing to build the qualifying non-residential use in phase 3, the timing of which is undetermined, the applicant will be required to provide a form of agreement and/or financial guarantees acceptable to the City that assure the commercial component will be built within a certain time as suggested by applicant and approved by the City. This is one of the conditions of Planning Commission's recommendation.

Deviations

Although staff worked diligently with the applicant over several months to reduce the number of deviations that the plan would require, the site plan currently requires a long list of waivers and variances as noted in the suggested motion. The subject parcel has an atypical long but shallow shape that limits conformance to certain code requirements. The applicant has offered to dedicate approximately an acre of the property for the Flint Street realignment plans which further decreased the depth of the property and made it even shallower.

Density and Total Number of Rooms:

There are several major considerations by the City Council as a part of the Site Plan Review. Of them, four of them are a result of temporary gravel parking the applicant is proposing as a benefit to cemetery visitors. One is a minor deviation, supported by staff, to the proposed building setback. One is to approve an increase in maximum number of total rooms allowed.

In the Town Center district, density is calculated on the total number of rooms proposed in the residential development. The development proposes a mix of 1, 2 and 3 bedroom units. In the TC-1 District, the maximum allowable rooms is calculated by taking the area of the parcel in square feet, divided by a factor of 800 (for a mixed use development). For the subject parcel, the maximum number of rooms allowed is 421 rooms (7.74 acres = 336,718 sq. ft. / 800). The applicant is proposing a total of 627 rooms. The City Council may approve the increase in the room count (421 allowed, 627 proposed) up to twice the number of rooms otherwise permitted by the ordinance.

Street Name Change

The applicant has made a request to rename the existing public road, Flint Street, to Bond Street. The City's Project and Street Naming Committee has recommended approval of the new street name, as it does not conflict with any other street names in the City. The names of public streets may be changed by resolution of the City Council, after review and recommendation from the City's Administrative Street Name Review Committee. If the City Council is inclined to approve the development, staff will present a resolution to rename Flint Street at a subsequent meeting, after providing notice to nearby property owners.

Other Reviews:

Engineering

The proposed site plan reflects the Alternative 1A alignment of Flint Street included in the City's Capital Improvement Plan (CIP) for repaving and construction of Flint Street between Novi Road and Grand River Avenue for Fiscal Year 2019-2020. The applicant is working with City staff to identify the timeline for the construction of the proposed realignment in order to coordinate with the construction of the development (if it is approved). A memo from the Engineering Division addressing this item is attached.

The applicant is proposing to dedicate the necessary right of way (approximately 1 acre) along the project's Flint Street frontage in order to accommodate the City's plans to reconstruct and realign Flint Street in the future. On-street parking is proposed along the realigned public road, similar to the on-street parking that is currently available along Main Street, east of Novi Road. The applicant has further clarified that the proposed sidewalk on the southwest side of Flint Street will be eight feet in width, consistent with the Planning Commission's recommendation, and the plan will be modified at the time of Final Site Plan Review to show an eight foot wide sidewalk.

Typically, a Right-of-way dedication has to be completed prior to Final Stamping Sets approval. In this case, if the City Council approves the development plan, Engineering may request the Right-of-Way earlier to start construction of road improvements.

Landscape

There are a number of City Council waivers required as listed below. The applicant has worked to eliminate many of the waivers and reduce the impact of others to the point where the following waivers can be supported:

- Lack of berm and screening from a non-residential property
- Reduction in the Right-of-Way greenbelt width
- Reduction in multifamily trees
- Reduction in interior roadway perimeter trees
- Deficiency for building foundation landscape for the parking garage
- Lack of parking lot perimeter trees for the temporary gravel parking spaces

Wetlands & Woodlands

The site does not appear to contain regulated wetlands; however the Walled Lake Branch of the Middle Rouge River flows through the southeast section of the site. The regulated woodlands trees are located in the southeast section of the site, along the river, and in the northwest part of the property. The woodland trees are predominantly cottonwood trees. The applicant is proposing to remove 103 trees, or approximately 70 percent of the total regulated trees on site. 139 replacement tree credits will be required. It is expected that the applicant will make a payment into to the tree fund due to lack of available space on site for woodland plantings.

Traffic

The applicant has provided a Traffic Impact Study that provided a comparative analysis between the City's current Flint Street realignment plans and the proposed Flint Street design from the developer. Left-turn passing lanes are not warranted based on future traffic volumes. The study also determined that a 75 foot northbound left-turn lane should be provided at the intersection of Flint Street and Grand River Avenue. The applicant has revised the plan to eliminate the deviations identified in earlier reviews. Traffic is recommending approval with additional details to be provided at the time of Final Site Plan review.

Façade

The proposed building elevations would require multiple deviations as listed below:

- Underage of brick, overage of EIFS, and underage of combined percentages of brick and stone for Building 1 and Building 2
- Lack of brick and overage of cast stone for the proposed parking structure
- Overage of ribbed metal and overage of cast stone for the proposed commercial building

The Façade review notes that, in general, the buildings exhibit interesting massing and a creative use of materials and colors. The deviations are minor in nature and the overall appearance of the buildings would not be significantly improved by strict application of the percentages of materials listed in the Ordinance. The deviations are recommended. The applicant has provided a façade board, a picture of which is included in the packet.

Fire

The Fire Marshal is recommending approval with additional items to be addressed at the time of Final Site Plan submittal.

Planning Commission Action

On June 27, 2018, Planning Commission held the required public hearing and recommended approval to the City Council of the Preliminary Site Plan, Phasing Plan, Woodlands Permit, and Storm Water Management Plan based on the motion listed in the action summary attached. Draft meeting minutes are also attached.

According to Section 3.27, when a site under development in the TC or TC-1 District is five (5) acres or more in area, Preliminary Site Plan approval shall be by the City Council after review and recommendation by the Planning Commission. Following the City Council's approval, the Final Site Plan approval may be granted administratively.

If the matter is approved by the City Council, the applicant is tentatively scheduled to go before Zoning Board of Appeals on August 14, 2018 to seek the necessary variances.

RECOMMENDED ACTION: Two part motion:

Part 1

Approval of the request of DTN Management/Tricap Holdings for JSP 18-10 for the Preliminary Site Plan, Phasing Plan, Woodland Permit, and Storm Water Management Plan, subject to and based on the following:

1. The applicant shall provide a form of agreement and/or financial guarantees as acceptable to the City, at the time of Final Site Plan submittal, to assure that the commercial component will be built within a certain time as suggested by applicant and approved by the City.
2. City Council finding per Section 4.82.2.b. for allowing an increase of the maximum number of rooms allowed (421 allowed, 627 proposed) *based on justification provided by the applicant in their response letter dated June 22, 2018;*
3. A City Council waiver for exceeding the maximum allowable front yard building setback per Section 3.1.26.D (10 ft. maximum allowed, approximately 15 ft. proposed) *due to the unusual and shallow shape of the subject property;*
4. City Council approval according to Sec. 3.6.2.Q. for allowing an increase in the minimum required parking setback as listed in Sec. 3.1.26.D for six parking spaces designated for public use (10 ft. maximum allowed, approximately 7 ft. proposed) *as the applicant has clearly demonstrated that the minimum parking setback area is met in the remainder of the site;*
5. City Council variance from Sec. 11-239(b)(1),(2) of Novi City Code for the absence of hard surface for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1 *as the requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
6. City Council variance from Sec. 11-239(b)(1),(2) of Novi City Code for absence of curb and gutter for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1 *as the requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
7. City Council variance from Sec. 11-239(b)(3) of Novi City Code for absence of pavement markings and layout including end islands for proposed temporary parking lot of six spaces in Phase 1 *as the requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
8. A Section 9 waiver for the following deviations *as the overall appearance of the building would not be significantly improved by strict application of the percentage listed in the Ordinance:*

- a. Not providing the 30 percent minimum required brick on the facades for Building 1 and 2 as follows: east (28% proposed), north (28% proposed) and south (26% proposed);
 - b. Exceeding the 25 percent maximum allowed percentage of EIFS on all facades for Building 1 and 2 (proposed: East - 28%, North - 38%, South - 35% and West - 48%);
 - c. Not providing the 50 percent minimum required brick and stone for TC-1 district on the north façade for Building 1 and 2 (48% proposed);
 - d. Not providing the minimum 30 percent required brick on all facades for the Commercial Building (proposed: North - 23%, West - 8%, South - 8% and East - 17%);
 - e. Exceeding the 50 percent maximum allowed for Cast Stone on all facades for the Commercial Building (proposed: North - 55%, West - 76%, South - 76% and East - 64%);
 - f. Exceeding the maximum allowed percentage for Ribbed Metal (0% allowed) on all facades providing the ribbed metal for the Commercial Building (proposed: North - 12%, West - 6%, South - 6% and East - 9%);
 - g. Exceeding the maximum allowed concrete for west facade of the parking structure (0% allowed, 100% proposed) in lieu of providing the minimum required brick (30% minimum required, 0% provided);
 - h. Exceeding the maximum allowed cast stone for north and south facades of the parking structure (0% allowed, 100% proposed) in lieu of providing the minimum required brick (30% minimum required, 0% provided);
9. Landscape waiver from Sec. 5.5.3.B.ii for the lack of a berm and screening as *the applicant proposed a line of arborvitae along the property line to soften the view toward the railroad tracks and industrial site beyond in lieu of required landscape screening;*
 10. Landscape waiver from Sec. 5.5.3.B.ii for a reduction in the required greenbelt width between the right-of-way and parking areas along Flint/Bond Street (20 ft. width required, a range of 10 ft. to 20 ft. provided). A 2.5 foot brick wall screening the parking and additional landscaping in the narrower areas help to compensate for the lack of space in the areas with just a 10 foot greenbelt;
 11. Landscape waiver from Sec. 5.5.3.F.ii.b(1) for a reduction in the total number multifamily unit trees provided (147 trees required, 127 provided) *as the reduction is only 14% from the total requirements and the site is otherwise well-landscaped;*
 12. Landscape waiver from Sec. 5.5.3.F.ii.B(2) for the reduction in the number of interior roadway perimeter trees provided (1 tree short) *due to conflict with fire access lane (grass pavers);*
 13. Landscape waiver from Sec. 5.5.3.D. for the deficiency in the foundation landscaping coverage around the parking deck *due to limited space available along the southwest side, along the railroad. Large arborvitae are proposed in that area to help screen the view of the railroad and the industrial site;*
 14. Landscape waiver from Sec. 5.5.3.C.(3) Chart footnote for not proposing the required parking lot perimeter trees for the temporary gravel parking proposed to be constructed for use by visitors to Novi Cemetery in Phase 1 (11 trees required, 0 proposed) *as the landscape requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
 15. The following variances would require Zoning Board of Appeals approval:
 - a. A Zoning Board of Appeals variance from Section 4.82.2 for increasing the maximum percentage of one bed room units allowed for this development (50% maximum allowed, 58% proposed) *based on applicants response that a 60% unit mix is recommended based on their internal marketing survey and assessment;*
 - b. A Zoning Board of Appeals variance from Section 3.27.1.D for allowing parking in side yard for commercial building (around 49 spaces) *due to the unusual shallow shape of the subject property and the inability to park in the rear yard;*

- c. A Zoning Board of Appeals variance from Section 3.27.1.D for allowing parking in front yard for residential section (around 38 spaces, 9% of total 432 spaces) *due to the unusual shallow shape of the subject property and the inability to park in the rear yard;*
 - d. A Zoning Board of Appeals variance from Section 3.27.1.D for allowing parking in side yard for residential section (around 50 spaces, 12% of total spaces in east and 35 spaces 12% of total spaces in west) *due to the unusual shallow shape of the subject property and the inability to park in the rear yard;*
 - e. A Zoning Board of Appeals variance from Section 4.82.2.e for a reduction of the minimum building setback for Building 1 on the east side (15 ft. required, a minimum of 12 ft. with overhang of 8.8 ft. proposed for an approximate length of 12 ft., total building length is 283 ft.) *due to the unusual shallow shape of the subject property;*
 - f. A Zoning Board of Appeals variance from Section 4.82.2.e for a reduction of the minimum building setback for Building 2 on the east side (15 ft. required, a minimum of 8 ft. with overhang of 3.8 ft. proposed for an approximate length of 16 ft., total building length is 283 ft.) *due to the unusual shallow shape of the subject property;*
 - g. A Zoning Board of Appeals variance from Section 4.82.2.e for a reduction of the minimum building setback for the parking garage on the west side (15 ft. required, 5 ft. proposed for entire structure, total building length is 283 ft.) *due to the unusual shallow shape of the subject property;*
 - h. A Zoning Board of Appeals variance from Section 5.7.3.E. for allowing an increase of the average to minimum light level ratio for the site (4:1 maximum allowed, 4.81 provided) *due to site layout and the site's shallow depth;*
 - i. A Zoning Board of Appeals variance from Section 5.7.3.K for exceeding the maximum allowed foot candle measurements along the south property line abutting the railroad tracks (1 foot candle is maximum allowed, up to 1.7 foot candles is proposed for a small area);
 - j. A Zoning Board of Appeals variance from Section 3.27.1.H. and Sec. 5.4.2 for allowing two loading areas in the side yard for the residential section *due to the unusual shallow shape of the subject property;*
 - k. A Zoning Board of Appeals variance from Section Sec. 5.4.2 for a reduction in the minimum required loading area for each of the two loading spaces in the residential section (2,830 square feet required, 644 square feet provided) *due to residential nature of the development that does not require larger loading areas;*
 - l. A Zoning Board of Appeals variance from Section 3.27.1.I. for a reduction in width of the sidewalk along a non-residential collector (12.5 feet required on both sides, 8 feet proposed on west side and 10 feet asphalt path proposed on east) *as it aligns with City's current plans for Flint Street realignment;*
 - m. A Zoning Board of Appeals variance from Section 5.3.2. for a reduction of the minimum parking bay depth for spaces proposed in the parking garage (19 ft. minimum required, 18 ft. proposed) *as the depth is limited by the pre-fabricated manufacturers specifications; and*
16. The findings of compliance with Ordinance standards in the staff and consultant review letters and the conditions and the items listed in those letters being addressed on the Final Site Plan.

This motion is made because the plan is otherwise in compliance with Article 3, Article 4, and Article 5 of the Zoning Ordinance, Chapter 11 and Chapter 37 of the Code of Ordinances and all other applicable provisions of the Ordinance.

Part 2

Adoption of the attached Resolution to Change Street Name from Flint Street to Bond Street.

MAPS

Location

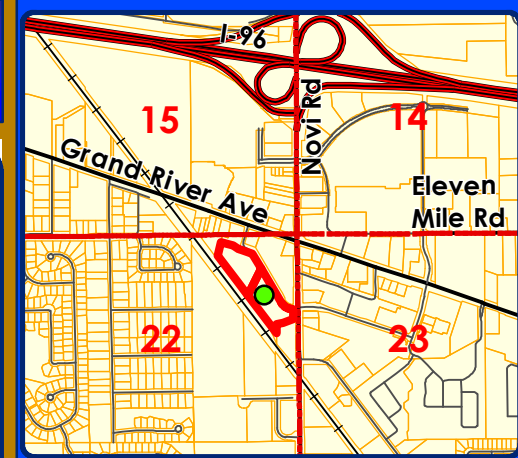
Zoning

Future Land Use

Natural Features

JSP 18-10 THE BOND fka THE DISTRICT

Location Map



LEGEND

 Sections



City of Novi

Dept. of Community Development
City Hall / Civic Center
45175 W Ten Mile Rd
Novi, MI 48375
cityofnovi.org

Map Author: Sri Komaragiri
Date: 06/21/18
Project: JSP 18-10 THE BOND fka THE DISTRICT
Version #: 1



1 inch = 282 feet

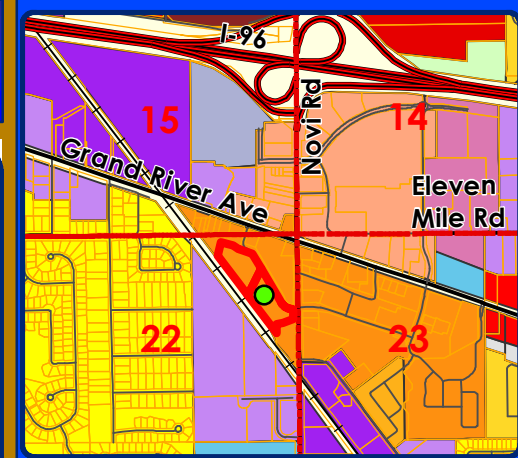
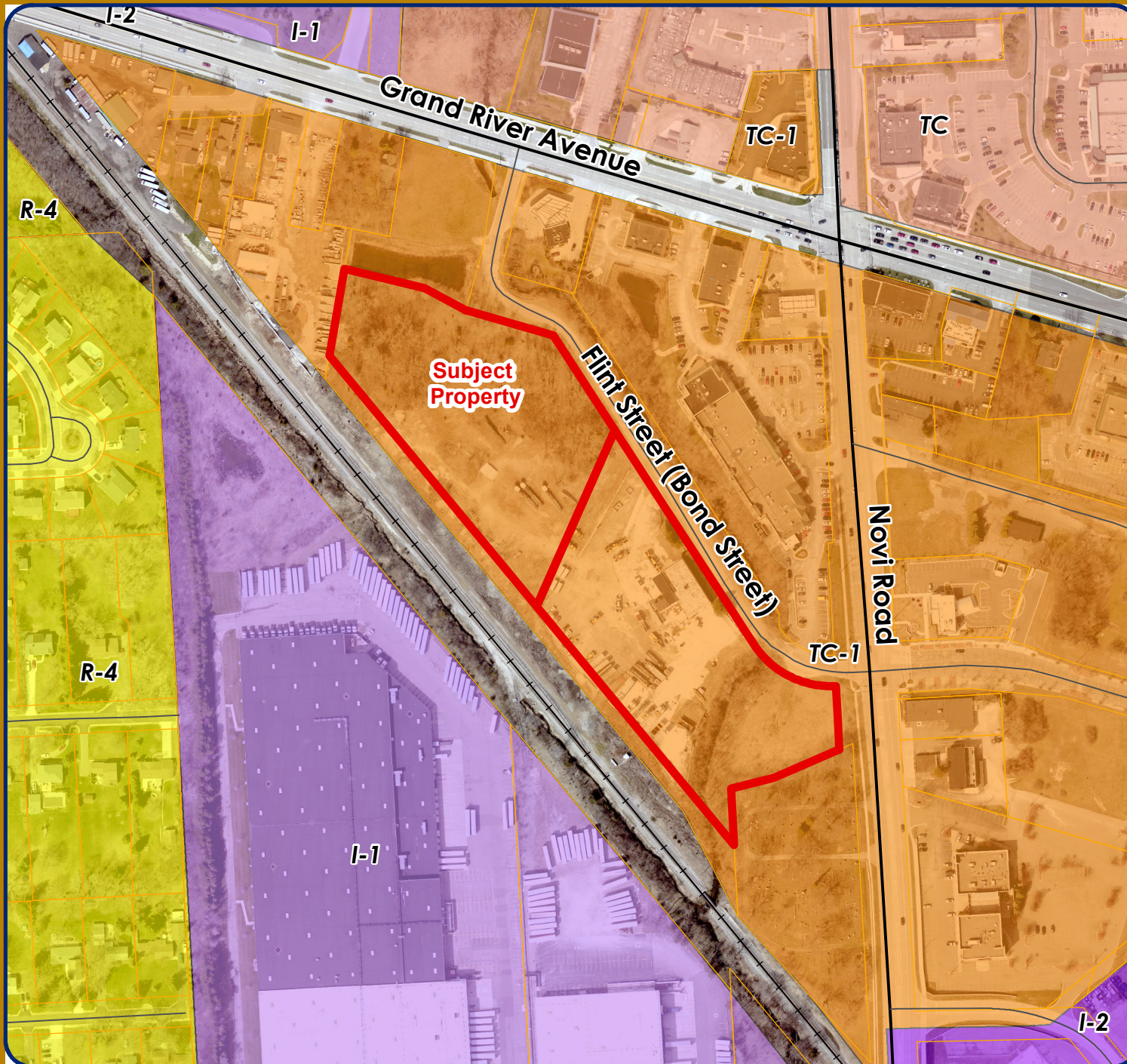


MAP INTERPRETATION NOTICE

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JSP 18-10 THE BOND fka THE DISTRICT

Zoning



LEGEND

- Sections
- R-A: Residential Acreage
- R-4: One-Family Residential District
- RM-1: Low-Density Multiple Family
- RM-2: High-Density Multiple Family
- B-3: General Business District
- C: Conference District
- EXPO: EXPO District
- I-1: Light Industrial District
- I-2: General Industrial District
- OS-1: Office Service District
- OSC: Office Service Commercial
- OST: Office Service Technology
- RC: Regional Center District
- P-1: Vehicular Parking District
- TC: Town Center District
- TC-1: Town Center -1 District



City of Novi
 Dept. of Community Development
 City Hall / Civic Center
 45175 W Ten Mile Rd
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Feet

0 62.5 125 250 375

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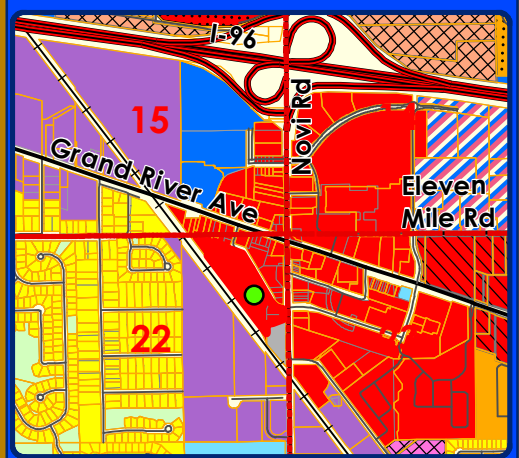
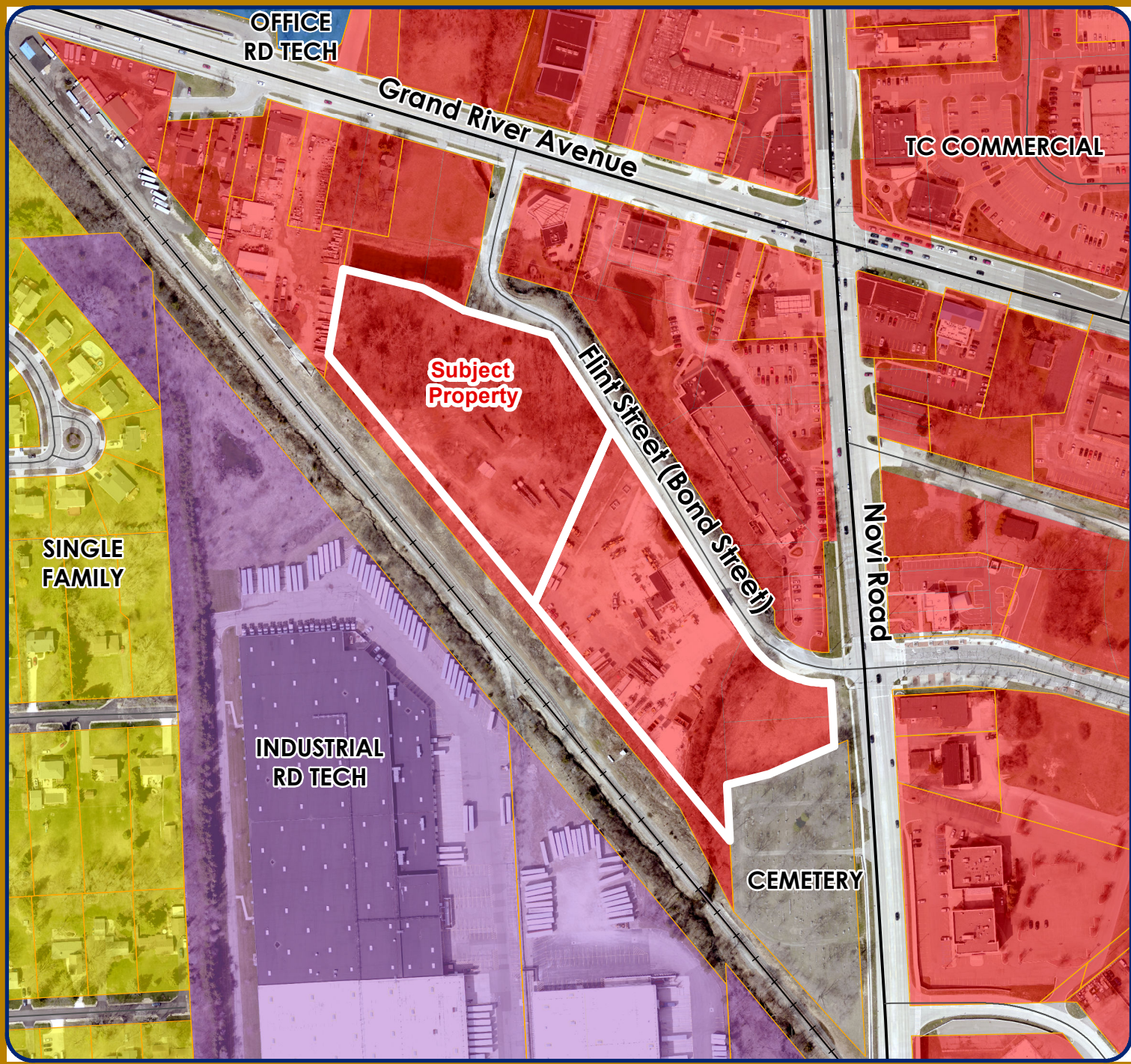


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JSP 18-10 THE BOND fka THE DISTRICT

Future Land Use



LEGEND

- Sections
- FUTURE LAND USE**
- Single Family
- Multiple Family
- PD1
- Office RD Tech
- Office Commercial
- Industrial RD Tech
- Heavy Industrial
- Regional Commercial
- TC Commercial
- TC Gateway
- Public
- Private Park
- Cemetery

CITY OF NOVI
 Dept. of Community Development
 City Hall / Civic Center
 45175 W Ten Mile Rd
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0 62.5 125 250 375 Feet

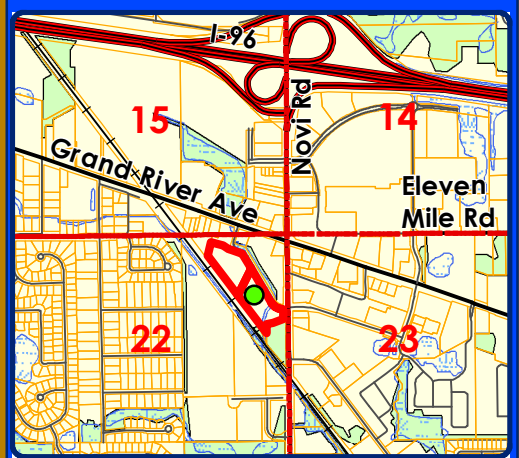
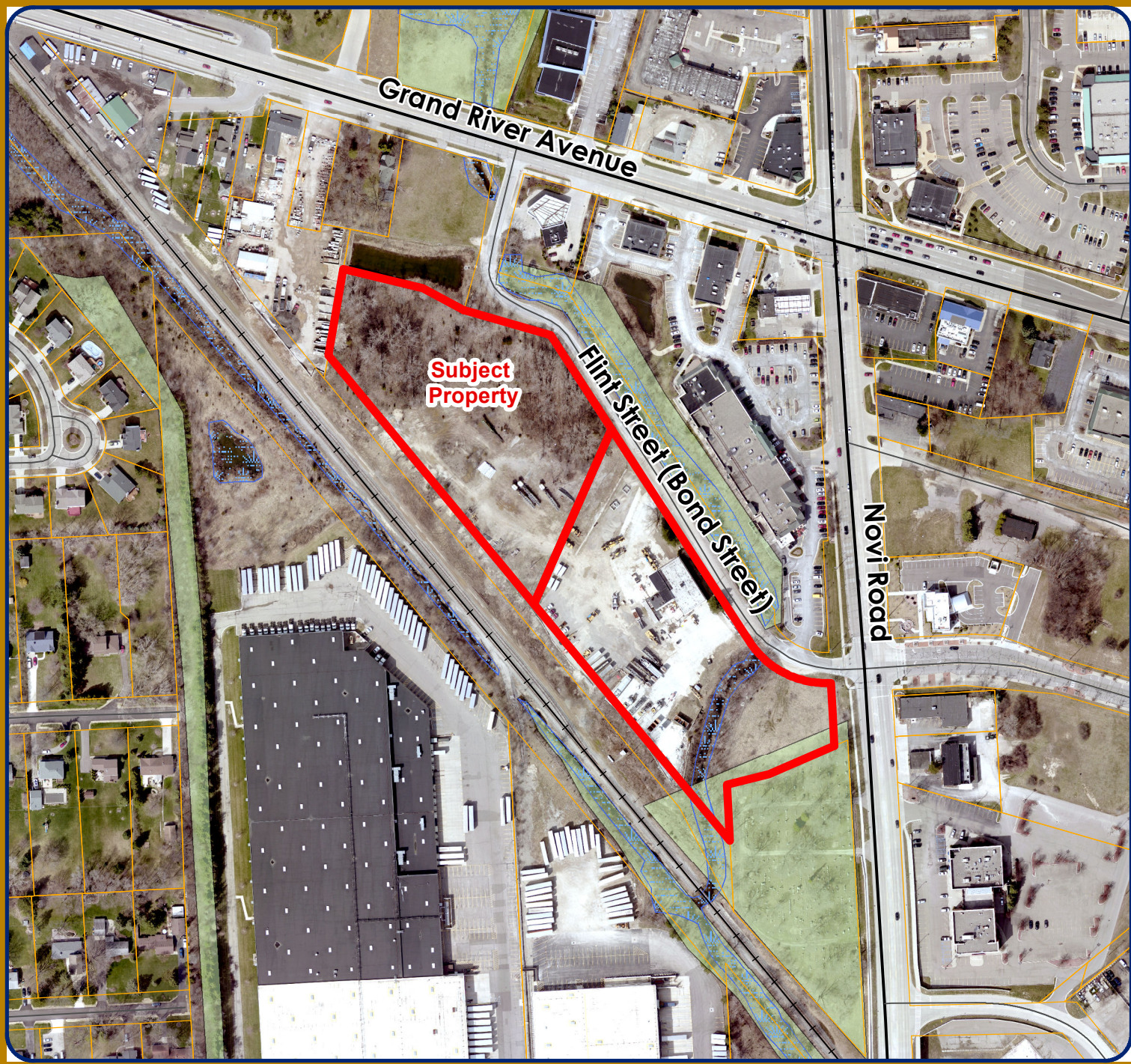
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JSP 18-10 THE BOND fka THE DISTRICT

Natural Features



- LEGEND**
- Sections
 - WETLANDS
 - WOODLANDS



City of Novi

Dept. of Community Development
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SITE PLAN

(Full plan set available for viewing at the Community Development Department.)

PRELIMINARY SITE PLANS FOR:

THE BOND

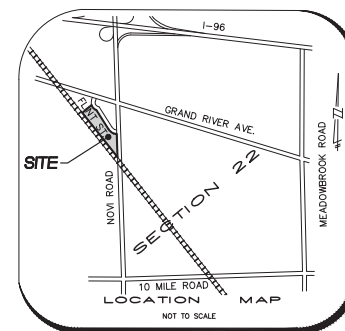
SECTION 22, TOWN 1 NORTH, RANGE 8 EAST,
CITY OF NOVI, OAKLAND COUNTY, MICHIGAN
PREPARED FOR:



DTN MANAGEMENT COMPANY
2502 LAKE LANSING ROAD, SUITE C
LANSING, MICHIGAN 48912
PHONE: 517.371.5300



TRICAP HOLDINGS LLC
30600 NORTHWESTERN, SUITE 430
FARMINGTON, MICHIGAN 48334
PHONE: 248.538.1389 EXT. 236



LEGAL DESCRIPTIONS

PARCELS 1 & 2 COMBINED (8.731 ACRES)
THE LAND REFERRED TO IN THIS COMMITMENT, SITUATED IN THE COUNTY OF OAKLAND, CITY OF NOVI, STATE OF MICHIGAN, IS DESCRIBED AS FOLLOWS:

PARCEL 1 (AS RECORDED AND SURVEYED)
LOTS, 6, 7 AND 8 "RAILROAD SUBDIVISION", ACCORDING TO THE PLAT THEREOF AS RECORDED IN LIBER 92 OF PLATS, PAGE 16 OAKLAND COUNTY RECORDS.

PARCEL 2 (AS RECORDED AND SURVEYED)
PART OF LOT 9 OF "RAILROAD SUBDIVISION", ACCORDING TO THE PLAT THEREOF AS RECORDED IN LIBER 92 OF PLATS, PAGES 16, 17, AND 18, OAKLAND COUNTY RECORDS, DESCRIBED AS: BEGINNING AT THE SOUTHEAST CORNER OF LOT 7 OF "SUPERVISORS PLAT NO.3", AS RECORDED IN LIBER 54A OF PLATS, PAGE 84, OAKLAND COUNTY RECORDS, ALSO BEING THE NORTHERLY CORNER OF SAID LOT 9; THENCE 558'39"32" (EIRECORDED) 558'38"11" E (MEASURED) 91.79 FEET; THENCE ALONG THE SOUTHWESTERLY LINE OF FLINT STREET, 90.00 FEET WIDE, S70°38'00" E, 176.30 FEET; THENCE ALONG THE SOUTHWESTERLY LINE OF FLINT STREET, 75.00 FEET WIDE, S29°51'45" E, 219.68 FEET; THENCE ALONG THE EASTERLY LINE OF SAID LOT 9, S27°03'36" W, 375.00 FEET; THENCE ALONG THE SOUTHERLY LINE OF SAID LOT 9, N38°26'24" W, 633.32 FEET; THENCE ALONG THE WESTERLY LINE OF SAID LOT 9, N13°04'21" E, 171.48 FEET; THENCE ALONG THE SOUTHERLY LINE OF SAID LOT 7 AND THE NORTHERLY LINE OF SAID LOT 9, S73°24'45" E, 160.50 FEET TO THE POINT OF BEGINNING.

NOTE: THE FOLLOW DESCRIPTION IS BASED ON SURVEY BY JOSEPH C. KAPELCZAK, PS ON 5-17-2011 JOB NO. 10018 WITH A BASIS OF BEARING ON NAD83 (COR 96) SPC MICHIGAN SOUTH ZONE. THE FOLLOWING BEARINGS DO NOT RELATE TO THE RECORD PLAT FOR THE "RAILROAD SUBDIVISION", THEREFORE THE ABOVE RECORDS DESCRIPTION FOR PARCEL 2 WAS USED FOR THIS SURVEY ON THIS DATE.

ALSO DESCRIBED BY SURVEY JOSEPH C. KAPELCZAK, JOB NO. 10018:
PART OF LOT 9 "RAILROAD SUBDIVISION", A SUBDIVISION OF PART OF LOTS 17 OF "SUPERVISORS PLAT NO.3" LIBER 54A, PAGE 84 OF PART OF THE SOUTHEAST 1/4 OF SECTION 15 AND PART OF THE NORTHEAST 1/4 OF SECTION 22, TOWN 1 NORTH, RANGE 8 EAST, CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, AS RECORDED IN LIBER 92 OF PLATS, PAGE 16, 17, AND 18, OAKLAND COUNTY RECORDS, MORE PARTICULARLY DESCRIBED AS: BEGINNING AT A POINT LOCATED 503°10'57" E, ALONG THE EAST LINE OF SAID SECTION 22, 138.97 FEET AND N73°48'57" W ALONG THE NORTH LINE OF SAID "SUPERVISORS PLAT NO.3" 704.61 FEET AND 509°00'28" W, 168.50 FEET AND N76°27'32" W 100.00 FEET AND 507°56'01" W, 99.43 FEET FROM THE NORTHEAST CORNER OF SAID SECTION 22, BEING THE POINT OF BEGINNING AND THE SOUTHEAST CORNER OF LOT 7 OF SAID "SUPERVISORS PLAT NO.3"; THENCE S 61°37'16" E, 92.05 FEET; THENCE S74°04'10" E, 176.38 FEET; THENCE S32°39'31" E, 218.62 FEET; THENCE S23°38'55" W, 377.04 FEET; THENCE N 39°42'28" W, 633.32 FEET; THENCE N 10°02'34" E, 171.48 FEET; THENCE S77°38'25" E, 161.11 FEET TO THE POINT OF BEGINNING.

PARCEL 1 #22-22-226-003
PARCEL 2 #22-22-226-005



- ### INDEX
1. COVER SHEET
 2. OVERALL SITE PLAN
 3. R.O.W. TAKING PLAN AND OPEN SPACE PLAN
 4. STORM WATER MANAGEMENT PLAN
 5. PHASING PLAN

- ### BENCHMARKS
- RR SPIKE IN SOUTH FACE OF POWER POLE LOCATED ON THE WEST SIDE OF FLINT STREET & 88' SOUTHEAST OF THE SOUTHEAST BUILDING CORNER.
 - ELEVATION 908.44 (CITY OF NOVI DATUM)
 - RIM OF SANITARY MANHOLE AT NORTHERMOST POINT OF PROPERTY
 - ELEVATION 908.51 (CITY OF NOVI DATUM)



SEIBER, KEAST ENGINEERING, L.L.C.

CONSULTING ENGINEERS
100 MAINCENTRE • SUITE 10 • NORTHVILLE, MICHIGAN • 48167
PHONE: 248.308.3331

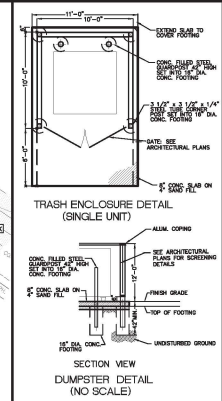
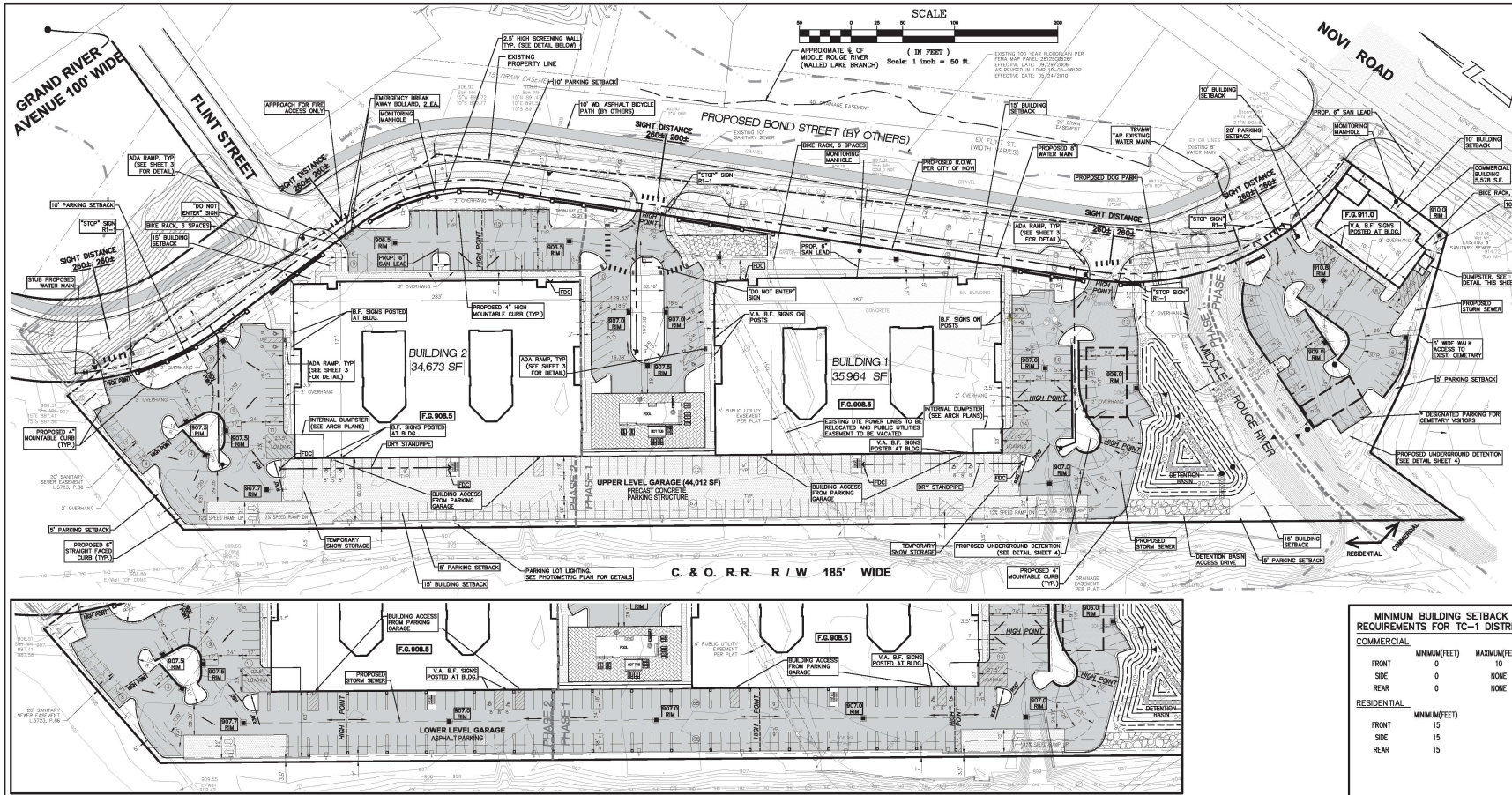
ARCHITECTURAL DESIGN PREPARED BY:
HUMPHREYS & PARTNERS ARCHITECTS, L.P.
53359 ALPHA ROAD
SUITE 300 • DALLAS, TX 57240
PHONE: 972.701.9636

BOUNDARY SURVEY PREPARED BY:
FAZAL KHAN & ASSOCIATES, INS
43279 SCHOENHERR
STERLING HEIGHTS, MICHIGAN 48313
PHONE: 586.739.8007
FAX: 586.739.6994

LANDSCAPE PLANS PREPARED BY:
ALLEN DESIGN, LLC
LANDSCAPE ARCHITECTURE
557 CARPENTER
NORTHVILLE, MICHIGAN 48167
PHONE: 248.467.4668

PROPERTY BOUNDARY & TOPO INFORMATION
ALPINE ENGINEERING, INC.
46892 WEST ROAD, SUITE 109
NOVI, MICHIGAN 48377
PHONE: 248.926.3765

08-10-18		DESIGNED BY: A.A. JOB NUMBER: 17-058
		CHECKED BY: R.B. DRAWING FILE: J-17030-CV.DWG
COVER SHEET		
	SEIBER, KEAST ENGINEERING, L.L.C. CONSULTING ENGINEERS 100 MAINCENTRE • SUITE 10 • NORTHVILLE, MI • 48167 PHONE: 248.308.3331	
	SHEET 1	



SITE DATA:

- EXISTING ZONING = TC-1
- EXISTING AREA OF SITE GROSS = 8.731 AC
- AREA OF SITE GROSS (AFTER R.O.W. TAKING) = 7.744 AC
- AREA OF COMMERCIAL SITE = 1.07 AC
- AREA OF RESIDENTIAL SITE = 6.67 AC

LOT AREA COVERAGE:

- BUILDING 1 (PHASE 1) = 35,964 SF
- BUILDING 2 (PHASE 2) = 34,673 SF
- PARKING GARAGE = 44,012 SF
- COMMERCIAL BUILDING = 5,578 SF
- TOTAL AREA = 120,227 SF**
- 120,227 SF / 337,154 SF = **35.66%**

RESIDENTIAL (PHASES 1 & 2):

- TOTAL NO. OF RESIDENTIAL UNITS = 253 UNITS
- BUILDING 1 (PHASE 1): 127 UNITS
- BUILDING 2 (PHASE 2): 126 UNITS
- TOTAL NUMBER OF RESIDENTIAL PARKING REQUIRED (TC DISTRICT) = 432
- TOTAL NUMBER OF RESIDENTIAL PARKING PROVIDED (INCLUDING 14 MP. PARKING SPACES) = 431

PHASE 1:

- 2-LEVEL GARAGE SURFACE = 152
- BOND STREET PARALLEL = 20
- PHASE 1 RESIDENTIAL PARKING SPACES PROVIDED = 231

TOTAL:

- 2-LEVEL GARAGE SURFACE = 270
- BOND STREET PARALLEL = 142
- TOTAL RESIDENTIAL PARKING SPACES PROVIDED = 431

COMMERCIAL (PHASE 3):

- TOTAL FLOOR AREA (USE IS UNDETERMINED) = 5,578 SF
- PARKING SPACES PROVIDED = 49

MINIMUM BUILDING SETBACK REQUIREMENTS FOR TC-1 DISTRICT

COMMERCIAL	MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	0	10
SIDE	0	NONE
REAR	0	NONE

RESIDENTIAL

MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	15
SIDE	15
REAR	15

MINIMUM PARKING SETBACK

COMMERCIAL	MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	20	20
REAR	10	10
SIDE	10	10

RESIDENTIAL

MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	10
REAR	10
SIDE	5

BIKE PARKING PROVIDED:

- BUILDING 1: 100
- BUILDING 2: 100
- TOTAL RESIDENTIAL PARKING SPACES: 52 (20 REQUIRED)
- COMMERCIAL: 100 (2 REQUIRED)

MINIMUM BUILDING SETBACK REQUIREMENTS FOR TC-1 DISTRICT

COMMERCIAL	MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	0	10
SIDE	0	NONE
REAR	0	NONE

RESIDENTIAL

MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	15
SIDE	15
REAR	15

MINIMUM PARKING SETBACK

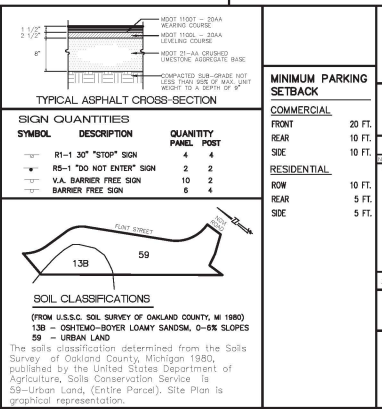
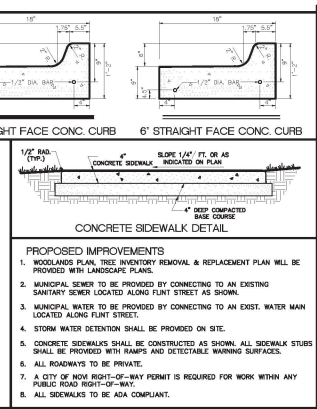
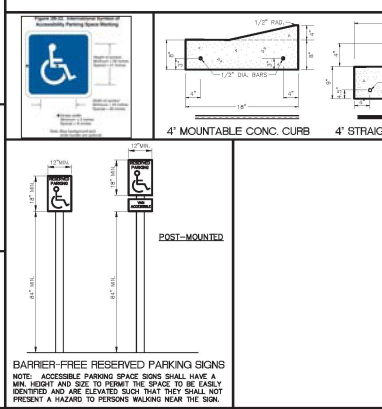
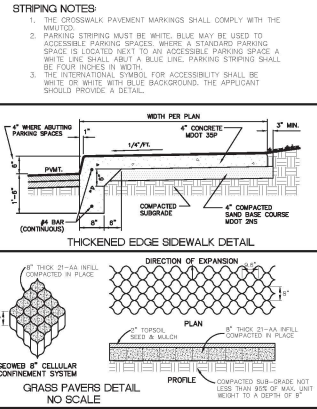
COMMERCIAL	MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	20	20
REAR	10	10
SIDE	10	10

RESIDENTIAL

MINIMUM(FEET)	MAXIMUM(FEET)
FRONT	10
REAR	10
SIDE	5

BIKE PARKING PROVIDED:

- BUILDING 1: 100
- BUILDING 2: 100
- TOTAL RESIDENTIAL PARKING SPACES: 52 (20 REQUIRED)
- COMMERCIAL: 100 (2 REQUIRED)



THE BOND

SECTION 22, TOWN 1 NORTH, RANGE 8 EAST
CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

REVISIONS

NO.	DATE	DESCRIPTION
1	05-10-18	ISSUED FOR PERMITS
2	05-10-18	ISSUED FOR PERMITS

UTILITY WARNING

UNDERGROUND UTILITY LOCATIONS AS SHOWN ON THE PLAN, WERE OBTAINED FROM UTILITY OWNER AND NOT FIELD VERIFIED WITH CONSTRUCTION.

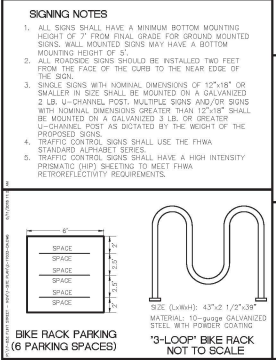
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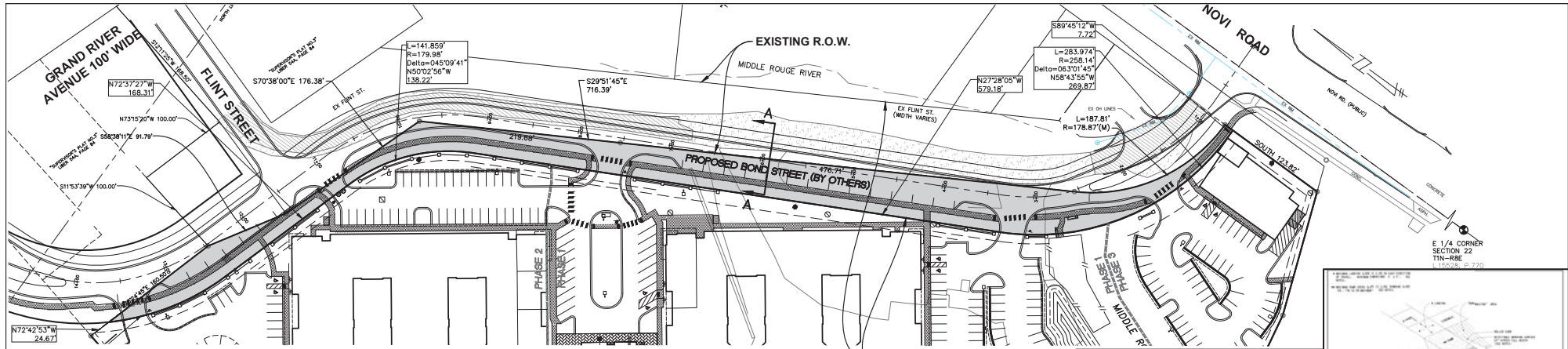
OVERALL SITE PLAN

SEIBER, KEAST ENGINEERING, L.L.C.

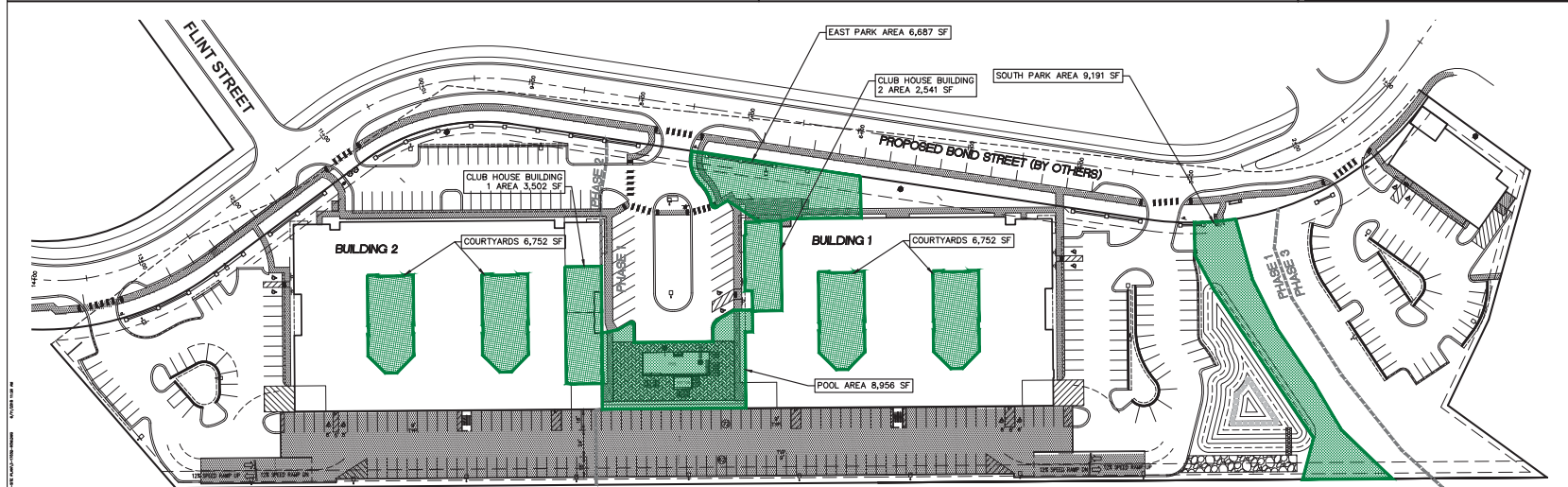
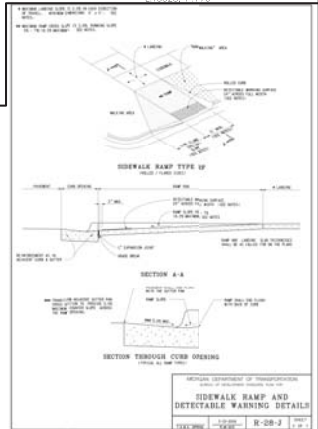
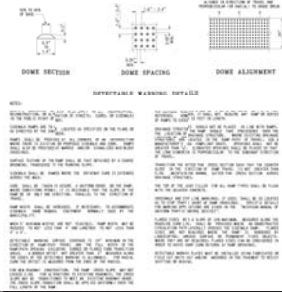
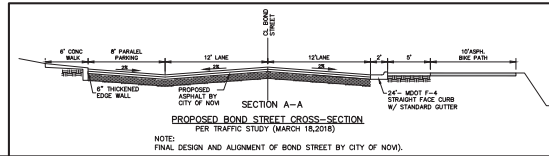
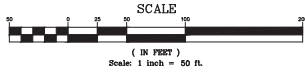
CONSULTING ENGINEERS
100 MANICENTRE A SUITE 104 NORTHVILLE, MI 48167
PHONE: 248.508.3331

SHEET 2





RIGHT OF WAY TAKING PLAN
 PROPOSED R.O.W. APPROXIMATE AREA TO BE DONATED TO CITY OF NOVI = 100 A.C.±



OPEN SPACE PLAN

OPEN SPACE CALCULATIONS

OPEN SPACE REQUIREMENTS:
 TOTAL PARCEL X 15% = 7.74 AC X .15 = **1.16 AC**

OPEN SPACE PROVIDED:
 EAST PARK AREA (AS SHOWN) 6,687 SF
 SOUTH PARK AREA (AS SHOWN) 9,191 SF
 POOL AREA (AS SHOWN) 8,956 SF

ADDITIONAL OPEN SPACE:
 BUILDING 1
 BALCONIES 7,690 SF
 CLUB HOUSE (AS SHOWN) 3,502 SF
 COURTYARDS (AS SHOWN) 6,752 SF
 BUILDING 2
 BALCONIES 7,297 SF
 CLUB HOUSE (AS SHOWN) 2,541 SF
 COURTYARDS (AS SHOWN) 6,752 SF
59,368 SF = 1.36 AC

THE BOND
 SECTION 22, TOWN 1 NORTH, RANGE 8 EAST
 CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

REVISIONS		UTILITY WARNING
NO.	DATE	CALL BEFORE YOU DIG
		UNDERGROUND UTILITY LOCATIONS AS SHOWN ON THE PLAN, WERE OBTAINED FROM UTILITY OWNER AND NOT FIELD LOCATED.

811 Know what's below. Call before you dig.

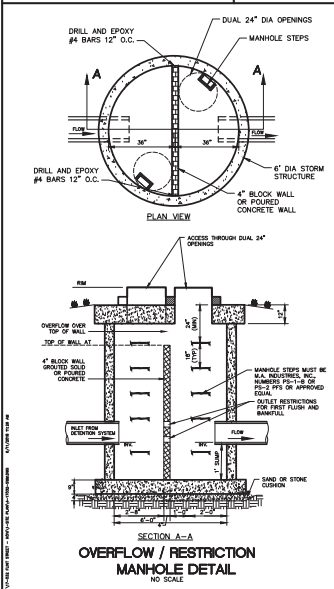
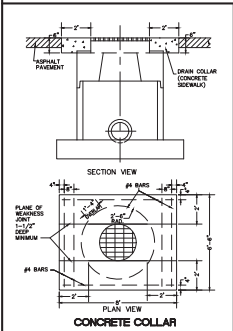
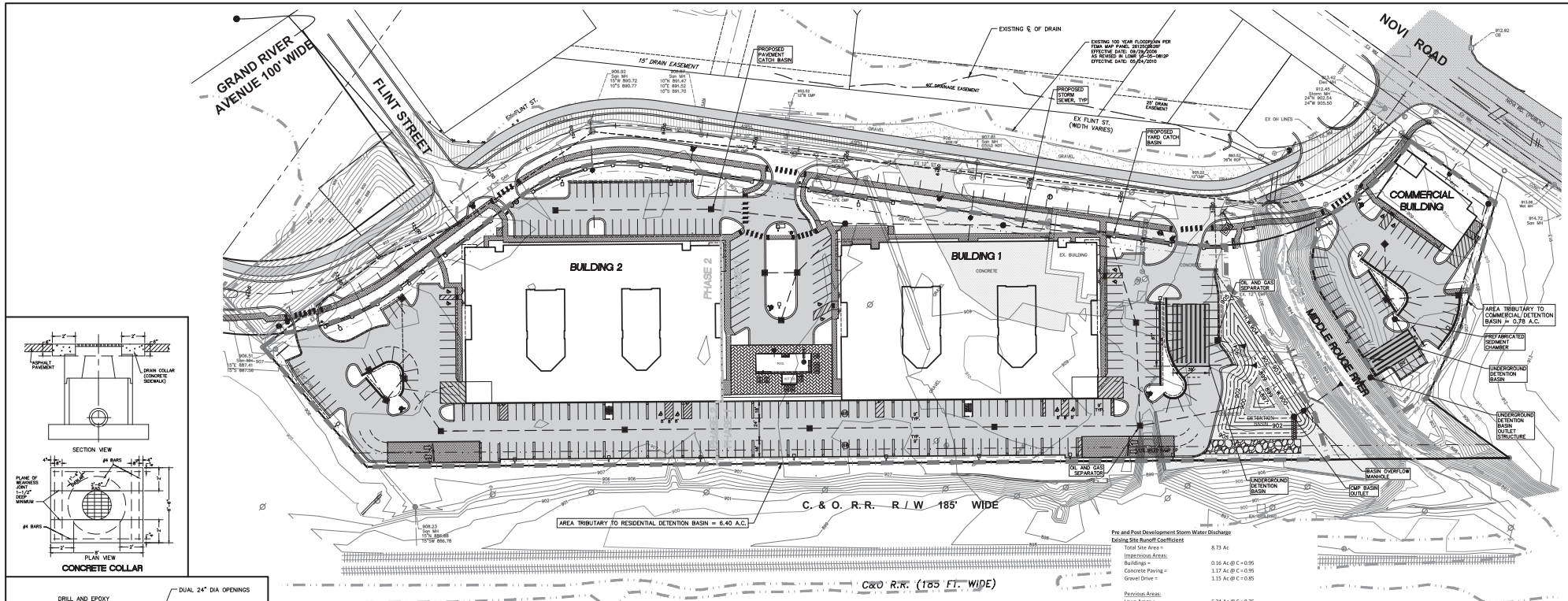
THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF AND/OR RELOCATION OF ALL UTILITIES THAT MAY INTERFERE WITH CONSTRUCTION.

DATE: 06-10-18 DESIGNED BY: A.A. FOR NUMBER: 17-008
 CHECKED BY: R.E. DRAWING FILE: 17-17008-BOV.DWG

R.O.W. TAKING PLAN AND OPEN SPACE PLAN

SEIBER, KEAST ENGINEERING, L.L.C.
 CONSULTING ENGINEERS
 100 MCMURTRY # SUITE 10 NORTHVILLE, MI # 48167
 PHONE: 248.308.3331

SHEET **3**



Station 6 at Novi Town Center
 Job No. 03-02
 Novi, Oakland County, MI
 Preliminary Site Plan - Storm Water Management Plan

Residential Site Runoff Coefficient		Commercial Site Runoff Coefficient	
Total Residential Site Area =	6.40 Ac	Total Commercial Site Area =	0.78 Ac
Impervious Areas:		Impervious Areas:	
Sidewalks / Paving =	3.93 Ac	Sidewalks / Paving =	0.45 Ac
Buildings =	2.50 Ac	Buildings =	0.33 Ac
Pavement Areas:	4.82 Ac @ C = 0.35	Pavement Areas:	0.58 Ac @ C = 0.35
Lawn Areas =	1.52 Ac @ C = 0.15	Lawn Areas =	0.20 Ac @ C = 0.35
Basin Water Area =	0.06 Ac @ C = 1.00		
Composite Runoff Coefficient C =	0.81	Composite Runoff Coefficient C =	0.80

Detention Basin Sizing (Bank Full Flood)		Detention Basin Sizing (Bank Full Flood)	
A =	6.40 Ac	A =	0.78 Ac
C =	0.81	C =	0.80

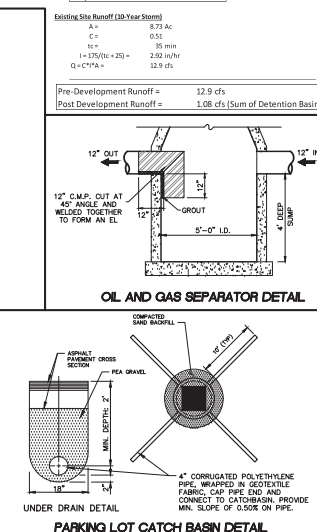
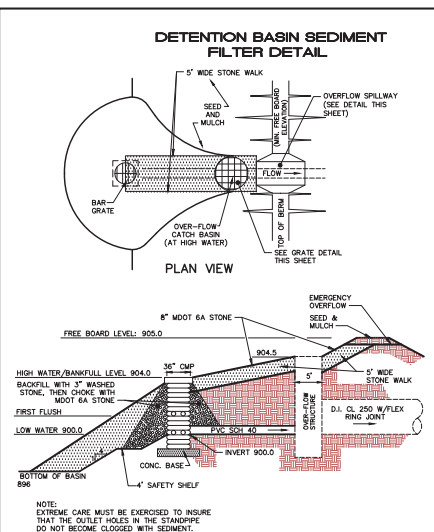
FIRST FLUSH VOLUME REQUIRED		FIRST FLUSH VOLUME REQUIRED	
V _{FF} = 1815 A * C * C =	9886 cf	V _{FF} = 1815 A * C * C =	1130 cf
First Flush Flood will be retained in the basins for a min. of 24 hours		First Flush Flood will be retained in the basins for a min. of 24 hours	

BANK FULL VOLUME REQUIRED		BANK FULL VOLUME REQUIRED	
V _{BF} = 5100 A * C * C =	26955 cf	V _{BF} = 5100 A * C * C =	3213 cf
Bank Full Flood will be retained in the basins for a min. 24 hours and no more than 40 hours.		Bank Full Flood will be retained in the basins for a min. 24 hours and no more than 40 hours.	

Detention Basin		Detention Basin	
Above Ground Detention Pond Volume		The Bank Full Flood volume will be detained in an underground Detention System	
Elevation - Area - Volume		Storage Vol =	3213 cf
900 - 2640		Storage Pipe Dia =	48 in
902 - 2579	2022	Vol / ft =	12.9 cf / ft
903 - 6363	5624	Pipe Length Required =	256 L.F. CMP Detention Pipe Req.
904 - 2960	1642		
	29336		

Maximum Allowable Basin Outlet Flow		Maximum Allowable Basin Outlet Flow	
Q _{all} =	0.12 cfs (0.15 ch/A)	Q _{all} =	0.36 cfs (0.15 dv/A)

The remaining Bank Full Flood volume will be detained in an underground Detention System
 Remaining Vol = 6649 cf
 Storage Pipe Dia = 48 in
 Vol / ft = 12.9 cf / ft
 Pipe Length Required = 528 L.F. CMP Detention Pipe Req.



Pre and Post Development Storm Water Discharge

Existing Site Runoff Coefficient	
Total Site Area =	6.73 Ac
Impervious Areas:	
Buildings =	0.58 Ac @ C = 0.35
Concrete Paving =	1.17 Ac @ C = 0.35
Gravel Drive =	1.15 Ac @ C = 0.85
Pavement Areas:	
Lawn Areas =	0.24 Ac @ C = 0.35
Composite Runoff Coefficient C =	0.51

Existing Site Runoff (30-Year Storm)	
A =	6.73 Ac
C =	0.51
t _c =	35 min
1.475(t _c + 25) =	2.82 min
Q = C * A * I =	12.9 cfs

Pre-Development Runoff =		Post Development Runoff =	
Q =	12.9 cfs	Q =	1.08 cfs (Sum of Detention Basin Outlets)

NOTE: ALL PARKING LOT CATCH BASINS HAVE UNDERGRASIS (SEE DETAIL THIS SHEET)

SCALE
 (IN FEET)
 Scale: 1 Inch = 50 Ft.

THE BOND
 SECTION 22, TOWN 1 NORTH, RANGE 8 EAST
 CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

NO.	ITEM	DATE	UTILITY WARNING
			UNDERGROUND UTILITY LOCATIONS AS SHOWN ON THE PLAN WERE OBTAINED FROM UTILITY OWNER AND NOT FIELD LOCATED.

811 Know what's below. Call before you dig.

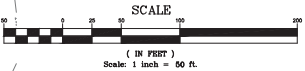
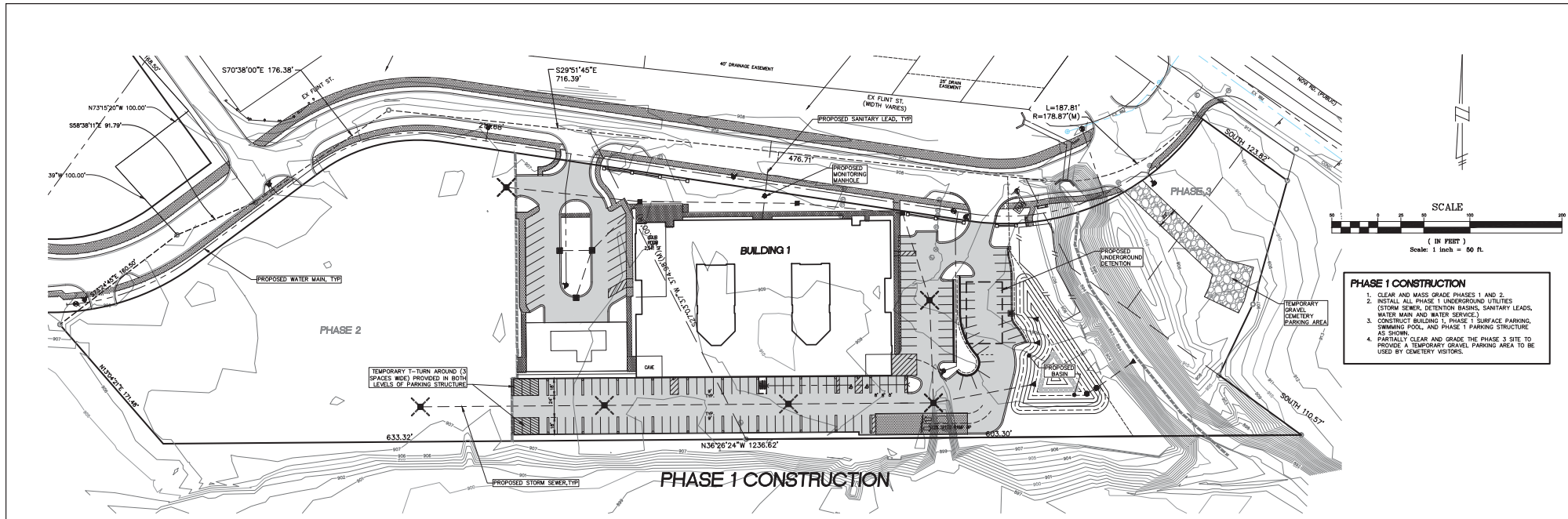
THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF AND/OR RELOCATION OF ALL UTILITIES THAT MAY INTERFERE WITH CONSTRUCTION.

DATE: 06-10-18 DESIGNED BY: A.A. JOHNSON CHECKED BY: E.E. DRAWING FILE: C-17838-NPM-DWG

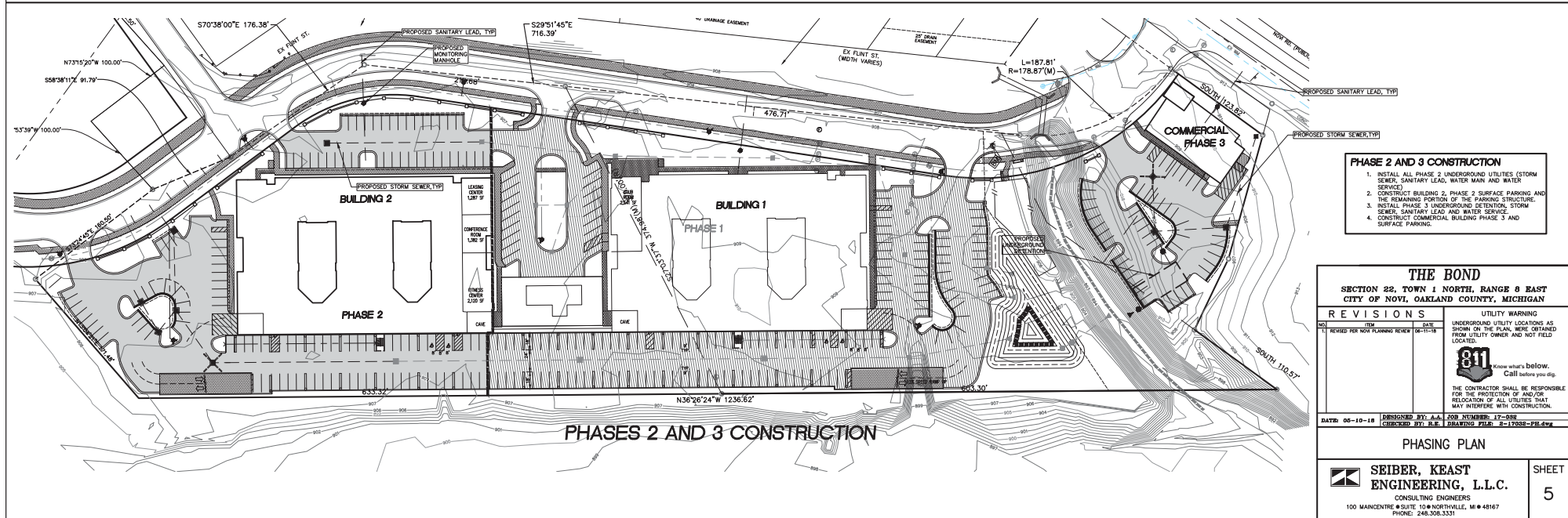
STORM WATER MANAGEMENT PLAN

SEIBER, KEAST ENGINEERING, L.L.C.
 CONSULTING ENGINEERS
 100 MANCOURT # SUITE 108 NORTHVILLE, MI # 48167
 PHONE: 248.208.3331

SHEET **4**



- PHASE 1 CONSTRUCTION**
1. CLEAR AND MASS GRADE PHASES 1 AND 2.
 2. INSTALL ALL PHASE 1 UNDERGROUND UTILITIES (STORM SEWER, DETENTION BASIN, SANITARY LEAD, WATER MAIN AND WATER SERVICE).
 3. CONSTRUCT BUILDING 1, PHASE 1 SURFACE PARKING, SWIMMING POOL, AND PHASE 1 PARKING STRUCTURE AS SHOWN.
 4. PARTIALLY CLEAR AND GRADE THE PHASE 3 SITE TO PROVIDE A TEMPORARY GRAVEL PARKING AREA TO BE USED BY CEMETERY VISITORS.



- PHASE 2 AND 3 CONSTRUCTION**
1. INSTALL ALL PHASE 2 UNDERGROUND UTILITIES (STORM SEWER, SANITARY LEAD, WATER MAIN AND WATER SERVICE).
 2. CONSTRUCT BUILDING 2, PHASE 2 SURFACE PARKING AND THE REMAINING PORTION OF THE PARKING STRUCTURE.
 3. INSTALL PHASE 3 UNDERGROUND DETENTION, STORM SEWER, SANITARY LEAD AND WATER SERVICE.
 4. CONSTRUCT COMMERCIAL BUILDING PHASE 3 AND SURFACE PARKING.

THE BOND
SECTION 92, TOWN 1 NORTH, RANGE 8 EAST
CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

REVISIONS		UTILITY WARNING
NO	DATE	UTILITY WARNING
01	06-11-18	UNDERGROUND UTILITY LOCATIONS AS SHOWN ON THE PLAN, WERE OBTAINED FROM UTILITY OWNER AND NOT FIELD LOCATED.

811 Know what's below. Call before you dig.

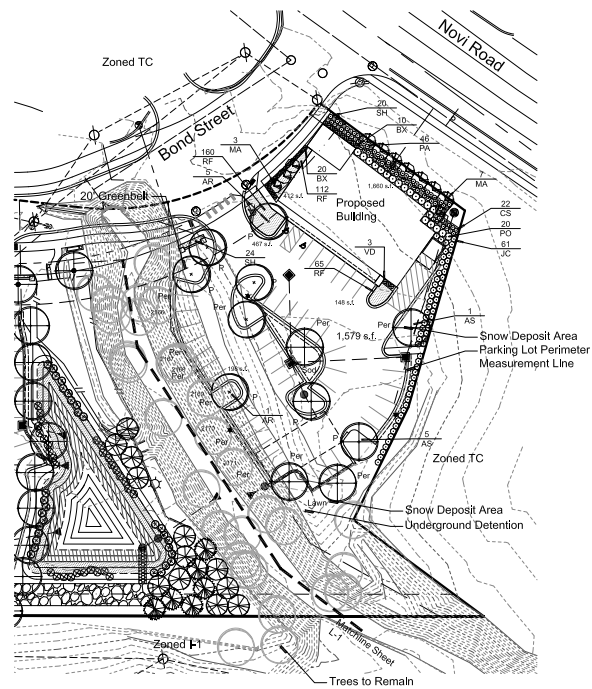
THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY/OR RELOCATION OF ALL UTILITIES THAT MAY INTERFERE WITH CONSTRUCTION.

DATE: 06-10-18 DESIGNED BY: J.A. FOR NUMBER: 17-096
CHECKED BY: R.L. LEAVELLE, P.E. 2-1008-218.dwg

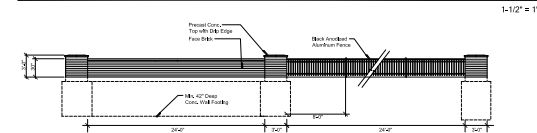
PHASING PLAN

SEIBER, KEAST ENGINEERING, L.L.C.
CONSULTING ENGINEERS
100 MANCENTRE # SUITE 10 NORTHVILLE, MI 48167
PHONE: 248.308.3331

SHEET
5



Wall Detail



Seal:



Title:
Landscape Plan

Project:

**The Bond
 Novi, Michigan**

Prepared for:

Tri-Cap Holdings, LLC
 30600 Northwestern Highway, Suite 430
 Farmington Hills, Michigan 48334

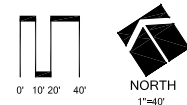
Revision: Issued:
 Submission: May 10, 2018
 Revised: June 11, 2018

Job Number:

18-032

Drawn By: Checked By:

jca jca



Sheet No.

L-2



Landscape Summary - Commercial

Existing Zoning	TC-1	Foundation Landscaping	
Greenbelt		Building Perimeter	329 l.f.
Street Frontage Adjacent to Pkg.	20'	Landscape Required	2,632 s.f. (329' x 8')
Trees Required	1 Tree (20' / 25')	Landscape Provided	2,653 s.f.
Trees Provided	1 Tree (1 Existing)	Requested Waiver:	
Ornamental Trees Required	1 Tree (20' / 15')	1. Sec 5.5.3.D.i.b. Requiring Planting Beds Along 75% of the	
Ornamental Tree Provided	0 Trees	Building Footprint. A Waiver of 25% Is Requested Based on the Current	
Street Frontage not Adjacent to Pkg.	271'	Building Footprint. The Footprint is Conceptual and Additional	
Less Drive Openings	50'	Planting Opportunities Exist as the Building Architecture is	
Net Frontage	221'	Refined. Planters can also be Added once the building Elevation	
Trees Required	7 Trees (221' / 30')	is Known.	
Trees Provided	0 Trees	Notes:	
Ornamental Trees Required	11 Trees (221' / 20')	A. Transformer to be Screened per Detail on Sheet L-3.	
Ornamental Trees Provided	11 Trees	B. Plantings Shall be Located no Closer than 4' to Property Lines.	
Parking Lot Landscaping		C. Plantings Shall be no Closer than 10' to Hydrants and Utility Structures.	
Vehicular Use Area	17,757 s.f.		
(Exclusive of Deck)			
VUA up to 50,000 s.f.	1,349 s.f. (17,757 s.f. x 7.5%)		
Landscape Area Required	1,331 s.f.		
Landscape Area Provided	1,579 s.f.		
Trees Required	6.6 Trees (1,331 s.f. / 200)		
Trees Provided	7 Trees		
Parking Lot Perimeter Length	381 l.f.		
Trees Required	10.5 Trees (381 / 35)		
Trees Provided	11 Trees (7 Existing)		

Plant List

Sym.	qty	botanical name	common name	caliber	spacing	root	height	price	total
AR	6	Acer rubrum 'October Glory'	October Glory Red Maple	3.0"	as shown	B&B		\$ 400.00	\$ 2,400.00
AS	6	Acer saccharum 'Green Mountain'	Green Mountain Sugar Maple	3.0"	as shown	B&B		\$ 400.00	\$ 2,400.00
BX	30	Buxus x. Green Velvet'	Green Velvet Boxwood		as shown	24"		\$ 50.00	\$ 1,500.00
CS	22	Chrysanthemum x superbum 'Alaska'	Alaska Shasta Daisy		as shown	#2 cont.		\$ 15.00	\$ 330.00
JC	61	Juniperus sp. 'Keteleer'	Keteleer Juniper		as shown	B&B	6'	\$ 50.00	\$ 3,050.00
MA	10	Malus 'Adirondack'	Adirondack Crab	2.5"	as shown	B&B		\$ 250.00	\$ 2,500.00
PA	46	Pennisetum alopecuroides 'Miami'	Dwarf Fountain Grass		as shown	#2 cont.		\$ 15.00	\$ 690.00
PO	20	Physocarpus opulifolius 'Coppertina'	Coppertina Ninebark		as shown	36"		\$ 50.00	\$ 1,000.00
RF	337	Rudbeckia ligula speciosa 'Goldsturm'	Black Eyed Susan		as shown	#2 cont.		\$ 15.00	\$ 5,055.00
SIH	44	Sporobolus heterostachys	Prairie Dropseed		as shown	#2 cont.		\$ 15.00	\$ 660.00
VD	3	Viburnum dentatum	Arrowwood Viburnum		as shown	36"		\$ 50.00	\$ 150.00
	461	Kentucky Blue Grass, (S.Y.)						\$ 6.00	\$ 2,766.00
Mulch		42 s.y. 4" Deep Shredded Hardwood Bark Mulch					335's y.		\$ 1,470.00
Irrigation		175 Sod					56' s.y.		\$ 1,050.00
									\$ 7,000.00
									Total
									\$ 32,021.00

54.5% Native Species.

Seal:



Title:
Landscape Details

Project:

**The Bond
Novi, Michigan**

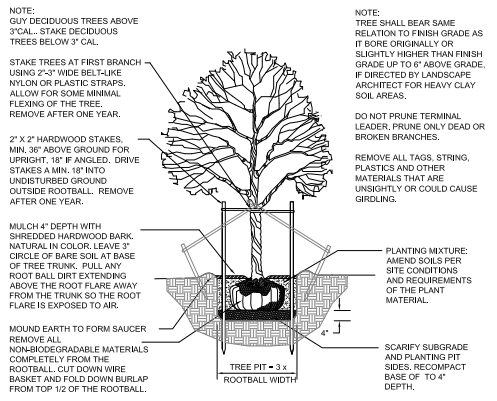
Prepared for:
Tri-Cap Holdings, LLC
30500 Northwestern Highway, Suite 430
Farmington Hills, Michigan 48334

Revision:	Issued:
Submission	May 10, 2018
Revised	June 11, 2018

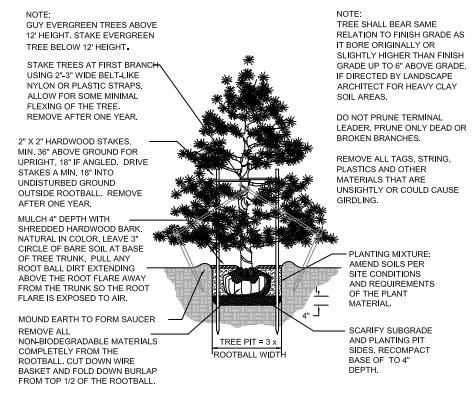
Job Number:
18-032

Drawn By: Jca
Checked By: Jca

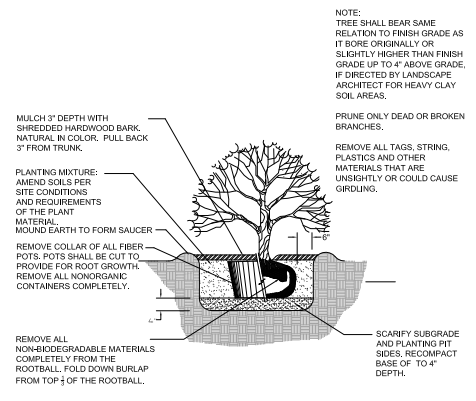
Sheet No.



DECIDUOUS TREE PLANTING DETAIL



EVERGREEN TREE PLANTING DETAIL



SHRUB PLANTING DETAIL

NOT TO SCALE

LANDSCAPE NOTES

- All plants shall be north Midwest American region grown, No. 1 grade plant materials, and shall be true to name, free from physical damage and wind burn.
- Plants shall be full, well-branched, and in healthy vigorously growing condition.
- Plants shall be watered before and after planting is complete.
- All trees must be staked, fertilized and mulched and shall be guaranteed to exhibit a normal growth cycle for at least two (2) full years following City approval.
- All material shall conform to the guidelines established in the most recent edition of the American Standard for Nursery Stock.
- Provide clean backfill soil, using material stockpiled on site. Soil shall be screened and free of any debris, foreign material, and stones.
- "Agiform" tabs or similar slow-release fertilizer shall be added to the planting pits before being backfilled.
- Amended planting mix shall consist of 1/3 screened topsoil, 1/3 sand and 1/3 compost, mixed well and spread to the depth as indicated in planting details.
- All plantings shall be mulched per planting details located on this sheet.
- The Landscape Contractor shall be responsible for all work shown on the landscape drawings and specifications.
- No substitutions or changes of location, or plant types shall be made without the approval of the Landscape Architect.
- The City of Novi's Landscape Architect shall be notified of any discrepancies between the plans and field conditions prior to installation.
- The Landscape Contractor shall be responsible for maintaining all plant material in a vertical condition throughout the guaranteed period.
- The City of Novi's Landscape Architect shall have the right, at any stage of the installation, to reject any work or material that does not meet the requirements of the plans and specifications. If requested by owner.
- Contractor shall be responsible for checking plant quantities to ensure quantities on drawings and plant list are the same. In the event of a discrepancy, the quantities on the plans shall prevail.
- The Landscape Contractor shall seed and mulch or soil (as indicated on plans) all areas disturbed during construction, throughout the contract term.
- A pre-emergent weed control agent, "Premer" or equal, shall be applied uniformly on top of all mulching in all planting beds.
- All landscape areas shall be provided with an underground automatic sprinkler system.
- Soil shall be two year old "Baron/Cherish/diplm" Kentucky Blue Grass grown in a soil nursery on loam soil.

CITY OF NOVI NOTES

- All landscape Islands shall be backfilled with a sand mixture to facilitate drainage.
- All proposed landscape Islands shall be curbed.
- All landscape areas shall be irrigated.
- Overhead utility lines and poles to be relocated as directed by utility company of record.
- Evergreen and canopy trees shall be planted a minimum of 10' from the curb and manhole, 15' from overhead wires.
- All plant material shall be guaranteed for two (2) years after City Approval and shall be installed and maintained according to City of Novi standards. Replace Falling Material Within Three Months or Next Growing Period, whichever is Sooner.
- All proposed street trees shall be planted a minimum of 4' from both the back of curb and proposed walks.
- All tree and shrub planting beds shall be mulched with shredded hardwood bark, spread to minimum depth of 4". All lawn area trees shall have a 4" diameter circle of shredded hardwood mulch 2' away from trunk. All perennial, annual and ground cover beds shall receive 2" of clean colored bark mulch as indicated on the plant list. Mulch is to be free from debris and foreign material, and shall contain no pieces of inconsistent size.
- All Substitutions or Deviations from the Landscape Plan Must be Approved in Writing by the City of Novi Prior to their Installation.

NOTES:
THE APPROXIMATE DATE OF INSTALLATION FOR THE PROPOSED LANDSCAPE WILL BE MARCH 15 - NOVEMBER 15 OF 2019 OR 2020.
THE SITE WILL BE MAINTAINED BY THE DEVELOPER IN ACCORDANCE WITH THE STANDARDS SET FORTH BY THE CITY OF NOVI ZONING ORDINANCE. THIS INCLUDES WEEDING AND WATERING AS REQUIRED BY NORMAL MAINTENANCE PRACTICES. THIS INCLUDES ONE CULTIVATION BETWEEN JUNE-AUGUST.

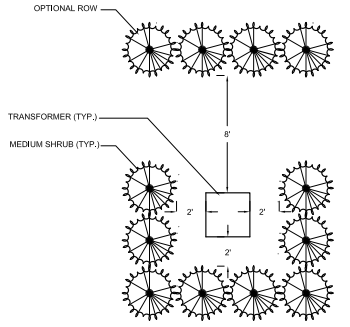
DEVELOPER SHALL BE RESPONSIBLE FOR REPLACING ANY TREES WITHIN UTILITY EASEMENTS THAT ARE DAMAGED THROUGH NORMAL MAINTENANCE OR REPAIRS.

PLANT MATERIALS SHALL BE GUARANTEED FOR 2 YEARS AND SHALL BE MAINTAINED IN ACCORDANCE WITH CITY ORDINANCES. WARRANTY PERIOD BEGINS AT THE TIME OF CITY APPROVAL, WATERING AS NECESSARY SHALL OCCUR DURING THE WARRANTY PERIOD.

Bench and Refuse Detail

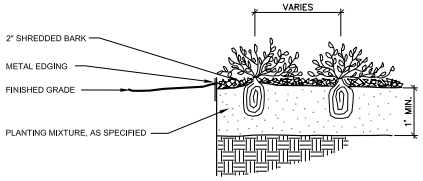


Scarborough Bench and Refuse by Landscape Forms



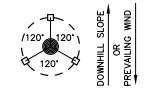
TRANSFORMER SCREENING DETAIL

Not to scale

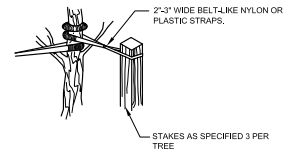


PERENNIAL PLANTING DETAIL

Not to scale



NOTE:
ORIENT STAKING/GUYING TO PREVAILING WINDS, EXCEPT ON SLOPES GREATER THAN 3:1 ORIENT TO SLOPE.
USE SAME STAKING/GUYING ORIENTATION FOR ALL PLANTS WITHIN EACH GROUPING OR AREA

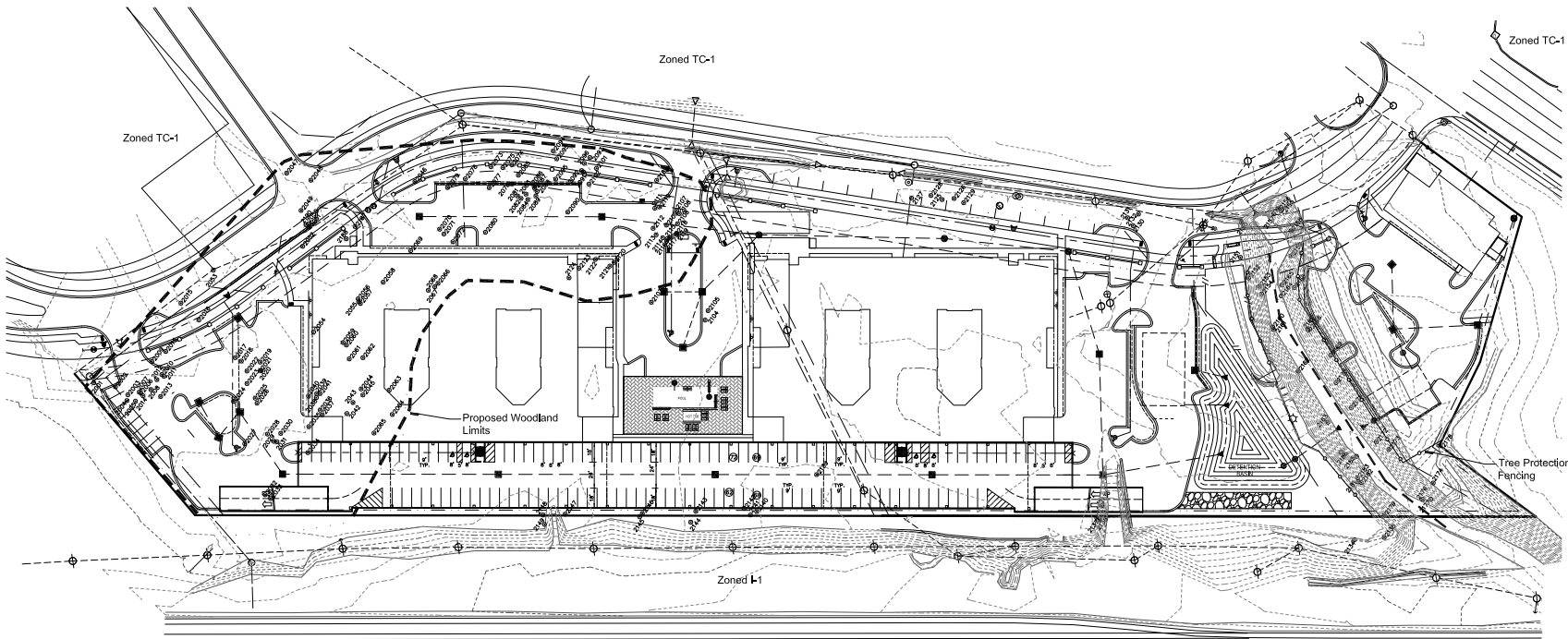


GUYING DETAIL

STAKING DETAIL

TREE STAKING DETAIL

Not to scale



Seal:



Title:
Woodland Plan

Project:

The Bond
 Novi, Michigan

Prepared for:

Ti-Cap Holdings, LLC
 30500 Northwestern Highway, Suite 430
 Farmington Hills, Michigan 48334

Revision:

Issued:

Submission May 10, 2018
 Revised June 11, 2018

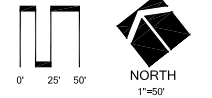
Job Number:

18-032

Drawn By: jca Checked By: jca

jca

jca



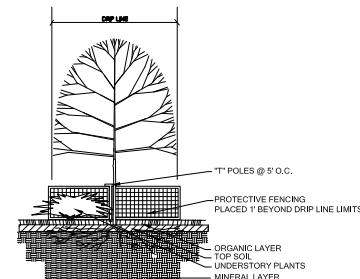
Woodland Summary

TAG NO.	DIA METER	COMMON NAME	BOTANICAL NAME	CONDI TION	RE MARKS	RE QUIRE D REPLACEMENT CREDITS
2001	15	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2002	11	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2003	11	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2004	11	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2005	23.5	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2006	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2007	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2008	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2009	11	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2010	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2011	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2012	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2013	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2014	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2015	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2016	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2017	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2018	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2019	11	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2020	11	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2021	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2022	13	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2023	13	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2024	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2025	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2026	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2027	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2028	14	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2029	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2030	17	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2031	15	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2032	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2033	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2034	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2035	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2036	15	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2037	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2038	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2039	15	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2040	15	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2041	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2042	16	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2043	12	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2044	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2045	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2046	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2047	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2048	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2049	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2050	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2051	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2052	8	Eastern Cottonwood	Populus deltoides	Good	Remove	1
2053	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2054	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2055	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2056	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2057	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2058	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2059	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2060	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2061	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
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2063	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
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2065	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
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2071	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2072	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2073	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2074	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2
2075	10	Eastern Cottonwood	Populus deltoides	Good	Remove	2

Woodland Summary

Total Trees	185 Trees
Less:	
Dead, Off-site, ROW Trees	37 Trees
Net Trees	148 Regulated Trees
Regulated Trees Removed	103 Trees
Replacement Required	
Trees 8" - 11"	43 trees x 1= 43 Trees
Trees 11" - 20"	36 trees x 2= 76 Trees
Trees 20" - 30"	3 trees x 3= 9 Trees
Trees 30"+	0 trees x 4= 0 Trees
Multi-Stemmed Trees (19 Trees)	77 Trees
Sub-total Replacement Required	205 Trees
Less Credits	66 Trees
Required Replacements	139 Trees

Key
 X Removed Tree



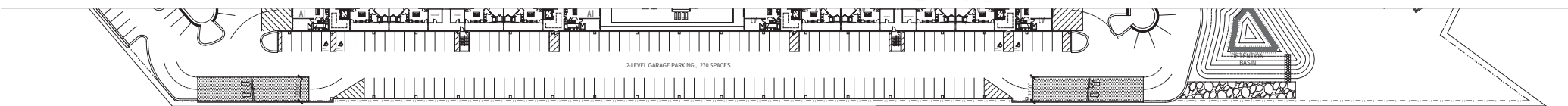
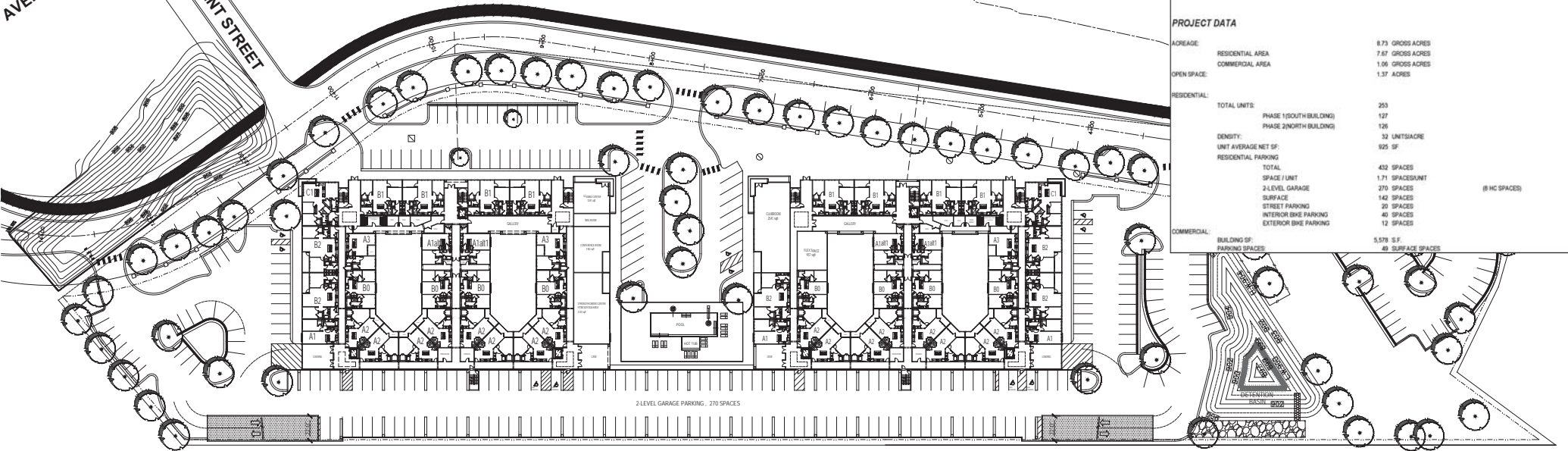
- Other blocks of wood (logs) shown fencing shall be located at or beyond the 6" x 6" x 6" unless more substantial fencing is required.
- Protective fencing shall be placed 1' beyond drip line limits.
- Fencing shall not be installed closer to the tree than the drip line of those trees to be saved. Special consideration shall be given to the City.
- Fencing shall be erected prior to construction. The City shall be notified once the fencing is installed for review.
- Under no circumstances shall the removal of a tree be allowed without proper approval from the City. No removal of a tree shall be allowed by any person or contractor without approval from the City.
- No blocking materials or construction equipment shall be placed within the protective areas.
- No ground anchors, drilled holes, or other ground anchors shall be placed within the protective areas.
- Any required single tree to be removed shall be removed from the protected area. Any tree to be removed shall be removed through a protected area. No ground anchors shall be placed within the protective area.
- Regulated trees or Regulated Trees Adjacent to the Property are Also Required to be Protected Whether or not they are shown on the Plan.

TREE PROTECTION DETAIL
 NO SCALE

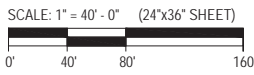


- Remarks Key:
- | | |
|--------|---|
| Save | Tree will be saved |
| Credit | Tree is located outside of a woodland area and will be saved. |
| Remove | Tree is located in a regulated woodland and will be removed. |
| Exempt | Tree is dead or located outside of a woodland area. |

GRAND RIVER AVENUE 100' WIDE
FLINT STREET



STATION 6							DTN MANAGEMENT CO.		2017659
UNIT TABULATION- 4 STORY RESIDENTIAL									
UNIT NAME	UNIT TYPE	NET AREA(SF)	UNIT COUNT	BED COUNT	TOTAL AREA	UNIT PERCENTAGE	% UNIT BREAKDOWN		
E1	1br/1ba	823	24	24	14,472	9%			
A1	1br/1ba	742	27	27	20,034	11%			
A1-a11	1br/1ba	750	4	4	3,168	2%	58%		
A2	1br/1ba	820	64	64	52,480	26%			
A3	1br/1ba	864	27	27	23,328	11%			
B0	2br/2ba	941	32	64	30,208	15%			
B1	2br/2ba	1,122	32	64	36,504	19%	37%		
B2	2br/2ba	1,259	29	58	36,511	11%			
C1	3br/3ba	1,277	14	42	17,878	6%			
TOTALS			253	374	233,993	100%	100%		
UNIT AVERAGE NET SF :						925			
* NET AREA IS COMPUTED TO INCLUDE SQUARE FOOTAGE FROM EXTERIOR FACE OF ALL EXTERIOR FRAME WALLS THAT ENCLOSE A/C SPACE. IT DOES NOT INCLUDE PATIOS, BALCONIES, PATIO/BALCONY STORAGE.									
PROJECT DATA									
ACREAGE:		8.73 GROSS ACRES							
RESIDENTIAL AREA		7.67 GROSS ACRES							
COMMERCIAL AREA		1.06 GROSS ACRES							
OPEN SPACE:		1.37 ACRES							
RESIDENTIAL:									
TOTAL UNITS:		253							
PHASE 1(SOUTH BUILDING)		137							
PHASE 2(NORTH BUILDING)		116							
DENSITY:		32 UNITS/ACRE							
UNIT AVERAGE NET SF:		925 SF							
RESIDENTIAL PARKING:									
TOTAL:		432 SPACES							
SPACE / UNIT		1.71 SPACES/UNIT							
2.0 LEVEL GARAGE		270 SPACES							
SURFACE		142 SPACES							
STREET PARKING		20 SPACES							
INTERIOR BIKE PARKING		40 SPACES							
EXTERIOR BIKE PARKING		12 SPACES							
COMMERCIAL:									
BUILDING SF:		5,578 S.F.							
PARKING SPACES:		48 SURFACE SPACES							



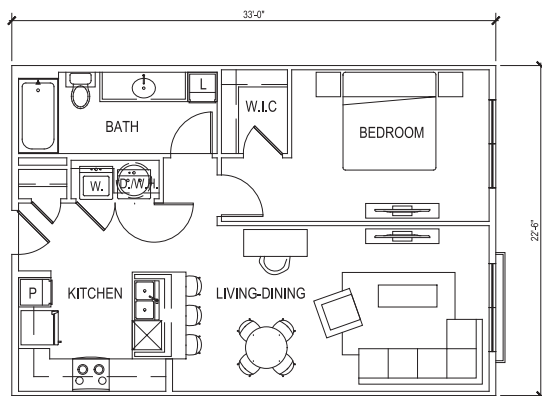
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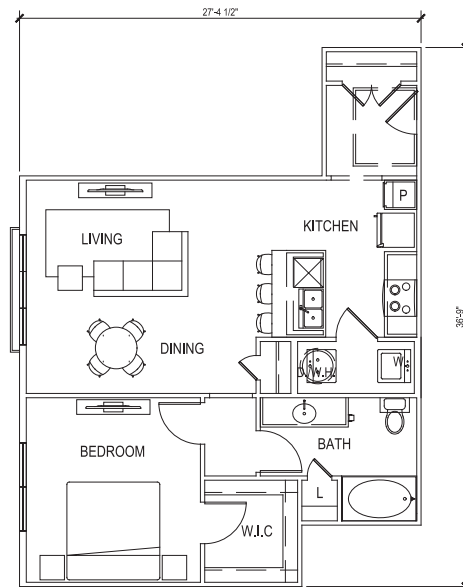
ARCHITECTURAL SITE PLAN
SCHEME 05
June 11, 2018

SP05
THE BOND
Novi, MI
HPAF 17659

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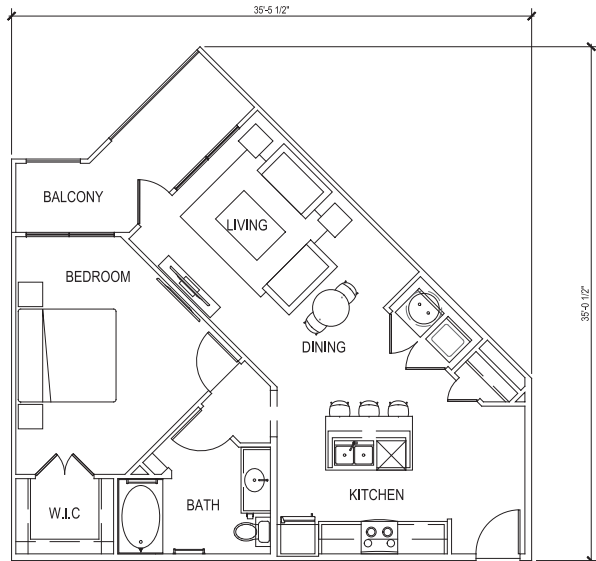
UNIT A1
NET - 742 SQ. FT.



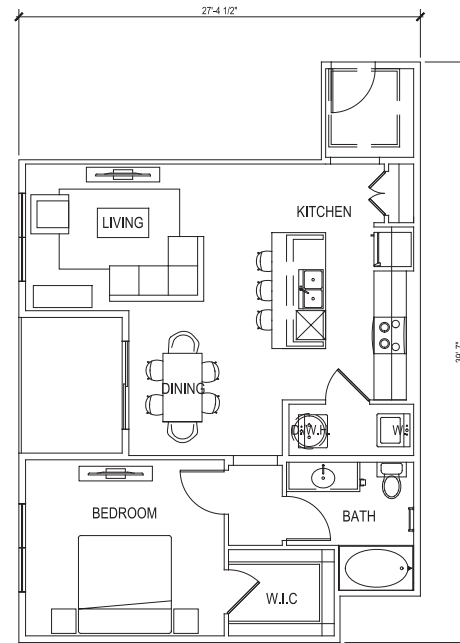
UNIT A1-alt1
NET - 792 SQ. FT.

SCALE: 1/4" = 1'-0" (24"x36" SHEET)





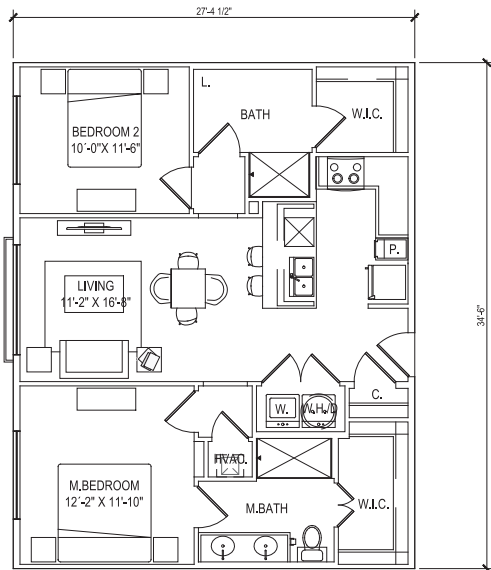
UNIT A2
NET - 820 SQ. FT.



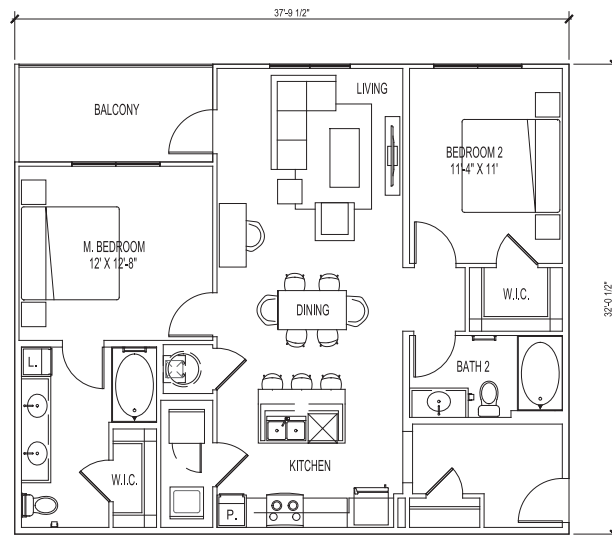
UNIT A3
NET - 864 SQ. FT.

SCALE: 1/4" = 1'-0" (24"x36" SHEET)

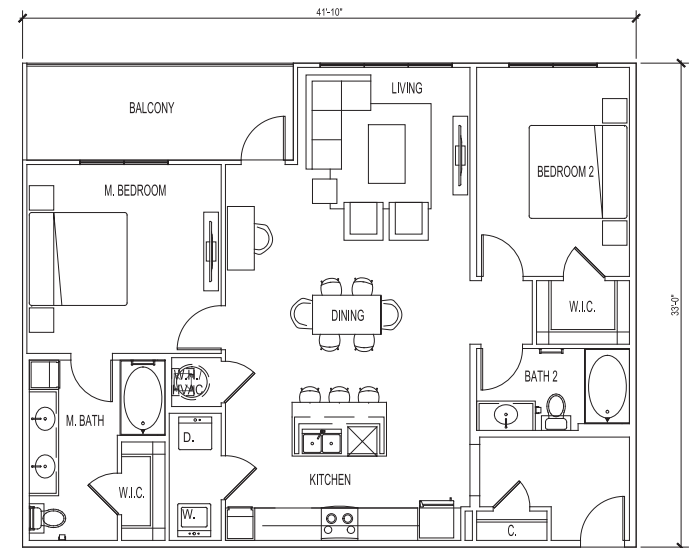




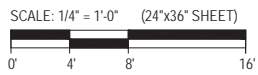
UNIT B0
NET - 944 SQ. FT.

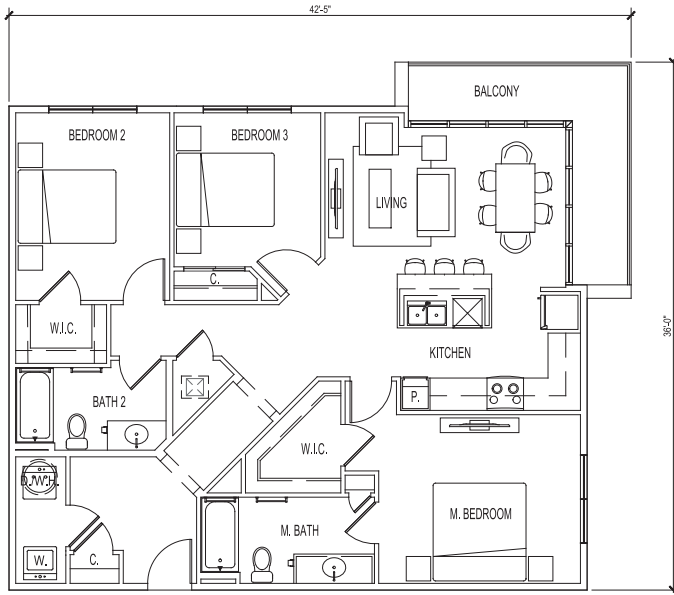


UNIT - B1
NET - 1121 SQ. FT.

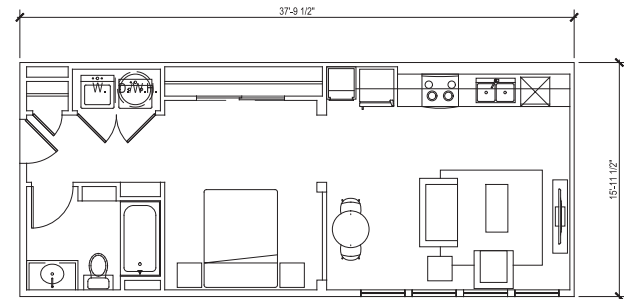


UNIT - B2
NET - 1259 SQ. FT.





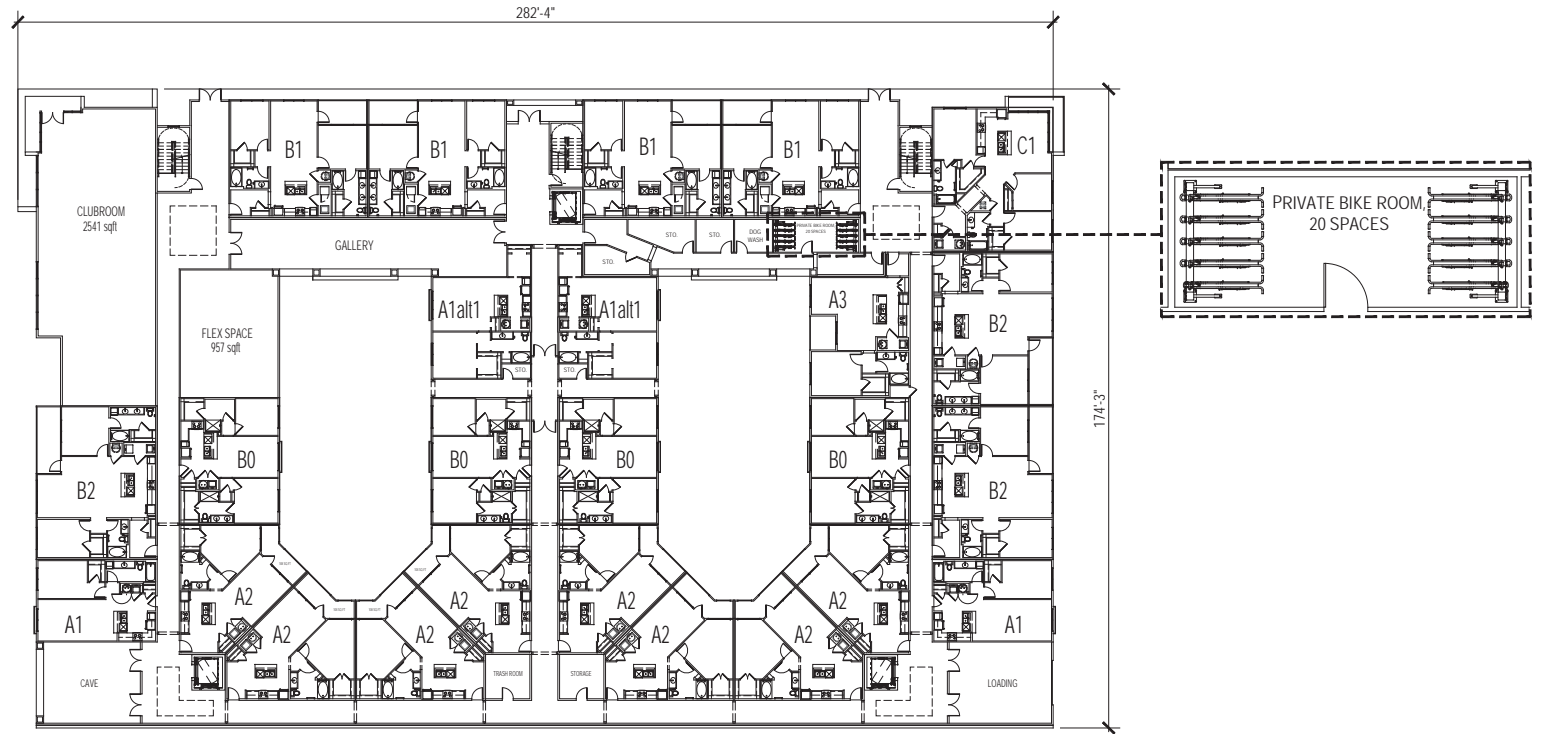
UNIT C1
NET -1277 SQ. FT.



UNIT E1
NET - 603 SQ. FT.

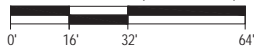
SCALE: 1/4" = 1'-0" (24"x36" SHEET)





BUILDING TYPE-II 1ST FLOOR PLAN

SCALE: 1/16" = 1'-0" (24"x36" SHEET)



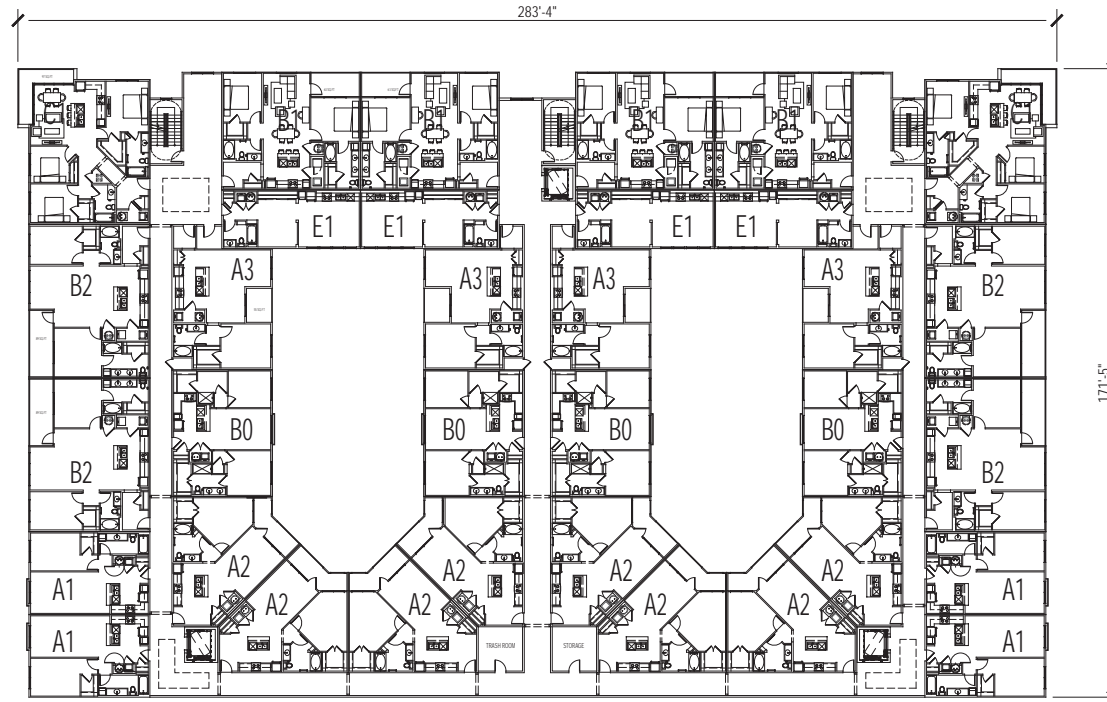
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BLDG TYPE 2 - 1ST FLOOR PLAN
SCHEME 05
June 11, 2018

A410
THE BOND
Novi, MI.
HPAF 17659

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BUILDING TYPE-II 2ND FLOOR PLAN

SCALE: 1/16" = 1'-0" (24"x36" SHEET)



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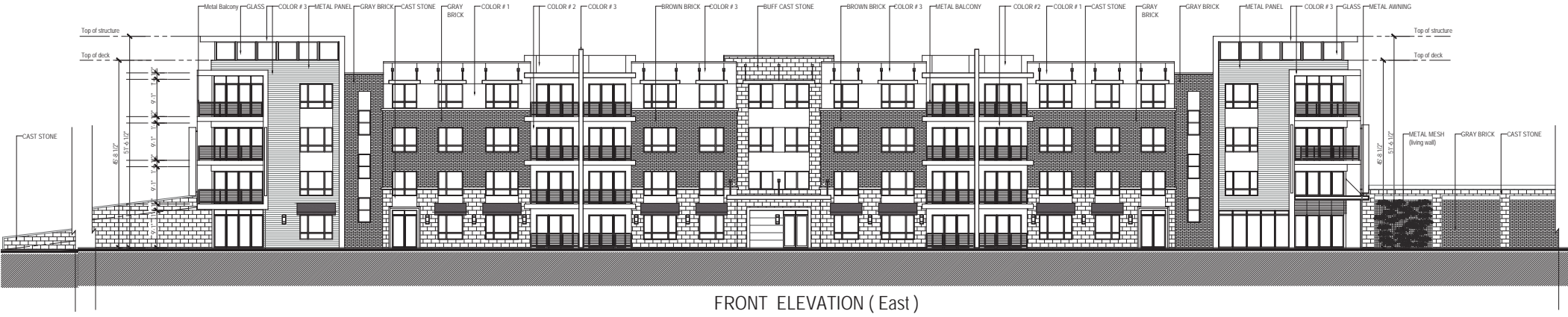


BLDG TYPE 2 - 2ND FLOOR PLAN
 SCHEME 05
 June 11, 2018

A411

THE BOND
 Novi, MI
 HPAF 17659

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FRONT ELEVATION (East)

STUCCO %35
 BRICK %28
 CAST STONE %22
 PANEL %15

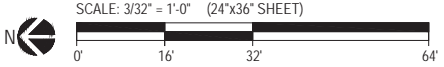


SIDE ELEVATION (North/pool courtyard)

STUCCO %45
 BRICK %28
 CAST STONE %20
 PANEL %7

- COLOR #1 SW 7543
- COLOR #2 SW 7548
- COLOR #3 SW 7656
- GRAY BRICK
- BROWN BRICK
- BUFF CAST STONE
- METAL PANEL FAUX WOOD COLOR

BUILDING TYPE I ELEVATIONS



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ARCHITECTURAL SITE PLAN
 SCHEME 05
 June 12, 2018

A412
THE BOND
 Novi, MI.
 HPA# 17659

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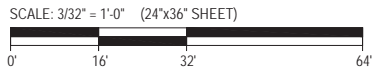


SIDE ELEVATION (South)

STUCCO %41
 BRICK %27
 CAST STONE %24
 PANEL %8



BUILDING TYPE I ELEVATIONS



SCALE: 3/32" = 1'-0" (24"x36" SHEET)



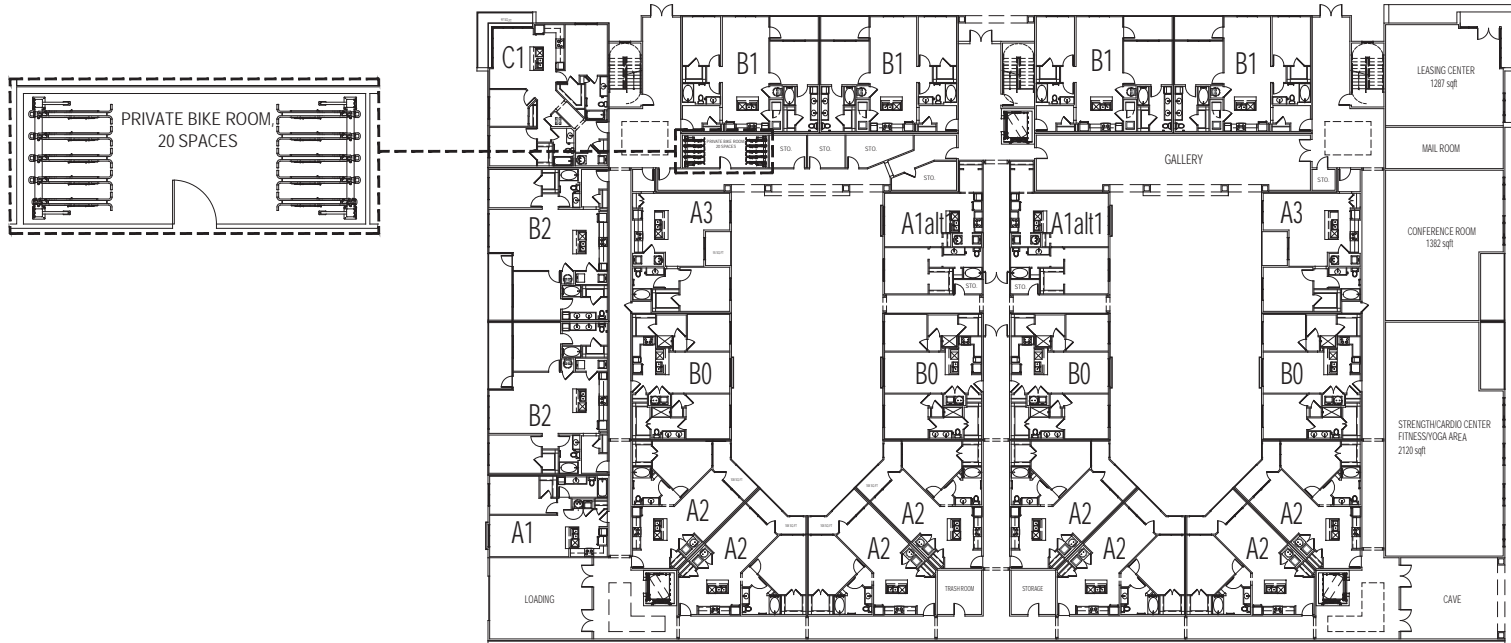
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ARCHITECTURAL SITE PLAN
 SCHEME 05
 June 11, 2018

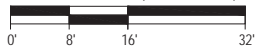
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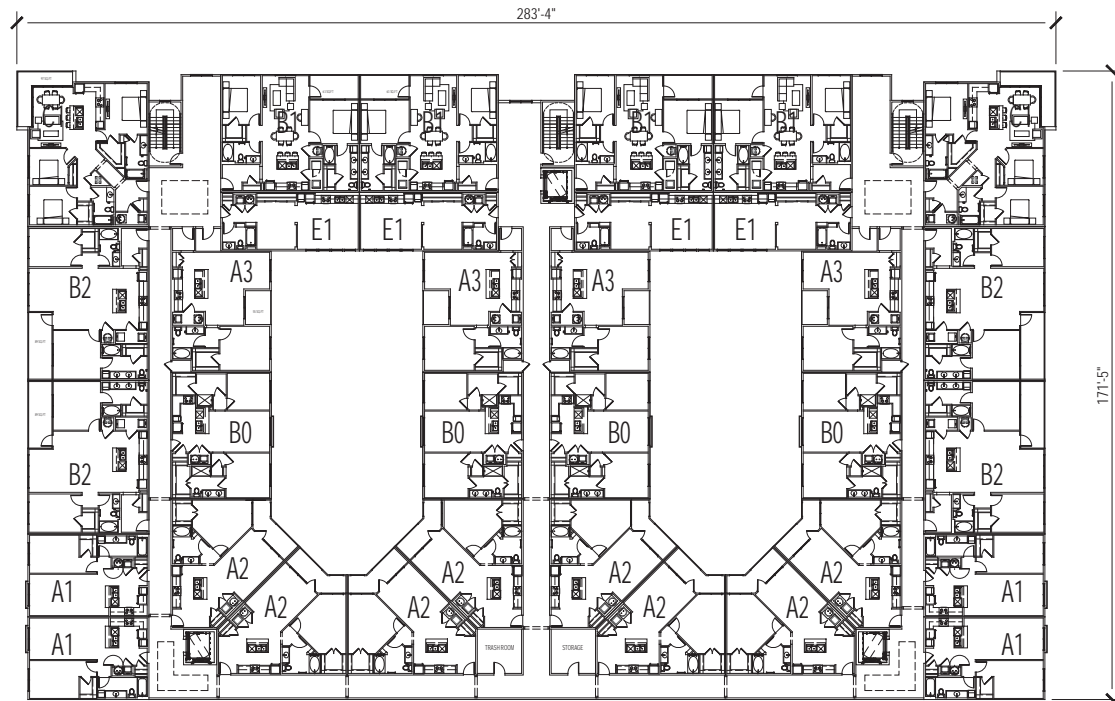
THE BOND
 Novi, MI.
 HPA# 17659



BUILDING TYPE-I 1ST FLOOR PLAN

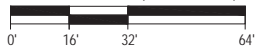
SCALE: 1/8" = 1'-0" (24"x36" SHEET)





BUILDING TYPE-1 2ND FLOOR PLAN

SCALE: 1/16" = 1'-0" (24"x36" SHEET)



HUMPHREYS & PARTNERS ARCHITECTS, L.P.
5339 Alpha Rd., Suite 300, Dallas, TX 75240 | 972.701.9636 | www.humphreys.com



BLDG TYPE 1 - 2ND FLOOR PLAN
SCHEME 05
June 11, 2018

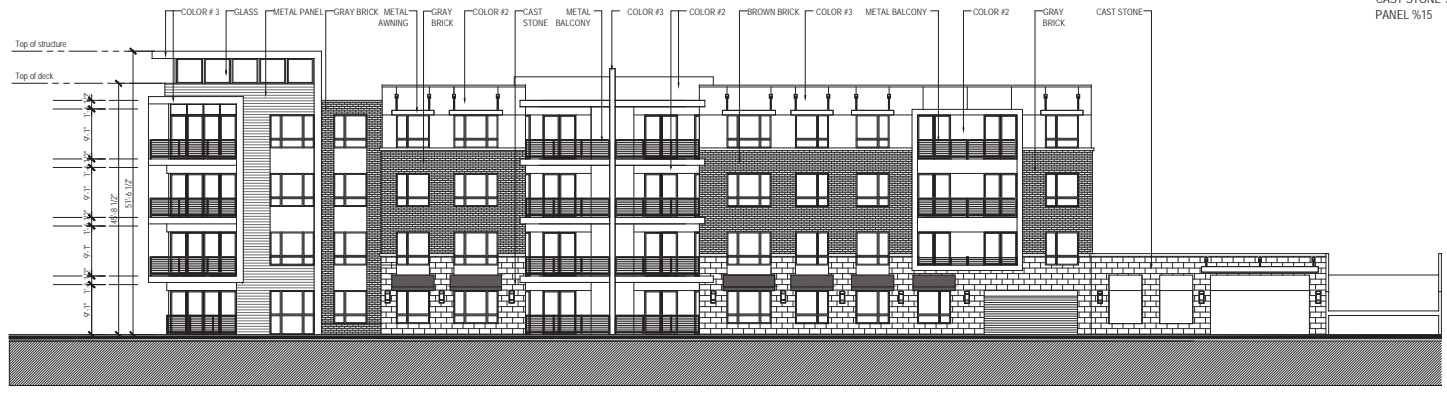
A421

THE BOND
Novi, MI
HPAF 17659

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FRONT ELEVATION (East)

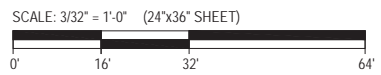


SIDE ELEVATION (North)

STUCCO %35
BRICK %28
CAST STONE %22
PANEL %15

STUCCO %41
BRICK %27
CAST STONE %24
PANEL %8

- COLOR #1
SW 7543
- COLOR #2
SW 7548
- COLOR #3
SW 7656
- GRAY BRICK
- BROWN BRICK
- BUFF CAST STONE
- METAL PANEL
FAUX WOOD COLOR



BUILDING TYPE II ELEVATIONS



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ARCHITECTURAL SITE PLAN
SCHEME 05
June 12, 2018

A422
THE BOND
Novi, MI.
HPA# 17659

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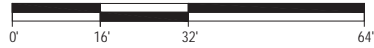


SIDE ELEVATION (South/ pool courtyard)

STUCCO %45
BRICK %28
CAST STONE %20
PANEL %7

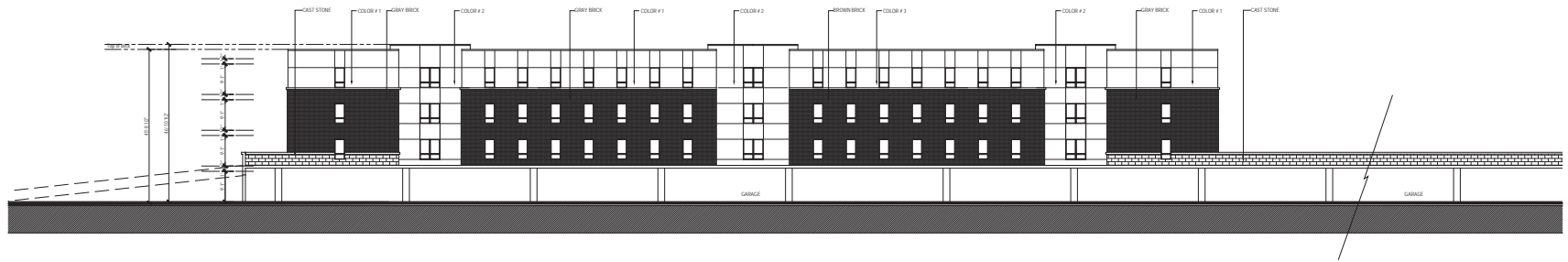


SCALE: 3/32" = 1'-0" (24"x36" SHEET)



BUILDING TYPE II ELEVATIONS





REAR ELEVATION (West)

STUCCO %48
 BRICK %47
 CAST STONE %5
 PANEL %0

- COLOR #1
SW 7543
- COLOR #2
SW 7548
- COLOR #3
SW 7656
- GRAY BRICK
- BROWN BRICK
- BUFF CAST STONE
- METAL PANEL
FAUX WOOD COLOR

BUILDING TYPE I&II ELEVATIONS

SCALE: 1/16" = 1'-0" (24"x36" SHEET)
 0' 16' 32' 64'



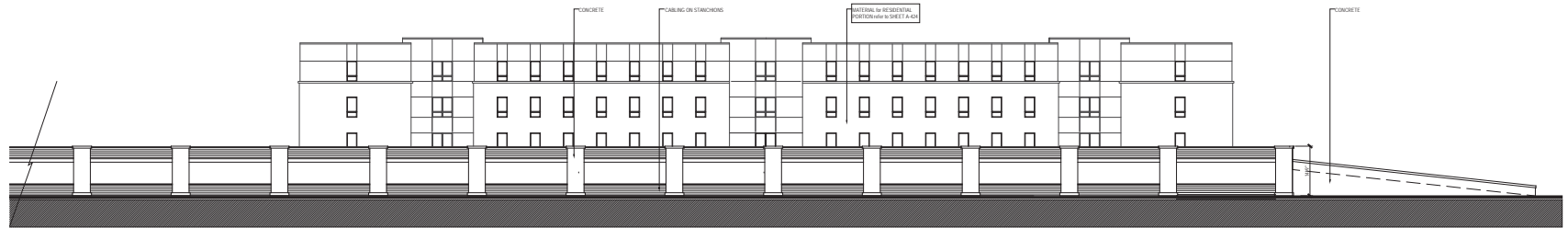
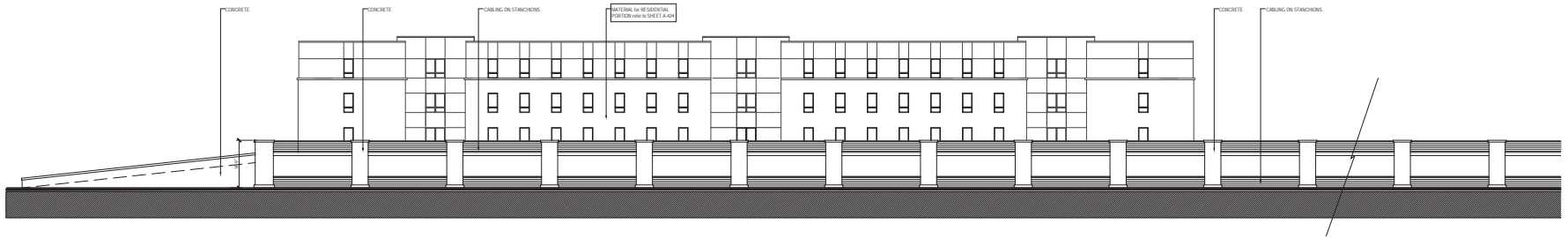
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ARCHITECTURAL SITE PLAN
 SCHEME 05
 June 11, 2018

A424
THE BOND
 Novi, MI
 HPA# 17659

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PARKING ELEVATION (West)

- COLOR #1 SW 7543
- COLOR #2 SW 7046
- COLOR #3 SW 7056
- GRAY BRICK
- BROWN BRICK
- BUFF CAST STONE
- METAL PANEL FAUX WOOD COLOR

BUILDING TYPE I&II ELEVATIONS

SCALE: 1/16" = 1'-0" (24"x36" SHEET)

A425



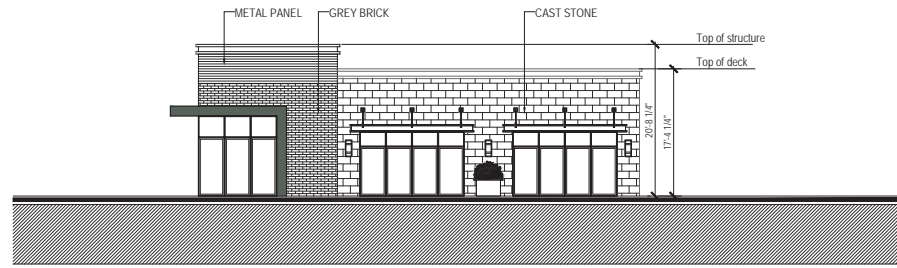
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ARCHITECTURAL SITE PLAN
SCHEME 05
June 11, 2018

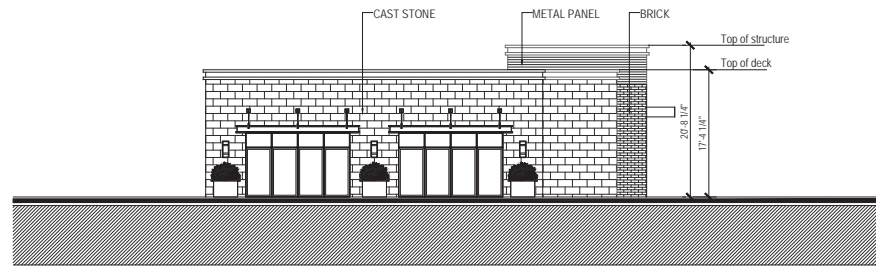
THE BOND
Novi, MI
HPA# 17659

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FRONT ELEVATION

BRICK 23%

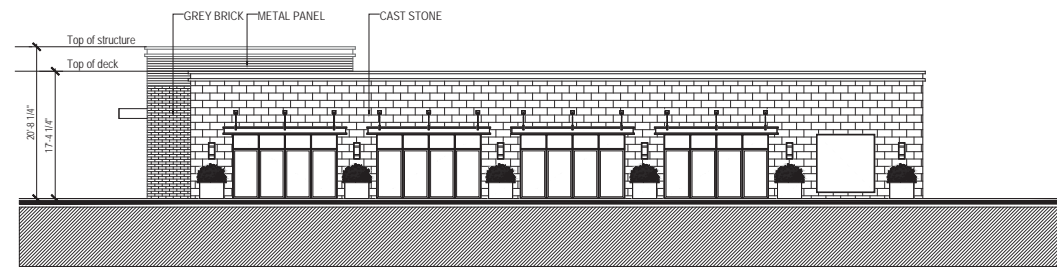


REAR ELEVATION

BRICK 8%

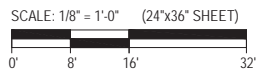


SIDE ELEVATION
BRICK 17%
CAST STONE 74%
METAL PANEL 9%



SIDE ELEVATION

BRICK 8%
CAST STONE 86%
METAL PANEL 6%





OVERALL EAST ELEVATIONS



EAST ELEVATIONS-POOL COURTYARD









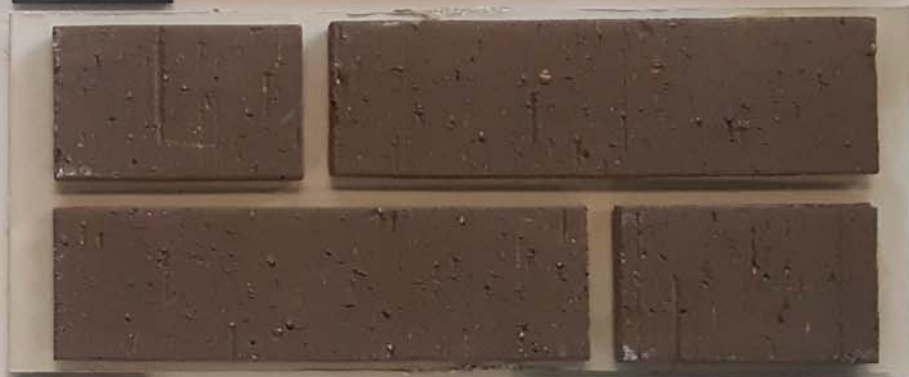
STUCCO



METAL PANEL



BROWN BRICK



GRAY BRICK



BALCONY POUR STOP



BALCONY RAILING



BUFF CAST STONE

**ENGINEERING MEMO
FLINT STREET COORDINATION UPDATE**

MEMORANDUM



TO: BARBARA MCBETH, AICP, CITY PLANNER
FROM: GEORGE D. MELISTAS, ENGINEERING SR. MANAGER
SUBJECT: FLINT STREET COORDINATION UPDATE
DATE: JULY 16, 2018

The realignment and reconstruction of Flint Street is part of the Capital Improvement Program (CIP) for FY 2019-20. Flint Street is the third of four road construction phases and completes the southwest quadrant of the ring road that will connect Grand River Avenue and Novi Road. The work includes acquiring the necessary right-of-way (R.O.W.) and easements from property owners and extending the road to the west along Flint Street. The existing gravel road will be replaced with an asphalt road with concrete curb and gutter. Flint Street will include two eleven-foot-wide lanes and two eight-foot-wide bump-outs with parking lanes along the south side of Flint Street. These bump-outs will accommodate enough parking for 20 vehicles.

The City's Engineering Division intends on working with the developer of "The Bond" to acquire the additional R.O.W. required to construct Flint Street. "The Bond" is a development proposed on the two parcels south of Flint Street and will be comprised of two multi-family units (totaling 253 units) on these parcels.

The Engineering Division's proposed plan has the City constructing a ten-foot-wide asphalt pathway on the north side of Flint Street. The pathway will extend from Novi Road to Grand River Avenue. The developer of "The Bond" will be constructing an eight-foot-wide concrete sidewalk in conjunction with "The Bond" along the south side of the Flint Street.

At this time, Flint Street is planned for construction after July 1, 2019. "The Bond" is anticipating starting construction by May 2019, and is estimated to take approximately 14 months to complete. Timing of the construction between these two projects will be vital to the construction schedule of Flint Street.

There are two options available for the Flint Street construction, with the selection dependent upon the cost of either option. The first option would be to only place the base course and the leveling course of asphalt, allowing all construction equipment associated with "The Bond" to continue to use the roadway as a means of a construction route without destroying the wearing course (top layer) of the pavement. The second option is to finish Flint Street using a less expensive wearing course known as a "sacrificial layer" which can be removed and replaced after construction of "The Bond" is complete.

Please let me know if you have any questions or comments regarding this memorandum.

cc: Peter Auger, City Manager
Victor Cardenas, Assistant City Manager
Jeffrey Herczeg, Director of Public Works
Aaron Staup, Construction Engineer
Matt Wiktorowski, Field Operations Senior Manager

RESOLUTION TO RENAME
A SECTION OF FLINT STREET
TO BOND STREET

CITY OF NOVI

OAKLAND COUNTY, MICHIGAN

RESOLUTION TO CHANGE STREET NAME
FROM FLINT STREET
TO BOND STREET
IN THE CITY OF NOVI

Minutes of a Regular Meeting of the City Council of the City of Novi, Oakland County, Michigan, held on the Twenty-third day of July, 2018, at the City Hall, 45175 Ten Mile Road, Novi, Michigan 48375.

The following resolution was offered by _____ and supported by _____.

WHEREAS, Flint Street currently extends from Novi Road to Grand River Avenue in the southwest quadrant of the Grand River and Novi Road intersection; and

WHEREAS, the City of Novi intends to reconstruct and realign Flint Street from its current intersection with Novi Road, to a point approximately 350 feet west of the existing Flint Street intersection with Grand River Avenue; and

WHEREAS, the existing section of Flint Street south of Grand River, west of the existing gas station will remain in its current location; and

WHEREAS, the applicants for “The Bond” development have made a request to rename the section of Flint Street to Bond Street, from Novi Road to where its projected new connection will be made at Grand River Avenue, as a part of the likely road reconstruction and realignment project, in order for the new road name to be consistent with the proposed name of the development; and

Proposed Renaming of Flint Street to Bond Street

Location Map




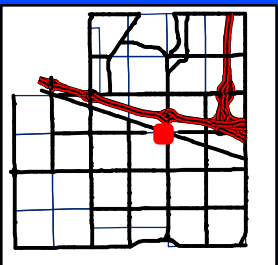
Map Author: Joseph Akers
 Date: July 9, 2018
 Project: Bond Street Renaming
 Version #: 1.2

Amended By:
 Date:
 Department:

MAP INTERPRETATION NOTICE
 Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.

Legend


 Proposed Road Renaming



CITY OF NOVI
 Engineering Division
 Department of Public Services
 26300 Lee BeGole Drive
 Novi, MI 48375
 cityofnovi.org

City of Novi
 Engineering Division
 Department of Public Services
 26300 Lee BeGole Drive
 Novi, MI 48375
 cityofnovi.org

Scale: 0 30 60 120 180 Feet
 1 inch = 167 feet



PROJECT & STREET NAMING COMMITTEE
MINUTES
May 17, 2018 at 10:00 A.M.

45175 W. Ten Mile
248-347-0475

Members: Larry Butler, Alan Patterson, Andy Copeland
Also Attending: Barb McBeth, Keri Blough, Brian Riley, Hannah Smith

1. Approval of Project and Street Names Draft Minutes from March 22, 2018
Motion to approve the Minutes from 3-22-18 made by Alan Patterson, seconded by Andy Copeland. Motion carried 3-0.

2. The applicant, Triangle Development Co, Inc., is requesting the name **Gateway Townhomes of Novi** for the development project north of Grand River Ave and west of Meadowbrook Rd. Parcel number is 50-22-23-226-044 in Section 23. This project was previously named Gateway Village of Novi, but due to condominium regulations, the final phase requires a different name. Street names are already existing and approved, no changes requested.
Motion to approve Gateway Townhomes of Novi made by Alan Patterson, seconded by Andy Copeland. Motion carried 3-0.

3. The applicant, Tricap Holdings LLC, is requesting to change the name The District to **Station 6 at Novi Town Center** for the development project west of Flint St near the southwest corner of Grand River and Novi Rd (43443 Flint St). The parcel numbers are 50-22-22-226-003 and 50-22-22-226-005. The sign at the development would only say **Station 6**. This project was formerly named Flint Street Development, the name was changed to The District after Committee approval in December 2017. The applicant also applied to change Flint St to Main St in December 2017, but the Committee denied that request.

The Committee discussed concerns about naming a development Station 6 as it may cause confusion with fire stations, and would also be an issue if a sixth fire station were to be built (Station 6).

Motion to deny Station 6 at Novi Town Center made by Alan Patterson, seconded by Andy Copeland.

a. The applicant is also requesting approval of the street names listed below.

i. Bond Street

1. The preferred address is **6 Bond Street East** and **6 Bond Street West**

The Committee decided that Bond Street would be the new name of the road as it goes from Novi Road to Grand River Avenue as part of the ring road realignment. The small part of Flint Street that is existing off of Grand River will remain Flint Street separate from the ring road entrance off Grand River. Committee approval of Bond St is a recommendation to City Council for the change of a street name.

Motion to recommend approval to City Council of street name change from Flint Street to Bond Street made by Larry Butler and seconded by Andy Copeland. Motion carried 3-0.

The Committee determined that the addressing of 6 Bond Street East and 6 Bond Street West are not consistent with the City's addressing and street numbering standards.

Motion to deny the addressing of 6 Bond Street East and 6 Bond Street West made by Larry Butler and seconded by Alan Patterson. Motion carried 3-0.

Motion to adjourn made by Alan Patterson, seconded by Andy Copeland.
Meeting adjourned 10:22 am.

EXCERPT FROM
PLANNING COMMISSION
DRAFT MINUTES



**EXCERPT FROM
PLANNING COMMISSION
DRAFT MINUTES**

CITY OF NOVI

Regular Meeting

June 27, 2018 7:00 PM

Council Chambers | Novi Civic Center
45175 W. Ten Mile (248) 347-0475

ROLL CALL

Present:

Member Anthony, Member Avdoulos, Member Greco, Member Howard, Member Lynch, Member Maday, Chair Pehrson

Absent:

None

Also Present:

Barbara McBeth, City Planner; Sri Komaragiri, Planner; Rick Meader, Landscape Architect; Darcy Rechten, Staff Engineer; Thomas Schultz, City Attorney; Doug Necci, Façade Consultant; Maureen Peters, Traffic Consultant

1. THE BOND FKA THE DISTRICT JSP 18-10

Public hearing at the request of DTN Management/Tricap Holdings for JSP 18-10 Planning Commission's recommendation to City Council for Preliminary Site Plan, Phasing Plan, Woodlands Permit, and Storm Water Management Plan Approval. The subject property is currently zoned TC-1 (Town Center One) and is approximately 7.74 acres. It is located on the west side of Flint Street in the south west corner of Grand River Avenue and Novi Road in Section 22. The applicant is proposing a mixed use development with two four-story multi-family residential buildings with a total of 253 apartments and a single-story commercial building (5,578 SF).

Planner Komaragiri said the subject property is located behind City Center Plaza between Flint Street and the railroad. There is an existing building on the property, which is not actively used at this time. The property is zoned Town Center One (TC-1) surrounded by the same on all sides except with Light Industrial (I-1) the south side across the railroad tracks. The Future Land Use Map indicates similar uses for the subject property and surrounding parcels. The applicant is currently not seeking a rezoning.

The site does not appear to contain regulated wetlands; however, the Walled Lake Branch of the Middle Rouge River flows through the southeast section of the subject site. Few of the regulated woodlands area are located in the southeast section of the site, along the Walled Lake Branch of the Middle Rouge River. They are predominantly cotton wood trees. The applicant is proposing to remove about 103 trees, about 70%, which would require 139 replacements, most likely paid into tree fund due to lack of space on site.

The subject property is approximately 7.74 acres. The applicant is proposing to redevelop the former Fendt Transit Mix Concrete Plant into a mixed use development with two four-story multifamily residential buildings with a total of 253 apartments and a single-story commercial building (5,578 SF). A minimum of 10% of commercial use of total development is required to qualify for a mixed use development.

Planner Komaragiri said the applicant is proposing a total 432 spaces for residential development, as recommended. The site improvements include a two level parking structure, site amenities such as a swimming pool, landscaped courtyards and related landscape improvements. The applicant is proposing a phased construction in three phases. The building's orientation is primarily toward Flint Street, with only a few of the building's windows opening onto the rear property line adjacent to the railroad tracks.

On-street parking is proposed along the realigned public road, similar to the onstreet parking that is currently available along Main Street, east of Novi Road. The applicant is proposing to dedicate six parking spaces as a benefit to the Novi Public Cemetery visitors to provide convenient access to the cemetery through their property. The applicant and staff will continue to work together to coordinate construction timelines of the Flint Street realignment and of the proposed construction.

The site plan qualifies for a mixed use development and higher densities as the applicant is proposing 10%. However, the applicant is proposing to build the qualifying non-residential use in phase 3, of which the timing is undetermined. The applicant will be required to provide a form of agreement and/or financial guarantees acceptable to the City that assure the commercial component will be built within a certain time as suggested by applicant and approved by the City, which the applicant agreed to do at the time of Final Site Plan.

The applicant has been working with City staff for over a year trying to identify issues and trying to co-ordinate their design efforts with the City's Flint Street realignment plans. They have eliminated about 7 deviations since the pre-application meeting.

Planner Komaragiri said the applicant is proposing to dedicate the necessary right of way (approximately 1 acre) along the project's Flint Street frontage in order to accommodate the City's plans. It is indicated as the area shaded in grey in the image on top. A majority of the deviations that relate to items such as building setbacks and parking setbacks are a result of the shallow shape of the lot. Those areas are indicated as red in the bottom image. As you can see, they are very negligible encroachments into the setbacks. The motion sheet you have lists approximate distances into the setbacks and we will work with the applicant to identify the right number before they go to Zoning Board of Appeals.

The site plan currently requires an unusually long list of deviations from Planning, Engineering, Landscape and Facade for a site which is being developed as a permitted use. However, as mentioned, the subject parcel has an atypical shallow shape that limits conformance to certain code requirements.

Items in green are a result of shallow lot discussed earlier. Items in blue, which refer to

unit density and unit mix are subject to further discussion by Planning Commission and City Council. Items in green are supported by staff, as we understand that alternate options are not available. In the Town Center (TC) District, the total number of rooms dictates the maximum density that can be attained for a specific site. Staff has determined that in order to not exceed the maximum allowable room count of 421 rooms, the development for the subject property cannot exceed 201 units, with a density of 23 dwelling units per acre. This number is calculated based on the site acreage of 7.74 acres, the percentage of unit mix the applicant is proposing (58% 1 BR units, 37% 2 BR units and 6% 3 BR units), and the recommended density by the code. The applicant is proposing 627 rooms with a total density of 33 DUA (Dwelling Units per Acre). City Council may approve the increase in the room count (421 allowed, 627 proposed) up to twice the number of rooms allowed and thus the increase in density proposed (23 DUA approximate allowable, 33 DUA proposed). The Master Plan for Land Use recommends a density of up to 20 DUA for the subject property.

The applicant is exceeding the maximum percentage of 1 bedroom units (50% maximum, 58% proposed), which would require a Zoning Board of Appeals variance. The applicant has provided a narrative explaining the reasons for exceeding the maximum allowable percentage. The applicant states that their target renters mostly prefer to have smaller living spaces but more on-site amenities for active and passive recreation. They further state that the proposed unit mix tends to provide a more urban apartment living style than the traditional suburban style living.

Planner Komaragiri said items in maroon are temporary deviations which are a result of temporary gravel parking proposed by the applicant for the benefit of cemetery visitors. There are number of Landscape waivers required, but the applicant has worked to eliminate many and reduce the impact of others to the point where the waivers now can be supported.

Multiple deviations for Façade are being requested for all building on site including the parking garage. The façade review notes that in general the buildings exhibit interesting massing and the creative use of materials and colors, that these deviations are minor in nature and that the overall appearance of the building would not be significantly improved by strict application of the percentages listed in the Ordinance. The applicant has provided a façade board, which is in front of the podium. Our façade consultant, Doug Necci, is here tonight if you have any questions for him.

All reviews are recommending approval. The development is over 5 acres and is located in Town Center One (TC-1) District, which would require City Council approval based on your recommendation.

The Planning Commission is asked tonight to hold the scheduled Public Hearing, and is asked to make a recommendation to City Council to either approve or deny the applicant request. The applicants Albert Ludwig, Glenn Cantor and John Woods are here with their design engineer, Bob Emerine, if you have any questions for them and so are we. Thank you again for your time.

Albert Ludwig from TriCap Holdings said with me from TriCap is Michael Horowitz and

Glenn Cantor and the three of us have been working together since the 80's. We were with a little company called the Selective Group that got sold but we stayed together and we have been developing for a really long time. We were a bunch of young guys back then. We've developed thousands of homes and dozens of commercial buildings, office buildings over time and our three guys have been doing this for a really long time and together for most of that time.

This project was big so we thought we'd find ourselves a partner and we were lucky enough to come across a company out of Lansing called DTN Management. And from DTN, I have John Woods and James Chen here. They do stuff like this, they've done a couple recently in the state and other parts of the state and Jon is going to get up and tell you a little bit about them, he knows more about them and I can tell you about us.

But together we're going to do this project hopefully and we also assembled a team of consultants that are all very familiar with Novi. Bob Emerine from Seiber Keast, he seems like he knows every site in Novi backwards and forwards. And all the rest of our consultants – the traffic, the Village Green who consulted with us on the unit mix, King and MacGregor the wetlands and woodlands people, all the way down the line. Our consultant team, with the exception of the architect, knows Novi. They know how it works, they know what Novi is looking for, so we think we put together a really strong team there. For the architect, we're bringing in somebody out of Houston – a world-class architect that DTN was familiar with. They do projects like this all over the world, not just here. The architect couldn't be here tonight because he's in London, they're doing something in Dubai, they're a big company and they really know their stuff. But they're not here tonight.

Mr. Ludwig said as Sri said, we've been working on this project since March of last year and our initial concept was to do an urban project. We weren't interested in building a traditional suburban apartment community, we didn't think that was the market but there's nothing really new and vibrant for the younger people that are today looking for these smaller units with lots and lots of open space available and amenities, so that's been our vision since day one. We recognize that this site had issues in terms of its narrowness and with the road coming in and we understood that as we worked with staff, it got narrower with the land going to the right-of-way. So we were getting squeezed this way on an already narrow site.

Initially, we had three residential buildings and because of the narrowness and the loss of the land to the right-of-way, the third building was eliminated and the two other buildings became a little bit larger to come up with the plan as it sits today. This is result of many, many meetings with staff and their consultants and we've reached a point to where everybody is recommending approval, which we think is terrific. This is our first non-staff meeting and we hope that you guys support it, as well.

We were able to come up with cemetery parking but we don't want to pave that area now because we're not going to build the shopping center first, the shopping center needs to follow the residential units. We think that it's going to be a much stronger center with these buildings behind it so that is why it's Phase 3. At that time, we will pave the lot and put in that crossing to the cemetery, but as part of Phase 1 we have

agreed to put in a gravel lot which requires more variances naturally but at least there will be a place where people can come and park. And my understanding is that the cemetery people will restrict or eliminate access to the current roads so they can use that just for maintenance equipment. That's why the temporary road is suggested and the temporary parking spaces at this point in time.

Mr. Ludwig said I'm going to turn this over to John who can tell you more about the buildings and the lifestyle amenities that are involved, and more about DTN. And if you have any questions regarding the site, Bob is here to answer those.

John Woods with DTN Management said thanks Albert. I'm also here with my colleague James Chen, who is our portfolio analyst. Before I get started I'd like to thank Sri, Barb, Rick, and the rest of the talented group in the Planning Department. This has probably been one of the more challenging projects I've worked on personally, probably one of the more challenging for DTN just from a planning perspective because as Albert noted, there are some challenges on the site.

We've recently done, and recently meaning the last couple years, three urban projects right downtown Grand Rapids on a pretty tight site so I would stack this one up there as far as complexity, but probably also from an opportunity standpoint. We really look at this as being probably even a better opportunity for us as an organization, even more than the urban sites in downtown Grand Rapids.

I'd like to thank TriCap, fortunately they invited us to come be involved in this project several months ago. Just a little bit about DTN, we're a 45-year-old company founded in 1972 by two electrical engineers that really hated their jobs, so they thought when we were in school we paid a heck of a lot of money for student housing, so let's figure out how to raise some money and we'll get into the student housing game. So they did, DTN at this point is probably one of the largest privately-owned student housing operators and owners in the country. Although it's not a huge portfolio, it is privately owned and so we own and operate about half the beds on Michigan State's campus. Over time, that morphed into market-rate apartments in Lansing, it morphed into commercial and retail, and then we eventually ended up in Grand Rapids. So as we've bought a lot in Lansing, we've looked in other areas of the state and decided it made sense to invest and diversify. We had been looking at southeastern Michigan for about eighteen months when this opportunity came to us so we're extremely excited about it.

Mr. Woods said we're a company of about 700 employees, again primarily in Grand Rapids and Lansing with a portfolio a little under a billion dollars and we have 120 properties. I think something that is important to note, particularly for Novi, is that we're a very committed and passionate investor. So in 45 years, we've bought 122 properties and sold four, and each one had a very specific reason as to why it was sold so even thought at times it makes sense to sell properties because you can take profits or trade it in for something better, it's never been our philosophy. The first property that was sold, which was only twelve years ago, they literally interviewed the buyer for two and a half hours to make sure they understood how to effectively run the property and also understood the commitment to the community because it was a small community that they bought this first property in Holt, Michigan and it was a very important relationship

that they had developed over the years. And that is something that I don't see that will be any different for us here.

Personally, I live close, James lives close as well, our owners are over here quite often. And no different than Holt, Michigan in 1972, we'll be just as committed to Novi, Michigan in 2018. On behalf of TriCap and DTN, we're really excited to present this project this evening. It's a very different type of residential housing design. We think it's absolutely great timing for Novi.

Mr. Woods said it is that mix, and I'm careful in how I use this term but internally we call it a suburban mix – it's kind of a suburban urban building. But every community is different, so you can't just take an urban building you see in Atlanta or you see in downtown Detroit or even downtown Grand Rapids and just plug it into a community and think that it's going to work. And so part of the time that we invest into a project like this and that we spent with TriCap, we spent with engineers, and we spent with Sri and her team is trying to figure out what that balance is. Fortunately for us, you've got a pretty good ordinance to start with that really helped shape the elevation of this building. And of course through your façade consultant and Doug, and by the way Doug took my calls on a Saturday morning at 9 o'clock when he was on vacation with his family so I really appreciate that. But this project was just that complicated where it just took a team of this magnitude to develop what we did and personally we feel really good about it. We love the market, we think it's a great mix.

And what I think is really important to note about these buildings, and this what we've seen doing three of these downtown Grand Rapids, we've got about \$140 million investment down there right across from Van Andel and we've got another one that's a little bit more suburban around the corner, but these units and the reason they're developed the way that they're developed and the buildings look the way they do and the way we program the common areas, and common area is a loose term for all of the really cool spaces that you don't live in, is that people in a building like this and a community like Novi, they don't just live in their unit. They live in the entire community, and that includes the building, but that includes the Town Center District. And that's why we feel this is such a great fit for it because people will not just live in their 900 or 1,000 square foot unit, they're going to live in the courtyards that are programmed both actively and passively, meaning if you want to go down and do some gaming in an open courtyard you can do that or if you want some quiet time you can go to a different courtyard. We have four of those designed into this building and I'd be happy to talk about those later because I think that's a very different programmatic element that does not exist in this community as far as I can tell.

The walkability or what I will call the semi-walkability of Novi, so people can walk over to Main Street, across the corner to the east side of Novi Road, or they can hop in an Uber and run over to Fountain Walk. It's very convenient. And so all of that is a really integral part of the design of this building. It's not just looking at the floor plate and saying what does this 600 square foot or 900 square foot or 1200 square foot unit represent, it's the totality of the design of the building.

And also, people are living differently in apartments and you've probably all heard this

in the last couple of months but the percentage of household formation is now 70% rental of new household formations. So there still is, and I'm sure a lot of that was driven from the downturn, but it's also been a transition to the Millennial generation and I'm sure there will eventually be another transition but people are living in apartments much different than they have historically ever lived.

Mr. Woods said and one thing that we've seen when operating almost 9,000 apartment units and 15,000 student housing beds, some of which are hybrid, people years ago when they didn't have a choice. These are renters by choice, these are people that are making a clear decision that they want to live in an apartment unit. Our median incomes in our three buildings that are comparable to this, we have one in Lansing that is very similar to this, is almost \$9,000 a month. Those people can clearly buy homes, making over \$108,000 a year, so these are renters by choice. These are people that want to be here. And they've got other choices, whether they're condos, other apartments, but they want to be here.

Another thing we see in a renter by choice community is that you don't have the bunking up in rooms, and so you don't need 1,100 square foot or 1,000 square foot single units because you don't have two roommates. As a matter of fact, what we're seeing in Grand Rapids, which was very surprising to us, is even the two-bedrooms have only got singles in them. So people that have a little more discretionary income are turning the second bedroom into a den or a hobby room or whatever it happens to be, allow family to come bunk with them. It's being lived in differently when you're in a renter by choice environment. It's really a combination of all of those things that I've described that makes a project like this function but only in a community that it can function effectively, and we really feel strongly about Novi. We spend a lot of time thinking about and looking at these projects. And I'll tell you, there are very few communities in southeast Michigan that we think it will work. So again, when we were approached the better part of eight or nine months ago, we were like wow, yeah we'd love to talk about that because Novi is one of those communities.

Some other important features to note in a project like this, there are many more resident amenities in this building and areas than you'll see in a typical suburban rental community. For example, a typical suburban rental community may have a 5,000 or 6,000 square foot clubhouse, some walking trails, maybe a dog park, some grilling stations. This building all-in probably has 25,000 square feet of common area in it. And a big chunk of that is the courtyards and those courtyards are fairly substantial.

In this building, not only will there be a traditional fitness facility but in our building in Lansing, we have a yoga and on-demand fitness room, which is really nice because if you don't want to go and push weights around you can go into the on-demand fitness room and hit a button to either join a live group in New York City or Detroit or wherever, or you can do a pre-recorded class. Actually, we've found that to be more popular than the people that want to go in and hit an elliptical for 45 minutes. A lot of active social engagement, either by choice – there's plenty of space here for people to be active in the community, and then we have a lot of space where people can have quiet time also, so you don't have to be socially over the top to live in a building like this. You can be somebody that doesn't need all of that, but this building

accommodates both types of lifestyle.

Mr. Woods said multiple resident club rooms, I've tried to think of a better word to use than that, but our space, and I believe our Lansing building is an example of that, these are basically converted units that have a warming kitchen in them. People can use them for private parties, gathering spaces, and they're really kind of cool spaces. They might have some game rooms in them or some gaming tables, an expensive resort-style pool and gathering space, maybe not significantly different than some high-end suburban projects, but usually and particularly on this specific project, it's going to be a pretty over-the-top pool and gathering space, very cool. So the pool area opens up, you've got two big club rooms on either end where there are big doors that you can basically open it up into like an open-air environment for your pool area and it really changes the vibe, particularly when you're in the middle of the summer and you have hopefully not 60 and raining, but sunny and 85 degrees.

Business centers with high-speed technology; technology is a huge issue in these buildings. What we're finding in a couple of our buildings, we run fibered everything, we've been running fiber for fifteen years. We happen to own a technology company and we've got probably eleven miles of fiber strung around Lansing, Grand Rapids, and a couple other markets. There's a real high demand for technology, people in these buildings are sometimes self-employed, sometimes they're working from home, but one of the biggest complaints we've gotten is, and this may sound crazy, but if you can't deliver Wi-Fi at high speed then you're out of luck, they don't want to live there. And so the business centers in our building in Lansing, actually a local Apple store uses it as a training facility because we have a one-gig Ethernet fiber cable in there and they can do some really cool business meetings in there. So they're very functional spaces in there, they're not just spaces that are colored up that we put fancy furniture in and it sits and collects dust and you clean it once a week. These are very functional, high-utilization areas.

Mr. Woods said and as Sri mentioned and Albert alluded to, it's tough on this site and so we went through a lot of iterations and tried to mitigate as many of those deviations as we could. I don't know how many we started with but it was many more than that and fortunately, the Planning Department helped give us suggestions and recommendations on how to narrow those down and quite frankly tell us this is what makes sense to them and this is what doesn't make sense to them.

So I will tell you that as a developer, we are very appreciative of that and the process is very involved for planning review. To spend \$150,000 to be here tonight, we had to have a pretty good feeling that there was a good opportunity that we would get a recommendation. But there was a tremendous amount of work that was provided to us to help us get here, as well and also turnaround time was absolutely incredible so thanks again, I can't emphasize that enough and again, we're really excited about this, we're really excited about being a part of your community at DTN, we're excited to be partners with TriCap, and thank you.

Mr. Ludwig said I thought John was going to get into this, but he didn't mention it. So I wanted to add on the unit mix, which is one of the items up before you. Early on, we

had Village Green do a market study for us and their recommendation within that market study was 60% one-bedrooms. Now, one-bedrooms that we propose on this range in size from 600 to 1,000 square feet and anywhere in between, so there's a whole bunch of different kinds of one-bedroom. But it's important to us to meet the need of the prospective tenant and what we've been told is that 50% isn't enough, that's what the professionals are telling us for the market that we're going to attract with this building. So it worked out to be 58% is what we're asking for, but the study said that we should have 60% and nobody knows the market like they do.

Chair Pehrson asked if there was anyone in the audience that wished to address the Planning Commission regarding this project. Seeing no one, he asked if there was any correspondence.

Member Lynch said yes, we have one correspondence in support from Joseph Chuang, 25750 Novi Road.

Chair Pehrson closed the public hearing and turned it over to Planning Commission for their consideration.

Member Avdoulos said thank you, Chair Pehrson. One thing about this piece of property that was favorable is that we weren't looking at a rezoning, so the idea was to take the property and work with it and work with the City. When I first opened our packet and saw all the variances, it was like ok, let's go through this step by step and see what the issues are. And knowing a little bit about that area and what is being proposed and how you have been working with the City to come up with this project, I think this is going to be very good for the community and I think it'll be a great benefit.

I like it; I think it's a nice, modern type of building that I think is appropriate basically for the area. I think that the scale of it works well with where it will be sited. The building and the property, the project within itself, has a lot of amenities and then right in front of it with the plaza where Panera is and all the other businesses, those are additional amenities that just are extended through the site.

There's a similar building in Detroit on Woodward, sort of by the Max Fisher building, called The Scott. I had toured that building with the developer and it did have a lot of one-bedrooms, those came furnished and I don't know if you'll provide furnished apartments, they had the amenities of the dog grooming, the bike racks, the pools, the lobby area, and so I get it and I think that this is something that will attract the right crowd. My daughter lives in DC and she lives in a 450 square foot studio that is probably more expensive than these just because she is in DC, but it's the same thing – that particular complex offers the amenities that she doesn't need a huge space. So I think it's a positive thing for the City.

The Façade Ordinance – I'm glad you've been working with Doug, our City Architect – that acts as our baseline to make sure that we maintain a quality level of materials. These materials may be not in the range of the percentages that we were looking for, but based on the design, the aesthetic of where the materials are being placed and how they're being used are appropriate, I think. I'm hoping that because you're up

against the railroad track acoustically, I don't know if you're using more soundproof windows because I know that the back of the building that faces the train tracks don't have a lot of openings and the garage is there. I just want to make sure that as you're detailing and finalizing, you pay attention to some of those concerns so that as people are renting these, you're not getting any issues and the building doesn't become un-rentable.

Member Avdoulos said we've seen in other parts of the country similar developments for the type of business that we're into. Our architectural firm has done projects like this in Atlanta and all over the country and I think you hit it on the head with Novi being not quite urban, being suburban. This intersection between Grand River and Novi Road is a kind of interesting being, with Main Street not fully developed. Maybe this would help act as a catalyst for that.

We've got the living area to the east of this, which I think is working very well to so I think it's a good add and even though there's a lot of variances and it will go to Zoning Board of Appeals and City Council for waivers, I think based on the geometry of the site and everything that you've been doing for the last year or so working with the City, I'm glad that we've been able to work together and to put forward a good product.

Member Lynch said first of all, I'm glad to see the site develop and this really looks good. I noticed that there's a lot of deviations and that you'll go to City Council. The only thing I worry about is the density thing and I don't want to set precedent with that, but I think being in the Town Center District asking for 65% more density, it's really a City Council decision but I would just be cautious. This Commission has been very liberal, if you will, on allowing more density but not to the level of 65%. I know it's a difficult site. I just want Council, since they're the ones that will be making a decision as it says on the motion sheet and I am totally in support of the motion sheet, just be cautious that we don't set precedent without some justification when they do decide to allow whatever density is decided.

And the only other thing that stuck out to me, other than the beauty of the project, was this little stream that you have back there. I don't know how you're going to mitigate that. What we've done consistently is there is always going to be some type of conservation easement, and I know we're at virtually the beginning of the Rouge watershed, and it looks like you'll be working with the DEQ to figure out how you can mitigate some of that runoff, especially with the dog park being right there. I think it can be accomplished, I don't think it's a big sticking point but I do want you to focus a little bit of attention on what you do with that stream.

Member Lynch said other than that, I think the project is going to be beautiful. It is unique to Novi, it does fit that space. I do agree that it does fit that space and when I went over there and looked and tried to envision all of this that you've presented to us and what it is going to look like on that particular parcel, I think it's going to be great. It's going to be beautiful. I think it's a winner, it's beautiful, it's unique, I think it's really going to be a nice project. My only concern is that we don't set precedent by allowing such a large increase in density without some reasonable justification, and just being in the Town Center District may justify that. I just wanted it to make that clear in the

minutes so that when Council reads the minutes, they can at least see that the only concern that I have as a Commissioner is that I don't want to set precedent on allowing significant increases in density without clear justification.

Member Howard said I think this is a wonderful project. I am very excited to have something like this in Novi. I am almost scared to admit this, but I am a Millennial so this is very appealing to me. It was very interesting to go through and see some of those concerns, I think that you're completely spot on. When I talk to friends or colleagues or associates, what they're looking for even in terms of the density, it makes a lot of sense to me in terms of the façade. I wasn't necessarily happy that we need the deviations but when you see them in place, it's a gorgeous building. If it makes sense, then it makes sense.

My only concern would be traffic and I'm kind of going back to the density issue but if we are allowing such a high density in this area, I want to know that the traffic impact study is not just thinking about this from where we want to be in the future, but where we are now and how this is going to be addressed and sustained as we go through the phases of this project. I am also a fan of Panera and traffic in that area can just be a pain and while I understand that there are plans in place, I guess my concern is where we want to be versus where we are and as the project moves forward in those phases, making sure that we're able to kind of have a handle on those things.

Member Anthony said I think this is a great project, we've been waiting for this for a while. Just to follow up on the traffic question to the City, we've talked about the ring road development and that infrastructure and altering Flint Street. It looked from the Preliminary plans as though that may be a part of this. Did I read that correctly?

Planner Komaragiri said there are two different projects. The Flint Street realignment is a City project, and the developer is going to be doing residential mixed-use within his site. So the discussions have started and are ongoing to coordinate the timelines of construction, so both may run parallel or one might go after the other. We are still working on the details.

Member Anthony said ok, so that was my question was the coordination because it seems like it's vital on this one. I think a good justification for high density in this area is that this is our ring road area and that in the days of not being able to get a huge automotive manufacturing plant to pay taxes anymore, all communities now and especially suburbs need an area that's an urban village and high density and I think this is a perfect spot for that. With that, I really support it.

Member Greco said I have a question for the developer. Regarding the commercial development aspect of this, what is it? The only mention I heard was a shopping center, but what is the commercial development that is envisioned?

Mr. Ludwig said it could go in a couple different directions. It could be a stand-alone restaurant, it could be a small strip center with two or three or four retail spaces or service spaces – your typical hair salons or that type of use. You don't get shoe stores anymore, everybody gets that stuff online. So in our strip centers, it's mostly service

tenants that we have. So if it does go that way, we would envision a multi-tenant building. AT&T, a hair salon, something like that. But again, it could be a single restaurant, like a Big Boy or Applebee's or something like that. We just don't know yet.

Member Greco said and with that, I do have a question for our staff and maybe our attorney. With there being an agreement, and we don't really have a timeline yet on this, and the commercial part being an aspect of a development like this. So we have a restaurant or a strip mall that's not really in a high traffic area, it's off to the side. I know the ring road issue that Member Anthony brought up is a good one because that would significantly have an impact here. But a commercial development is subject to business conditions, right, so we've got two buildings that are very attractive, two buildings that a lot of Millennials are getting in and hanging out at the pool and the business center and then what do we do with a building that is maybe a restaurant that doesn't survive because the tenants aren't supporting it that much or a strip mall that ends up being empty. Does the developer have an obligation to fill it or does it just sit there once they set the rent and it doesn't get filled? Is there anything that we can do, or what are the tools available to make sure that it's a commercial development building that has commerce?

City Attorney Schultz said in the same sense that we can't control the occupancy of any building that you approve through site plan approval, the same is going to be true here. So what we've put in the motion is at least an indication that they are going to have an obligation to build something at some point, we don't know exactly what the agreement to that effect is going to be. A phasing agreement, essentially, which would also pick up the spaces for the cemetery. But I don't know that there's anything that we can do to make sure that they fill the space, but we can make sure that they make the space available.

Member Greco said with regard to the agreement or the development in general, is it required for there to be a single owner for the entire development and this way the property management will be run by the commercial or will it just have to be the commercial separately?

City Attorney Schultz said there's no requirement that it remain the same owner. They or some successor will have an obligation to fulfill the site plan or whatever is in the agreement will kick in. If they transfer it, we're fine with that as long as the obligation goes with the land.

Member Maday said back to the commercial development part, I envision with this development the commercial building supplying things that the people living in the community that we're developing need, like nail salon, like carry-out food. I mean, if we're looking at the Millennials, that's kind of what they're after that they don't want to drive some place. It would be nice if it had tenants that made sense for the tenants.

I love the building. I think everyone that talked kind of addressed my concerns. The traffic is a big concern for me; we can't stop development but I'm hoping and it sounds like the City is doing everything we can to work with the development – I'm not quite sure what all of it meant, but I think it meant that the lights are going to be timed in

certain ways to help with the traffic. I'm assuming we're going to do everything that we can as the City to help with that because we all know that intersection is awful. But I can't argue with the density because it is exactly what works in that area.

Member Maday said the other question that I had, and it's not really a concern, but it's bothering me. So we're not worried about the one-bedrooms because that's what everybody wants and I agree and think you guys know better than we do. But then why are we worried about the impacts on the school? Why does the study say that there could be 60+ kids going to Parkview if we're really trying to develop this for the Millennials instead of the families?

Mr. Woods said this very issue came up in a Planning Commission meeting I was at about a year ago because the local residents were concerned about the strain on the school system. And the reality is, there aren't a lot of school aged children in these buildings and they typically contribute, even in the more suburban-style apartments, they contribute about a third of what a single-family development contributes. So it's far less, and I can forward you that information, but that's from the National Multi-Housing Council and those are some statistics that we shared because of that level of concern. In this building, you're not going to have many school-aged children, I'd be very surprised. And yes to your point on the one-bedrooms, realistically you could do 100% one-bedrooms and we could fill immediately, but the projects don't work economically because we need more rental income from the two's and the three's. We've got projects in Grand Rapids that are close to 70%, but the rents are \$3.20 a foot, so if it's a 380 square foot unit you're paying \$1300 a month for, and people get sick of them after about a year. When they make enough money to move out, then they move out and you're constantly backfilling. This isn't like that, these aren't 380 square foot units, they're 600 square foot units so we're expecting a little more of a stable rent.

Member Maday said that's kind of what I was hoping you'd say because you've had experience with these types of properties and I would hope that they're for the Millennials and not for the families.

Mr. Woods said and I'll share that in a previous life before DTN, I worked for one of the largest property management companies in the country – we managed 40,000 units in 22 markets and it's consistent across the board, whether you're in Atlanta or you're in Novi. Your mixes will probably be comparable but your sizes and styles will vary.

Chair Pehrson said I, too, support this particular application. This is one time where I think a couple of the members have mentioned about density; typically, that's the one thing that none of us like to see, but here in this particular location, it's what this area needs for the 'downtown.' We need that kind of density, we need more of that. Unfortunately, it brings some more traffic, but I think from what I saw is the Traffic Impact Study that the City is doing and we will do everything we can to try to eliminate some of that. I'm very impressed with this particular project and the renderings I saw, and I wish you great success.

Motion made by Member Greco and seconded by Member Avdoulos.

ROLL CALL VOTE TO RECOMMEND APPROVAL TO CITY COUNCIL THE PRELIMINARY SITE PLAN MADE BY MEMBER GRECO AND SECONDED BY MEMBER AVDOULOS.

In the matter of The Bond fka The District JSP18-10, motion to recommend approval to City Council the Preliminary Site Plan based on and subject to the following:

1. The applicant shall provide a form of agreement and/or financial guarantees, along with final site plan submittal, acceptable to the City to assure that the commercial component will be built within a certain time as suggested by applicant and approved by the City.
2. City Council finding per Section 4.82.2.b. for allowing an increase of maximum number of rooms allowed (421 allowed, 627 proposed) *based on justification provided by the applicant in their response letter dated June 22, 2018;*
3. A City Council waiver for exceeding the maximum allowable front yard building setback per Section 3.1.26.D (10 ft. maximum allowed, approximately 15 ft. proposed) *due to unusual shallow shape of the subject property;*
4. City Council approval according to Sec. 3.6.2.Q. for allowing an increase in the minimum required parking setback as listed in Sec. 3.1.26.D for seven parking spaces designated for public use (10 ft. maximum allowed, approximately 7 ft. proposed) *as the applicant has clearly demonstrated that the minimum parking setback area is met in the remainder of the site;*
5. City Council variance from Sec. 11-239(b)(1),(2) of Novi City Code for absence of hard surface for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1 *as the requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
6. City Council variance from Sec. 11-239(b)(1),(2) of Novi City Code for absence of curb and gutter for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1 *as the requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
7. City Council variance from Sec. 11-239(b)(3) of Novi City Code for absence of pavement markings and layout including end islands for proposed temporary parking lot of six spaces in Phase 1 *as the requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
8. A section 9 waiver for the following deviations *as the overall appearance of the building would not be significantly improved by strict application of the percentage listed in the Ordinance:*
 - a. not providing the minimum required brick (30% minimum required) on the east (28% proposed), north (28% proposed) and south (26% proposed) facades for Building 1 and 2;
 - b. exceeding the maximum allowed percentage of EIFS (25% maximum allowed) on all facades (proposed: East-28%, North-38%, South- 35% and West- 48%) for Building 1 and 2;
 - c. not providing the minimum required brick and stone (50% minimum required) for TC-1 district on the north façade (48% proposed) for Building 1 and 2;

- d. not providing the minimum required brick(30% minimum required) on all facades (proposed: North -23%, -West 8%, South- 8% and East- 17%) for Commercial Building;
 - e. exceeding the maximum allowed for Cast Stone (50% maximum allowed)on all facades (proposed: North-55%, West-76%, South- 76% and East- 64%) for Commercial Building;
 - f. exceeding the maximum allowed percentage for Ribbed Metal (0% allowed) on all facades providing the ribbed metal (proposed: North-12%, West-6%, South- 6% and East- 9%) for Commercial Building;
 - g. exceeding the maximum allowed concrete for west facade for parking structure (0% allowed, 100% proposed) in lieu of providing the minimum required brick (30% minimum required, 0% provided);
 - h. exceeding the maximum allowed cast stone for north and south facades for parking structure (0% allowed, 100% proposed) in lieu of providing the minimum required brick (30% minimum required, 0% provided) ;
9. Landscape waiver from Sec. 5.5.3.B.ii for lack of berm and screening as *the applicant proposed a line of arborvitaes along the property line to soften the view toward the railroad tracks and industrial site beyond in lieu of required landscape screening;*
 10. Landscape waiver from Sec. 5.5.3.B.ii for reduction in required greenbelt width between right-of-way and parking areas along Flint/Bond Street (20 ft. width required, a range of 10 ft. to 20 ft. provided). A 2.5 foot brick wall screening the parking and additional landscaping in the narrower areas help to compensate for the lack of space in the areas with just a 10 foot greenbelt;
 11. Landscape waiver from Sec 5.5.3.F.ii.b(1) for reduction in number of total number multifamily unit trees provided (147 required, 127 provided) *as the reduction is only 14% from the total requirements and the site is otherwise well-landscaped;*
 12. Landscape waiver from Sec. 5.5.3.F.ii.B(2) for reduction in number of interior roadway perimeter trees(1 tree short) provided *due to conflict with fire access lane (grass pavers);*
 13. Landscape waiver from Sec 5.5.3.D. for deficiency in foundation landscaping coverage around parking deck *due to limited space available along the southwest side, toward the railroad. Large arborvitaes are proposed in that are to help screen the view to the railroad and industrial site;*
 14. Landscape waiver from Sec. 5.5.3.C.(3) Chart footnote for not proposing required parking lot perimeter trees for temporary gravel parking proposed to be constructed for use by visitors to Novi Cemetery in Phase 1 (11 trees required, 0 proposed) *as the landscape requirements will be met at the time of Phase 3 construction within a certain time mutually agreed between the applicant and the City;*
 15. The followings variances would require Zoning Board of Appeals approval:
 - a. A Zoning Board of Appeals variance from section 4.82.2 for increasing the maximum percentage of one bed room units allowed for this development (50% maximum allowed, 58% proposed) *(based on applicants response that a 60% unit mix is recommended based on their internal marketing survey and assessment);*
 - b. A Zoning Board of Appeals variance from section 3.27.1.D for allowing parking in side yard for commercial building(around 49 spaces) *due to*

unusual shallow shape of the subject property and the inability to park in the rear yard;

- c. A Zoning Board of Appeals variance from section 3.27.1.D for allowing parking in front yard for residential section (around 38 spaces, 9% of total 432 spaces) *due to unusual shallow shape of the subject property and the inability to park in the rear yard;*
- d. A Zoning Board of Appeals variance from section 3.27.1.D for allowing parking in side yard for residential section (around 50 spaces, 12% of total spaces in east and 35 spaces 12% of total spaces in west) *due to unusual shallow shape of the subject property and the inability to park in the rear yard;*
- e. A Zoning Board of Appeals variance from section 4.82.2.e for reduction of minimum building setback for Building 1 on east side (15 ft. required, a minimum of 12 ft. proposed for an approximate length of 12 ft., total building length is 283 ft.) *due to unusual shallow shape of the subject property;*
- f. A Zoning Board of Appeals variance from section 4.82.2.e for reduction of minimum building setback for Building 2 on east side (15 ft. required, a minimum of 8 ft. proposed for an approximate length of 16 ft. , total building length is 283 ft.) *due to unusual shallow shape of the subject property;*
- g. A Zoning Board of Appeals variance from section 4.82.2.e for reduction of minimum building setback for parking garage on west side (15ft. required, 5 ft. proposed for entire structure, total building length is 283 ft.) *due to unusual shallow shape of the subject property;*
- h. A Zoning Board of Appeals variance from section 5.7.3.E. for allowing an increase of average to minimum light level ratio for the site (4:1 maximum allowed, 4.81 provided)*due to site layout and site shallow depth;*
- i. A Zoning Board of Appeals variance from section 5. 7.3.K for exceeding maximum allowed foot candle along south property line abutting railroad tracks (1 fc maximum allowed, up to 1.7 is proposed for a small area);
- j. A Zoning Board of Appeals variance from section 3.27.1.H. and Sec. 5.4.2 for allowing two loading areas in the side yard for residential section *due to unusual shallow shape of the subject property;*
- k. A Zoning Board of Appeals variance from section Sec. 5.4.2 for reduction in minimum required loading area for each of the two loading spaces in residential section (2,830 square feet required, 644 square feet provided) *due to residential nature of the development that does not require larger loading areas;*
- l. A Zoning Board of Appeals variance from section 3.27.1.I. for reduction in width of the sidewalk along a non-residential collector (12.5 feet required on both sides, 8 feet proposed on west side and 10 feet asphalt path proposed on east) *as it aligns with City's current plans for Flint street realignment;*
- m. A Zoning Board of Appeals variance from section 5.3.2. for reduction of minimum parking bay depth for spaces proposed in Parking garage (19 ft. minimum required, 18 ft. proposed) *as the depth is limited by the pre-fabricated manufacturers specifications;*

16. The findings of compliance with Ordinance standards in the staff and consultant review letters and the conditions and the items listed in those letters being addressed on the Final Site Plan.

This motion is made because the plan is otherwise in compliance with Article 3, Article 4, and Article 5 of the Zoning Ordinance and all other applicable provisions of the Ordinance. *Motion carried 7-0.*

ROLL CALL VOTE TO APPROVE PHASING PLAN MADE BY MEMBER GRECO AND SECONDED BY MEMBER AVDOULOS.

In the matter of The Bond fka The District JSP18-10, motion to recommend approval of the Phasing Plan based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan. This motion is made because the plan is otherwise in compliance with Article 3, Article 4 and Article 5 of the Zoning Ordinance and all other applicable provisions of the Ordinance. *Motion carried 7-0.*

City Attorney Schultz said just for clarification, this is also a recommendation for approval. Just to clarity, it's not in the motion sheet.

ROLL CALL VOTE TO APPROVE WOODLAND PERMIT MADE BY MEMBER GRECO AND SECONDED BY MEMBER AVDOULOS.

In the matter of The Bond fka The District JSP18-10, motion to recommend approval of the Woodland Permit based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan. This motion is made because the plan is otherwise in compliance with Chapter 37 of the Code of Ordinances and all other applicable provisions of the Ordinance. *Motion carried 7-0.*

ROLL CALL VOTE TO APPROVE STORMWATER MANAGEMENT PLAN MADE BY MEMBER GRECO AND SECONDED BY MEMBER AVDOULOS.

In the matter of The Bond fka The District JSP18-10, motion to recommend approval of the Stormwater Management Plan based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan. This motion is made because the plan is otherwise in compliance with Chapter 11 of the Code of Ordinances and all other applicable provisions of the Ordinance. *Motion carried 7-0.*

PLANNING REVIEW



PLAN REVIEW CENTER REPORT

June 20, 2018

Planning Review

The Bond fka The District

JSP 18-10

PETITIONER

The Bond, fka The District

REVIEW TYPE

Revised Preliminary Site Plan

PROPERTY CHARACTERISTICS

Section	22	
Site Location	west side of Flint Street in the south west corner of Grand River Avenue and Novi Road ; 50-22-22-226-005 and 50-22-22-226-003;	
Site School	Novi Community School District	
Site Zoning	TC-1: Town Center One	
Adjoining	North	TC-1: Town Center One
	East	TC-1: Town Center One
	West	I-1 Light Industrial across rail road
	South	TC-1: Town Center One
Current Site	Vacant	
Adjoining Uses	North	Commercial
	East	Commercial: City Center Plaza
	West	Gen Mar and CVS warehouse
	South	Novi Cemetery
Site Size	8.73 Acres, after ROW dedication 7.74 acres	
Plan Date	Revised June 11, 2018 (Original: May 10, 2018)	

PROJECT SUMMARY

The subject property is approximately 7.74 acres and is located west side of Flint Street near the south west corner of Grand River Avenue and Novi Road (Section 22). The applicant is proposing to redevelop the former Fendt Transit Mix Concrete Plant into a mixed use development with two four-story multi-family residential buildings with a total of 253 apartments and a single-story commercial building (5,578 SF). The site improvements include a two level parking structure, surface parking, site amenities such as a swimming pool, landscaped courtyards and related landscape improvements. The applicant is proposing a phased construction in three phases. The building's orientation is primarily toward Flint Street, with only a few of the building's windows opening onto the rear property line adjacent to the railroad tracks.

The applicant is proposing to dedicate the necessary right of way (approximately 1 acre) along the project's Flint Street frontage in order to accommodate the City's plans to reconstruct and realign Flint Street in the future. On-street parking is proposed along the realigned public road, similar to the on-street parking that is currently available along Main Street, east of Novi Road. The applicant is proposing to dedicate six parking spaces in the Commercial parking lot as a benefit to the Novi Public Cemetery visitors to provide convenient access the cemetery through their property.

RECOMMENDATION

Approval of revised Preliminary Site Plan is **recommended**.

The Planning Commission will be asked to make a recommendation to the City Council for approval, approval subject to conditions, or denial of the Preliminary Site Plan, Phasing Plan, Wetland permit, Woodland permit and Storm Water Management Plan.

ORDINANCE REQUIREMENTS

This project was reviewed for conformance with the Zoning Ordinance with respect to Article 3 (Zoning Districts), Article 4 (Use Standards), Article 5 (Site Standards), and any other applicable provisions of the Zoning Ordinance. **Please see the attached chart for information pertaining to ordinance requirements.** Items in **bold** below must be addressed and incorporated as part of the Final Site Plan submittal:

1. **Density and Total Number of Rooms:** In the Town Center district, total number of rooms dictates the maximum density that can be attained for a specific site. The current ordinance provides clear guidelines if the development contains only one type of bedroom units, and uses a factor if a mix of different types of units are proposed. This development proposes a mix of 1, 2 and 3 bedroom units. In TC and TC-1, Maximum allowable rooms is calculated by taking the area of the parcel in square feet, divided by a factor of 800 for a mixed use development. For the subject parcel, the maximum number of rooms allowed for this property is 421 rooms (7.74 acres = 336, 718 sq. ft. / 800).

Staff has determined that in order to not exceed the maximum allowable room count of 421 rooms, the development for the subject property cannot exceed 201 units, with a density of 23 dwelling units per acre. This number is calculated based on the site acreage of 7.74 acres, the percentage of unit mix the applicant is proposing (58% 1 BR units, 37% 2 BR units and 6% 3 BR units), and the recommended density for each type of unit specified in the Sec. 4.82.2. Please note that the total number of units may differ from 201 (and the corresponding density), if the percentage mix is revised.

The applicant is proposing 627 rooms with a total density of 33 DUA (Dwelling Units per Acre). City Council may approve the increase in the room count (421 allowed, 627 proposed) up to twice the number of rooms allowed and thus the increase in density proposed (23 DUA approximate allowable, 33 DUA proposed). The Master Plan for Land Use recommends a density of up to 20 DUA for the subject property.

2. **Percentage of 1-Bedroom units:** The applicant is exceeding the maximum percentage of 1 bedroom units (50% maximum, 58% proposed), which would require a Zoning Board of Appeals variance. The applicant has provided a narrative explaining the reasons for exceeding the maximum allowable percentage. **The applicant states that their target renters mostly prefer to have smaller living spaces but more on-site amenities for active and passive recreation. The applicants are proposing a large number of amenities and services on site, such as dog park, bike repair, dog wash, gyms, studios and conference rooms. They further state that the proposed unit mix tends to provide a more urban apartment living style than the traditional suburban style living.**
3. **Total Parking required and Proposed:** The proposed development would require a total of 360 parking spaces according to TC-1 standards for a mixed use development (1 per each 1 bedroom unit and 2 per each 2 and 3 bedroom units). The site plan proposes a mixed use development; however, the uses are physically separated and function as individual uses. There is no shared parking proposed between the uses. A typical multi-family development in another zoning district would have required a total of 513 spaces for a similar development (2 per each 2 or less bedroom units and 2.5 for 3 or more bedroom units).

The applicant provided some parking data that compared the proposed development and demographics with similar developments in other similar communities. After reviewing all information provided, staff recommends that while 360 is the minimum required per TC-1 code, the applicant should demonstrate that the site can hold for a 20 percent additional parking for contingency (additional 72 spaces bringing the total to 432). The contingency would address the unknown

factors such as renters demographic and occupancy rate, etc. **The applicant is proposing a total 432 spaces for residential development, as recommended.**

4. Mixed Use Development: The site plan qualifies for a mixed use development and higher densities it offers as the applicant is proposing 10% of total development as a non-residential use per section 4.25 (amended from 20% to 10% with a text amendment effective dated February 07, 2017). However, the applicant is proposing to build the qualifying non-residential use in phase 3, of which timing is undetermined. **The applicant will be required to provide a form of agreement and/or financial guarantees acceptable to the City that assure the commercial component will be built within a certain time as suggested by applicant and approved by the City.**
5. Photometric Plan: **Specification sheets should be placed on the sheets. Photometric data and fixture data should be shown on the building facades.** The applicant has submitted a revised photometric plan on June 20, 2018 via e-mail that indicates the Avg/Min ratio for the entire site. Avg/Min ratio is exceeding the maximum allowable ratio of 4:1 (Proposed 4.8: 1). The applicant has worked with the staff on multiple options and staff understands that the intensity of development and the shape of the lot determined that maximum ratio that is being proposed. **Please refer to Planning Review Chart for additional comments. An insignificant are along south property line has higher foot candle values; Please consider revising the plan to keep it under 1 fc to avoid seeking a ZBa deviation.**
6. Sheet Title: **Sheet No.3 is referred to as 'Right-of-way Taking Plan.'** The City is not "taking" the area indicated; rather, the applicant is offering the land voluntarily to accommodate Flint/Bond street realignment. **Please rename the sheet accordingly.**
7. Planning Review Chart: **Please refer to Planning Review chart for additional minor comments that needs to be addressed for further clarification.**
8. Phasing: The applicant is proposing to phase the construction in three phases. Per sheet 5, the phases are listed as follows:

Phase 1 (East building)	Phase 2 (West Building)	Phase 3 (Commercial building)
Building 1 2-level garage: 152 spaces Surface Parking: 59 spaces Parallel on-street parking: 20 spaces Temporary gravel parking for Cemetery visitors: 6 spaces	Building 2 and remaining parking for Residential development	Commercial building and associated parking
9. Street Name Change: The applicant has requested to rename Flint Street as Bond Street. Project Naming and Street Naming Committee has approved the new name. City Council final approval is required to adopt the new street name. The request for approval of Bond Street will be made at the time of Site Plan approval request.
10. Exterior Signage: Exterior Signage is not regulated by the Planning Division or Planning Commission. Sign permit applications that relate to construction of a new building or an addition to an existing building may submitted, reviewed, and approved as part of a site plan application. In that case, the proposed signs shall be shown on the Preliminary Site Plan. Alternatively, an applicant may choose to submit a sign application to the Building Official for administrative review after Site plan approval. Following Preliminary Site Plan approval, any application to amend a sign permit or for a new or additional sign shall be submitted to the Building Official. Please contact the Ordinance Division 248.735.5678 for information regarding sign permits.
11. Conservation Easements: Draft conservation easements are required along with Final Site Plan submittal.

IDENTIFIED LIST OF DEVIATIONS:

Following are list of the items staff has identified as deviating from the Ordinance. Staff supports some of the items listed below due to limitations posed by unusual shallow shape of the lot. The applicant has provided a narrative which expands on reasons for requesting the deviations.

Identified City Council Waivers/DCS variances:

For developments in Town Center district City Council may make findings and allow certain deviations from ordinance standards. The following two would require a City Council determination based on certain conditions listed in Ordinance.

1. City Council finding per Section 4.82.2.b. for allowing an increase of maximum number of rooms allowed (421 allowed, 627 proposed); **Please refer to comments provided on page 2.**
2. A City Council waiver for exceeding the maximum allowable front yard building setback per Section 3.1.26.D (10 ft. maximum allowed, approximately 15 ft. proposed); City Council can allow the increase in setback at intersections where necessary to obtain a clear vision area for vehicular traffic per Sec. 3.27.1.C. **The applicant is asked to demonstrate whether the increase is requested for above reason or revise the layout to conform to the requirement. Staff supported.**
3. City Council approval according to Sec. 3.6.2.Q. for allowing an increase in the minimum required parking setback as listed in Sec. 3.1.26.D for seven parking spaces designated for public use (10 ft. maximum allowed, approximately 7 ft. proposed). City Council may modify parking setback requirements provided such modification of the setback requirements does not reduce the total area of setback on a site below the minimum setback area requirements of Section 3.6.2.Q. **If the applicant cannot provide information that satisfactorily addresses the above items, then a Zoning Board of Appeals variance should be requested. Staff supported.**

The applicant is proposing to dedicate six parking spaces in the Commercial parking lot as a benefit to the Novi Public Cemetery visitors to provide convenient access the cemetery through their property. The applicant proposes to build a permanent parking lot as part of Phase 3 improvements. The timeline for Phase 3 is not yet determined. In the interim, the applicant proposes to build the six spaces as a temporary gravel lot. A gravel parking area would require the following City Council variances. These variances would be considered temporary until Phase 3 is built. **The Applicant is asked to indicate the tentative timeline for construction of phase 3.**

4. City Council variance from Sec. 11-239(b)(1),(2)of Novi City Code for absence of hard surface for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1;
5. City Council variance from Sec. 11-239(b)(1),(2)of Novi City Code for absence of curb and gutter for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1;
6. City Council variance from Sec. 11-239(b)(3) of Novi City Code for absence of pavement markings and layout including end islands for proposed temporary parking lot of six spaces in Phase 1;

Traffic review has identified the following possible deviations that may be approved by City Council. **The applicant is asked to provide the additional information and the values indicated in red below to determine whether any of these deviations are required or not.**

7. City Council waiver for variance from Design and Construction Standards Section 11-216(d) for reduction in distance opposite side commercial driveways(**xxx** required, **xxx** proposed);
8. City Council waiver for reduction in curb height when not fronting the 17 feet parking spaces (6 inches required, 4 " proposed) for approximately **xxx** feet;
9. City Council waiver for reduction in sidewalk width to access the bike parking for Commercial area (6 feet required, 5 feet clear sidewalk in addition to 2 feet overhang);

Facade review identified deviations from the Façade ordinance and recommends a section 9 waiver for all of the items listed below as it enhances the overall design and is consistent with the intent and purpose of the Ordinance. **All these are staff supported.**

10. A section 9 waiver for

- a. not providing the minimum required brick(30% minimum required) on the east (28% proposed), north(28% proposed) and south(26% proposed) facades for Building 1 and 2;
- b. exceeding the maximum allowed percentage of EIFS (25% maximum allowed) on all facades (proposed: East-28%, North-38%, South- 35% and West- 48%) for Building 1 and 2;
- c. not providing the minimum required brick and stone (50% minimum required) for TC-1 district on the north façade (48% proposed) for Building 1 and 2;
- d. not providing the minimum required brick(30% minimum required) on all facades (proposed: North -23%, -West 8%, South- 8% and East- 17%) for Commercial Building;
- e. exceeding the maximum allowed for Cast Stone (50% maximum allowed)on all facades (proposed: North-55%, West-76%, South- 76% and East- 64%) for Commercial Building;
- f. exceeding the maximum allowed percentage for Ribbed Metal (0% allowed) on all facades providing the ribbed metal (proposed: North-12%, West-6%, South- 6% and East- 9%) for Commercial Building;
- g. exceeding the maximum allowed concrete for west facade for parking structure (0% allowed, 100% proposed) in lieu of providing the minimum required brick (30% minimum required, 0% provided);
- h. exceeding the maximum allowed cast stone for north and south facades for parking structure (0% allowed, 100% proposed) in lieu of providing the minimum required brick (30% minimum required, 0% provided) ;

Landscape review has identified the following waiver, which are all staff supported.

11. Landscape waiver from Sec. 5.5.3.B.ii for lack of berm and screening as the applicant proposed a line of arborvitae along the property line to soften the view toward the railroad tracks and industrial site beyond in lieu of required landscape screening;
12. Landscape waiver from Sec. 5.5.3.B.ii for reduction in required greenbelt width between right-of-way and parking areas along Flint/Bond Street (20 ft. width required, a range of 10 ft. to 20 ft. provided). A 2.5 foot brick wall screening the parking and additional landscaping in the narrower areas help to compensate for the lack of space in the areas with just a 10 foot greenbelt;
13. Landscape waiver from Sec 5.5.3.F.ii.b(1) for reduction in number of total number multifamily unit trees provided (147 required, 127 provided) as the reduction is only 14% from the total requirements and the site is otherwise well-landscaped;
14. Landscape waiver from Sec. 5.5.3.F.ii.B(2) for reduction in number of interior roadway perimeter trees(1 tree short) provided due to conflict with fire access lane (grass pavers);
15. Landscape waiver from Sec 5.5.3.D. for deficiency in foundation landscaping coverage around parking deck due to limited space available along the southwest side, toward the railroad. Large arborvitae are proposed in that are to help screen the view to the railroad and industrial site;
16. Landscape waiver from Sec. 5.5.3.C.(3) Chart footnote for not proposing required parking lot perimeter trees for temporary gravel parking proposed to be constructed for use by visitors to Novi Cemetery in Phase 1 (11 trees required, 0 proposed);

Zoning Board of Appeals Variances:

If approval is recommended by the City Council, the applicant should seek the following variances with the Zoning Board of Appeals.

1. A Zoning Board of Appeals variance from section 4.82.2 for increasing the maximum percentage of one bed room units allowed for this development (50% maximum allowed, 58% proposed) . **Please refer to comments provided on page 2. Staff supported.**

The subject parcel has an atypical shallow shape that limits conformance to certain code requirements. The applicant has dedicated approximately an acre of the property for Flint street realignment plans which further decreased the depth of the property and made it even shallower. The applicant is seeking the following variances to setbacks and loading space location due to limitations posed by the shape of the lot in order to maximize the developable area. All these are staff supported.

Parking Setbacks

2. A Zoning Board of Appeals variance from section 3.27.1.D for allowing parking in side yard for commercial building(approximately 49 spaces);
3. A Zoning Board of Appeals variance from section 3.27.1.D for allowing parking in front yard for residential section (38 spaces, 9% of total 432 spaces);
4. A Zoning Board of Appeals variance from section 3.27.1.D for allowing parking in side yard for residential section (50 spaces,12% of total spaces in east and 35 spaces 12% of total spaces in west);

Building Setbacks

5. A Zoning Board of Appeals variance from section 4.82.2.e for reduction of minimum building setback for Building 1 on east side (15 ft. required, a minimum of 12 ft. proposed for an approximate length of 12 ft., total building length is 283 ft.);
6. A Zoning Board of Appeals variance from section 4.82.2.e for reduction of minimum building setback for Building 2 on east side (15 ft. required, a minimum of 8 ft. proposed for an approximate length of 16 ft. , total building length is 283 ft.);
7. A Zoning Board of Appeals variance from section 4.82.2.e for reduction of minimum building setback for parking garage on west side(15ft. required, 5 ft. proposed for entire structure, total building length is 283 ft.);

Lighting and Photometric Plan

8. A Zoning Board of Appeals variance from section 5.7.3.E. for allowing an increase of average to minimum light level ratio for the site (4:1 maximum allowed, 4.81 provided); **This is supported as the applicant has clearly demonstrated all alternates have been explored to minimize the overage of the ratio;**
9. A Zoning Board of Appeals variance from section 5. 7.3.K for exceeding maximum allowed foot candle along south property line abutting railroad tracks (1 fc maximum allowed, up to 1.7 is proposed for a small area); **This is supported as the overage for an insignificant area along south property line;**

Loading Areas

10. A Zoning Board of Appeals variance from section 3.27.1.H. and Sec. 5.4.2 for allowing two loading areas in the side yard for residential section;
11. A Zoning Board of Appeals variance from section Sec. 5.4.2 for reduction in minimum required loading area for each of the two loading spaces in residential section (2,830 square feet required, 644 square feet provided); **This is supported as the development is residential in nature;**

Other

12. A Zoning Board of Appeals variance from section 3.27.1.I. for reduction in width of the sidewalk along a non-residential collector (12.5 feet required on both sides, 8 feet proposed on west side and 10 feet asphalt path proposed on east); **This is supported as it aligns with the City's design for Flint Street realignment;**
13. A Zoning Board of Appeals variance from section 5.3.2. for reduction of minimum parking bay depth for spaces proposed in Parking garage (19 ft. minimum required, 18 ft. proposed); **Staff supported as the reduction is requested due to manufacturers specification for pre-fabricated structures and additional green space provided.**

The applicant has been working with staff and **eliminated the following list of deviations** since the first submittal.

1. End Islands (Sec. 5.3.12): A City Council Waiver to allow painted end islands in lieu of required end islands as listed in Section 5.3.12;
2. Commercial Parking front yard setbacks (Sec. 3.1.25.D): 20 feet required; 8.3 feet proposed;
3. Traffic waivers: The site plan may require a waiver for same side driveway spacing requirements. Provide additional information to determine whether a waiver is required. Refer to Traffic review for more details.
4. Parking Setbacks Off-street Parking (Sec. 4.82.2.f): A minimum of 10 ft. from any wall of any dwelling structure, which contains openings involving living areas; A minimum of 8 ft. proposed.

5. Parking stall located adjacent to a parking lot entrance (public or private)(Sec. 5.3.13): A parking space shall not be located closer than twenty-five (25) feet from the street right-of-way (ROW) line. Some of the spaces are located closer than 25 feet.
6. Parking Setbacks Off-street Parking (Sec. 4.82.2.f): 10 ft. from ROW required, 6.5 feet minimum proposed.
7. Parking Screening (Sec. 3.27.1.D): Surface parking areas must be screened by either a 2.5 ft. brick wall or a semi-transparent screening or a landscaped berm from all public ROW. **The applicant has proposed a 2.5 feet brick wall, but it does not adequately screen all the parking spaces. If the plan is not revised to meet the code, a ZBA variance maybe required.**

OTHER REVIEWS

- a. Engineering Review (06-04-18): Additional comments to be addressed with Final Site Plan. Engineering is currently recommending approval conditional upon coordination with the City for Flint street realignment.
- b. Landscape Review (Revised with this submittal): Landscape review has identified waivers that may be required. Refer to review letter for more comments. Landscape is recommending approval. Additional comments to be addressed with Final Site Plan.
- c. Wetlands Review (06-04-18): A Wetlands Buffer Authorization is required for the proposed impacts to regulated wetland setbacks. Additional comments to be addressed with Final Site Plan. Wetlands recommend approval.
- d. Woodlands Review (05-30-18): A City of Novi Woodland permit is required for the proposed impacts to regulated woodlands. Additional comments to be addressed with Final Site Plan. Woodlands recommend approval.
- e. Traffic Review(Revised with this submittal): Additional comments to be addressed with Final Site Plan. Traffic recommends approval.
- f. Traffic Study (03-28-18): Traffic recommends approval.
- g. Facade Review(Revised with this submittal): Façade is recommending approval of Section 9 waiver. A sample board is provided.
- h. Fire Review(Revised with this submittal): Additional comments to be addressed with revised Preliminary Site Plan. Additional comments to be addressed with Final Site Plan.

NEXT STEP: PLANNING COMMISSION MEETING

All reviews are recommending approval. The site plan is scheduled for consideration on **June 27th** meeting. Please provide the following no later than **10 am on June 22, 2018**.

1. Original Site Plan submittal in PDF format. Staff has already received this item.
2. A response letter addressing ALL the comments from ALL the review letters and **a request for waivers as you see fit**.
3. A color rendering of the Site Plan or building elevations the applicant would like to be included in the Planning Commission packet (Optional).

CITY COUNCIL MEETING

The site plan will be place on City Council's agenda once Planning Commission recommends approval. No additional information is required prior to City Council meeting, unless Planning Commission provides comments that would require a resubmittal.

ZONING BOARD OF APPEALS MEETING

When City Council approves the site plan, the applicant should then seek a Zoning Board of Dimensional Variances. The application can be found at this [link](#). Please contact Kate Oppermann at 248-347-0459 for meeting and deadline schedule. **The application deadline to be on the agenda for August 14th meeting is July 5th.**

FINAL SITE PLAN SUBMITTAL

After receiving the Preliminary Site Plan approval from City Council and variances approved by ZBA, the applicant should submit the following for Final site plan review and approval

1. Seven copies of Final Site Plan addressing all comments from Preliminary review
2. Response letter **addressing all comments and refer to sheet numbers where the change is reflected**
3. [Final Site Plan Application](#)
4. [Final Site Plan Checklist](#)
5. Engineering Cost Estimate
6. Landscape Cost Estimate
7. [Other Agency Checklist](#)
8. [Hazardous Materials Packet](#) (Non-residential developments)
9. [Non-Domestic User Survey](#) (Non-residential developments)
10. [No Revision Façade Affidavit](#) (if no changes are proposed for Façade)
11. Legal Documents as required
12. Drafts of any legal documents (note that off-site easements need to be executed and any on-site easements need to be submitted in draft form before stamping sets will be stamped)

ELECTRONIC STAMPING SET SUBMITAL AND RESPONSE LETTER

After receiving Final Site Plan approval, please submit the following for Electronic stamping set approval:

1. Plans addressing the comments in all of the staff and consultant review letters in PDF format.
2. Response letter addressing all comments in ALL letters and ALL charts and **refer to sheet numbers where the change is reflected.**

STAMPING SET APPROVAL

Stamping sets are still required for this project. After having received all of the review letters from City staff the applicant should make the appropriate changes on the plans and submit **10 size 24" x 36" copies with original signature and original seals,** to the Community Development Department for final Stamping Set approval.

SITE ADDRESSING

A new address is required for this project. The applicant should contact the Building Division for an address prior to applying for a building permit. Building permit applications cannot be processed without a correct address. The address application can be found by clicking on this [link](#).

Please contact the Ordinance Division 248.735.5678 in the Community Development Department with any specific questions regarding addressing of sites.

STREET AND PROJECT NAME

This project requires approval from the Street and Project Naming Committee. The meeting is scheduled for June 21 for approval. Please contact Hannah Smith (248-347-0579) in the Community Development Department for additional information. The address application can be found by clicking on this [link](#).

PRE-CONSTRUCTION MEETING

A Pre-Construction meeting is required for this project. Prior to the start of any work on the site, Pre-Construction (Pre-Con) meetings must be held with the applicant's contractor and the City's consulting engineer. Pre-Con meetings are generally held after Stamping Sets have been issued and prior to the start of any work on the site. There are a variety of requirements, fees and permits that must be issued before a Pre-Con can be scheduled. If you have questions regarding the checklist or the Pre-Con itself, please contact Sarah Marchioni [248.347.0430 or smarchioni@cityofnovi.org] in the Community Development Department.

CHAPTER 26.5

Chapter 26.5 of the City of Novi Code of Ordinances generally requires all projects be completed within two years of the issuance of any starting permit. Please contact Sarah Marchioni at 248-347-0430 for additional information on starting permits. The applicant should review and be aware of the requirements of Chapter 26.5 before starting construction.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or skomaragiri@cityofnovi.org.



Sri Ravali Komaragiri – Planner



PLANNING REVIEW CHART: TC-1 - Town Center-1 District

Review Date: June 15, 2018
Review Type: Revised Preliminary Site Plan
Project Name: **18-10 BOND FKA THE DISTRICT**
Plan Date: June 11, 2018
Prepared by: Sri Ravali Komaragiri, Planner
E-mail: skomaragiri@cityofnovi.org **Phone:** 248.735.5607

Items in **Bold** in the comments column need to be addressed by the applicant and/or the Planning Commission/City Council before approval of the Preliminary Site Plan. Underlined items need to be addressed on the Final Site Plan. Items in **bold and underline** are not conforming to the code.

Item	Required Code	Proposed	Meets Code	Comments
Zoning and Use Requirements				
Master Plan <i>(adopted July 26, 2017)</i>	TC Commercial	Mixed Use: Retail, Residential, and Parking	Yes	
Area Study	Town Center Study 2014	Preferred Uses: Office, restaurants, retail, outdoor cafes abutting Middle Rouge Creek Other uses to be considered: Upper story residential or live/work units	Yes	
Flint Street Realignment	Town Center Area Study provided recommendations for Flint Street realignment	Proposed plan proposes realignment that matches the layout City has proposed. However, proposed cross section by the applicant is different from that City proposed. Applicant proposes a two lane road with parallel parking on one side. City proposed two lanes with center turn lane through.	Yes?	
Zoning <i>(Effective Dec. 25, 2013)</i>	TC-1: Town Center-1	No Change	Yes	
Uses Permitted <i>(Sec 3.1.26.B & C)</i>	Sec. 3.1.25.B. - Principal Uses Permitted. Sec. 3.1.25.C. – Special Land Uses Permitted.	5,578 SF if retail 253 Apartments 142 1-BR,	Yes	This development is considered a mixed use.

Item	Required Code	Proposed	Meets Code	Comments
	Retail (4.78.3) and Residential Dwellings (4.82)	93 2-BR and 15 3-BR units Area for Commercial site: 1.07 acres Area for residential site: 6.67 acre		
Density <i>Future Land Use Map(adopted July 26, 2017)</i>	Maximum 20.0 DUA	Total site area: 7.74 acres 33 dwelling Units per Acre	No	20 DUA is maximum recommended per our Master Plan for Land Use; Refer to Plan review letter for more comments on density
Phasing	Show proposed phasing lines on site plan.	Three phases are being proposed Phase 1(Building 1) 127 units Pool and amenities 2-level garage: 152 spaces Surface Parking: 59 spaces Parallel on-street parking: 20 spaces Temporary gravel cemetery parking area: approximately 6spaces Phase 2 (Building 2) 126 units 2-level garage: 152 spaces Phase 3 (Commercial building) Commercial building and associated parking	Yes	Please include the building and parking counts for each phase on the phasing plan as well. A pedestrian access to cemetery should be provided with phase 1 <u>City Council variance is required for gravel parking</u>
Height, bulk, density and area limitations				
Frontage on a Public Street <i>(Sec. 5.12)</i> Access To Major Thoroughfare <i>(Sec. 5.13)</i>	Frontage upon a public street. Access to major thoroughfare.	The site has frontage and access to Flint Street (public)	Yes	<i>Flint street is not a major thoroughfare; however this site qualifies to have an access to other than a major thoroughfare based on section 5.13</i>
Usable Open Space for Multiple Dwelling Units <i>(Sec. 3.1.26.D)</i>	Usable Open Space is defined as balconies, courts and yards that are private recreational uses, and no dimension	4 Amenity courtyards, Pool and Hot tub. It appears to be in conformance	Yes	<i>Information provided on sheet 3</i>

Item	Required Code	Proposed	Meets Code	Comments
	is less than 50 ft. 200 sq. ft. per dwelling unit 200 x 250 = 50,000 sq. ft.			
Maximum % of Lot Area Covered (By All Buildings) (Sec. 3.6.2 D)	No Maximum	Building 1: 34, 673 sf Building 2: 35, 964 sf Garage: 44, 012 sf Commercial: 5,578 sf Total 120, 227 sf (35.66%)	Yes	
Building Height (Sec.3.1.26.D) (Sec. 3.27.1.A)	5 stories or 65 ft, whichever is less** ** See Section 3.27.2.A for exceptions and additional requirements to exceed 65 stories	4 stories proposed; Approximately 43 feet	Yes?	Specify the maximum height under site data on sheet 2
Residential portion of this development is subject to conditions and requirements of Section 4.82: Residential Dwellings in TC and TC-1 districts (Ordinance Amendment 18.279)				
Commercial Portion is subject to TC and TC-1 requirements				
Commercial Building Setbacks (Sec 3.1.26 D) and (Sec. 3.27.1.C)				
Non-residential collectors and Local Streets <i>Additional setbacks may also be required by Planning Commission or City Council if deemed necessary for better design or functionality</i>				
Front (Flint Street)	0 ft. minimum 10 ft. maximum *Setback may be increased where necessary to obtain clear vision area for vehicular traffic.	Total length: 55 ft. Maximum setback provided: approx. 15 ft. Length of building exceeding maximum setbacks: 10 ft. (18 %)	Yes?	<u>City Council can approve the increase in setback at intersections where necessary to obtain a clear vision area for vehicular traffic.</u> The phase line indicated on the plans is treated as a property line to calculate setbacks. Provide the maximum setback for the commercial building at northeast corner.
Exterior Side Yard (Novi Road) <i>See 3.27.1.C for waiver conditions for City Council</i>	Commercial building is fronting on Novi Road	10 ft.	Yes	
Side Yard	0 ft. minimum None	10 ft.	Yes	
Rear Yard (Railroad tracks)	0 ft. minimum None	15 ft.	Yes	
Commercial Parking Setback (Sec 3.1.26.D)				
Front Flint Street	20 ft. from ROW	Meets the minimum	Yes	Show the setback distances on plans to verify conformance
Exterior Side Yard (Novi Road)	20 ft. from ROW	Meets the minimum	Yes	

Item	Required Code	Proposed	Meets Code	Comments
Side Yard, west	10 ft.	Unable to determine	Yes	We are treating the phase line as a working property line. Parking is considered to be proposed in side yard. Parking is not allowed in side yard on any non-residential collector. <u>A Zoning Board of Appeals variance is required to allow parking in side yard.</u> <u>City Council can allow for parking setback deviation, if the applicant demonstrates compliance with Sec. 3.6.2.Q.</u>
Rear Yard (Railroad tracks)	10 ft.	5 ft. proposed for a small area. Approximately 7 parking spaces do not meet the minimum	No	
Note To District Standards (Sec 3.6.2)				
Exterior Side Yard Abutting a Street (Sec 3.6.2.C)	All exterior side yards abutting a street shall be provided with a setback equal to front yard.	Exterior side yard along Novi Road.	Yes?	
Minimum lot area and width (Sec 3.6.2.D)	Except where otherwise provided in this ordinance, the minimum lot area and width, maximum percentage of lot coverage shall be determined by the requirements set forth.	Proposed	Yes	Provide the lot boundaries for Commercial area. Will it be a separate parcel?
Distance between buildings (Sec 3.6.2.H&L)	If site abuts a residential zone, buildings must be set back at least 3' for each 1' of building height, but in no case can be less than 20' setback	Does not abut residential zoning	NA	
Wetland/Watercourse Setback (Sec 3.6.2.M)	A setback of 25 ft. from wetlands and from high watermark course shall be maintained	Middle Rouge creek runs through the site	Yes?	Refer to Wetland review letter for more details.
Parking setback screening (Sec 3.6.2.P)	Required parking setback area shall be landscaped per sec 5.5.3.	Front yard parking is not sufficiently screened per Sec 3.27.1 D	No	Refer to landscape review for more details.
Modification of parking setback requirements	The Planning Commission may modify parking	Site plan does not conform with rear parking setback	Yes?	<u>City Council can allow for parking setback deviation, if the applicant</u>

Item	Required Code	Proposed	Meets Code	Comments
(Sec 3.6.2.Q)	setback requirements based on its determination according to Sec 3.6.2.Q.	requirements for commercial building		<u>demonstrates compliance with Sec. 3.6.2.Q. for Commercial building only</u>
The Planning Commission may modify parking setback requirements in those instances where it determines that such modification may result in improved use of the site and/ or in improved landscaping; provided, however, that such modification of the setback requirements does not reduce the total area of setback on a site below the minimum setback area requirements of this Section.				
TC-1 District Required Conditions (Sec 3.27)				
Site Plans (Sec. 3.27.1.A.)	Site area under 5 acres: Requires Planning Commission approval; Site area over 5 acres: Requires City Council approval upon Planning Commission recommendation	Site is over 5 acres (8.73 acres)	Yes	<i>Site plan requires City Council approval upon Planning Commission recommendation.</i>
Parking Setbacks (3.27.1 D)	20 ft. from ROW	Does not meet the minimum required	No	Refer to comments on page 3 and 4
	Surface parking areas must be screened by either a 2.5 ft. brick wall or a semi-transparent screening or a landscaped berm from all public ROW	A 2.5 foot screening wall is proposed	Yes	Sheet L-2 provides the wall detail. The applicant proposes a wall and black anodized aluminum fence.
	No front yard or side yard parking on any non-residential collector.	<u>Commercial:</u> All 49 spaces are proposed in side yard <u>Residential:</u> Of 432 spaces proposed, 38 spaces (9%) are proposed in front yard and 50 (12%) spaces in eastern side yard and 35 spaces (8%) in western side yard.	No	<u>A Zoning Board of Appeals variance is required for proposing parking in front yard and side yard. This can be supported by staff due to smaller depth of the parcel.</u>
Architecture/Pedestrian Orientation (3.27.1 E)	No building in the TC-1 district shall be in excess of one-hundred twenty-five (125) feet in width, unless pedestrian entranceways are provided at least every one-hundred twenty-	This only applies to Commercial building. The building is 101 feet long	Yes	<i>This only applies to Commercial building as the length of the building for residential units is subject to Sec. 4.82</i>

Item	Required Code	Proposed	Meets Code	Comments
	five (125) feet of frontage.			
Open Space (3.27.1 F)	15% (permanently landscaped open areas and pedestrian plazas) Required: 57,041 sq. ft.	4 Amenity courtyards, Pool and Hot tub. It appears to be in conformance Open space: 1.36 acres	Yes	<i>Information provided on sheet 3</i>
Façade materials (Sec. 3.27.1 G)	All sides of the building and accessory buildings must have the same materials. Façade materials may deviate from brick or stone with PC approval.	Section 9 waivers required which are supported by our Façade consultant	Yes	<u>City Council approval of Section 9 waivers is required.</u>
Parking, Loading, Signs, Landscaping, Lighting, Etc (Sec. 3.27.1 H)	All loading in TC-1 shall be in rear yards.	Residential: Side yard Commercial: Rear Yard Flint/Bond Street is considered a front yard	Yes	Loading area cannot be located in side yard. <u>A Zoning Board of Appeals variance is required for proposing loading area in side yard. Staff can support the variance due to smaller depth of the parcel.</u>
	Off-street parking counts can be reduced by the number of on-street parking adjacent to a use	20, on-street parking on Flint street proposed	Yes	
	PC may allow parking requirement reduction when parking areas serve dual functions.	The development proposes mixed uses. However, they are served by separate entrances and are not connected.	Yes	
	Special assessment district for structured park	Not proposed	NA	
Sidewalks required (Sec. 3.27.1 I)	Sidewalks required along non-residential collector to be 12.5 ft. wide	It appears that a 10 feet multi use path proposed south of Flint street 6 feet sidewalk proposed north of Flint Street	Yes	<u>The applicant should consider providing 12.5 feet wide walks along Flint Street.</u> <u>A Zoning Board of Appeals variance would be required</u>

Item	Required Code	Proposed	Meets Code	Comments
	Direct pedestrian access between all buildings and adjacent areas	Appears to be provided.	Yes?	Additional details are required to verify conformance
Bicycle Paths (Sec. 3.27.1 J)	Bike paths required to connect to adjacent residential & non-residential areas.	10 ft. wide asphalt bike path proposed along south side of Flint Street	Yes	
Development amenities (Sec. 3.27.1 L)	All sites must incorporate amenities such as exterior lighting, outdoor furniture, and safety paths in accordance with Town Center Study Area.	The development appears to be proposing sufficient and significant amenities such as pool and interior courtyards; A dog park is proposed in the green space east of proposed detention pond	Yes	Refer to wetlands review for comments on dog park location Proposed fence is approximately 10 feet within the buffer. This is considered a permanent wetland buffer impact; Please indicate the area of the impact; Show the type of vegetation and proposes pet refuse pick up stations on the park.
Combination of use groups within a single structure (Sec. 3.27.1 M) (Sec.3.27.2.B)	7,500 sq. ft. GLA max may exceed when: - All floors above 1 st floor permitted in TC-1 - No retail above 2 nd floor - 2 nd floor retail is less than 12,000 sq. ft. or 25% of the floor area - Single user max. is 15,000 sq. ft. - 50% of retail commercial space on 1 st floor is devoted to users of 5,000 sq. ft. or less	5,578 square feet of commercial space if provided in a separate building within the same site	NA	
Street and Roadway Rights-Of-Way (Sec. 3.27.1 N)	Nonresidential collector and local streets shall provide ROWs consistent with DCS standards	Flint Street realignment is proposed	Yes?	Coordination with Engineering department is required to determine the feasibility of proposed cross-section of Flint Street.
Mixed-Use Developments (Sec. 4.25) To qualify as a mixed-use development, a project must meet the following requirements.				

Item	Required Code	Proposed	Meets Code	Comments
Each use shall comprise of at least 10% in the TC-1 district of either a. The net site area or b. The total gross floor area of all buildings		Gross site area: 8.73 acres Gross site area after ROW taking: 7.73 acres Residential Site Area: 6.67 acres Commercial site area: 1 acre (11.5% of total site area)	Yes?	Phase line is considered the property line for all intent and purposes.
A development with both conventional multi-family and senior, age-qualified, independent multi-family uses shall not be considered mixed use unless a non-residential use is also included		Not applicable	NA	
A performing arts facility unconditionally dedicated to the public use, under separate agreement with the City, shall be considered a second use, provided that it is a fully enclosed structure with a minimum of 500 seats.		Not applicable	NA	
Residential Dwellings / Mixed-Use in TC/TC-1 (Sec. 4.82)				
Multiple-Housing Dwellings Units (Sec. 4.82.2)		Must meet RM-1 district requirements.	Not Applicable	
Mixed Use Guidelines (Sec. 4.82.2)				
Number of Rooms and Area of Parcel (Sec. 4.82.2.a) <i>TC/TC-1, Multiple Family, and Mixed-Use</i>	Total number of rooms shall not have more than the area of the parcel in square feet, divided by a factor of 1200. For mixed use, it is divided by factor of 800.	For 7.74 acres 336, 718 sq. ft. / 800 = 421 rooms Applicant has provided floor plans 146 1 BR @ 2 rooms = 292 93 2 BR @ 3 rooms = 279 14 3 BR@ 4 rooms= 56 Total 627 rooms proposed	No	The proposed number of rooms exceeds the maximum allowed rooms for this site. Refer to Planning review letter for more comments
Allowing increase in number of rooms (Sec. 4.82.2.b)	Planning Commission (for sites <5 acres) or City Council (for sites >5 acres) can approve increase in number of rooms subject to conditions listed in Sec. 4.82.2.b. The increase cannot exceed more than two times the rooms otherwise allowed	Allowed: 421 rooms Proposed: 627 rooms (staff estimated) Increase in rooms in less than two times otherwise allowed	No?	<u>City Council should make the finding for allowing increase in number of rooms</u> Please provide a narrative addressing the items in this section.
Floor plans for Mixed Use developments	Conceptual floor plans layouts for each	Floor plans are provided; 1 BR @ 2 rooms	Yes	<i>Floor plans did not indicate dens or extra</i>

Item	Required Code	Proposed	Meets Code	Comments
(Sec. 4.82.2.c)	dwelling unit is required to establish maximum number of rooms permitted, subject to minor modifications	2 BR @ 3 rooms 3 BR@ 4 rooms Floor plans indicate five styles for 1-BR, 3 styles for 2-BR and 1 style for 3-BR units.		living spaces
Minimum Distance between Buildings (Sec. 4.82.2.d)	10 ft.	129.33 ft.	Yes	
Building Setbacks (Sec. 4.82.2.e)	- 15ft. minimum, unless conflicts with corner clearance - 75 ft, if adjacent to single family	<u>Building 1:</u> Total length: 283 ft. Minimum setback provided: 12.2 ft. Length of building not meeting the minimum setbacks: 12 ft. (4%) <u>Building 2:</u> Total length: 283 ft. Minimum setback provided: 8.1 ft. Length of building not meeting the minimum setbacks: 16 ft. (6%) <u>Parking Structure:</u> Minimum setback provided: 5 ft. Length of building not meeting the minimum setbacks: entire parking structure (approximately 700 ft. long)	No?	The applicant provided an encroachment diagram that indicates insignificant encroachment into front yard setback. <u>A Zoning Board of Appeals variance is required for not meeting the minimum required building setback requirements for the parking garage and the residential units.</u>
Parking Setbacks Off-street Parking (Sec. 4.82.2.f) Residential dwelling are subject to this section, not Sec. 3.1.26.	10 ft. minimum from any wall of any dwelling structure, which contains openings involving living areas;	A minimum of 10 feet is maintained except for parking in front of Building 2. However, floor plans indicate that façade does not include any openings	Yes	
	5 ft. from any wall with no openings	Meets the minimum	Yes	
	10 ft. from any ROW (includes drives and loading)	Meets the minimum from ROW	Yes	
	5 ft. from all other property lines	Meets the minimum for other property lines	Yes	

Item	Required Code	Proposed	Meets Code	Comments
	30 ft. from property lines adjacent to Single family homes	Not applicable	NA	
Business and Office Uses (Sec. 4.82.3)	<ul style="list-style-type: none"> - Not occupy same floor as residential - No office use above a residential use - Separate entrance, private pedestrian entrance to residential shall be provided 	Not applicable	NA	
Parking Location (Sec. 4.82.5)	Off-street parking shall be provided within a building, parking structure physically attached, or designed off-street parking within 300 ft. of building.	Off-street proposed on-street, surface parking and parking structure	Yes	
Open Space (Sec. 4.82.6)	Open space required for each multiple unit has to be met Rooftop open space can be modified	Open space plan is provided and it is in conformance	Yes	

Sec. 4.82.2. Residential Guidelines for Development

Note: Staff has made a determination for mixed use guidelines that is consistent with non-mixed use guidelines. For purpose of determining compliance, the minimum square footages are associated with number of bedroom as follows: 1 BR- 500 SF min; 2 BR- 750 SF min; 3 BR – 750 SF min; 4+ BR- 1,000 SF min ;

The applicant has proposed a mix of 1, 2, and 3 bedroom units. One bed room units range from 603 sf to 864 sf. Two bedroom units range from 944 sf to 1259 sf; 3 br are at 1277 sf. The applicant has provided floor plans.

Maximum Room Count : Mixed Use Guidelines(Sec. 4.82.2)

Efficiency-400	1	Not proposed		See note above
1 BR: 500 sq. ft.	2	2		
2 BR: 750sq. ft.	3	3		
3 BR: 900 sq. ft.	4	4		
4 BR: 1000 sq. ft.	5	Not proposed		

Maximum Density: Mixed Use Guidelines(Sec. 4.82.2)

Efficiency-400	--	Proposed density: 33 DUA Allowable Density: 23 DUA; Allowable density is calculated based on maximum number of	No	Please see Planning review letter for more details. Density for residential dwellings in TC-1 is based on the maximum number of rooms allowed.
1 BR: 500 sq. ft.	27.3 DUA (a)			
2 BR: 750sq. ft.	18.15 DUA			
3 BR: 900 sq. ft.	13.61 DUA			
4 BR: 1000 sq. ft.	10.89 DUA			

Item	Required Code	Proposed	Meets Code	Comments
		rooms allowed for this property (421 rooms)		<u>City Council can approve the increase of maximum number of rooms and thus the increase in density.</u>
Maximum Percentage of Units : Mixed Use Guidelines(Sec. 4.82.2)				
Efficiency-400	5%	Not proposed		<u>A zoning board of appeals variance is required for exceeding the maximum allowable percentage for 1 bedroom units</u>
1 BR: 500 sq. ft.	50%	1 BR @ 146 units : 58 %	No	
2 BR: 750sq. ft.	100%	2 BR @ 93 units : 37 %	Yes	
3 BR: 900 sq. ft.	100%	3 BR @ 14 units : 6 %	Yes	
4 BR: 1000 sq. ft.	100%	Not proposed		
Minimum Off-street parking per unit: Mixed Use Guidelines(Sec. 4.82.2)				
Efficiency-400	1 per unit	146 spaces @ 1 BR 186 spaces @ 2 BR 28 spaces @ 3 BR Total 360 spaces required plus 20% contingency parking Total 432 spaces proposed		
1 BR: 500 sq. ft.	1 per unit		Yes	
2 BR: 750sq. ft.	2 per unit		Yes	
3 BR: 900 sq. ft.	2 per unit		Yes	
4 BR: 1000 sq. ft.	2 per unit			
Parking, Loading, and Dumpster Requirements (5.3 site specific review required)				
Required Parking Calculation (Sec. 5.2.12) (Sec. 4.82.2)	<u>Commercial</u> 1 per 250 sq. ft. of gfa 5,578 / 250 = 23 spaces <u>Residential Development</u> 360 spaces minimum 72 spaces 20% contingency Total of 432 spaces	<u>Commercial</u> 49 spaces Of which, four are dedicated for public parking for cemetery <u>Residential Development</u> 459 spaces 20 On street 270 garage 142 surface parking	Yes	
Parking Space Dimensions and Maneuvering Lanes (Sec. 5.3.2)	- 90° Parking: 9 ft. x 19 ft. - 24 ft. two way drives - 9 ft. x 17 ft. parking spaces allowed as long as detail indicates a 4" curb at these locations - 60° 9 ft. x 18 ft.	- 9 ft. x 17 ft. parking spaces allowed as long as detail indicates a 4" curb at these locations - 60° 9 ft. x 18 ft. - 9 ft. x 18 ft.	No	<u>A Zoning board of appeals variance is required for not meeting the minimum depth requirement for the parking spaces in the garage.</u>
Parking lot entrance offset	Parking lot entrances must be set back 25'	Not applicable	NA	

Item	Required Code	Proposed	Meets Code	Comments
(Sec. 5.3.6)	from any single-family residential district.			
End Islands (Sec. 5.3.12)	<ul style="list-style-type: none"> - End Islands with landscaping and raised curbs are required at the end of all parking bays that about traffic circulation aisles. - The end islands shall generally be at least 8 ft. wide, have an outside radius of 15 ft., and be constructed 3 ft. shorter than the adjacent parking stall 	Appears to be in conformance.	Yes	Refer to traffic review for additional comments.
Parking stall located adjacent to a parking lot entrance (public or private) (Sec. 5.3.13)	- Shall not be located closer than twenty-five (25) feet from the street right-of-way (ROW) line, street easement or sidewalk, whichever is closer	All entrances appear meet the requirements	Yes	
Barrier Free Spaces <i>Barrier Free Code</i> <i>*No deviations since this is a Michigan Building Code requirement</i>	Residential Portion: A total of 2% of 432 required parking = 9 barrier free 49 spaces for retail requires: 2 barrier free (1 van accessible)	<u>Commercial</u> 2 regular barrier free <u>Residential Development</u> 6 barrier free4 regular and 2 van accessible on surface parking lot 6 van accessible in garage Total of 12 barrier free	Yes	
Barrier Free Space Dimensions <i>Barrier Free Code</i>	<ul style="list-style-type: none"> - 8' wide with an 8' wide access aisle for van accessible spaces - 8' wide with a 5' wide access aisle for regular accessible spaces 	Spaces are distributed into five locations with two spaces each	Yes?	Please make sure there is at least one van accessible space for each location
Barrier Free Signs <i>Barrier Free Code</i>	One sign for each accessible parking space.	Signs indicated	Yes	
Minimum number of Bicycle Parking (Sec. 5.16.1)	<u>Multiple-Family:</u> 1 for each 5 dwellings 250/5 = 50 bike spaces	<u>Multiple-Family:</u> Building 1: 20 indoor spaces; 6 outdoor spaces	Yes	

Item	Required Code	Proposed	Meets Code	Comments
	<p><u>Commercial:</u> Five (5) percent of required automobile spaces, min. of 24 spaces = 2 bike spaces Total = 52 bike spaces</p>	<p>Building 2: 20 indoor spaces; 6 outdoor spaces Total 52 spaces <u>Commercial:</u> 2 spaces Total 54 provided</p>		
<p>Bicycle Parking General requirements (Sec. 5.16)</p>	<ul style="list-style-type: none"> - No farther than 120 ft. from the entrance being served - When 4 or more spaces are required for a building with multiple entrances, the spaces shall be provided in multiple locations - Spaces to be paved and the bike rack shall be inverted "U" design - Shall be accessible via 6 ft. paved sidewalk 	<p>Appears to be within 120 ft.</p>	<p>Yes</p>	
<p>Bicycle Parking Lot layout (Sec 5.16.6)</p>	<p>Parking space width: 6 ft. One tier width: 10 ft. Two tier width: 16 ft. Maneuvering lane width: 4 ft. Parking space depth: 2 ft. single, 2 ½ ft. double</p>	<p>Details provided, but are not complete</p>	<p>Yes?</p>	<p>Refer to Traffic review for more details comments about the sidewalk width and indoor bike space dimensional requirements.</p> <p><u>Any deviations from standards should be requested prior to Planning Commission meeting.</u></p>
<p>Loading Space Area (Sec. 5.4.2)</p>	<p>Within TC zoning, loading space shall be provided in the rear yard (or in the interior side yard beyond the side yard setback for double frontage lots) in the ratio of 10 sq. ft. per front foot of building.</p> <p>For 283 feet building, 2830 square feet of loading area is required for residential building</p> <p>For 55 feet long commercial building,</p>	<p><u>Residential:</u> Two spaces measuring approximately 644 square feet is proposed for residential buildings.</p> <p>Approximately 560 square feet of loading space is proposed for commercial space.</p> <p>Loading area is located in the side yard for residential portion.</p> <p><u>Commercial:</u> One space provided in</p>	<p>No</p>	<p><u>Loading area location for residential requires ZBA Variance</u></p> <p><u>Provide the length of the building to calculate the minimum required loading space for residential and commercial buildings. Lack of minimum requires square footage may also require ZBA variances</u></p>

Item	Required Code	Proposed	Meets Code	Comments
	550 square feet of loading area is required	rear yard for commercial portion		
Loading Space Screening (Sec. 5.4.2 B)	Loading area must be screened from view from adjoining properties and from the street.	<u>Residential</u> loading areas are screened adequately. <u>Commercial</u> loading spaces require additional screening.	Yes	
Dumpster Sec 4.19.2.F	<ul style="list-style-type: none"> - Located in rear yard - Attached to the building or no closer than 10 ft. from building if not attached - Not located in parking setback (20 ft.) - Rear lot abuts ROW, 50 ft. setback required. - Away from Barrier free Spaces 	<u>Residential:</u> Dumpsters are located inside the building <u>Commercial:</u> Dumpster located in rear yard. Flint/Bond street frontage is considered front.	Yes	
Dumpster Enclosure Sec. 21-145. (c) Chapter 21 of City Code of Ordinances	<ul style="list-style-type: none"> - Screened from public view - A wall or fence 1 ft. higher than height of refuse bin - And no less than 5 ft. on three sides - Posts or bumpers to protect the screening - Hard surface pad. - Screening Materials: Masonry, wood or evergreen shrubbery 	Located internally within the building	NA	
Lighting and Photometric Plan (Sec. 5.7)				
Intent (Sec. 5.7.1)	Establish appropriate minimum levels, prevent unnecessary glare, reduce spill-over onto adjacent properties & reduce unnecessary transmission of light into the night sky	Proposed	Yes ?	<p>Some of the items as noted do not conform to the code. Please revise accordingly.</p> <p>Most of the information is provided in the response letter. Please include it on the sheets.</p>
Lighting Plan (Sec. 5.7.2 A.i)	Site plan showing location of all existing &			

Item	Required Code	Proposed	Meets Code	Comments
	proposed buildings, landscaping, streets, drives, parking areas & exterior lighting fixtures			
Building Lighting (Sec. 5.7.2.A.iii)	Relevant building elevation drawings showing all fixtures, the portions of the walls to be illuminated, illuminance levels of walls and the aiming points of any remote fixtures.	Not provided	No	Please provide photometric for building lighting with final site plan
Lighting Plan (Sec.5.7.2 A.ii)	Specifications for all proposed & existing lighting fixtures	Mostly provided	Yes	Please add spec sheets to the set
	Photometric data	Mostly provided	Yes?	Provide foot candle values along property line
	Fixture height	10 ft., 12 ft. and 35 ft.	Yes	
	Mounting & design	Wall mounted/pole mounted	Yes	
	Glare control devices	Unable to determine	Yes	Provide spec sheets
	Type & color rendition of lamps	LED	Yes	
	Hours of operation	Site employees 8 am to 6 pm. Building available for 24 hours	Yes	Please provide hours of operation on lighting plan
	Photometric plan illustrating all light sources that impact the subject site, including spill-over information from neighboring properties	Unable to determine	No?	Please provide fc values along property line
Required Conditions (Sec. 5.7.3.A)	Light pole height not to exceed maximum height of zoning district (65 ft. for TC)	Maximum height 25 ft.	Yes	Please include this information on the sheet.
Required Conditions (Sec. 5.7.3.B&G)	- Electrical service to light fixtures shall be placed underground - Flashing light shall not be permitted - Only necessary lighting for security purposes &	Unable to determine	No?	Please add the notes to the plan

Item	Required Code	Proposed	Meets Code	Comments
	limited operations shall be permitted after a site's hours of operation			
Security Lighting (Sec. 5.7.3.H) Lighting for security purposes shall be directed only onto the area to be secured.	- All fixtures shall be located, shielded, and aimed at the areas to be secured. - Fixtures mounted on the building and designed to illuminate the facade are preferred.	Unable to determine	No?	Please provide a photometric plan with just lights intended for security purposes
Average to Minimum light level ratio (Sec.5.7.3.E)	Average light level of the surface being lit to the lowest light of the surface being lit shall not exceed 4:1	Avg/min ratio exceeds 4:1 for east side parking and drive and north parking and drive. The applicant provided an updated photometric via e-mail. Overall site avg/min ratio is 4.8:1	Yes?	Provide Avg/Min for the entire site A ZBA variance is required if the avg/min ratio is not revised to not exceed 4:1
Type of Lamp Fixtures (Sec. 5.7.3.F)	Use of true color rendering lamps such as metal halide is preferred over high & low pressure sodium lamps	LED lighting proposed		
Min. Illumination (Sec. 5.7.3.K)	Parking areas: 0.2 min	0.6 min	Yes	
	Loading & unloading areas: 0.4 min	0.8 min	Yes	
	Walkways: 0.2 min	0.9 min	Yes	
	Building entrances, frequent use: 1.0 min	1.0 min	Yes	
	Building entrances, infrequent use: 0.2 min	0.2 min	Yes	
Max. Illumination adjacent to Non-Residential (Sec. 5.7.3.K)	When site abuts a non-residential district, maximum illumination at the property line shall not exceed 1 foot candle	Foot candles exceed 1 fc south side of Building 1	No	<u>The applicant is seeking a Zoning Board of Appeals variance</u>

Item	Required Code	Proposed	Meets Code	Comments
Cut off Angles (Sec. 5.7.3.L)	When adjacent to residential districts: - All cut off angles of fixtures must be 90° - maximum illumination at the property line shall not exceed 0.5 foot candle	Not adjacent to residential districts	NA	
Building Code and Other Requirements				
Accessory Structures (Sec. 4.19)	- Each accessory building shall meet all setback requirements for the zoning district in which the property is situated - Shall meet the façade ordinance standards	No accessory structures i.e. carports are proposed	NA	
Exterior Building Wall Façade Materials (Sec. 5.15) (Sec. 3.27.1.G)	Façade Region: 1 Primarily brick with materials that complement	Elevation drawings submitted; requires section 9 waivers supported by Doug	Yes?	
Roof top equipment and wall mounted utility equipment Sec. 4.19.2.E.ii	All roof top equipment must be screened and all wall mounted utility equipment must be enclosed and integrated into the design and color of the building	Rooftop equipment is proposed to be hidden behind the parapet.	Yes	Add a note on the plan
Building Code	Building exits must be connected to sidewalk system or parking lot.	Sidewalks illustrated	Yes	
Design and Construction Standards Manual	Land description, Sidwell number (metes and bounds for acreage parcel, lot number(s), Liber, and page for subdivisions).	Mostly provided	Yes	Refer to all reviews for missing information

Item	Required Code	Proposed	Meets Code	Comments
General layout and dimension of proposed physical improvements	Location of all existing and proposed buildings, proposed building heights, building layouts, (floor area in square feet), location of proposed parking and parking layout, streets and drives, and indicate square footage of pavement area (indicate public or private).	Mostly provided;	Yes	Refer to review letters for missing information
Economic Impact	<ul style="list-style-type: none"> - Total cost of the proposed building & site improvements - Number of anticipated jobs created (during construction & after building is occupied, if known) 	Not provided	No	Required prior to Planning Commission meeting
Signage	<ul style="list-style-type: none"> - Signage if proposed requires a permit. - <u>Signage is not regulated by the Planning Commission or Planning Division.</u> 	A monument sign is indicate between the two residential building	NA	<u>Please contact ordinance department for sign permit requirements and process</u>
Property Address	The applicant should contact the Building Division for an address prior to applying for a building permit.	Not required at this time	NA	<u>Submit address application after Final Site Plan approval.</u>
Project and Street Naming Committee	Some projects may need approval from the Street and Project Naming Committee.	Station 6 is not approved; The applicant is requesting a 'The Bond' as the new project names	No?	A meeting is scheduled for June 21 to consider the new name.
Property Split/Combination	The proposed property split must be submitted to the Assessing Department for approval.	Lot combination required	No	Lot split required prior to final site plan approval. Contact Assessing 248-347-0492
Traffic Study (Site Plan and Development Manual)	Traffic Impact Statement Required for more than 105 units	A traffic study was provided and reviewed under separate packet in March 15	Yes	Refer to Traffic review letter dated March 28, 2018
Community Impact	Community Impact	Dated May 10, 2018	Yes	Staff agrees with the findings

Item	Required Code	Proposed	Meets Code	Comments
Statement (Site Plan and Development Manual)	Statement Required for more than 150 units			of the statement. The utility sections are generally fine, but did not use the right factors for the sanitary sewer calculations. It is not of a concern. Please work with Engineering to update the numbers.
Easements	All draft easements are required to be submitted along with electronic stamping sets	Indicate the easement boundaries on final site plan submittal	Yes?	Conservation easement may be required

NOTES:

1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.
2. The section of the applicable ordinance or standard is indicated in parenthesis. Please refer to those sections in Article 3, 4, and 5 of the zoning ordinance for further details.
3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.

ENGINEERING REVIEW



PLAN REVIEW CENTER REPORT

June 4, 2018

Engineering Review

The Bond fka The District (Flint Street)
JSP18-0010

Applicant

Tricap Holdings

Review Type

Preliminary Site Plan

Property Characteristics

- Site Location: West of Novi Road, south of Flint Street
- Site Size: 7.74 acres
- Plan Date: 05/10/2018
- Design Engineer: Seiber Keast Engineering, LLC

Project Summary

- Proposed development including one commercial building at Novi Road and two multi-family apartment buildings with an attached parking deck.
- Water service is available in an existing 8-inch stub in Flint Street just west of Novi Road.
- Sanitary sewer service is available in existing 15-inch sanitary sewer in Flint Street.
- Storm water will be collected on site, with bank full detention provided on site, and discharged via the Walled Lake Branch of the Middle Rouge River to the C&O District regional detention basin.

Recommendation

Approval of the Preliminary Site Plan and Preliminary Storm Water Management Plan is recommended.

Comments:

The Preliminary Site Plan meets the general requirements of the design and construction standards as set forth in Chapter 11 of the City of Novi Codified Ordinance, the Storm Water Management Ordinance and the Engineering Design Manual with the following items to be addressed at the time of Final Site Plan submittal (further engineering detail will be required at the time of the final site plan submittal):

Additional Comments (to be addressed upon Final Site Plan submittal):

General

1. The plan set shall reference at least one city established benchmark. An interactive map of the City's established survey benchmarks can be found under the 'Map Gallery' tab on www.cityofnovi.org.
2. The current site plan reflects the Alternative 1A alignment of Flint Street included in the City's current Capital Improvement Plan (CIP) for repaving and construction of Flint Street between Novi Road and Grand River Avenue for Fiscal Year 2019-2020. The applicant is asked to provide the AutoCAD drawing file showing the road alignment and development to confirm that the right-of-way taking plan aligns with the City's roadway design that is in progress.
3. The Non-domestic User Survey form shall be completed for the non-residential portion of the development and submitted to the City so it can be forwarded to Oakland County. This form was included in the original site plan package.
4. Right-of-way permits will be required from the City of Novi and from Road Commission for Oakland County.
5. Provide a traffic control sign table listing the quantities of each sign type proposed for the development. Provide a note along with the table stating all traffic signage will comply with the current MMUTCD standards.
6. Traffic signs in the RCOC right-of-way will be installed by RCOC.
7. Provide a traffic control plan for the proposed road work activity (City roads).
8. Provide a note that compacted sand backfill shall be provided for all utilities within the influence of paved areas, and illustrate on the profiles.
9. Provide a construction materials table on the Utility Plan listing the quantity and material type for each utility (water, sanitary and storm) being proposed.
10. Provide a construction materials table on the Paving Plan listing the quantity and material type for each pavement cross-section being proposed.
11. Provide a utility crossing table indicating that at least 18-inch vertical clearance will be provided; or that additional bedding measures will be utilized at points of conflict where adequate clearance cannot be maintained.
12. Provide a note stating if dewatering is anticipated or encountered during construction a dewatering plan must be submitted to the Engineering Department for review.
13. Generally, all proposed trees shall remain outside utility easements. Where

- proposed trees are required within a utility easement, the trees shall maintain a minimum 5-foot horizontal separation distance from any existing or proposed utility. All utilities shall be shown on the landscape plan, or other appropriate sheet, to confirm the separation distance.
14. Soil borings shall be provided for a preliminary review of the constructability of the proposed development (roads, basin, etc.). Borings identifying soil types, and groundwater elevation should be provided with the site plan submittal.
 15. The standard detail sheets are not required with Final Site Plan submittal. Include the City's standard detail sheets for water main (5 sheets-rev. 02/16/2018), sanitary sewer (3 sheets- rev. 02/16/2018), storm sewer (2 sheets-rev. 02/16/2018), and paving (2 sheets-rev. 03/05/2018) in the printed Stamping Set submittal. These details can be found on the City's website at this location: <http://cityofnovi.org/Government/City-Services/Public-Services/Engineering-Division/Engineering-Standards-and-Construction-Details.aspx>
 16. A letter from either the applicant or the applicant's engineer must be submitted with the Final Site Plan submittal highlighting the changes made to the plans addressing each of the comments in this review.

Water Main

17. Show the locations of separate domestic and fire leads for each building with a unique shut-off valve for each.
18. Note that a tapping sleeve, valve and well will be provided at the connection to the existing water main.
19. Provide a profile for all proposed water main 8-inch and larger.
20. The water main stub at the west end of the development shall terminate with a hydrant followed by a valve in well.
21. Provide three (3) signed and sealed sets of utility plans along with the MDEQ permit application (1/07 rev.) for water main construction. The Streamlined Water Main Permit Checklist should be submitted to the Engineering Division for review, assuming no further design changes are anticipated. Utility plan sets shall include only the cover sheet, any applicable utility sheets and the standard detail sheets.

Sanitary Sewer

22. Provide a sanitary sewer monitoring manhole, unique to the commercial building, within a dedicated access easement or within the road right-of-way. If not in the right-of-way, provide a 20-foot wide access easement to the monitoring manhole from the right-of-way (rather than a public sanitary sewer easement).
23. Provide a note on the construction materials table that 6-inch sanitary leads shall be a minimum SDR 23.5.
24. Provide a note on the Utility Plan stating that sanitary leads shall be buried at least 5 feet deep where under the influence of pavement.
25. Indicate the invert elevation at the building for each sanitary sewer lead.

26. Include a sanitary sewer basis of design on the utility plan, using the attached Sewer Unit Factor chart. These unit factors should also be referenced in the waste water section of the Community Impact Statement.

Storm Sewer

27. A minimum cover depth of 3 feet shall be maintained over all storm sewers.
28. Provide storm sewer sizing calculations.
29. Provide storm sewer profiles with the 10-year HGL shown, and ensure the HGL remains at least 1-foot below the rim of each structure.
30. Provide a schedule listing the casting type and other relevant information for each proposed storm structure on the utility plan. Round castings shall be provided on all catch basins except curb inlet structures.
31. Show and label all roof conductors, and show where they will tie into the storm sewer system on the layout and on the profile.

Storm Water Management Plan

32. An adequate maintenance access route to the basin outlet structure and any other pretreatment structures shall be provided (15 feet wide, maximum slope of 1V:5H, and able to withstand the passage of heavy equipment). Verify the access route does not conflict with proposed landscaping.
33. Provide a 5-foot wide stone bridge allowing direct access to the standpipe from the bank of the basin during high-water conditions (i.e. stone 6-inches above high water elevation). Provide a detail and/or note as necessary.
34. A 4-foot wide safety shelf is required one-foot below the permanent water surface elevation within the basin.
35. A 25-foot vegetated buffer shall be provided around the perimeter of each storm water basin, or submit a request for variance from the Design & Construction Standards where the 25-foot buffer cannot be achieved around the storm water basin.
36. Provide an access easement for maintenance over the storm water detention system and the pretreatment structure. Also, include an access easement to the detention area from the public road right-of-way. These easements should be shown on the storm water management plan.
37. Provide a soil boring in the vicinity of the proposed underground detention system to determine bearing capacity and the high water elevation of the groundwater table.
38. The underground storage system shall include 4-foot diameter manholes at one end of each row for maintenance access.
39. Provide critical elevations (low water, first flush, bank full and pavement elevation) of the detention system on the underground detention system cross-section. Insure there is at least 1 ft. of freeboard between the 100-year elevation and the subgrade elevation under the pavement.
40. The underground detention system shall be kept outside the influence of any planting areas.

41. Restricted discharge to an off-site regional detention basin is proposed. Storm water tap fee amount will be determined with pre-construction checklists.

Paving & Grading

42. Refer to standard paving details and remove any redundant or conflicting details from the plan set.
43. Provide a site grading plan. Site grading shall be limited to 1V:4H (25-percent), excluding landscaping berms.
44. Curbing and walks adjacent to the end of 17-foot stalls shall be reduced to 4-inches high, rather than the standard 6-inch height to be provided adjacent to 19-foot stalls. Provide additional details as appropriate.

Soil Erosion and Sediment Control

45. A SESC permit is required. The review checklist detailing all SESC requirements is attached to this letter. An informal review will be completed with the Final Site Plan if SESC plans are included in the submittal.

Off-Site Easements

46. Any off-site utility easements anticipated must be executed **prior to final approval of the plans.**

The following must be submitted at the time of Final Site Plan submittal:

47. An itemized construction cost estimate must be submitted to the Community Development Department at the time of Final Site Plan submittal for the determination of plan review and construction inspection fees. This estimate should only include the civil site work and not any costs associated with construction of the building or any demolition work. **The cost estimate must be itemized** for each utility (water, sanitary, storm sewer), on-site paving, right-of-way paving (including proposed right-of-way), grading, and the storm water basin (basin construction, control structure, pretreatment structure and restoration).
48. Draft copies of any off-site easements, a recent title search, and legal escrow funds must be submitted to the Community Development Department for review and approved by the Engineering Division and the City Attorney prior to being executed.
49. A letter from either the applicant or the applicant's engineer must be submitted with the Final Site Plan submittal highlighting the changes made to the plans addressing each of the comments in this review.

The following must be submitted at the time of Stamping Set submittal:

50. A draft copy of the maintenance agreement for the storm water facilities, as outlined in the Storm Water Management Ordinance, must be submitted to the Community Development Department. Once the form of the agreement is approved, this agreement must be approved by City Council and shall be recorded in the office of the Oakland County Register of Deeds.

51. A draft copy of the 20-foot wide easement for the water main to be constructed on the site must be submitted to the Community Development Department.
52. If required, a draft copy of the 20-foot wide access easement for the sanitary sewer monitoring manhole must be submitted to the Community Development Department.
53. If required, executed copies of any required off-site utility easements must be submitted to the Community Development Department.

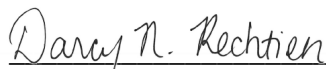
The following must be addressed prior to construction:

54. A pre-construction meeting shall be required prior to the commencement of any site work. Please contact Sarah Marchioni in the Community Development Department to setup a meeting (248-347-0430).
55. A City of Novi Grading Permit will be required prior to any grading on the site. This permit will be issued at the pre-construction meeting. There is no application or fee for this permit.
56. A Soil Erosion Control Permit must be obtained from the City of Novi. Contact Sarah Marchioni in the Community Development Department (248-347-0430) for forms and information.
57. An NPDES permit must be obtained from the MDEQ if disturbed area is over 5 acres in size. The MDEQ requires an approved plan to be submitted with the Notice of Coverage.
58. A permit for work within the right-of-way of Flint Street and Novi Road must be obtained from the City of Novi. The application is available from the City Engineering Division and should be filed at the time of Final Site Plan submittal. Please contact the Engineering Division at 248-347-0454 for further information.
59. A permit for work within the right-of-way of Novi Road must be obtained from the Road Commission for Oakland County. Please contact the RCOC (248-858-4835) directly with any questions. The applicant must forward a copy of this permit to the City. Provide a note on the plans indicating all work within the right-of-way will be constructed in accordance with the Road Commission for Oakland County standards.
60. A permit for water main construction must be obtained from the MDEQ. This permit application must be submitted through the Water and Sewer Senior Manager after the water main plans have been approved.
61. Construction Inspection Fees, to be determined once the construction cost estimate is submitted, must be paid prior to the pre-construction meeting.
62. Restricted discharge into a regional detention basin is planned for this site. Therefore, a storm water tap fee will be required prior to the pre-construction

- meeting. An exact figure will be determined at the time of Final Site Plan approval.
63. A storm water performance guarantee, equal to 1.2 times the amount required to complete storm water management and facilities as specified in the Storm Water Management Ordinance, must be posted with Community Development.
 64. An incomplete site work performance guarantee, equal to 1.2 times the amount required to complete the residential development (excluding the storm water detention facilities) as specified in the Performance Guarantee Ordinance, must be posted with Community Development.
 65. A street sign financial guarantee in an amount to be determined (\$400 per traffic control sign proposed) must be posted with Community Development.

To the extent this review letter addresses items and requirements that require the approval of or a permit from an agency or entity other than the City, this review shall not be considered an indication or statement that such approvals or permits will be issued.

Please contact Darcy Rechtien at (248) 735-5695 with any questions.



Darcy N. Rechtien, P.E.

cc: Theresa Bridges, Engineering
George Melistas, Engineering
Sri Komaragiri, Community Development

LANDSCAPE REVIEW



PLAN REVIEW CENTER REPORT

June 19, 2018

Revised Preliminary Site Plan - Landscaping The Bond

Review Type

Revised Preliminary Landscape Review

Property Characteristics

- Site Location: West side of Flint Street
- Site Acreage: 8.2 acres
- Site Zoning: TC-1
- Adjacent Zoning: North, East: TC-1; South, West: I-1
- Plan Date: 6/11/2018

Ordinance Considerations

This project was reviewed for conformance with Chapter 37: Woodland Protection, Zoning Article 5.5 Landscape Standards, the Landscape Design Manual and any other applicable provisions of the Zoning Ordinance. Items in **bold** below must be addressed and incorporated as part of the revised Preliminary/Final Site Plan submittal. Please follow guidelines of the Zoning Ordinance and Landscape Design Guidelines. This review and the accompanying Landscape Chart is a summary and not intended to substitute for any Ordinance.

Recommendation:

The project is **recommended for approval**. There are a number of waivers required, but the applicant has worked to eliminate many and reduce the impact of others to the point where the waivers can be supported. The remaining changes can be made on Final Site Plans.

Landscape Waivers Required:

1. Lack of berm or alternate screening between Residential and Industrial property/railroad to west – *Supported by staff because they have provided a line of arborvitae along the property line to soften the view toward the railroad tracks and industrial site beyond.*
2. Deficiency in required greenbelt between right-of-way and parking areas – *supported by staff because they have increased the greenbelt, have provided the required brick wall along the road, and have increased the landscaping in the areas with a greenbelt of only 10 feet.*
3. Deficiency in number of multifamily unit trees provided – *supported by staff because they have decreased the deficiency to only 20 trees (14%) (less with the requested plantings in the dog park) and have otherwise landscaped the site quite heavily.*
4. Deficiency in foundation landscaping coverage around parking deck – *supported by staff because the only place available for significant foundation landscaping is along the southwest side, toward the railroad. Large arborvitae have been planted in that location so the landscaping should be sufficient.*
5. Deficiency in number of parking lot access way perimeter trees provided (1). *This is supported by staff because the fire access lane (grass pavers) does not provide room for the missing tree.*
6. *No trees provided around temporary gravel cemetery parking lot on Commercial lot. This is supported by staff because the parking is provided as a benefit to the city and the*

trees would have to be removed or would be heavily impacted by any construction of the permanent commercial project on that lot.

A list of these requested waivers has been provided. The applicant is asked to revise that list with the next submittal to match these waivers required.

Ordinance Considerations

Existing Soils (Preliminary Site Plan checklist #10, #17)

Provided

Existing and proposed overhead and underground utilities, including hydrants.(LDM 2.e.(4))

1. Provided.
2. No overhead utility lines will remain in the vicinity of the project.

Existing Trees (Sec 37 Woodland Protection, Preliminary Site Plan checklist #17 and LDM 2.3 (2))

1. A tree survey is provided and woodland replacement calculations are provided.
2. No replacements will be planted on the site. A deposit to the tree fund will be made for all required replacements.

Stream Protection

1. **Please provide protection for stream and its buffers for during the construction process and afterward.**
2. **Please provide some means of keeping dog feces and other runoff from flowing directly into the adjacent stream.**

Residential Adjacent to Non-Residential - Buffer (Zoning Sec. 5.5.3.B.ii and iii)

1. Property abuts railroad which is backed by I-1, an active CVS warehouse with frequent large truck traffic.
2. As that property has been developed with an industrial use, the requirement for a 10-15 foot tall landscaped berm falls on the residential property.
3. A line of green giant arborvitae has been added to provide a visual buffer between the multi-family site and the railroad/industrial site beyond.
4. A landscape waiver for the lack of the berm is required. *It is supported by staff.*

Adjacent to Public Rights-of-Way – Berm (Wall) & Buffer (Zoning Sec. 5.5.3.B.ii and iii)

1. No berm along the right-of-way is required in the TC-1 district, but a 2.5' tall brick wall or decorative fence with brick pilasters is required between the parking areas and the right-of-way. A wall is provided along most of that frontage. It needs to be extended by about 15' toward the path leading north from Building 2.
2. A 20-foot deep greenbelt is required between the right-of-way and parking areas. Most of those frontages do not have the required 20 feet greenbelt. A landscape waiver is required for the lack of greenbelt depth. *As the applicant has increased the distance between the parking lot and the right-of-way to no less than 10 feet, and has added dense landscaping in the areas with the narrowest greenbelt, this waiver request is supported by staff.*

Street Tree Requirements (Zoning Sec. 5.5.3.E.i.c and LDM 1.d.)

Street trees are not required along the right-of-way in the TC-1 district.

Multi-family Unit Trees and Interior Street Trees (Sec 5.5.3.F.ii.b(1) and (2).

1. Based on 49 ground-level dwelling units, 147 deciduous canopy or large evergreen trees are required on the site.
2. Including the parking lot trees, perimeter trees, arborvitae along the parking deck, and evergreens near the detention basin, 127 trees are provided.

3. The applicant is asked to add at least 3 canopy trees in the dog park to provide shade.
4. A landscape waiver is required for the deficiency in trees provided. *It is supported by staff because the site is very heavily landscaped and adding more trees than requested would negatively impact the other trees on the site.*
5. Based on the length of the central driveway, three interior street trees are required along the central drive entrance. Two are provided, including a greenbelt tree that could be double-counted as a perimeter tree. A landscape waiver is required for the missing tree. *It is supported by staff as the missing tree would be located where the grass pavers for the fire access lane are located.*

Parking Lot Landscaping (Zoning Sec. 5.5.3.C.)

Multifamily:

Based on the vehicular use areas, 3919 sf of island area and 20 trees are required. 4,123 sf of islands and 20 trees are provided.

Commercial:

Based on the vehicular use areas, 1332 sf of island area and 7 trees are required. 1,579 sf of island area and 7 trees are provided.

Parking Lot Perimeter Canopy Trees (Zoning Sec. 5.5.3.C.(3) Chart footnote)

Multifamily:

1. As noted above, parking lot trees in addition to site landscaping trees are not required. The site landscaping trees can be used to fulfill the requirements for interior trees and perimeter canopy trees.
2. Based on the 975 linear feet of parking lot perimeter 28 perimeter trees are required and are provided. There is actually not enough room for the 4 perimeter trees proposed along the west property line at the north corner of the property due to the lack of space allowed, and the vehicular overhang. **That area needs to be expanded so the trees can be planted in a situation where they will survive. If there is a reason why they can't be located there, please give that reason in your response letter.**
3. Greenbelt canopy trees within 15 feet of the parking lot edge can be double-counted as perimeter trees. If this option is used, please note it in the calculations and clearly show which trees are being double-counted.

Commercial:

Based on the 381 linear feet of perimeter, 11 trees are required. 11 trees, including 7 existing trees on the western edge, are provided.

Loading Zone screening (Zoning Sec. 3.14, 3.15, 4.55, 4.56, 5.5)

The commercial loading zone is sufficiently screened from the cemetery by a double row of evergreens.

Building Foundation Landscape (Zoning Sec 5.5.3.D.)

Multifamily:

Greater than 35% of required foundation landscaping facing the road is provided for both buildings.

Commercial:

1. Based on the building perimeter, 2632 sf of foundation landscaping is required and 2653 sf are provided.
2. At least 60% of the building frontages facing public roads must be landscaped. 118/160 (74%) is landscaped.
3. At least 75% of the total building perimeter should be landscaped with at least a 4 foot wide strip of landscaping area. About 80% of the building's 5851f foundation has landscaping, including the evergreens between the building and the cemetery.
4. **Please add some planters on the paved area between the parking lot and the building to soften up the look of the building from the parking lot.**

Plant List (LDM 2.h. and t.)

Provided

Planting Notations and Details (LDM)

Provided.

Storm Basin Landscape (Zoning Sec 5.5.3.E.iv and LDM 1.d.(3))

Provided

Irrigation (LDM 1.a.(1)(e) and 2.s)

1. The proposed landscaping must be provided with sufficient water to become established and survive over the long term. Please note how this will be accomplished if an irrigation plan is not provided.
2. Per the Road Commission for Oakland County, no underground irrigation system may be installed in the Novi Road right-of-way.

Proposed topography. 2' contour minimum (LDM 2.e.(1))

Provided

Snow Deposit (LDM.2.q.)

1. Provided.
2. **Please be sure that the proposed snow deposit areas are consistent between plans, and that they will not be in positions that will harm the landscaping.**
3. **All curbing needs to be front-faced, versus mountable.**

Proposed trees to be saved (Sec 37 Woodland Protection 37-9, LDM 2.e.(1))

Tree fencing is provided around all trees to be saved near areas of disturbance.

Corner Clearance (Zoning Sec 5.9)

Provided

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5621 or rmeader@cityofnovi.org.



Rick Meader – Landscape Architect

LANDSCAPE REVIEW SUMMARY CHART – Revised Preliminary Site Plan

Review Date: June 19, 2018
Project Name: JSP18 – 0010: The Bond fka The District
Plan Date: June 11, 2018
Prepared by: Rick Meader, Landscape Architect E-mail: rmeader@cityofnovi.org;
 Phone: (248) 735-5621

Items in **Bold** need to be addressed by the applicant before approval of the Preliminary Site Plan.
Underlined items need to be addressed for Final Site Plan.

Landscape Waivers Required – all supported by staff:

1. Waiver to not provide the required 10-15 foot tall landscaped buffer between residential and industrial uses.
2. A landscape waiver for the areas with greenbelt depth less than required.
3. A landscape waiver for a deficiency in number of multifamily unit trees provided.
4. A landscape waiver to not provide 1 tree due to the grass pavers provided for fire access along the center entry.
5. A landscape waiver for the deficiency in landscape area along the parking deck foundation.
6. A landscape waiver for not providing the required perimeter trees around the temporary cemetery parking area in the commercial section.

Item	Required	Proposed	Meets Code	Comments
Landscape Plan Requirements – Basic Information (LDM (2))				
Landscape Plan <i>(Zoning Sec 5.5.2, LDM 2.e)</i>	<ul style="list-style-type: none"> ▪ New commercial or residential developments ▪ Addition to existing building greater than 25% increase in overall footage or 400 SF whichever is less. ▪ 1" -20' minimum with proper North. Variations from this scale can be approved by LA 	Scale 1" =40'	Yes	
Owner/Developer Contact Information <i>(LDM 2.a.)</i>	Name, address and telephone number of the owner and developer or association	Yes	Yes	
Landscape Architect contact information <i>(LDM 2.b.)</i>	Name, Address and telephone number of RLA	Yes	Yes	
Survey information <i>(LDM 2.c.)</i>	Legal description or boundary line survey	<ul style="list-style-type: none"> • Legal description on Cover Sheet • No existing topo is provided. 	No	Please provide a topographic survey/ existing conditions sheet.
Project Information <i>(LDM 2.d.)</i>	Name and Address	Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
Sealed by LA. (LDM 2.g.)	Requires original signature	Copy of seal and signature		<u>Provide on Final Site Plans.</u>
Miss Dig Note (800) 482-7171 (LDM.3.a.(8))	Show on all plan sheets	Yes	Yes	
EXISTING CONDITIONS				
Existing plant material Existing woodlands or wetlands (LDM 2.e.(2))	<ul style="list-style-type: none"> ▪ Show location type and size. ▪ Label to be saved or removed. ▪ Plan shall state if none exists. 	Sheet L-4	Yes	See ECT review for detailed discussion of woodlands/wetlands.
Stream protection				<ol style="list-style-type: none"> 1. Please be sure that proper buffers and protection for stream are provided during construction and afterward. 2. Due to its location next to the stream, the dog park has a high potential for polluting it. Please provide some sort of protection to keep dog feces from being washed into the stream. A small berm is one option.
Soil type (LDM.2.r.)	As determined by Soils survey of Oakland county	Sheet 2	Yes	
Zoning (LDM 2.f.)	Site: TC-1 North, East, Southeast: TC-1 South, West: RR, I-1	Site: TC-1 East, North: TC-1 West, South: RR, I-1	Yes	
PROPOSED IMPROVEMENTS				
Existing and proposed improvements (LDM 2.e.(4))	Existing and proposed buildings, easements, parking spaces, vehicular use areas, and R.O.W	Yes	Yes	
Existing and proposed utilities (LDM 2.e.(4))	Overhead and underground utilities, including hydrants	<ul style="list-style-type: none"> • Storm and water are shown • Overhead line is shown as being removed. • Light poles are shown on landscape plan 	Yes	Commercial section's detention system has been re-aligned out of the southwest corner.

Item	Required	Proposed	Meets Code	Comments
Proposed topography - 2' contour minimum (LDM 2.e.(1))	Provide proposed contours at 2' interval	Detention pond contours shown	Yes	
Clear Zones (LDM 2.e.(5))	25 ft. corner clearance required. Refer to Zoning Sec 5.5.9	Yes	Yes	
LANDSCAPING REQUIREMENTS				
Berms and ROW Planting				
<ul style="list-style-type: none"> ▪ All berms shall have a maximum slope of 33%. Gradual slopes are encouraged. Show 1ft. contours ▪ Berm should be located on lot line except in conflict with utilities. ▪ Berms should be constructed with 6" of top soil. 				
Residential Adjacent to Non-residential (Sec 5.5.3.A) & (LDM 1.a)				
Berm requirements (Zoning Sec 5.5.A)	<u>Residential adjacent to I-1 requires:</u> <ul style="list-style-type: none"> • 10-15 foot high landscaped berm with 6 foot wide crest. • Opacity 80% winter, 90% summer. 	A long row of green giant arborvitae is proposed along the southwest side of the parking deck.	No, but see comments.	<p>7. The row of green giant arborvitae will provide some visual buffering between the site and the railroad/industrial site. <i>While this alternative still requires a landscape waiver it is supported by staff.</i></p> <p>8. Please verify the cemetery property line and be sure that there are no graves on the commercial lot. If there are, work with the city on the issue.</p> <p>9. A second row of junipers has been added between the building and the cemetery to provide additional buffering for the cemetery. This is appreciated.</p>
Planting requirements (LDM 1.a.)	LDM Novi Street Tree List			See above.
Adjacent to Public Rights-of-Way (Sec 5.5.B) and (LDM 1.b)				
ROW Landscape Screening Requirements Chart (Sec 5.5.3.B. ii)				
Greenbelt width (2)(3) (5)	<ul style="list-style-type: none"> • Adjacent to parking: 20 ft • Not adjacent to parking: 0 ft 	<ul style="list-style-type: none"> • The minimum greenbelt width between the property line and parking has been increased 	No	<p>1. A landscape waiver is required for the areas with greenbelt depth less than required.</p> <p>2. The increased area,</p>

Item	Required	Proposed	Meets Code	Comments
		to at least 10 feet. <ul style="list-style-type: none"> Landscaping and a brick wall help to create greater visual separation in those narrow areas. 		<i>brick walls and landscaping provide sufficient screening from Bond Street where the greenbelt is less than 20 feet so the waiver request is supported by staff.</i>
Min. berm crest width	No berm is required in TC-1	None	Yes	
Min. berm height (9)	No berm is required in TC-1	None	Yes	
3' wall (Zoning section 3.27.D)	In the TC-1 district, an ornamental 2.5' brick wall or decorative fence with brick pilasters is required between surface parking lots and public rights-of-way.	A 2.5' wall is indicated along much of the frontage on Sheets 2 -4.	Mostly	<ol style="list-style-type: none"> Please extend the wall shown on Sheet L-1 northwest of Building 2 approximately 15 feet to the east, toward the path, to completely screen the parking area. Please add a 15 foot wall long west of the northern driveway to screen the northwestern parking bay from the road. In areas of conflict with utilities, dense shrubs can be used to fill the gaps in screening.
Canopy deciduous or large evergreen trees Notes (1) (10)	<p><u>Residential:</u></p> <ul style="list-style-type: none"> Adjacent to parking: 1/25 lf* 600 lf/25 = 25 trees Not adjacent to parking: 1/30lf* 430 lf/30 = 14 trees Total of 39 trees <p><u>Commercial:</u> <u>Flint/Bond Street</u></p> <ul style="list-style-type: none"> Adjacent to parking: 1/25 lf* (70-28)lf/25 = 2 trees Not adjacent to parking: 1/30lf* 72 lf/30 = 2 trees Total of 4 trees <p><u>Novi Road</u></p>	<p><u>Residential:</u> 19 trees</p> <p><u>Commercial:</u> Flint Street: 1 tree (if the P tree in the drive is reclassified as a greenbelt tree) Novi Road: 0 trees</p> <p>Note: Part of the greenbelt requirement for both sections was met with subcanopy trees (see below)</p>	No	<p>*Only large canopy tree or subcanopy tree requirement needs to be met in TC-1 district, not both.</p> <ol style="list-style-type: none"> Greenbelt trees can be double-counted perimeter trees if they are within 15 feet of the curb. If desired, some of the perimeter trees provided can be used as greenbelt trees to reduce the total number of greenbelt trees that need to be

Item	Required	Proposed	Meets Code	Comments
	<ul style="list-style-type: none"> • Not adjacent to parking: 1/20lf* • 115 lf/20 = 6 trees • Total of 7 trees 			<p>provided.</p> <p>Residential:</p> <ol style="list-style-type: none"> 1. If the double-counting is done as allowed, the total frontage, in conjunction with the subcanopy trees provided, has 4 extra canopy trees or 6 extra subcanopy trees. 2. Please clearly show greenbelt/perimeter canopy trees being double-counted. <p>Commercial:</p> <ol style="list-style-type: none"> 1. If the parking lot tree shown in the Flint Street greenbelt is changed to a greenbelt tree, and the double-counting is done as allowed, the total frontage, in conjunction with the subcanopy trees provided, has sufficient trees. 2. Please clearly show greenbelt/perimeter canopy trees being double-counted.
<p>Sub-canopy deciduous trees Notes (2)(10)</p>	<p><u>Residential:</u></p> <ul style="list-style-type: none"> • Adjacent to parking: 1/15 lf* • 600 lf/15 = 40 trees • Not adjacent to parking: 1/20lf* • 430 lf/20 = 22 trees • Total of 62 trees <p><u>Commercial:</u> <u>Flint/Bond Street</u></p> <ul style="list-style-type: none"> • Adjacent to parking: 1/15 lf* • 42 lf/15 = 3 trees • Not adjacent to parking: 1/20lf* • 72 lf/20 = 4 trees • Total of 7 trees <p><u>Novi Road</u></p>	<p><u>Residential:</u> 26 trees</p> <p><u>Commercial:</u> Flint Street: 3 trees Novi Road: 6 trees</p>	<p>Yes</p>	<p>See discussion above</p>

Item	Required	Proposed	Meets Code	Comments
	<ul style="list-style-type: none"> Not adjacent to parking: 1/20lf* 115 lf/20 = 6 trees 			
Canopy deciduous trees in area between sidewalk and curb	Not required in TC-1.	None	Yes	
Multi-Family Residential Zoning Sec 5.5.3.F.ii & LDM 1.d (2)				
Building Landscaping (Zoning Sec 5.5.3.F.ii.)	<ul style="list-style-type: none"> 3 deciduous canopy trees or large evergreen trees per dwelling unit on the first floor. 49 * 3 = 147 trees 	<ul style="list-style-type: none"> Site trees, including those used in and around parking lots and along parking deck, but not including greenbelt trees, total 127 trees provided. A landscape waiver for 20 trees is required as proposed. 	No	<ol style="list-style-type: none"> Please add at least 3 trees within the dog park to provide shading for the park, As the site is heavily landscaped and there is little room for more trees without overcrowding the trees that are provided, the waiver for 17 or fewer trees is supported by staff.
Interior Street Landscaping	<ul style="list-style-type: none"> 1 deciduous canopy tree along interior roads for every 35 lf (both sides), excluding driveways, interior roads adjacent to public rights-of-way and parking entry drives. 55*2 (central drive)/35 = 3 trees 	2 trees (one of the greenbelt trees at the center entry can be double-counted as a perimeter tree).	Yes	<ol style="list-style-type: none"> A landscape waiver is required to not provide 1 tree due to the grass pavers provided for fire access along the center entry. This waiver request is supported by staff.
Foundation Landscaping	<u>Multifamily:</u> <ul style="list-style-type: none"> 35% of building façade fronts in multi-family section should be landscaped 	All frontages exceed the 35% minimum landscaping requirement.	Yes	
Foundation Landscaping – not Multi-family (Sec. 5.5.3.D)				
<u>Parking Deck & Commercial Building:</u>	<u>Parking Deck:</u> <ul style="list-style-type: none"> 8 x west edge of parking deck (870-2*45)*8 = 6240 SF) <u>Commercial:</u> <ul style="list-style-type: none"> Required area = bldg. perimeter x 8 ft Minimum width of landscape area = 4 ft Entire building, less paved access points, shall be 	<u>Parking Deck:</u> <ul style="list-style-type: none"> 3938 SF A line of 61 green giant arborvitae is provided along the southwest edge of the deck and landscape areas are also proposed at 	<u>Deck:</u> No <u>Commercial:</u> Yes	<u>Parking Deck:</u> <ol style="list-style-type: none"> A landscape waiver is required for the deficiency in landscape area along the parking deck foundation. This waiver request is supported by staff as the only possible area for landscaping is along the

Item	Required	Proposed	Meets Code	Comments
	landscaped.(329-28-6*5) = 271*8=2168 SF ▪ Minimum of 60% of building frontage facing roads shall be landscaped.	both ends of the deck. <u>Commercial:</u> <ul style="list-style-type: none"> • 2,653 SF • 100 of 580 (17%) of total building perimeter is not landscaped, but the double-row of evergreen shrubs across the loading zone screens the building foundation from the cemetery and Novi Road. • Over 60% of building facing public roads is landscaped. 		southwest side of the deck where the line of arborvitae is provided. <u>Commercial:</u> Please provide planters at a minimum along the west side of the building to soften appearance of the building from the parking lot.
Parking Area Landscape Requirements (LDM 1.c. & Calculations (LDM 2.o.))				
General requirements (LDM 1.c)	<ul style="list-style-type: none"> ▪ Clear sight distance within parking islands ▪ No evergreen trees 	Yes	Yes	
Name, type and number of ground cover (LDM 1.c.(5))	As proposed on planting islands	Sod is proposed	Yes	
General (Zoning Sec 5.5.3.C.ii)				
Parking lot Islands (a, b. i)	<ul style="list-style-type: none"> ▪ A minimum of 200 SF to qualify ▪ 200sf landscape space per tree planted in island. ▪ 6" curbs ▪ Islands minimum width 10' BOC to BOC 	Yes	Yes/No	1. Mountable curbs are not allowed except for the fire access lane. 2. Please replace all other proposed mountable curbs with straight-face curbs.
Curbs and Parking stall reduction (c)	Parking stall can be reduced to 17' with 4" curb adjacent to a sidewalk of minimum 7 ft.	Yes	Yes	
Contiguous space limit (i)	Maximum of 15 contiguous spaces	No bay is greater than 15 spaces.	Yes	
Category 1: For OS-1, OS-2, OSC, OST, B-1, B-2, B-3, NCC, EXPO, FS, TC, TC-1, RC, Special Land Use or non-residential use in any R district (Zoning Sec 5.5.3.C.iii)				

Item	Required	Proposed	Meets Code	Comments
A = Total square footage of vehicular use areas x 7.5%	<u>Multifamily:</u> ■ A = 50000 SF x 7.5% = 3750 sf <u>Commercial:</u> ■ 17757 * 7.5% = 1332 sf			1. The parking deck is being treated for review as a building, not parking. 2. The endcap island at the southern corner of the commercial building needs to be at least 200sf in area and have a deciduous canopy tree planted in it.
B = Total square footage of additional paved vehicular use areas over 50,000 SF x 1 %	<u>Multifamily:</u> ■ B = (66925-50000)SF x 1% = 169 sf	TBD	TBD	See above.
All Categories				
C = A+B Total square footage of landscaped islands	<ul style="list-style-type: none"> • A + B = x SF <u>Multifamily:</u> <ul style="list-style-type: none"> • 3750 + 169 = 3919 sf <u>Commercial:</u> <ul style="list-style-type: none"> • 1332 sf 	<u>Multifamily:</u> 4123 sf <u>Commercial:</u> 1579 sf	Yes for both	
D = C/200 Number of canopy trees required	<ul style="list-style-type: none"> • $x/200 = y$ Trees <u>Multifamily:</u> <ul style="list-style-type: none"> • 3919/200 = 20 trees <u>Commercial:</u> <ul style="list-style-type: none"> • 1332/200 = 7 trees 	<u>Multifamily:</u> 20 trees <u>Commercial:</u> 7 trees	Yes	Multifamily section can use the required site landscaping trees within the parking lot and around the periphery but the lot needs to follow standard parking lot landscape guidelines.
Parking Lot Perimeter Trees	<ul style="list-style-type: none"> ■ 1 Canopy tree per 35 lf <u>Multifamily:</u> <ul style="list-style-type: none"> • 975 lf/35 = 28 trees <u>Commercial:</u> <ul style="list-style-type: none"> • 381/35 = 11 trees 	<u>Multifamily:</u> 975 lf/35 = 28 trees <u>Commercial:</u> 11 trees, including 7 existing trees on west edge.	<u>Multifamily:</u> Yes <u>Commercial:</u> Yes	<u>Residential:</u> 1. Greenbelt trees within 15 feet of the curb can be double-counted as perimeter trees. 2. As noted above, multifamily unit trees may be used to satisfy parking lot tree requirements..
Parking land banked	NA			
Miscellaneous Landscaping Requirements				
Plantings around Fire Hydrant (d)	No plantings with mature height greater than 12' within 10 ft. of fire hydrants, manholes,	No trees are proposed closer than 10 feet from hydrants or storm	Yes	

Item	Required	Proposed	Meets Code	Comments
	catch basins or other utility structures.	structures.		
Landscaped area (g)	Areas not dedicated to parking use or driveways exceeding 100 sq. ft. shall be landscaped	Yes	Yes	
Name, type and number of ground cover (LDM 1.c.(5))	As proposed on planting islands	Sod is indicated	Yes	
Snow deposit (LDM.2.q.)	Show leave snow deposit areas on plan in locations where landscaping won't be damaged	Numerous locations are provided.	TBD	Please be sure areas shown on different sheets in the set are consistent, and that they are areas where landscaping won't be damaged by snow deposits.
Transformers/Utility boxes (LDM 1.e from 1 through 5)	<ul style="list-style-type: none"> ▪ A minimum of 2 ft. separation between box and the plants ▪ Ground cover below 4" is allowed up to pad. ▪ No plant materials within 8 ft. from the doors 	No	No	<ol style="list-style-type: none"> 1. Please show transformers and other utility boxes when their locations are determined. 2. If box locations are not determined by final site plans, add a note to plan stating that all utility boxes are to be landscaped per the detail.
Detention/Retention Basin Planting requirements (Sec. 5.5.3.E.iv)	<ul style="list-style-type: none"> ▪ Clusters of large native shrubs shall cover 70-75% of the basin rim area ▪ 10" to 14" tall grass along sides of basin ▪ Refer to wetland for basin mix ▪ Include seed mix details on landscape plan 	Required coverage and species are provided.	Yes	
General Landscape Requirements (LDM 3)				
General Conditions (LDM 3.a)	Plant materials shall not be planted within 4 ft. of property line	Yes	Yes	Please add note near property lines.
Irrigation plan (LDM 2.s.)	A fully automatic irrigation system and a method of draining is required with Final Site	No		1. <u>Please add irrigation plan or information as to how plants will be watered</u>

Item	Required	Proposed	Meets Code	Comments
	Plan			<p><u>sufficiently for establishment and long- term survival.</u></p> <p>2. <u>If xeriscaping is used, please provide information about plantings included.</u></p> <p>3. <u>Per the Road Commission for Oakland County, no underground irrigation system may be installed in the Novi Road right-of-way.</u></p>
Other information (LDM 2.u)	Required by Planning Commission	NA		
Landscape tree credit (LDM3.b.(d))	Substitutions to landscape standards for preserved canopy trees outside woodlands/ wetlands should be approved by LA. Refer to Landscape tree Credit Chart in LDM	No		
Plant Sizes for ROW, Woodland replacement and others (LDM 3.c)	<ul style="list-style-type: none"> • Canopy Deciduous shall be 3" and sub-canopy deciduous shall be 2.5" caliper. • Refer to section for more details 	Sheet L-4	Yes	
Plant size credit (LDM3.c.(2))	NA	No		
Prohibited Plants (LDM 3.d)		None		
Recommended trees for planting under overhead utilities (LDM 3.e)	Label the distance from the overhead utilities	Note indicates that overhead lines will be removed.		
Collected or Transplanted trees (LDM 3.f)		None		
Nonliving Durable Material: Mulch (LDM 4)	<ul style="list-style-type: none"> ▪ Trees shall be mulched to 3" depth and shrubs, groundcovers to 2" depth ▪ Specify natural color, finely shredded hardwood bark mulch. ▪ Include in cost estimate. 	Details on Sheet L-3		

Item	Required	Proposed	Meets Code	Comments
	<ul style="list-style-type: none"> ▪ Refer to section for additional information 			
Landscape Notes and Details– Utilize City of Novi Standard Notes				
Plant List (LDM 2.h.) – Include all cost estimates				
Quantities and sizes		Yes	Yes	
Root type		Yes	Yes	
Botanical and common names	Refer to LDM suggested plant list	12 of 21 species (57%) used in the Multifamily area and 6 of 11 (55%) species used in the Commercial area are native to Michigan.	Yes	
Type and amount of lawn		Sod	Yes	
Cost estimate (LDM 2.t)	For all new plantings, mulch and sod as listed on the plan	Yes	Yes	
Planting Details/Info (LDM 2.i) – Utilize City of Novi Standard Details				
Canopy Deciduous Tree	Refer to LDM for detail drawings	Yes	Yes	
Evergreen Tree		Yes	Yes	
Shrub		Yes	Yes	
Multi-stem tree		Yes	Yes	
Perennial/ Ground Cover		Yes	Yes	
Tree stakes and guys	Wood stakes, fabric guys.	Yes	Yes	
Protective Tree Fence		Yes	Yes	
Cross-Section of Berms (LDM 2.j)				
Slope, height and width	<ul style="list-style-type: none"> ▪ Label contour lines ▪ Maximum 33% slope ▪ Constructed of loam ▪ 6" top layer of topsoil 	None		
Type of Ground Cover		Sod, special seed for detention pond	Yes	
Setbacks from Utilities	Overhead utility lines and 15 ft. setback from edge of utility or 20 ft. setback from closest pole, 10 feet from structures, hydrants	No overhead utilities will be on site.	Yes	
Walls (LDM 2.k & Zoning Sec 5.5.3.vi)				
Material, height and	Freestanding walls	2.5 ft screening	Yes	Provide dimensioned

Item	Required	Proposed	Meets Code	Comments
type of construction footing	should have brick or stone exterior with masonry or concrete interior	walls are provided along most of frontage.		wall details.
Walls greater than 3 ½ ft. should be designed and sealed by an Engineer		None indicated	No	<u>If walls are taller than 3 ½ feet, please have engineer design, sign and seal.</u>
Notes (LDM 2.i) – Utilize City of Novi Standard Details				
Installation date (LDM 2.i. & Zoning Sec 5.5.5.B)	<ul style="list-style-type: none"> ▪ Provide intended date ▪ Between Mar 15 – Nov 15 	Mar-Nov 2019/20	Yes	
Maintenance & Statement of intent (LDM 2.m & Zoning Sec 5.5.6)	<ul style="list-style-type: none"> ▪ Include statement of intent to install and guarantee all materials for 2 years. ▪ Include a minimum one cultivation in June, July and August for the 2-year warranty period. 	<ul style="list-style-type: none"> ▪ 2-year guarantee is included ▪ City of Novi note #6 indicates that failed plant material shall be replaced during next appropriate planting period 	No	
Plant source (LDM 2.n & LDM 3.a.(2))	Shall be northern nursery grown, No.1 grade.	Yes	Yes	
Establishment period (Zoning Sec 5.5.6.B)	2 yr. Guarantee	Yes	Yes	
Approval of substitutions. (Zoning Sec 5.5.5.E)	City must approve any substitutions <u>in writing</u> prior to installation.	Yes	Yes	
<p>NOTES:</p> <ol style="list-style-type: none"> 1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards. 2. The section of the applicable ordinance or standard is indicated in parenthesis. For the landscape requirements, please see the Zoning Ordinance landscape section 5.5 and the Landscape Design Manual for the appropriate items under the applicable zoning classification. 3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals. 				

WETLANDS REVIEW



ECT Project No. 180344-0100

June 4, 2018

Ms. Barbara McBeth, AICP
City Planner
Community Development Department
City of Novi
45175 W. Ten Mile Road
Novi, Michigan 48375

Re: The District (JSP18-0010)
Wetland Review of the Preliminary Site Plan (PSP18-0036)

Dear Ms. McBeth:

Environmental Consulting & Technology, Inc. (ECT) has reviewed the Preliminary Site Plan for the proposed The District (f.k.a. Flint Street) project prepared by Seiber, Keast Engineering, L.L.C. dated and stamped "Received" by the City of Novi Community Development Department on May 10, 2018 (Plan). The Plan was reviewed for conformance with the City of Novi Wetland and Watercourse Protection Ordinance and the natural features setback provisions in the Zoning Ordinance. ECT completed an on-site wetland evaluation on November 7, 2017.

The project is located south of Grand River Avenue and west of Novi Road in Section 22. The site is specifically located south of Flint Street and north of the existing railroad (C. & O. Railroad). The Plan includes the construction of two (2) multi-family residential buildings, a commercial building, associated parking decks, utilities, underground stormwater detention systems and a conventional stormwater detention basin. The Walled Lake Branch of the Middle Rouge River that is located directly adjacent to the site on the north side (i.e., north of Flint Street) and flows through the southeast section of the site.

ECT recommends approval of the Preliminary Site Plan for wetlands with the condition that the Applicant satisfactorily address the items noted in the "Wetland Comments" section of this letter at the time of Final Site Plan submittal.

The following wetland related items are required for this project:

Item	Required/Not Required/Not Applicable
Wetland Permit (specify Non-Minor or Minor)	Does not appear to be Required
Wetland Mitigation	No Required
Wetland Buffer Authorization	Required
MDEQ Permit	To Be Determined. It is the applicant's responsibility to contact the MDEQ in order to determine the need for a wetland use permit.
Wetland Conservation Easement	Not Required

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City of Novi Ordinance Requirements

The City of Novi Wetland and Watercourse Protection Ordinance (City of Novi Code of Ordinances, Part II, Chapter 12, Article V.; Division 2.) describes the regulatory criteria for wetlands and review standards for wetland permit applications.

The wetland essentiality criteria as described in the Wetland and Watercourse Protection Ordinance are included below. Wetlands deemed essential by the City of Novi require the approval of a use permit for any proposed impacts to the wetland.

All noncontiguous wetland areas of less than two (2) acres which appear on the wetlands inventory map, or which are otherwise identified during a field inspection by the city, shall be analyzed for the purpose of determining whether such areas are essential to the preservation of the natural resources of the city. . . . In making the determination, the city shall find that one (1) or more of the following exist at the particular site:

- (1) The site supports state or federal endangered or threatened plants, fish or wildlife appearing on a list specified in Section 36505 of the Natural Resources Environmental Protection Act (Act 451 of 1994) [previously section 6 of the endangered species act of 1974, Act No. 203 of the Public Acts of 1974, being section 229.226 of the Michigan Compiled Laws].*
- (2) The site represents what is identified as a locally rare or unique ecosystem.*
- (3) The site supports plants or animals of an identified local importance.*
- (4) The site provides groundwater recharge documented by a public agency.*
- (5) The site provides flood and storm control by the hydrologic absorption and storage capacity of the wetland.*
- (6) The site provides wildlife habitat by providing breeding, nesting or feeding grounds or cover for forms of wildlife, waterfowl, including migratory waterfowl, and rare, threatened or endangered wildlife species.*
- (7) The site provides protection of subsurface water resources and provision of valuable watersheds and recharging groundwater supplies.*
- (8) The site provides pollution treatment by serving as a biological and chemical oxidation basin.*
- (9) The site provides erosion control by serving as a sedimentation area and filtering basin, absorbing silt and organic matter.*
- (10) The site provides sources of nutrients in water food cycles and nursery grounds and sanctuaries for fish.*

After determining that a wetland less than two (2) acres in size is essential to the preservation of the natural resources of the city, the wetland use permit application shall be reviewed according to the standards in subsection 12-174(a).

Wetland Evaluation

ECT's in-office review of available materials included the City of Novi Regulated Wetland and Watercourse map, USGS topographic quadrangle map, NRCS soils map, USFWS National Wetland Inventory map, and historical aerial photographs. The City of Novi Regulated Wetland and Watercourse Map (see Figure 1) indicates one (1) watercourse in the southeastern section of the site. As noted above, this area is the Walled Lake Branch of the Middle Rouge River. This watercourse has significantly steep side slopes/banks and therefore lacks a wetland fringe along its edge. ECT conducted an on-site wetland evaluation with the applicant's wetland consultant, King & MacGregor Environmental, Inc, (KME), on November 7, 2017.

It should be noted the applicant's wetland consultant has reported that no wetland areas were found on the property. The property was the subject of a 2011 wetland boundary review conducted by ASTI Environmental (ASTI). At that time ASTI identified four (4) wetland areas. Historically, this property appears to have been filled and is generally highly disturbed. The Natural Resources Conservation Service (NRCS) soils map indicates that the wooded portion located in the northwest section of the property is the "Urban Land" categorization and not a natural soil type. KME notes that the on-line Michigan Department of Environmental Quality (MDEQ) Wetlands Map Viewer does not indicate any areas of on-site wetland.

KME conducted a site inspection for wetlands on September 12, 2017. They report that the site is largely dominated by invasive species. The wooded area in the northwest portion of the site is dominated by common buckthorn (*Rhamnus cathartica*) and eastern cottonwood trees (*Populus deltoides*). The more open and more-recently disturbed areas are dominated by wild carrot (*Daucus carota*), spotted knapweed (*Centaurea maculosa*), sweet clover (*Melilotus officinalis*) and some common reed (*Phragmites australis*). KME notes that there is little to no evidence of hydrology on the subject site. They noted that in the wooded areas, there are some areas that are slightly depressed in elevation with some slight water staining on the leaf cover, however the vegetation growing in these areas is identical to the surrounding areas of higher ground. KME dug exploratory soil pit in three (3) locations in the woods and hit refusal at 8-inches depth each time. The soil that was excavated did not appear to be wetland soil or native soil.

KME further notes that 2011 (the year that the previous on-site wetland delineation was conducted) was an unusually wet year. As of the June 6, 2011 date of the ASTI report, Detroit Metro Airport was more than 4 inches above normal precipitation and Milford was almost 7 inches above normal since April 1st of 2011. KME notes that this may have been a contributing factor in the previous (2011) wetland identification.

A wetland must have (1) wetland vegetation, (2) hydric (wetland) soils, and (3) hydrology or signs of hydrology. ECT did not observe any on-site wetlands at the time of our on-site wetland evaluation. A soil probe was used in order to assess the soil within any areas that had been previously identified as wetland. No areas of hydric (i.e., wetland) soil were observed on the site.

Wetland Mitigation

It should be noted that in those cases where an activity results in the impact to wetland areas of 0.25-acre or greater that are deemed essential under City of Novi Ordinance subsection 12-174(b) mitigation shall be required. The applicant shall submit a mitigation plan which provides for the establishment of replacement wetlands at a ratio of 1.5:1 for emergent/scrub-shrub wetland types and 2:1 for forested wetlands, if impacts meet or exceed the 0.25-acre threshold. The MDEQ's threshold for the requirement of wetland mitigation is 0.3-acre of wetland impacts.

The proposed project does not require wetland mitigation.

Regulatory Discussion

The site does not appear to contain regulated wetlands, however the Walled Lake Branch of the Middle Rouge River flows through the southeast section of the subject site. This water feature is regulated by the City of Novi as well as the Michigan Department of Environmental Quality (MDEQ). It is the applicant's responsibility to contact MDEQ in order to confirm the regulatory authority with respect to the on-site watercourse area, should any subsequent site plan submittals include proposed impacts or crossings of this natural feature. This watercourse is regulated by MDEQ under Part 301, Inland Lakes and Streams.

Any proposed impacts to this watercourse will require a City of Novi *Wetland and Watercourse Use Permit* as well as an *Authorization to Encroach the 25-Foot Natural Features Setback* for any proposed impacts to the 25-foot watercourse buffers.

The applicant is urged to minimize impacts to on-site watercourse and watercourse setbacks to the greatest extent practicable. The City regulates wetland and watercourse buffers/setbacks. Article 24, Schedule of Regulations, of the Zoning Ordinance states that:

“There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to require a minimum setback from wetlands and watercourses?”.

The applicant shall provide information on subsequent plans that clearly indicates the area of onsite watercourse as well as the area of the 25-foot watercourse buffers. The plans shall also clearly indicate the area (square feet or acres) of all watercourse and watercourse buffer impacts (both permanent and temporary, if applicable) and the volume (cubic yards) of all proposed impacts, if applicable.

Wetland Review Comments

ECT recommends that the Applicant address the items noted below in the Final Site Plan submittal:

1. A City of Novi *Wetland and Watercourse Use Permit* would be required for any proposed impacts to site wetlands or watercourses. Currently there do not appear to be any direct impacts (i.e., cut or fill) proposed.
2. ECT encourages the Applicant to minimize impacts to on-site wetlands, watercourses and the associated 25-foot wetland/watercourse setbacks to the greatest extent practicable. The applicant is urged to locate the ultimate stormwater outfall structures and the proposed dog park outside of all 25-foot wetland/watercourse setback boundaries.
3. A City of Novi *Authorization to Encroach the 25-Foot Natural Features Setback* would be required for any proposed impacts to on-site 25-foot wetland or watercourse buffers. It is currently unclear if there are impacts (temporary or permanent) proposed to the 25-foot setback of the Walled Lake Branch of the Middle Rouge River for the purpose of stormwater outlet construction and for the proposed dog park area. The applicant shall:

- a. Indicate and label the 25-foot watercourse setback location on the Plan,
 - b. Provide details regarding the two (2) proposed stormwater outfalls to the river and label and quantify (square feet or acres) and proposed impacts (both permanent and temporary) to the 25-foot setbacks. The applicant is urged to locate the ultimate stormwater outfall structures outside of all 25-foot wetland/watercourse setback boundaries.
 - c. The proposed fence for the dog park area appears to be located within the existing 25-foot watercourse setback of the Walled Lake Branch of the Middle Rouge River. The applicant is urged to locate the dog park outside of all 25-foot wetland/watercourse setback boundaries.
4. It should be noted that it is the Applicant's responsibility to confirm the need for a Permit from the MDEQ for any proposed wetland or watercourse impact. Final determination as to the regulatory status of the on-site wetlands and watercourses shall be made by MDEQ. The Applicant should provide a copy of the MDEQ Wetland Use Permit application to the City (and our office) for review and a copy of the approved permit upon issuance. A City of Novi Wetland and Watercourse Permit cannot be issued until the need for an MDEQ wetland permit has been clarified by the applicant and documentation provided to the City (including a copy of the issued MDEQ Wetland Permit, if applicable).
5. The applicant shall provide information on subsequent plans that clearly indicates the areas of all onsite wetlands and watercourses as well as the area of the 25-foot wetland and watercourse buffers. The plans shall also clearly indicate the area (square feet or acres) of all wetland and wetland buffer impacts (both permanent and temporary, if applicable) and the volume (cubic yards) of all wetland impacts. This information is required prior to issuance of the City of Novi Wetland Permit.

Conclusion

ECT did not observe any on-site wetlands at the time of our on-site wetland evaluation. The site does not appear to contain regulated wetlands, however the Walled Lake Branch of the Middle Rouge River flows through the southeast section of the subject site.

Any proposed impacts to this watercourse will require a permit from the MDEQ, a City of Novi *Wetland and Watercourse Use Permit*, and an *Authorization to Encroach the 25-Foot Natural Features Setback* for any proposed impacts to the 25-foot watercourse buffers.

Recommendation

ECT recommends approval of the Preliminary Site Plan for wetlands with the condition that the Applicant satisfactorily address the items noted in the "*Wetland Comments*" section of this letter at the time of Final Site Plan submittal.

The District (JSP18-0010)
Wetland Review of the Preliminary Site Plan – (PSP18-0036)
June 4, 2018 (Revision 1)
Page 6 of 8

As always, please feel free to contact our office if you have any questions.

Sincerely,
ENVIRONMENTAL CONSULTING & TECHNOLOGY, INC.



Pete Hill, P.E.
Senior Associate Engineer

cc: Lindsay Bell, City of Novi Planner
Sri Komaragiri, City of Novi Planner
Rick Meader, City of Novi Landscape Architect
Hannah Smith, City of Novi Planning Assistant

Attachments: Figure 1 – City of Novi Regulated Wetland and Woodland Map
Site Photos

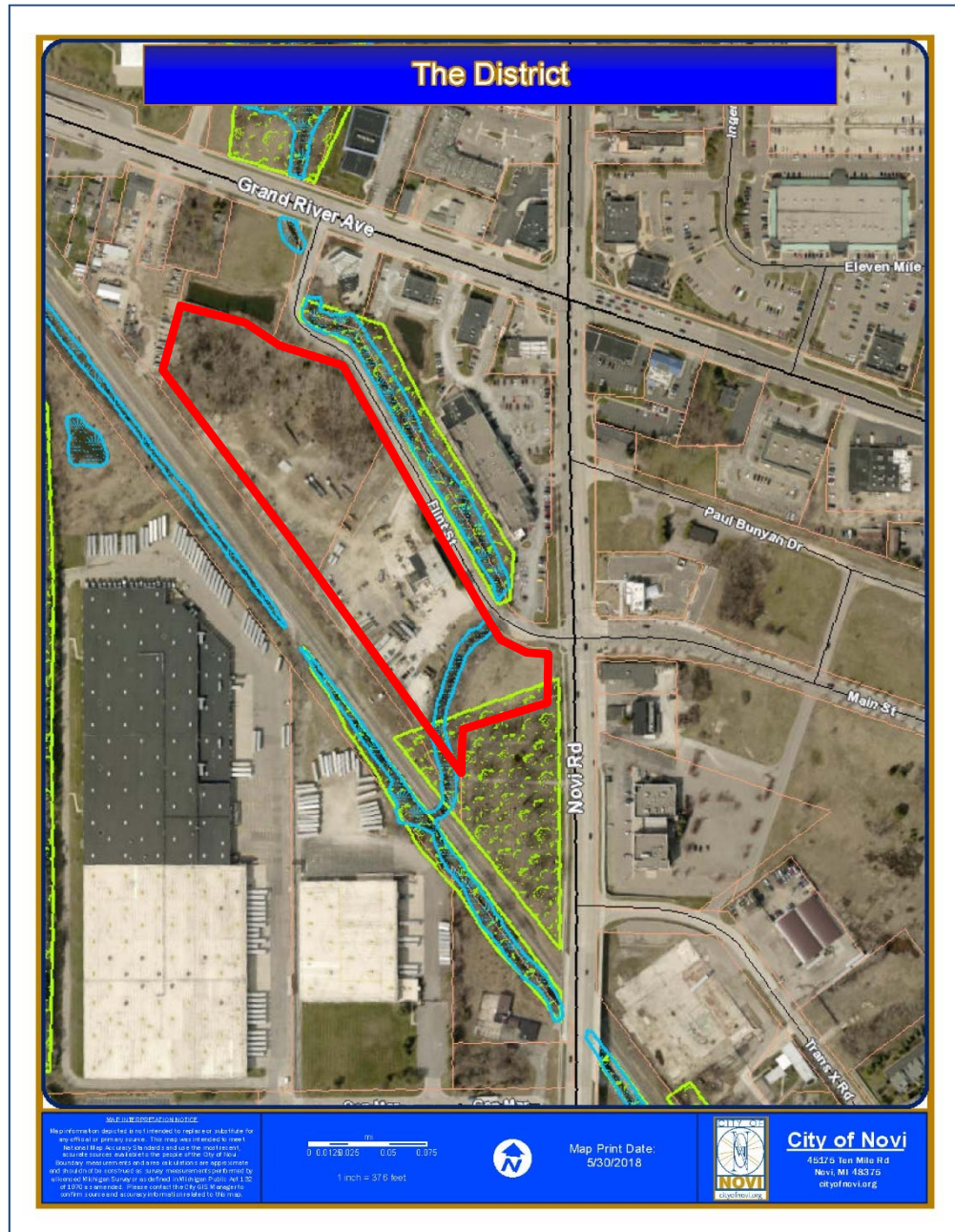


Figure 1. City of Novi Regulated Wetland & Woodland Map (approximate property boundary shown in red). Regulated Woodland areas are shown in green and regulated Wetland areas are shown in blue.

Site Photos



Photo 1. Looking southeast at the Walled Lake Branch of the Middle Rouge River (ECT, November 7, 2017).

WOODLANDS REVIEW



May 30, 2018
ECT Project No. 180344-0200

Ms. Barbara McBeth, AICP
City Planner
Community Development Department
City of Novi
45175 West Ten Mile Road
Novi, MI 48375

Re: The District (f.k.a. Flint Street) JSP18-0344
Woodland Review of the Preliminary Site Plan (PSP18-0036)

Dear Ms. McBeth:

Environmental Consulting & Technology, Inc. (ECT) has reviewed the Preliminary Site Plan for the proposed The District (f.k.a. Flint Street) development project prepared by Seiber, Keast Engineering, L.L.C. and Allen Design, L.L.C. dated and stamped "Received" by the City of Novi Community Development Department on May 10, 2018 (Plan). The Plan was reviewed for conformance with the City of Novi Woodland Protection Ordinance Chapter 37.

The project is located south of Grand River Avenue and west of Novi Road in Section 22. The site is specifically located south of Flint Street and north of the existing railroad (C. & O. Railroad). The Plan includes the construction of two (2) multi-family residential buildings, a commercial building, associated parking decks, utilities, underground stormwater detention systems and a conventional stormwater detention basin.

ECT recommends approval of the Preliminary Site Plan for woodlands with the condition that the Applicant satisfactorily address the items noted in the "Woodland Comments" section of this letter at the time of Final Site Plan submittal.

The following woodland related items are required for this project:

Item	Required/Not Required/Not Applicable
Woodland Permit	Required
Woodland Fence	Required
Woodland Conservation Easement	Not Required

What follows is a summary of our findings regarding on-site woodlands associated with the proposed project.

Woodland Evaluation

ECT completed an on-site woodland evaluation on November 7, 2017. The site appears to contain an area indicated as City of Novi Regulated Woodlands (see Figure 1). This area is located in the southeast section of the site, along the Walled Lake Branch of the Middle Rouge River. Areas mapped as Regulated Woodlands are also located adjacent to the site, across Flint Street.

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The current Plan includes a *Woodland Plan* (Sheet L-4) that includes a tree survey list as well as a *Woodland Summary* that indicates proposed tree removals and the associated required Woodland Replacement Credits.

It should be noted that the purpose of the City of Novi Woodland Protection Ordinance (Chapter 37) is to:

1. *Provide for the protection, preservation, replacement, proper maintenance and use of trees and woodlands located in the city in order to minimize disturbance to them and to prevent damage from erosion and siltation, a loss of wildlife and vegetation, and/or from the destruction of the natural habitat. In this regard, it is the intent of this chapter to protect the integrity of woodland areas as a whole, in recognition that woodlands serve as part of an ecosystem, and to place priority on the preservation of woodlands, trees, similar woody vegetation, and related natural resources over development when there are no location alternatives;*
2. *Protect the woodlands, including trees and other forms of vegetation, of the city for their economic support of local property values when allowed to remain uncleared and/or unharvested and for their natural beauty, wilderness character of geological, ecological, or historical significance; and*
3. *Provide for the paramount public concern for these natural resources in the interest of health, safety and general welfare of the residents of the city.*

The northern end of the site and the buffer along the Walled Lake Branch of the Middle Rouge River contain the largest concentrations of existing trees. The majority of the tree species located on the site are eastern cottonwood (*Populus deltoides*). Other tree species located on the project site include black willow (*Salix nigra*), black walnut (*Juglans nigra*), American elm (*Ulmus americana*), green ash (*Fraxinus pennsylvanica*), and Austrian pine (*Pinus nigra*). The majority of the trees are listed as being in Good condition and ECT was able to confirm this in our on-site evaluation.

While some trees located on-site appear to fall outside of the City of Novi's mapped Woodland Boundaries, the City's Woodland Ordinance contains the following:

Where uncertainty exists with respect to the boundaries of designated woodland areas shown on the regulated woodland map, the following rules shall apply:

- *Distances not specifically indicated on the map shall be determined by the scale on the map;*
- *Where physical or natural features existing on the ground are at variance with those shown on the regulated woodland map, or in other circumstances where uncertainty exists, the community development director or his or her designee shall interpret the woodland area boundaries;*
- *On any parcel containing any degree of regulated woodland, the applicant shall provide site plan documentation showing the locations, species, size and condition of all trees of eight-inch caliper or larger. Existing site understory trees, shrubs and ground cover conditions must be documented on the site plan or woodland use permit application plan in the form of a brief narrative. The woodland conditions narrative should include information regarding plant species, general quantities and condition of the woodland vegetation*

It is ECT's opinion that the areas containing trees on the Plan, including within the project's proposed limits of disturbance, should be considered as Regulated Woodland area. As such, there are physical and natural features existing on the site that are at variance with those shown on the regulated woodland map. The Woodland Ordinance also defines Woodland Areas as:

All lands (including all trees, shrubs and ground cover thereon regardless of size) which are subject to this chapter under section 37-4 as designated on the regulated woodland map and/or on an approved site plan. Woodlands areas are identified by such factors as: soil quality, habitat quality, tree species and diversity, health and vigor of tree stand, understory species and quality, presence of wildlife, and other factors such as the value of the woodland area as a scenic asset, windblock, noise buffer, healthy environment, and the value of historic or specimen trees.

Woodland Impact Review & Woodland Replacement Credits

The Woodland Plan indicates that a total of 185 trees were surveyed on the subject site. Of the trees surveyed, 148 trees meet the minimum 8-inch diameter-at-breast-height (DBH) requirement and are otherwise in good to fair condition and are considered regulated trees.

As shown, there are impacts proposed to regulated woodlands associated with the site construction. The Plan notes that a total of **103** of the **148** on-site, regulated trees (approximately 70% of the regulated trees) will be removed as a result of the proposed project.

As noted above, a *Woodland Summary* list has been included on the *Tree List* (Sheet 03). The Applicant has noted the following:

- Total Surveyed Trees 185
- Less Dead or Off-site Trees 37
- Total Regulated Trees 148
- Regulated Trees Removed: 103 (70% Removal)
- Regulated Trees Preserved: 45 (30% Preservation)

- Stems to be Removed 8" to 11": 43 x 1 replacement (Requiring 43 Replacements)
- Stems to be Removed 11" to 20": 38 x 2 replacements (Requiring 76 Replacements)
- Stems to be Removed 20" to 30": 3 x 3 replacements (Requiring 9 Replacements)
- Stems to be Removed 30"+: 0 x 4 replacements (Requiring 0 Replacements)
- Multi-Stemmed Trees (19 trees): (Requires 77 Replacements)

- Subtotal Replacement Trees Required: 205
- Less credits for preservation of non-woodland trees 66
- Total Woodland Replacement Credits Required 139

The Plan does not appear to provide any on-site Woodland Replacement plantings and does not appear to have the space available to meet this requirement given the extent of the proposed development.

City of Novi Woodland Review Standards and Woodland Permit Requirements

Based on Section 37-29 (*Application Review Standards*) of the City of Novi Woodland Ordinance, the following standards shall govern the grant or denial of an application for a use permit required by this article:

No application shall be denied solely on the basis that some trees are growing on the property under consideration. However, the protection and conservation of irreplaceable natural resources from pollution, impairment, or destruction is of paramount concern. Therefore, the preservation of woodlands, trees, similar woody vegetation, and related natural resources shall have priority over development when there are location alternatives.

In addition,

“The removal or relocation of trees shall be limited to those instances when necessary for the location of a structure or site improvements and when no feasible and prudent alternative location for the structure or improvements can be had without causing undue hardship”.

There are a significant number of replacement trees required for the construction of the proposed development. While, the overall ecological values of the existing woodlands cannot be immediately replaced through the planting of woodland replacement trees, it appears that the applicant will be prepared to meet the required Woodland Replacement requirements through a required payment to the City of Novi Tree Fund. After reviewing the Woodland Replacement calculations as noted above, the applicant shall clarify whether all of the required Woodland Replacement tree credits will be provided on-site or if a portion will be paid into the City of Novi Tree Fund.

Woodland Comments

Please consider the following comments when preparing subsequent site plan submittals:

1. A Woodland Permit from the City of Novi would be required for proposed impacts to any trees 8-inch diameter-at-breast-height (DBH) or greater and located within an area designated as City Regulated Woodland, or any tree 36-inches DBH regardless of location on the site. Such trees shall be relocated or replaced by the permit grantee. All deciduous replacement trees shall be two and one-half (2 ½) inches caliper or greater and count at a 1 replacement tree-to-1 credit replacement ratio. All coniferous replacement trees shall be six (6) feet in height (minimum) and count at a 1.5 replacement tree-to-1 credit replacement ratio. All Woodland Replacement trees shall be species that are listed on the City’s Woodland Tree Replacement Chart (attached). ECT recommends that the applicant take all steps feasible in order to provide as many of the required Woodland Replacement credits through the planting of on-site replacement trees.
2. If applicable, a Woodland Replacement Performance financial guarantee for the planting of replacement trees will be required. This financial guarantee will be based on the number of on-site woodland replacement trees (credits) being provided at a per tree value of \$400. Based on a successful inspection of the installed on-site Woodland Replacement trees, the original Woodland Financial Guarantee shall be returned to the Applicant. Twenty-five percent (25%) of the value of the Woodland Replacement material shall be kept for a period of 2-years after the successful inspection of the tree replacement installation as a *Woodland Maintenance and Guarantee Bond*.
3. Replacement material should not be located 1) within 10’ of built structures or the edges of utility easements and 2) over underground structures/utilities or within their associated easements. In addition, replacement tree spacing should follow the *Plant Material Spacing Relationship Chart for Landscape Purposes* found in the City of Novi *Landscape Design Manual*.

4. The Applicant will be required to pay the City of Novi Tree Fund at a value of \$400/credit for any Woodland Replacement tree credits that cannot be placed on-site.
5. The Applicant shall provide preservation/conservation easements as directed by the City of Novi Community Development Department for any areas of woodland replacement trees (if applicable). The applicant shall demonstrate that the all proposed woodland replacement trees and existing regulated woodland trees to remain will be guaranteed to be preserved as planted with a conservation easement or landscape easement to be granted to the city. This language shall be submitted to the City Attorney for review. The executed easement must be returned to the City Attorney within 60 days of the issuance of the City of Novi Woodland permit.

Recommendation

ECT recommends approval of the Preliminary Site Plan for woodlands with the condition that the Applicant satisfactorily address the items noted in the “*Woodland Comments*” section of this letter at the time of Final Site Plan submittal.

If you have any questions regarding the contents of this letter, please contact us.

Respectfully submitted,

ENVIRONMENTAL CONSULTING & TECHNOLOGY, INC.



Pete Hill, P.E.
Senior Associate Engineer

cc: Lindsay Bell, City of Novi Planner (lbell@cityofnovi.org)
Sri Komaragiri, City of Novi Planner (skomaragiri@cityofnovi.org)
Rick Meader, City of Novi Landscape Architect (rmeader@cityofnovi.org)
Hannah Smith, City of Novi Planning Assistant (hsmith@cityofnovi.org)

Attachments: Figure 1 – City of Novi Regulated Wetland and Woodland Map
Woodland Tree Replacement Chart
Site Photos

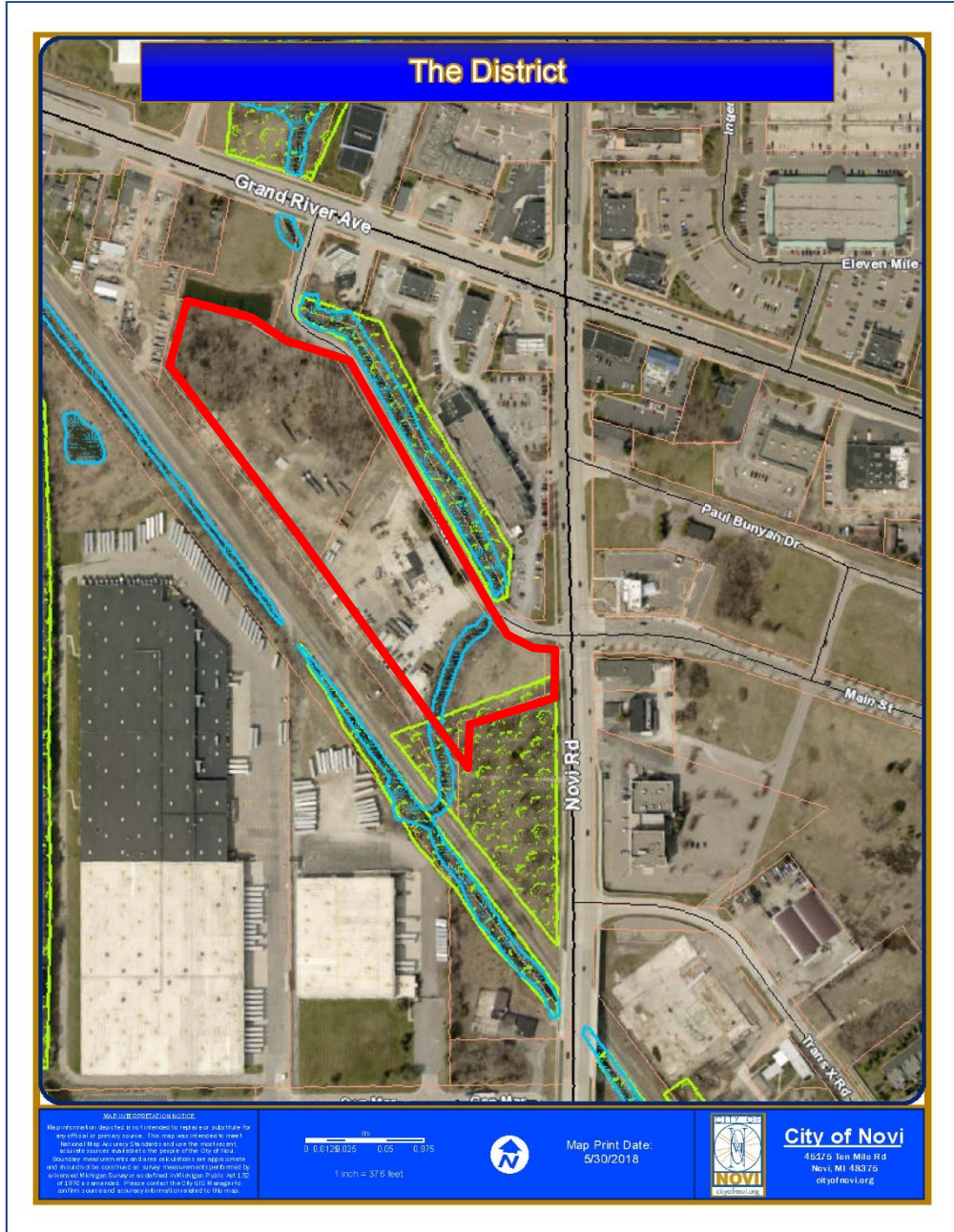


Figure 1. City of Novi Regulated Wetland & Woodland Map (approximate project area is highlighted in red). Regulated Woodland areas are shown in green and regulated Wetland areas are shown in blue.

Woodland Tree Replacement Chart

(from Chapter 37 Woodlands Protection)

(All canopy trees to be 2.5" cal or larger, evergreens as listed)

Common Name	Botanical Name
Black Maple	Acer nigrum
Striped Maple	Acer pennsylvanicum
Red Maple	Acer rubrum
Sugar Maple	Acer saccharum
Mountain Maple	Acer spicatum
Ohio Buckeye	Aesculus glabra
Downy Serviceberry	Amelanchier arborea
Yellow Birch	Betula alleghaniensis
Paper Birch	Betula papyrifera
American Hornbeam	Carpinus caroliniana
Bitternut Hickory	Carya cordiformis
Pignut Hickory	Carya glabra
Shagbark Hickory	Carya ovata
Northern Hackberry	Celtis occidentalis
Eastern Redbud	Cercis canadensis
Yellowwood	Cladrastis lutea
Beech	Fagus sp.
Thornless Honeylocust	Gleditsia triacanthos inermis
Kentucky Coffeetree	Gymnocladus dioicus
Walnut	Juglans sp.
Eastern Larch	Larix laricina
Sweetgum	Liquidambar styraciflua
Tuliptree	Liriodendron tulipifera
Tupelo	Nyssa sylvatica
American Hophornbeam	Ostrya virginiana
White Spruce_(1.5:1 ratio) (6' ht.)	Picea glauca
Black Spruce_(1.5:1 ratio) (6' ht.)	Picea mariana
Red Pine	Pinus resinosa
White Pine_(1.5:1 ratio) (6' ht.)	Pinus strobus
American Sycamore	Platanus occidentalis
Black Cherry	Prunus serotina
White Oak	Quercus alba
Swamp White Oak	Quercus bicolor
Scarlet Oak	Quercus coccinea
Shingle Oak	Quercus imbricaria
Burr Oak	Quercus macrocarpa
Chinkapin Oak	Quercus muehlenbergii
Red Oak	Quercus rubra
Black Oak	Quercus velutina
American Bladdernut	Staphylea trifolia
Bald Cypress	Taxodium distichum
American Basswood	Tilia americana
Hemlock (1.5:1 ratio) (6' ht.)	Tsuga canadensis

Site Photos



Photo 1. Looking northwest at wooded area in the northwest portion of the site (ECT, November 17, 2017).



Photo 2. Looking north from within wooded area in the northwest portion of the property (ECT, November 17, 2017).

TRAFFIC REVIEW



AECOM
 27777 Franklin Road
 Southfield
 MI, 48034
 USA
 aecom.com

Project name:
 JSP18-0010 The Bond fka The District
 Revised Preliminary Site Plan Traffic Review

From:
 AECOM

Date:
 June 19, 2018

To:
 Barbara McBeth, AICP
 City of Novi
 45175 10 Mile Road
 Novi, Michigan 48375

CC:
 Sri Komaragiri, Lindsay Bell, George Melistas,
 Theresa Bridges, Darcy Rechten, Hannah Smith

Memo

Subject: JSP18-0010 - The Bond fka District Revised Preliminary Site Plan Traffic Review

The revised preliminary site plan was reviewed to the level of detail provided and AECOM **recommends approval** for the applicant to move forward with the condition that the comments provided below are adequately addressed to the satisfaction of the City.

GENERAL COMMENTS

1. Tricap Holdings, LLC, is proposing a combined residential and retail development on the south side of Flint Street between Novi Road and Grand River Avenue. The residential development will consist of 253 apartment units and the commercial development is 5,578 square feet (SF).
2. Flint Street is under the jurisdiction of the City of Novi.
3. The site is under Town Center (TC-1) zoning.
4. Summary of traffic-related waivers/variances:
 - a. The applicant is seeking a Zoning Board of Appeals (ZBA) variance for the 18' parking space length in the parking garage.
 - b. Other deviations may be required pending the applicants response to this review letter.

TRAFFIC IMPACTS

1. AECOM performed an initial trip generation estimate based on the ITE Trip Generation Manual, 10th Edition, as follows:

ITE Code: 220 Apartments – Mid-Rise; 820 – Shopping Center
 Development-specific Quantity: 253 units; 5,578 SF
 Zoning Change: N/A

Trip Generation Summary				
	Estimated Trips (Residential+Commercial)	Estimated Peak-Direction Trips (Residential+Commercial)	City of Novi Threshold (Directional Trips)	Above Threshold?

AM Peak-Hour Trips	116+5=121	89+3=92	100	No
PM Peak-Hour Trips	142+64=206	89+33=122	100	Yes
Daily (One-Directional) Trips	1852+844=2696	N/A	750	Yes

2. The applicant submitted a traffic impact study (TIS) dated March 15, 2018. The TIS was reviewed in a separate letter dated March 28, 2018.

EXTERNAL SITE ACCESS AND OPERATIONS

The following comments relate to the external interface between the proposed development and the surrounding roadway(s).

1. The width and radii of the site driveways are in compliance with City standards.
2. The TIS indicated that the proposed driveways do not require right-turn lanes or tapers; nor do the driveways require a left-turn bypass lane.
3. The applicant has indicated a sight distance of 260± at each driveway, which is in accordance with the requirements provided in Figure VIII-E in the City's Code of Ordinances for a 25 mph, two (2) lane roadway. The applicant has reviewed the proposed on-street parallel parking and has removed some of the spaces in order to provide adequate sight distance at each driveway.
4. **The applicant is required to provide same-side and opposite-side driveway spacing for each driveway in accordance with the requirements provided in Section 11-216.d.1.d and Figure IX.12 in the City's Code of Ordinances. If driveway spacing requirements are not met, deviations may be required.**
 - a. The northwesternmost driveway has been moved to the west to provide more distance from existing Flint Street. **The applicant should provide the distance between the proposed driveway and Flint Street to review whether or not it meets City standards and to determine if a waiver is required.**

INTERNAL SITE OPERATIONS

The following comments relate to the on-site design and traffic flow operations.

1. General Traffic Flow
 - a. The width of the ramp leading to the second floor of the parking garage is acceptable for two-way traffic operations.
 - b. The applicant has provided turning radii dimensions for all routes to the parking garage entrances.
 - i. The applicant met with the Fire Department and indicated that the Department will not require access into the parking garage or the parking area between buildings 1 and 2.
 - c. The applicant should provide pavement markings on the 2L garage entry/exit ramps. The applicant should also consider pavement markings and delineation in the area near the ramps.
 - d. The applicant should indicate if there will be any sort of protection or markings provided at the end of the parking deck ramp wall.
 - e. The applicant should provide turning radii throughout the development in addition to pavement markings and signing that detail proposed traffic operations in certain areas. Specifically, the middle parking area between buildings 1 and 2 that has proposed one-way traffic.
 - f. The proposed loading zones are required to be 10 SF for each front foot of building. The applicant has provided the dimensions for each proposed loading zone. The applicant should also indicate the type of vehicles that may use the loading zones to confirm that they can accommodate such vehicles.

- i. Specifically, the loading zones are situated in front of the internal dumpsters. The applicant should provide turning paths to indicate that the applicable trash collection vehicles and any other delivery vehicles have sufficient access to/from the collection locations.
 - ii. The applicant has indicated that trash will be collected once per week and any resident scheduled moves will be coordinated with the site operations personnel. The applicant has also indicated that the trash collection dates and times will be posted.
- g. The applicant has indicated that the mailboxes will be internal to the buildings.
- h. The applicant has indicated that snow removal of the upper deck will temporarily be stored within the parking spaces. It should be noted that during temporary snow storage, five to six parking spaces may be unavailable; however, the use of these parking spaces for snow removal does not reduce the number of available parking spaces below the minimum requirement. The applicant has stated that the snow will be removed from the temporary storage areas as soon as possible, but should further indicate on the plans what the snow removal process and timeline is expected to be so the impact may be further assessed.
- i. The applicant has indicated in the response letter that 4" mountable curb is to be placed in certain areas so that snow plows can push the snow into collection areas behind the curb.
 - i. Further review of the landscape plans in conjunction with the 4" mountable curb indicates potential conflicts. The proposed snow removal areas have landscaping elements that limit the snow storage possibilities. Additionally, the straight face curb is preferred to better protect landscape areas from vehicles entering into them.
 - ii. **The applicant should replace the 4" mountable curb with straight face 6" or 4" curb, as applicable based on location, with the exception of the 4" mountable curb adjacent to the landscape paver area required by Fire near Building 1, which may remain as 4" mountable curb.**
 - iii. **If the curb heights are not adjusted, the applicant should seek a City Council variance for the use of 4" curb in lieu of 6" curb when not in front of a 17' parking space, which would not be supported by staff.**

2. Parking Facilities

- a. Please reference the planning department letter for parking calculation information and feedback.
- b. The parking space dimensions are generally in compliance with Section 5.3.2 of the City's Zoning Ordinance, with the exception of the parking garage spaces.
 - i. The applicant has proposed parking around curved islands. The applicant should indicate that the narrowest width of each parking space shall be nine feet.
 - ii. The 8' dimension of the parallel parking along Bond Street is in compliance with Section 5.3.2 of the City's Zoning Ordinance.
 - iii. Per Section 5.3.2 of the City's Zoning Ordinance, the 18' length along the angled parking and width of the aisles is in compliance.
 - iv. The 18' length of the parking spaces in the parking garage does not meet the standard 19' dimension per Section 5.3.2.
 - 1. The applicant is seeking a ZBA variance for the reduced parking space length.
- c. Based on the City's parking requirements, the applicant is required to provide a total of 2%, or 11, of the total number of parking spaces in accessible parking for the residential development and one accessible parking space for the commercial development. The applicant has indicated a total of 16 accessible spaces for the residential development and two for the commercial development.
- d. One van accessible parking space is required for every six accessible parking spaces. The applicant has provided 10 spaces that are van accessible.
- e. The applicant has indicated the proposed accessible parking spaces are located near building entrances.
- f. **The applicant has indicated six spaces marked as "designated for cemetery visitors," but should reposition the note and arrow to point to the designated spaces appropriately.**
- g. The curb heights throughout the development are generally in compliance with City standards, with the exception of two areas:

SIGNING AND STRIPING

1. All on-site signing and pavement markings shall be in compliance with the Michigan Manual on Uniform Traffic Control Devices (MMUTCD). The following is a discussion of the proposed signing and striping.
 - a. The applicant should indicate the size and MMUTCD designation of ALL proposed signs in the sign quantity table.
 - b. The applicant could add a one-way (R6-1) sign in the parking area between buildings 1 and 2.
 - c. The applicant should update signing note 2 to state "All roadway signs should be installed two feet from the face of the curb or edge of sidewalk to the near edge of the sign."
 - d. The applicant should update the striping notes to reflect actual installation instructions.
 - i. It appears as though the notes were copied verbatim from a previous review letter, which served as guidance to the applicant.
 - ii. The applicant should remove the statement in striping note number 3 that states "the applicant should provide a detail."
 - e. The applicant should rotate the international symbol of accessibility pavement markings by 180 degrees to be oriented in the same direction as the detail shown in the MMUTCD.
 - f. The applicant should provide a crosswalk marking detail for review, rather than only stating that it shall be consistent with the MMUTCD.
 - g. The applicant could make use of pavement markings throughout the site in order to properly delineate traffic.
 - i. Hollow arrows should be used to indicate traffic flow and solid arrows should be used to indicate pavement markings.

Should the City or applicant have questions regarding this review, they should contact AECOM for further clarification.

Sincerely,

AECOM



Maureen N. Peters, PE
Senior Traffic/ITS Engineer



Paula K. Johnson, PE
Senior Traffic Engineer

TRAFFIC IMPACT STUDY REVIEW



AECOM
27777 Franklin Road
Southfield
MI, 48034
USA
aecom.com

Project name:
The District (Flint Street Development) Traffic
Impact Study (TIS) Review

From:
AECOM

Date:
March 28, 2018

To:
Barbara McBeth, AICP
City of Novi
45175 10 Mile Road
Novi, Michigan 48375

CC:
Sri Komaragiri, Lindsay Bell, George Melistas,
Theresa Bridges, Darcy Rechten, Hannah Smith

Memo

Subject: The District (Flint Street Development) Traffic Impact Study (TIS) Review

The traffic impact study (TIS) was reviewed to the level of detail provided and AECOM **recommends approval** for the applicant to move forward with the condition that the comments provided below are adequately addressed to the satisfaction of the City.

The study presented a comparative analysis between the City's current Flint Street realignment plans and the proposed Flint Street design from the developer. The developer is proposing two 14 foot lanes with on street parking whereas the City is proposing two lanes with a two-way left-turn lane with no on-street parking. Generally, the two designs have minimal effect on the operations of vehicles exiting the site driveways. However, the City's design experiences smaller left-turn queue lengths in to the site by approximately 6 feet. Left-turn passing lanes are not warranted based on future traffic volumes. The study also determined that a 75 foot northbound left-turn lane should be provided at the intersection of Flint Street and Grand River Avenue.

INTRODUCTION

1. The development includes 250 apartment units and 6,000 square feet (SF) of retail space.
2. The development is proposing a total of three full-access site driveways and one exit-only driveway. The development will be accessed from Flint Street, which will have access to both Novi Road and Grand River Avenue.
3. The TIS included the following study intersections:
 - a. Grand River Avenue & Novi Road
 - b. Grand River Avenue & Flint Street
 - c. Grand River Avenue & Crescent Boulevard (Proposed)
 - d. Main Street/Flint Street & Novi Road
 - e. Site Driveways

DATA COLLECTION

1. Traffic turning movements and volume data were collected on Thursday, September 22, 2016, during the periods of 7:00-9:00 AM and 4:00-6:00 PM. SCATS data from 2018 was used to adjust the 2016 counts to reflect existing traffic conditions.
2. Traffic volumes were balanced between study intersections.
3. Existing signal timing information was obtained from the Road Commission for Oakland County (RCOC).

EXISTING CONDITIONS

1. Synchro traffic analysis software was used to calculate peak hour vehicle delays and levels of service (LOS).
2. Typically, LOS D or above (LOS A representing minimal delay and LOS F indicating failing conditions) is considered acceptable.
3. Under existing conditions, the intersection of Novi Road and Grand River Avenue operates below acceptable conditions during peak hours of traffic. (LOS E – AM, LOS F – PM). All other intersections operate with an acceptable overall LOS given existing conditions.
4. Long vehicle queues were detected for eastbound, westbound, and northbound left-turn movements at the intersection of Novi Road and Grand River Avenue. It should be noted that left-turn phasing is protected-only at the intersection. The study should also indicate the length that the queue exceeds the existing provided storage.
5. Long vehicle queues were detected for the southbound through movement at the intersection of Novi Road and Grand River Avenue. The study should also indicate the length that the queue exceeds the existing provided storage.
6. The study evaluated the following mitigation strategies in order to improve traffic operations:
 - i. Provide permissive/protected left-turn phasing at all left-turn movements at the intersection of Novi Road and Grand River Avenue.
 - ii. Provide a 70-second cycle length during the peak periods at Novi Road and Flint Street in order to reduce minor street vehicle delays while maintaining coordination with adjacent 140-second cycle length signals.
 1. The TIS should provide additional detail in the text regarding how this modification affects the available storage lengths on northbound and southbound Novi Road.
 - iii. Construct an eastbound left-turn lane at Flint Street and Novi Road.
7. The proposed improvements are expected to improve the LOS from E to D and F to E at the intersection of Novi Road and Grand River Avenue for the AM and PM peak periods, respectively. The improvements will also increase the overall LOS from C to B for the intersection of Novi Road and Flint Street during both peak periods.

BACKGROUND CONDITIONS

1. The TIS used a project build-out year of 2021 and used a growth rate of 1.5% based on SEMCOG data from 2012 to 2016.
2. The City of Novi and AECOM provided trip generation and trip distribution numbers for the old Novi Expo site project that is located in the northwest quadrant of Novi Road and Grand River Avenue. The report indicates that this data was attached, but it is not. It should be included as an appendix.
3. The anticipation of the new ring road connection may divert existing trips away from the Novi Road and Grand River Avenue intersection. The study re-distributed trips through to the ring road connection and away from Novi Road and Grand River Avenue based on percentages that were previously approved by the City and AECOM (5% ingress traffic, 10% egress traffic).
4. There is no change to the LOS at the study intersections given the addition of background traffic and the mitigation measures mentioned above; however, there is a non-discernable increase in delay. It should be noted that without the proposed mitigation measures, the intersection of Novi Road and Grand River Avenue may fall below an acceptable LOS. It should be noted that the intersection of Novi Road and Grand River Avenue remained at an LOS E.
5. The analysis still detected long vehicle queues for left-turn movements at the intersection of Novi Road and Grand River Avenue. The study should also indicate the length that the queue exceeds the existing provided storage.

SITE TRIP GENERATION ANALYSIS

1. The TIS utilized the *Trip Generation Manual*, published by the Institute of Transportation Engineers, 9th edition to estimate the number of trips produced by the proposed development. It should be noted that the 9th edition of the *Trip Generation Manual* yields more conservative numbers than the 10th edition of the manual.
2. The *Trip Generation Manual* estimated a total of 2,516 new trips per day and 196 trips during the PM peak hour. The estimate also included 215 trips during the peak hour on Saturday (it should be noted that Saturday scenarios were not included as part of the study). The study should also include AM peak hour trips for the apartments. It is

understood that the retail development will not likely incur trips during the AM peak hour; however, the apartments will have an effect on traffic during that time, and the AM trip generation for any applicable land uses should be included

3. New trips were distributed to the roadway network based on existing peak hour traffic patterns and methodologies published by ITE.

FUTURE CONDITIONS

1. The TIS analysis indicates that the study intersections will operate in a similar manner to existing and background conditions given the suggested mitigation measures with no change in LOS and a non-discernable increase in delay. The new intersection of Crescent Drive and Grand River Avenue is expected to operate at a LOS C for both peak hours of traffic. The existing intersection of Flint Street and Grand River Avenue is expected to operate at LOS C and LOS D for the AM and PM peak hours, respectively. It should be noted that without the proposed mitigation measures the intersection of Novi Road and Grand River Avenue fall below an acceptable LOS.
2. Site driveways are expected to operate at LOS B or above.
3. The right-turn lanes nor left-turn passing lanes are required based on City standards at the site driveways.
4. The TIS references that Flint Street eastbound left-turn lane at Novi Road is not expected to exceed the available storage length; however, there is not an existing left-turn storage length. The TIS should elaborate on the left-turn storage length that is being referred to. The Flint Street northbound left-turn lane is expected to experience 95th percentile queues of 74 feet and 58 feet during the AM and PM peak hours, respectively. The study suggests that a 75-foot left-turn lane is provided at Flint Street and Grand River Avenue for the northbound approach.
5. The TIS completed a comparative analysis of the City's Flint Street re-alignment plans and the proposed design for Flint Street based on the District development. The analysis indicated that the site driveways are expected to experience similar delays and LOS for both designs. However, the City's design generally offered shorter queue lengths during both the AM and PM peak hours.

SUMMARY

1. The applicant should confirm whether or not the proposed mitigation measures with regard to signal timing/phasing adjustments will be acceptable to the City and RCOC.
2. The study should be updated to include AM peak hour trip generation estimates.
3. The study should be reviewed to confirm that the trips were distributed to the correct driveways in accordance with the trip generation and existing volumes on the roadways. There seem to be some intersections where volumes appear to be inconsistent. For example, the retail development is generating several trips during the AM period according to Figure 4 and even more trips according to Figure 5. Because this driveway does not exist in current conditions, the volumes should be consistent between the two Figures. If this is not the case, the study preparer should provide justification for the values presented.
4. The proposed street alignment and cross section is expected to operate similar to the City's roadway design for Flint Street. The intersections of Flint Street at Grand River and at Novi Road were modeled to have exclusive left turn lanes and shared right-thru lanes, and operations are expected to be acceptable under future conditions.

Should the City or applicant have questions regarding this review, they should contact AECOM for further clarification.

Sincerely,

AECOM

Memo

A handwritten signature in blue ink, appearing to read "Sterling Frazier".

Sterling Frazier, PE
Reviewer, Traffic/ITS Engineer

A handwritten signature in blue ink, appearing to read "Maureen Peters".

Maureen N. Peters, PE
Senior Traffic/ITS Engineer

FAÇADE REVIEW



June 19, 2018

City of Novi Planning Department
 45175 W. 10 Mile Rd.
 Novi, MI 48375-3024

Attn: Ms. Barb McBeth – Director of Community Development

Re: FACADE ORDINANCE Revised Final Site Plan
The Bond (FKA The District), JSP18-0010
 Façade Region: 1, Zoning District: TC-1

Dear Ms. McBeth:

The following is the updated Façade Review for the above referenced project based on the drawings provided by Humphreys & Partners, Architect, dated 6/12/18 (Scheme 5). The percentages of materials proposed for each façade are as shown below. The maximum allowable and minimum required percentages of each material are indicated in the right-hand column. Materials in non-compliance are highlighted in bold.

Building Type 1	East (Front)	North	South	West	Façade Ordinance Section 5.15 Maximum (Minimum)
Brick	28%	28%	26%	47%	100% (30% Min)
Cast Stone	22%	20%	24%	5%	50%
EIFS	28%	38%	35%	48%	25%
Metal Panel (Woodgrain)	15%	7%	8%	0%	50%
Spandrel Glass	3%	3%	3%	0%	50%
Fabric Awnings	3%	3%	3%	0%	10%
Flat Metal (Canopies)	1%	1%	1%	0%	50%
Combined Brick and Stone	50%	48%	50%	52%	TC-1 Ordinance 3.26.1.G, 50% Minimum

Building Type 2	East (Front)	North	South	West	Façade Ordinance Section 5.15 Maximum (Minimum)
Brick	28%	28%	26%	47%	100% (30% Min)
Cast Stone	22%	20%	24%	5%	50%
EIFS	28%	38%	35%	48%	25%
Metal Panel (Woodgrain)	15%	7%	8%	0%	50%
Spandrel Glass	3%	3%	3%	0%	50%
Fabric Awnings	3%	3%	3%	0%	10%
Flat Metal (Canopies)	1%	1%	1%	0%	50%
Combined Brick and Stone	50%	48%	50%	52%	TC-1 Ordinance 3.26.1.G, 50% Minimum

Building Types 1 and 2 - Section 3.26.1.G of the TC-1 Ordinance required that facades be constructed “primarily of Brick and Stone”. As shown above the minimum amounts of Brick (30%) is not provided on the east, north and south facades, the combined percentage of Brick and Stone (50%) is not provided on the north facades, and the percentage of EIFS exceeds the Ordinance on all facades. Façade Waivers in accordance with Section 5.15.9 of the Ordinance would be required for these deviations.

Parking Structure	West	North	South	East (Btwn. Bldg. 1 & 2)	Façade Ordinance Section 5.15 Maximum (Minimum)
Brick	0%	0%	0%	45%	100% (30% Min)
Concrete	100%	0%	0%	0%	0%
Living Wall (Must have Brick or Stone behind)	0%	0%	0%	17%	50%
Cast Stone	0%	100%	100%	38%	50%
Combined Brick and Stone	0%	100%	100%	83%	TC-1 Ordinance 3.26.1.G, 50% Minimum

Parking Structure – The applicant has revised the drawings in response to prior comments to indicate Brick and Stone in the visible portions of the north and south facades and the portion of the east façade located at the west end of the courtyard. As shown above the minimum amount of Brick (30%), is not provided on the west, north and south facades, the combined percentage of Brick and Stone (50%) is not provided on the west façade, and the proposed percentage of Concrete exceeds the maximum amount allowed by the Ordinance by 100% on the west facade. In this case the west façade is located adjacent to the railroad right of way with a warehouse building beyond and as such will not readily visible to the public for the foreseeable future. The Cast Stone (100%) on the north and south facades will visually appear as a continuation of the adjacent building. A Section 9 Waiver for the west façade may be justified on this basis.

Commercial Building	North Front	West	South	East	Façade Ordinance Section 5.15 Maximum (Minimum)
Brick	23%	8%	8%	17%	100% (30% Min)
Cast Stone	55%	76%	76%	64%	50%
Ribbed Metal (Horizontal)	12%	6%	6%	9%	0%
Flat Metal (Canopies & Cornice)	10%	10%	10%	10%	50%
Combined Brick and Stone	78%	84%	94%	91%	TC-1 Ordinance 3.26.1.G, 50% Minimum

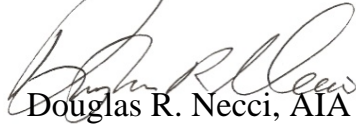
Commercial Building - As shown above the minimum percentage of Brick and Stone (50%) is provided on all elevations. The percentage of Horizontal Ribbed Metal exceeds the maximum percentage allowed by the Ordinance on all facades. A Section 9 Waiver would be required for these deviations. In this case the used of Horizontal Ribbed Metal enhances the overall design and is consistent with the intent and purpose of the Ordinance. A Section 9 Waiver is therefore recommended fort the overage of Horizontal Ribbed Metal Siding.

Recommendation – In general the buildings exhibit interesting massing and the creative use of materials and colors. The applicant has revised the facades in response to prior comments to increase the percentage of Brick and reduce the percentage of EIFS on Building Types 1 and 2. The combined percentage of brick and stone is now at or near 50% on all facades. We believe that these deviations are minor in nature and that the overall appearance of the building would not be significantly improved by strict application of the percentage listed in the Ordinance. Therefore, it is our recommendation that the designs are consistent with the intent and purpose of the Façade Ordinance and that Section 9 Waivers be granted for the following deviations;

1. For not providing the minimum required percentage of Brick (30%) on the east (28% proposed), north (28% proposed) and south (26% proposed) facades of Buildings 1 and 2;
2. For exceeding the maximum allowed percentage of EIFS (25%) on the east (28% proposed, north (38% proposed), south (35% proposed) and west (48% proposed) facades of Buildings 1 and 2;
3. For not providing the minimum combined percentage of Brick and Stone required for the TC-1 District (50% minimum required, 48% proposed) on the north façade of Buildings 1 and 2;
4. For not providing the minimum required percentage of Brick (30% minimum required) on the east (23% proposed, north (8% proposed), south (8% proposed) and west (17% proposed) facades of the Commercial Building;

5. For exceeding the maximum allowed percentage of Cast Stone (50% maximum allowed) on all facades (Proposed: East-55%, North-76%, South- 76% and West-64%) of the Commercial Building;
6. For exceeding the maximum allowed percentage for Ribbed Metal (0% allowed) on all facades (Proposed: East-12%, North-6%, South- 6% and West- 9%) of the Commercial Building;
7. For exceeding the maximum allowable percentage of Concrete (0% allowed, 100% provided, and not providing the minimum required percentage of Brick (30% minimum required, 0% provided) on the west facade of the Parking Structure;
8. For exceeding the maximum allowable percentage of Cast Stone (50% allowed, 100% proposed) and not providing the minimum percentage of Brick (30% required, 0% provided) on the north and south facades of the parking structure. Note that these elevations essentially appear as a continuation of the adjacent buildings.

Sincerely,
DRN & Architects PC



Douglas R. Necci, AIA

FIRE REVIEW



June 14, 2018

TO: Barbara McBeth- City Planner
Sri Ravali Komaragiri- Plan Review Center
Lindsay Bell-Plan Review Center
Hannah Smith- Plan Review Center

CITY COUNCIL

Mayor
Bob Gatt

Mayor Pro Tem
Dave Staudt

Andrew Mutch

Wayne Wrobel

Laura Marie Casey

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City Manager
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David E. Molloy

Director of EMS/Fire Operations
Jeffery R. Johnson

Assistant Chief of Police
Erick W. Zinser

Assistant Chief of Police
Scott R. Baetens

RE: The District/The Bond (FKA Flint Street Development)

PSP#18-0089

Project Description:

Build 2 multi-story/multi family structures off of Flint St., and 1 commercial building property off Novi Rd x Flint St.

Comments:

- MUST provide water-mains and sizes on a site plan for review.
- **CORRECTED 3/22/18** - MUST provide drawings to scale for turning radius review.
- **Turning radius in the middle and south parking lots do not city standards for 50' outside and 30' inside.**
- **CORRECTED 6-14-18 Building >55' MUST be built to High Rise specifications.**
- **CORRECTED 6-14-18 KSP.** Hydrant spacing is 300' from hydrant to hydrant (Not as the crow flies). Novi City Ordinance 11-68(F)(1)C.
- FDC locations MUST be with-in 100' from a fire hydrant. (Novi City Ordinance Sec15-17 912.3). **However FDC locations are NOT included for the residential buildings on this submittal. They must also be located within 100" of hydrants and not obstructed by landscaping.**
- Fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all-weather driving capabilities supporting thirty-five (35) tons (Novi City Ordinance 503.2.3 Surface).
- **CORRECTED Dry standpipes/FDC's on each end of parking structure KSP 6-14-18.** For the parking structures: The parking structures lengths are (710') longer than the fire departments pre connect hose lays. Need to figure out a solution for this issue. Possible dry stand pipe connections from both ends towards the middle.

Novi Public Safety Administration
45125 Ten Mile Road
Novi, Michigan 48375
248.348.7100
248.347.0590 fax

cityofnovi.org

Recommendation:
APPROVED WITH CONDITIONS

Sincerely,

A handwritten signature in black ink, appearing to read 'KSP', with a long horizontal flourish extending to the right.

Kevin S. Pierce-Fire Marshal
City of Novi – Fire Dept.

cc: file

APPLICANT RESPONSE LETTER
At the time of site plan submittal



June 11, 2018

RECEIVED

JUN 11 2018

CITY OF NOVI
COMMUNITY DEVELOPMENT

City of Novi
Att: Sri Ravali Komaragiri
Planner
Community Development Department
45175 W. Ten Mile Road
Novi, MI 48375

Re: Petitioner/Applicant for The Bond (fka The District) Response to the Plan Center Report (i.e. Planning Review Letter) Dated June 1, 2018 (JSP 18-0010 The District)

Sri:

The following summarizes responses to the Plan Review Center Report dated June 1, 2018.

Project Background

This letter provides categorical responses to the Plan Review Center Report issued by the Planning Department ("Staff"), dated June 1, 2018. However, prior to addressing Staff comments, we felt it was important to revisit discussions that occurred between Staff and Tricap Holdings (Applicant) as part of the planning and pre - development process, which began in early 2017.

The Bond (fkaThe District) proposed mixed - use project includes an approximate 6,000 square foot commercial building on one acre, as well as two four story, luxury mid - rise apartment building encompassing approximately 275,000 square feet, supported by a 270 space, multi - level parking deck that is attached to the building. From the beginning it was the Applicant's intent to introduce and develop a market rate luxury product type that had yet to be built in Novi, in keeping with the spirit of the intent of the Town Center/urban vision and offering a unique, local residential experience within walking distance of all of the Town Center ("TC") District. A project that would readily co-exist, compliment and support the surrounding retail, restaurant and commercial environment.

This concept appeared to be consistent with the type of project the City was looking to attract in the TC District, but it was also acknowledged by all parties that current and even recently enacted residential ordinances may not completely align with the described product design parameters. However, given the desire to introduce a stronger residential component to the Town Center area, combined with the desire to redevelop an area of the City with much potential, yet with numerous challenges, The Bond appeared to be an ideal candidate for the location.

The luxury mid - rise apartment building is not a new residential concept. Thousands of units are built each year, predominantly in core urban cities and many vibrant suburban communities offering close proximity to retail, restaurant and entertainment districts. However, it is accurate to say this residential concept is somewhat new to Novi and specifically, the Town Center District, which currently consists primarily of retail, restaurant and commercial businesses, with few, if any "urbanesque" residential options.

Residents that choose to live in this style of apartment community are looking for a more robust lifestyle experience than offered in a traditional suburban apartment community. They tend to live and use the entire building and its amenities and not just the apartment unit. The property and entire building becomes their "unit". The typical amenities and demand drivers in a luxury mid-rise property like The Bond include:

- fitness centers (often more than one)
- business centers
- resident clubrooms (often more than one)
- active and passive outdoor and courtyard gathering spaces with grilling spaces, kitchens, outdoor lounge, exercise and and gaming areas
- active social programming and regularly scheduled resident events
- extensive outdoor pool areas with socially focused seating and gathering spaces
- secure bike repair and storage rooms within the building
- sheltered parking with direct access to the building
- close proximity to restaurants, retail and entertainment venues
- technology with accessible connectivity (i.e. fiber, wi - fi, etc)
- smaller unit sizes in lieu of more resident amenities

Based on these characteristics, this building type tends to be more efficient with respect to unit size and mix with a much higher percentage of common area relative to total unit square footage. Also, while many common in many urban cores, the building is more dense with respect to land coverage than the current Novi ordinances have allowed, and common areas are larger and units are somewhat smaller. By example, The Bond will offer approximately 25,000 – 30,000 square feet of indoor and outdoor common area and resident gathering spaces that are accessible to tenants 24 hours per day, compared to a more traditional 250 unit suburban style apartment community that might offer a 5,000 – 6,000 square foot clubhouse with perhaps a few other outdoor resident spaces.

In addition to discussing a more modern and urban product type the City indicated their desire and need to realign and reconstruct Flint Street as part of the City's comprehensive plan to connect the southwest loop (i.e. Flint Street) to the TC District. While a new street will provide a mutual benefit to the City and the project, the Applicant is gifting the property necessary to achieve the desired road profile and realignment. This approximate one acre of land provides a benefit for all, but also creates design and ordinance challenges for the project when combined with the shallow configuration of the parcel. This is more evident as we approach the Planning Review process, particularly as it relates to parking, building setbacks, landscaping requirements, parking setbacks and other site related issues.

From the time first introduced to the City, it was always understood that to get to the outcome that was mutually desired on this challenging parcel, many deviations from existing ordinances might be required.

Applicant Response to Plan Center Report Dated June 1, 2018

Categorical Response to Staff Comments Regarding Conformance with the Zoning Ordinance with Respect to Article 3 (Zoning Districts), Article 4 (Use Standards, Article 5 (Site Standards and any other Applicable Provisions of the Zoning Ordinance are as follows:.

1. **Narrative** – Submit a narrative providing explanation and supporting visuals to address the requested deviations.

Response – submitted as part of the comprehensive response

2. **Density and Total Number of Rooms and Maximum Percentage of 1 Bedroom Units that Exceeds the Ordinance (Requires ZBA Approval):**

Code Deviatlon #1 – The applicant is proposing 627 rooms with a total density of 33 DUA. City Council can approve the increase in the room count (421 allowed, 627 proposed) up to twice the number of rooms allowed and thus the increase in density proposed (23 DUA approximate allowable, 33 DUA proposed). The master plan for the land use recommends a density of 20 DUA for the subject property.

Response #1 - Consistent with Applicant's comments in the Project Background above, The Bond is a lifestyle oriented urban/suburban designed building offering a desirable mix of resident amenities different from traditional luxury market rate apartment communities. The scale/density of the project is imperative to justify the scope of amenities and legitimize the project's unique yet isolated location in this area of the TC. As a result of The Bond's isolated location on Flint Street it has no notable impact on adjacent property owners or businesses, particularly as it relates to traffic. This appeared to be consistent with the product type sought by the City for this location.

Code Deviatlon #1 – The maximum percentage of 1 bedroom units is 50% (58% is proposed). Staff recommends revising the unit mix to conform to 50%.

Response #2 - The unit mix of 1, 2 and 3 bedroom units was based on an internal marketing survey and assessment of the Novi luxury rental market that indicates a desired target mix of approximately 60% 1 bedroom units. This was based on a number factors including the target demographic and renter profile, indicated rental rates, residential amenities and experiences that will be offered to residents. The mix is also consistent with similar, recently built and stabilized properties owned by the applicant in similar Michigan markets (i.e. Lansing and Grand Rapids). Other key factors contributing to the unit mix determination:

- A key demographic for the property will include young professionals, both singles and couples, demanding 1 bedroom units under \$1400 per month.
- A unit mix that offers a number of 1 bedroom configurations, covering a range of almost 200 square feet, offering a variety of floor plans and premium locations within the property.
- A limited number of luxury 1 bedroom units currently offered in the Novi market that do not offer like amenities or resident experiences (e.g. private parking deck, active and passive outdoor courtyard experiences, resident club rooms, bike repair centers, walking distance to Novi Town Center).
- The shift to a larger percentage of 1 bedroom units is consistent with the industry overall as it relates to the urban-suburban luxury rate market given the design attributes of the product type.

As noted below, applicant will seek a variance from the Zoning Board of Appeals for an increase from 50% to 58% 1 bedroom units.

3. **Total Parking Required and Proposed**

Code Devlation – The new established minimum for parking is 360 spaces with an additional 72 for future parking, or a total of 432. Applicant has revised the site plan and is now proposing 432 parking spaces (reduced from 461) to provide for more open space and landscaping enhancements.

Response - No deviation sought as proposed parking is consistent with parking requirement.

4. **Photometric Plan (Sec 5.7.1)** – The Plan Center Report indicates refers to to Planning Review Chart for additional comments that need to be addressed prior to approval of Photometric Plan. Those comments in the Chart indicating Applicant response are as follows:
- a. **Intent** - some of items as noted in photometric plan do not conform to code.
 - *Response* - The product type and parcel size are both unique and Applicant requests an evaluation of the requested deviations/variances on the overall merits of the project and intent of Applicant to limit deviations and variances.
 - b. **Lighting Plan** – show all existing & proposed buildings, landscaping, streets, drives, parking areas, etc.
 - *Response* – Current Photometric Plan includes all street, drives and parking areas. Landscaping lighting and existing is not shown and will be completed prior to Final Site Plan submittal.
 - c. **Building Lighting** – Relevant building elevation drawings showing all fixtures, portions of walls to be illuminated, illumination levels, etc.
 - *Response* – Building mounted lighting is included, but complete lighting design is limited at this stage, but will be completed prior to final site plan submittal.
 - d. **Lighting Plan** – The following is requested regarding specifics related to the Lighting Plan:
 - **Specifications** – provide for all proposed and existing lighting fixtures, photometric data and fixture height.
 - *Response* – spec sheets were provided with the original submittal on May 10, 2018.
 - **Hours of Operation** – provide hours of operation.
 - *Response* – site employees will be on site from 8am to 6pm, but the building will be functional and available to for resident use 24 hours.
 - **Photometric Plan** - illustrating all light sources, including spill over from neighboring properties
 - *Response* – Information is attached, including foot candle values
 - e. **Required Conditions** – Light pole height not to exceed 65 feet
 - *Response* – maximum light pole height is 25 feet.
 - f. **Required Conditions** – Service to fixtures must be underground, no flashing light permitted and only necessary lighting for security purposes and limited operations after hours

- *Response* – all service to be underground, there is no flashing lighting and limited lighting after hours is planned.
 - g. **Security Lighting** – Lighting for security shall be directed only onto the area to be secured. Please provide a photometric plan for security.
 - *Response* – The photometric plan includes all lighting, including security. All building lighting is shielded and no floods which require aiming are being used.
 - h. **Average to Minimum Light Level Ratio** - Exceeds 4:1 for east side of parking and drive and north parking and drive.
 - *Response* – Applicant requests a deviation/variance if necessary.
 - i. **Type of Lamp Fixtures** – Use of true color required and LED is proposed
 - *Response* – LED use is requested and Applicant can provide 3,000k or 4,000 k CCT.
 - j. **Maximum Illumination** – the site abuts a non-residential district and illumination at property line shall not exceed 1 foot candle.
 - *Response* - Although the property abuts non - residential, the property as located on Flint Street is isolated and there are no adjacent improvements to impact. Additionally, this will hurt the max/min 4:1 ratio even more.
5. **Planning Review Chart** – Please refer to Planning Review chart for additional minor comments that need to be addressed for further clarification.

Response – minor comments are addressed as requested.

6. **Project Phasing (Clarification Required)** – The applicant is proposing to phase construction and more clarification with respect to phasing was requested.

Applicant Response – The following clarification is provided:

- *Question:* How is phased construction proposed to coordinate with Flint Street construction.
 - o *Response* - Applicant recommends this item be addressed at the preconstruction meeting should the project be approved. However, it seems practical the City could commence reconstruction of Flint Street from the Novi Road access (i.e south) and applicant could access and begin construction from the from Grand River access (i.e. north). This is based on the assumption the Flint Street culvert is scheduled to be replaced upon commencement of reconstruction of Flint Street.
- *Question:* Provide a separate phasing plan with detailed phasing notes.
 - o *Response* – Applicant has added a phasing plan with applicable notes. **See additional phasing plan attached as part of the Revised Site Plan dated June 11, 2018.**

- *Question:* How is site grading and stabilization proposed with phasing plan?
 - o *Response* – all site grading and stabilization will be completed as part of Phase I.

 - *Question:* Provide more information on interim improvements between phases
 - o *Response* - There are no planned interim improvements at this time.

 - *Question:* How does site circulation function with a half - built garage?
 - o *Response* - **See Revised Site Plan dated June 11, 2018** and notes attached, which illustrates a temporary parking plan for the Phase I ramp construction.

 - *Question:* Cemetery parking is requested with Phase I. Is that possible?
 - o *Response* - Yes, temporary parking can be constructed to support cemetery access during Phase I. **See Revised Site Plan dated June 11, 2018 for changes.** Also, additional cemetery parking spaces were added per City request.
7. **Exterior Signage** – Applicant will submit a sign application after Site Plan approval.
8. **Conservation Easement** – to be submitted with Final Site Plan submittal, if required

Identified City Council Waivers/DCS Variances

(Please note: Applicant has worked with Staff with the intent of eliminating or reducing as many deviations as possible).

Ordinance deviations are noted as follows:

1. **Number of Rooms and Area of Parcel (Sec 4.82.2a)** - See comments and applicant response on Page 1 and above.

2. **End Islands (Sec. 5.3.12)** – A City Council Waiver is required to allow painted end islands in lieu of required end islands as listed in Sec 5.3.12.

Applicant Response – No deviation or variance requested. Applicant eliminated the painted end islands. **See revised site plan dated June 11, 2018.** .

3. **Commercial Parking Front Yard Setbacks (Sec. 3.1.25.D)** – 20 feet is required and 8 feet is proposed.

Applicant Response – No deviation or variance sought as Applicant revised the site plan and is now providing a 20 foot front yard setback. **See revised site plan dated June 11, 2018.**

4. **Commercial Parking Rear Yard Setbacks (Sec. 3.1.25.D)** – 10 feet is required and 7 feet is proposed. Staff recommends reducing parking and enhancing or improving landscaping.

Applicant Response - As noted on page 1 and as requested by the City, applicant agreed to add additional parking along the rear yard to provide for cemetery parking access. A deviation/waiver is requested to provide for the spaces needed for cemetery parking spaces. Also, additional cemetery spaces (i.e. 6 in total) have been made available in the revised site plan. **See revised site plan dated June 11, 2018.**

5. **Traffic Waivers** – The site plan indicated a same side driveway spacing issue that could require a waiver and refers to the Traffic Review Letter for further discussion.

Applicant Response – Consistent with the Traffic Review Letter, the applicant has agreed to relocate the northwestern most driveway farther northwest of the intersection on the opposite end of the bay along Flint Street. No waiver is requested.

6. **Façade Waivers** – The Façade Review identified a couple of deviations from the Façade ordinance and recommends a Section 9 waiver. Deviations and suggested changes are as follows:
- o Provide revised drawing of north and south of ends of parking structure, showing brick or stone on the elevations facing the public side of the building (east side)
 - o Reduce percentage of EIFS and increase the brick and stone on buildings 1 and 2.

Applicant Response – As suggested by façade consultant, applicant made the following revisions to the residential building:

- Revised drawings to show north and south ends of parking structure, adding brick and stone on public facing elevation.
- Replaced EIFS with brick at stair tower locations on each building, proportionately increasing brick and decreasing EIFS. The
- Applicant requests a Section 9 façade waiver to allow for any resulting overage of EIFS and underage of the combined brick and stone requirements consistent with the revised drawings dated June 11, 2018.
- Façade consultant indicated in his review letter he would support a Section 9 waiver assuming the recommended changes were adopted.

7. **Landscape Waivers** – The following landscape waivers are requested:

Residential Waivers Requested:

- a. Sec 5.5.A – Requiring a 10' – 15' high berm adjacent to the I-1 Zoning District. A two - story parking deck abuts the I-1 Zoning and is planted with evergreens and effectively buffering the residences.
- b. Sec 5.5.3.B.ii.f – Requiring a 20' greenbelt adjacent to parking. 119 lineal feet of parking is located within the 20' greenbelt. Evergreen hedges have been added to screen the parking.
- c. Sec 5.5.D.ii.b – Requiring 75% of foundation landscaping to be located in beds with a minimum 4' width. Due to the urban nature of this project, beds are less than 4'.
- d. Sec 5.5.F.ii.b(1) – Requiring 3 trees per unit. This requirement cannot be met due to the limited planting area available. A waiver of 11 trees is requested.
- e. Sec 5.5.F.ii.b(2) – Requiring internal street trees. Two trees cannot be planted due to the fire access lane.

Commercial Waiver Requested:

- f. Sec 5.5.3.D.ii.b – Requiring planting beds along 75% of the building. A waiver of 25% is requested based on the building footprint. The footprint is conceptual and additional planting opportunities could exist as the building design is finalized. Planters can also be added once the building elevation is confirmed.

Zoning Board of Appeals - Variance Requests

The following is a list of deviations that will require site plan revisions or a variance request from the Zoning Board of Appeals:

1. **Parking Screening (Sec 3.27.1.D)** – The applicant has proposed a 2.5 foot thick wall, but it does not adequately screen all parking spaces. Absent a revision, the ZBA variance may be required.

Applicant Response – Based on the revised site plan dated June 11, 2018, Applicant is in compliance with the parking screening ordinance.

2. **Parking Along Non-Residential Collector (Sec 3.27.1.D)** – Parking is not allowed in the front yard or the side yard. The applicant is proposing parking in front and side yards along Flint/Bond Street.

Applicant Response – Consistent with staff support and comments, the irregular shape of the parcel provides limited parking access in the rear of the property. Applicant reduced the deviation by eliminating 29 parking spaces in the front and side yards and included a 270 space parking deck in the rear yard.

3. **Loading Areas (Sec 3.27.1 H and Sec 5.4.2)** – All loading areas in TC – 1 shall be in rear yards. Loading is proposed in both side yards.

Applicant Response – Consistent with staff comments and support the shape and size of the parcel does not permit for rear yard loading. Responses to Traffic Review Letter concerns regarding loading is as follows:

- a. Dimensions to be provided for each loading zone
- b. Appropriate signage and turning paths to be provided for trash collection vehicles to avoid access issues
- c. Trash collection dates/times to be posted to avoid conflict between loading and trash collection. Trash will be collected once per week and any resident scheduled moves will be coordinated with the site operations personnel.
- d. Loading zone programming and challenges are consistent with Applicant's other urban properties requiring coordination and management of loading zone and trash collection routines, which are managed without incident.

4. **Sidewalk Along Bond fka Flint Street (3.27.1.1)** – Sidewalks required along non-residential collector to be 12.5 feet wide. Proposed sidewalk along Flint Street is 8' and therefore does not conform.

Applicant Response – Consistent with staff support and comments, the City is reconstructing and realigning Flint Street, supported by a gift of approximately one acre of land from the applicant, that will provide the character and practical effect of a residential collector. Also, it is the Applicant's understanding an additional 10' wide sidewalk will be constructed on the east side of Flint Street as part of the reconstruction project. A suggested way of looking at the sidewalk provided would be to look at the combined sidewalk width along Bond/Flint Street (i.e. 6') together with the sidewalk next to Building #2 (i.e. 7'), which totals approximately 13' of total sidewalk, or roughly the required sidewalk width.

5. **Building Setbacks (Sec 4.82.2e)** – Ordinance requires 15 feet and applicant proposes 8.1' for Building #2 and 12.2' feet for Building #1.

Applicant Response – Building Setback Variance Request – The building encroachment is dictated by taking of the ROW and the irregular shape of the parcel with the encroachment extending for approximately 3.5% of the total building road frontage (20' of encroachment, divided by 566' of building frontage along Flint Street), or less than 0.2% of the building footprint (120 sq ft divided by 72k square feet of building footprint). The setback, as designed, provides for what the applicant feels is the best balance between building, surface parking and structured parking location. The extent of the deviation will be clearly identified on the site plan.

In addition to the building not meeting the minimum setback requirement, the parking garage is also considered a building and is only setback 5 feet from the rear yard. The parking garage abuts a wide rail easement and industrial property consisting of a distribution center/warehouse with loading docks that face the parking garage.

6. **Parking Setbacks Off-Street Parking (Sec 4.82.2.f)** – 10 feet is required from ROW, minimum of 6.5 proposed.

Applicant Response – Based on the revised site plan dated June 11, 2018, parking setback deviations for off street parking have been eliminated.

7. **Parking Setbacks Off Street Parking (Sec 4.82.2.f)** – A minimum of 10 feet is required from any wall of any dwelling structure, which contains openings involving living areas. 8 feet is proposed.

Applicant Response – Based on the revised site plan dated June 11, 2018, parking setback deviations for off street parking have been eliminated, except for the 6 spaces designated for cemetery parking in the commercial area. Applicant is requesting a variance for the cemetery spaces.

8. **Maximum Allowable Percentage of Units (Sec 4.82.2)** – 50% maximum of 1 bedroom units allowed. 58% proposed.

Applicant Response - See discussion on page 2 – Code Deviations – Maximum percentage of 1 bedroom units of 50%.

9. **Parking Stall Located Adjacent to Parking Lot Entrance (Sec 5.3.13)** – A parking space shall not be located closer than 25 feet from the street ROW line. Some spaces are closer than 25 feet.

Applicant Response – Applicant eliminated all spaces within 25 feet from the street ROW and therefore, a deviation or variance will not be requested. Please see the revised site plan dated June 11, 2018.

10. **Loading Space Area (Sec 5.4.2)** – Loading space area should be provided in the ratio of 10 square feet per front foot of building. For residential, 2830 sq ft, per building would be required and approximately 644 sq ft of loading space is proposed for each building.

Applicant Response – consistent with staff comments and support, 644 square feet per building is adequate for residential buildings as loading needs are limited to resident move out and move ins. Therefore, a variance to support 644 square feet per building is requested.

11. **Average to Minimum Light Level Ratio (Sec 5.7.3.E)** – Average light level of the surface being lit to the lowest light of the surface being lit shall not exceed a ratio of 4:1. Some areas appear to exceed 4:1.

Applicant Response – It appears the average minimum ratio will exceed 4:1 and the applicant will be requesting a variance from the minimum.

12. **Parking Space Dimensions and Maneuvering Lanes (Sec 5.3.2)** – A ZBA variance is required for not meeting the minimum depth requirement for parking spaces in a garage/ramp. 19 feet is required and 18 feet is proposed.

Applicant Response – Consistent with staff support and discussion, a standard pre - cast parking structure is designed to accommodate parking spaces 18 feet deep and 24 foot drive aisles for turning and maneuvering. Modification is deemed costly and unnecessary given the dimensions are an industry standard.

Applicant Responses to Other Reviews

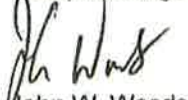
Please see responses attached as follows:

1. Engineering Review, Wetland Review, Traffic Review and Fire Review – Please see attached letter from Seiber Keast Engineering, LLC, dated June 11, 2018.
2. Landscape Review – Please see attached letter from Allen Design, LLC, dated June 11, 2018.
3. Woodland Review – No response required. Additional comments to be addressed with Final Site Plan approval.
4. Façade Review – Please see response and waiver request above - Identified City Council Waivers (Item #6) above.

Should you have any questions or want to discuss specific items noted above please call at your earliest convenience at 248-361-0718. All revision discussed at our June 5th meeting will be submitted on Monday, June 11th. Thanks again and we look forward to the June 27th Planning Commission meeting.

Sincerely,

DTN Development Group and
Tricap Holdings LLC



John W. Woods
DTN

Cc: Glenn Cantor, Tricap
Albert Ludwig, Tricap
Chuck Holman, DTN

SEIBER KEAST ENGINEERING, LLC
ENGINEERING CONSULTANTS

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100 MainCentre, Suite 10
Northville, MI 48167
Phone No. 248.308.3331
E-mail: be@seiberkeast.com

June 11, 2018

Ms. Sri Komaragiri, Planner
City of Novi
45175 W. Ten Mile Road
Novi, MI 48375

Re: The Bond
Preliminary Site Plan Engineering Review
JSP 18-0010

Dear Ms. Komaragiri:

In accordance with your staff Engineering Review dated June 4, 2018 for the Preliminary Site Plan submittal, we offer the following comments in response. The comment numbers shown below corresponds to the comments contained in the consultant or staff review letters where applicable.

ENGINEERING REVIEW

The Engineering Review letter from the City of Novi is recommending Approval of the Preliminary Site Plan and Preliminary Storm Water Management Plan. All engineering review comments discussed in the review letter will be addressed at the time of Final Site Plan submittal. The Soil Erosion and Sedimentation Control Plan checklist and application for permit will be submitted to the City of Novi at the Final Site Plan submittal.

WETLAND REVIEW

The Wetland Review letter from the City of Novi is recommending Approval of the Preliminary Site Plan. All wetland review comments discussed in the review letter will be addressed at the time of Final Site Plan submittal.

Impacts to the existing Middle Rouge River watercourse will be minimized to the extent possible. Currently the only impacts required are the outlets from the two detention basins, both of which will impact the 25-foot watercourse setback. Any MDEQ permits required for the detention basin outlets to the Middle Rouge River will be obtained prior to construction.

TRAFFIC REVIEW

External Site Access and Operations

3. Site distance requirements at each driveway have been revised in accordance with Figure VIII-E of the City's Code of Ordinances. SKE is in agreement that several of the parallel

parking spaces needed to be removed to provide adequate site distance at each entrance. The Site Plan has been revised to remove 11 of the parallel parking spaces as a result. Site distance was of particular concern at the bend in the proposed Bond Street alignment.

4. The driveway to the northwesternmost parking area has been revised as discussed in the review letter to provide better spacing between the drive approach and the existing Flint Street.

Internal Site Operations

1. General Traffic Flow.

- a) The dimension for the parking structure ramp has been revised to 22' in accordance with Novi Standards.
- b) All turning radii are provided on the Site Plan.
- c) Pavement markings will be provided at Final Site Plan. Key radii are provided on the Site Plan at locations where future paint lines can be located.
- d) Adequate parking lot dimensions are provided to allow Fire Department access.
- e) SKE will coordinate with the City of Novi to provide adequate markings in and around the parking deck and ramp areas at Final Site Plan.
- f) Adequate turning radii are provide on the site plan. All internal traffic through out the development is 2-way except the parking area between buildings 1 and 2.
- g) The proposed loading areas are adequate for the use of the residential buildings. Please see the Planning review for further discussion on the loading areas.
- h) Mailboxes are located internal to the buildings.
- i) Snow removal on the upper deck of the parking structures will utilized the ends of the structure for temporary storage of the snow. Normal snow removal and maintenance crews will remove the snow from the temporary storage areas as quickly as possible. Snow storage for the site will be provided in various lawn areas located behind the mountable curb provided throughout the site.

2. Parking Facilities

- b) A variance will be sought for the reduced parking lot space length (18') required in the parking structure.
- c) The parking calculations have been updated to provide the correct number of accessible spaces.
- d) Van accessible parking signage has been revised as requested. The aisle between the hcp spaces at the commercial building has been widened to 8' to provide V.A. spaces at the commercial building as requested.
- e) Barrier free spaces have been relocated to be nearer to entrances.
- f) The spaces marked as "designated for cemetery visitors" will be signed accordingly. The type of signage provided will be coordinated with the City of Novi prior to Final Site Plan.
- i) All of the landscape islands are designed to Novi Standards. The island detail originally shown on sheet 2 has been removed. Please note that the "3' short" dimension discussed in the review letter is not required at any of the parking areas in the Site Plan.
- j) The painted end island has been removed from the Site Plan.

Ms. Sri Komaragiri, Planner
June 11, 2018
Page 3

- k) A T-Turn around area has been added to the Site Plan and is shown in the new Phasing Plan Sheet for clarity.
3. Sidewalks adjacent to outdoor bike parking areas are 6' wide minimum. A 5' wide sidewalk is provided at the cemetery parking area.

Signing and Striping

1. Sign quantities have been revised as requested and the Signing and Striping notes have been added to the Site Plan.

FIRE DEPARTMENT REVIEW

SKE and TriCap Holdings previously met with the Fire Department on 2/5/2018 to discuss Fire Marshall Review Comments. In that meeting it was determined that middle parking area between buildings 1 and 2 as well as the commercial parking area would be exempt from the turning radius requirements for Fire Trucks. Since both of these areas are less than 150' in length from the nearest intersection, a Fire Truck is able to back up and turn around at the adjacent intersection.

Fire Hydrant spacing has been revised to a maximum of 300 LF of pipe between hydrants throughout the development.

As noted in the review letter, a FDC and dry standpipe is provided at each end of the Parking Structure. FDC connections to the building have also been indicated and are located within 100' of a fire hydrant.

Please feel free to contact me at 248.639.9442 or be@seiberkeast.com if you have any questions on the Preliminary Site Plan submittal.

Very Truly Yours,

SEIBER KEAST ENGINEERING, LLC



Robert J. Emerine, P.E.

June 11, 2018

Mr. Rick Meader, Landscape Architect
City of Novi Community Development
45175 West 10 Mile
Novi, MI 48375

RE: The Bond

Dear Mr. Meader:

Below are our responses to your review dated May 23, 2018.

Landscape Review

- Underground detention has been relocated under the parking lot.
- Proposed lighting is shown on the plans.
- Green Giant arborvitae have been added adjacent to the parking deck to screen the non-residential land use. A waiver is requested to not install the berm.
- The required wall has been extended to areas where parking points towards the street.
- Greenbelt areas have been increased with the elimination of non-required parking spaces.
- Required multi-family trees have been increased from 70 to 136. A waiver of 11 trees is requested due to limited planting area associated with an urban development.
- Two central driveway trees cannot be planted due to the fire access lane. A waiver is requested.
- Additional parking lot perimeter trees have been added based upon the revised perimeter length.
- The tree tags have been added for the existing trees counted towards the perimeter parking lot trees in the commercial area.
- A second row of evergreens has been added between the commercial building and the cemetery.
- A planting bed has been added in the front of Building 2.
- Native species have been increased meeting the 50% requirement.
- The dog park fencing currently encroaches on the 25' wetland buffer. The encroachment area is currently a mix of gravel and asphalt and will be planted with lawn.
- Existing zoning has been revised on Sheet L-1.
- 119' of parking is located within the 20' greenbelt. Evergreen hedges have been added to screen the parking. A waiver is requested.
- The screen wall has been increased to provide better screening. A detail is shown on sheet L-2.
- The parking lot perimeter length and calculations have been revised as suggested.
- The landscape notes have been revised.

If you have any questions or comments regarding this response, please contact me at your convenience.

Sincerely,

A handwritten signature in black ink, appearing to be 'J. Allen', written over the word 'Sincerely,'.

James C. Allen
Allen Design L.L.C.

COMMUNITY IMPACT STATEMENT

STATION 6 AT NOVI TOWN CENTER

A Mixed-Use Residential Multi-Family Development

Community Impact Assessment

May 10, 2018

Developed By:

**TriCap Holdings, LLC
30600 Northwestern Highway, Suite 430
Farmington Hills, MI 48334
248.538.1389**

**DTN Management Company
2502 Lake Lansing Road, Suite C
Lansing, MI 48912
517.371.5300**

Prepared By:

**Seiber Keast Engineering, LLC
100 MainCentre, Suite 10
Northville, MI 48167-1592
Telephone: 248.308.3331**

SITE DESCRIPTION

The Station 6 at Novi Town Center mixed-use residential multi-family development consists of an irregular shaped vacant parcel of land containing 8.731 acres and is located at the South West Corner of Grand River Avenue and Novi Road in Novi, Michigan.

The property is located along existing Flint Street and fronts Novi Road approximately 800 feet South of Grand River Avenue in Section 22 of the City of Novi. The subject property is zoned TC-1, Town Center - 1, which provides for a mixture of residential and commercial uses.

The City of Novi will be designing and reconstructing the existing Flint Street according to the Flint Street Realignment Study, prepared by URS in January of 2015. As a part of the Station 6 at Novi Town Center project, a Traffic Study was performed that proposes an alternate road cross section that will be adopted by the City of Novi in the redesign of Flint Street and the road will be renamed Bond Street.

The development road frontage includes 1319 feet along the proposed Bond Street which will be designed by Novi as a 2 lane asphalt roadway containing on-street parallel parking on the development side of the street. There is also 113 feet of frontage along Novi Road. Flint/Bond Street road frontage falls within the jurisdiction of the City of Novi and the Novi Road frontage is under the Road Commission for Oakland County jurisdiction. Access to the Site will be provided from the existing Flint Street connection to Grand River Avenue to the North and Novi Road from the East.

The Walled Lake Branch of the Middle Rouge River runs along the Eastern side of existing Flint Street and cuts through the parcel near the southern end of the site.

The development will be split into a commercial area (1.07 Ac) located at the South West intersection of the existing Flint Street and Novi Road on the South side of the Middle Rouge River. The 253-unit multi-family residential portion of the property (6.67 Ac) is located to the North of the Middle Rouge River and is accessed from Bond Street.

TOPOGRAPHY

Topographically, the site consists mostly of a gently to moderately sloping terrain, highest towards the center of the parcel sloping toward the existing Flint Street and the Walled Lake Branch of the Middle Rouge River. The incised banks of the Middle Rouge River are relatively steep (60%-70%) through the site and the river is roughly 14' below the remainder of the property. The sites highest elevation in the residential portion of the property 910.22 and slopes at approximately 1% toward the bank slope of the Middle Rouge River. The remainder of the residential portion of the property is relatively flat with poor drainage. The highest elevation in the commercial portion of the property is 913.55 on a hill adjacent to bank of the Middle Rouge River. The commercial portion of the site generally drains toward the South property line with the Cemetery at a slope of 1-3%. Approximately 5 to 10 feet from the South West property line the topography drops off significantly (5 to 10 feet) down to the existing C&O Rail Road with a se-

ries of existing large block retaining walls and 25 - 40% slopes. In addition, an earthen ramp from the site down to the rail road exists approximately 350 feet from the Middle Rouge River.

ADJACENT LAND USES

The proposed Station 6 at Novi Town Center Development is surrounded by commercial uses located within the TC-1 and I-1 zoning districts. Along the Northern side of the site across the existing Flint Street and the Middle Rouge River is the City Center Plaza Development that consists of several commercial businesses including a Sonoco and Mobil Gas Station, an Urgent Care Medical Facility, and numerous businesses within a strip mall including Joes Coney Island, Panera Bread, Scott Trade and others. The South West property line of the development is bordered by a 185 feet wide easement for C&O Rail Road. A CVS distribution and trucking facility is located across the rail road easement within the adjacent I-1 zoning district and the Novi Cemetery is located along the South property line abutting Novi Road.

DRAINAGE COURSES

As noted previously the Walled Lake Branch of the Middle Rouge River is located near the Eastern property line of the property between the existing Flint Street and the City Center Plaza Development. The Middle Rouge River then turns South and crosses underneath Flint Street approximately 180 feet from Novi Road using an existing box culvert and then crosses through the development. The river bed adjacent to and throughout the property is significantly lower than the surrounding land areas with steep banks approximately 8 to 14 feet deep. This portion of the river contains a FEMA regulated 100-Year flood plain that is generally contained within the banks of the river and will not be impacted by the proposed development according to the FEMA flood maps.

The site will be drained by means of sheet flow directed into a proposed storm sewer system. The storm sewer will lead to an open and underground detention basin in the residential portion of the property and into an underground detention system on the commercial portion of the property. Both detention basins will be sized for the Bank Full Flood storm event according to the City of Novi Engineering Design Manual. The detention basins will ultimately discharge on-site into the Middle Rouge River.

VEGETATION

Approximately 1/3 of the site is free of vegetation and is covered by an existing industrial development consisting of one building (7,000 SF +/-) with paved and gravel parking areas. A 2.3 Ac wooded area is located at the north end of the site containing mostly Eastern Cottonwoods and a few other tree varieties. Additionally, the banks of the Middle Rouge River and the Southern property line along the Novi Cemetery are wooded with a variety of trees. The remainder of the site has been previously cleared of trees and is now vegetated with field grasses.

WETLANDS

There are no wetlands located on the site.

WILDLIFE

Wildlife commonly found on the site consists of small mammals such as field mice, squirrels, raccoons, fox and rabbits. A variety of small birds normally populate the area.

SOILS CLASSIFICATIONS

The soils classification as provided by the United States Soil Conservation Services Soil Survey of Oakland County, indicate Oshtemo-Boyer Loamy Sand 0-6% Slopes and Urban Land.

MUNICIPAL WATER SUPPLY

Municipal water supply is available to the site by means of an existing 8-inch diameter water main stub within the existing Flint Street Right-of-Way at the North West corner of Flint Street and Novi Road. This stub will be extended South across Flint Street and along the full frontage of the development Bond Street Right-of-Way. The water main will be stubbed at the Northern property line of the development for future extension. The water main will also be extended into the site to provide domestic water service to the residential and commercial buildings as well as providing adequate fire hydrant coverage. Adequate water supply is anticipated for both domestic and firefighting purposes.

WASTE WATER DISPOSAL

An existing 8-inch to 15-inch sanitary sewer is located on-site for waste water disposal service. The existing sewer is located within the existing Flint Street Right-of-Way and runs down to the North West property corner of the development. The sewer then turns South and exits the site at the C&O Rail Road easement. 6-inch sanitary leads will connect the 2 residential buildings to this public sewer. An existing 8-inch sanitary sewer is also located within the Novi Road Right-of-Way and will be used to connect the commercial building to the public sewer. No off-site easements are required for the sewer connection.

The residential portion of the development has 253 Multiple Family Residences multiplied by a unit factor of 0.60 SF unit / MF unit, resulting in 152 equivalent Single Family units. At a rate of 3.2 people per Single Family residential unit the service population for the residential portion of the development is 486 people. The retail/commercial portion of the development has 5,578 square feet of leasable area multiplied by a unit factor of 0.40 units / 1,000 square feet of space, resulting in 2.25 equivalent single -family units. This equates to a total of 154 equivalent single-family units for the entire development with a total service population of 493 people. With a peaking factor of 4.0, the peak flow from the project would be 0.305 cubic feet per second. The capacity of an 8-inch diameter sanitary sewer is 0.76 cubic feet per second, therefore, capacity is sufficient.

PUBLIC UTILITIES

Public utilities such as electricity, telephone, gas and cable television, are available on Flint Street and Novi Road.

PROJECT DESCRIPTION

The proposed Station 6 at Novi Town Center Development consists of 2 four story residential buildings containing 253 Multiple Family residential rental units and a separate 5,578 SF commercial building. The project will include 146 one-bedroom units, 93 two-bedroom units and 14 three-bedroom units. The one bedroom units will be a minimum of 603 square feet, the two-bedroom units a minimum of 944 square feet and the three-bedroom units will be a minimum of 1277 square feet.

Of the 7.74 Acres of land within the site, 1.36 Ac (59,368 square feet) will be open space. This exceeds the minimum open space required of 1.16 Ac (Total Parcel x %15). Open spaces include balconies on all of the buildings, courtyards and clubhouse areas in Buildings 1 and 2, the pool area between Buildings 1 and 2, and the park areas.

Amenities include a 2,120 square foot exercise room, a 2,541 square foot club room, a 1,382 square foot conference room, a swimming pool and a Dog Park adjacent to the Middle Rouge River.

PHASING

The project will be constructed in 3 phases. Phase 1 includes Building 2 of the residential portion of the development, the above and below ground detention basins for the residential portion of the development, the parking area between Building 2 and the middle Rouge River, the parking area and swimming pool area between Buildings 1 and 2, and approximately 50% of the Parking located behind building 2 and the swimming pool area. Phase 2 will include Building 1 as well as the remainder of the parking structure and parking areas in the residential portion of the development. Phase 3 will include all of the commercial portion of the development.

ROADWAYS

All interior drives and parking areas are proposed to be private. The proposed public Bond Street is 28-feet wide and provides the main access to the Station 6 at Novi Town Center Development and residential/commercial parking areas. After the construction of the proposed Bond Street is completed, it will directly connect to both Novi Road and Grand River Avenue as a part of the Town Center Loop Road. Bond Street will provide direct access to the proposed development from both Novi Road and Grand River Avenue.

A traffic Impact Study dated March 15, 2018 has been prepared by the traffic engineering firm Fleis & Vanderbrink Engineering. This study indicated that the future traffic levels at the pro-

posed development entrances will be at acceptable levels after the construction of the proposed Bond Street is completed.

ENVIRONMENTAL CONCERNS

Upon full development, the proposed site will yield approximately 2.76 acres of building area. Therefore, Building Coverage will be 35.66 percent.

Ecologically, the development will affect the existing vegetation and ground cover to the extent that all existing field grasses and trees will be removed.

The ground water table will be affected slightly due to the extent of paving and building coverage. However, no deep excavations are planned which would contribute to the lowering of the ground water table. Soil erosion control will be provided on the site in accordance with the City of Novi requirements. Surface water run-off is expected to contain some road salts and oils carried by automobiles. Most suspended sediments will be removed in the storm water quality/detention basins, and oil and gas separators proposed in the development.

Air quality will be affected somewhat by automobile emissions and natural gas combustion gases from the apartment heating systems. In addition, the net ambient air temperature of the site will be increased slightly due to the loss of vegetation and the addition of pavement and buildings.

Noise levels will increase due to the additional automobile and truck traffic, and exterior air conditioning units.

An aesthetic impact will result from the introduction of man-made structures and site improvements.

Site lighting will be designed to maintain a low profile and prevent light spill and glare onto the adjacent property. A photometric plan and light fixture catalog cuts have been provided in the plan set.

Finally, landscaping will soften the overall impact of the development. A total of 146 trees are proposed to be planted. (See the Planting Schedule on the Landscape Plans Sheet L-1) No hazardous or toxic chemicals will be stored on-site except for household cleaners, chlorine tablets for the swimming pool, pesticides and fertilizers used for lawn and plant care. No underground storage tanks, wells, or septic tanks are proposed and none will be permitted.

STORM WATER DISPOSAL

Storm water generated on the proposed site will be collected by on site storm sewer and delivered to the on-site detention basins sized to detain the Bank Full Flood storm event. The detention basins will ultimately discharge on-site into the Middle Rouge River. The C&O District Regional Detention Basin is located downstream of the proposed development along the Middle Rouge River and will be used for storage of 100-Year storm for the site.

DEMANDS ON POLICE DEPARTMENT SERVICES

Based on the Police records for the year 2013 and the SEMCOG population estimate for the City of Novi for 2013 of 59,395 persons, the per capita response was one Police Department response for every 2.63 persons. Based on an expected residential population of 633 persons (2.5 persons per household), it is estimated that 240 annual Police Department calls would be made from the project.

DEMANDS ON FIRE DEPARTMENT SERVICES

After deducting a 30-percent factor for commercial, industrial, and office uses, the per capita response for the City of Novi during the year 2013, was 102.3 persons per Fire Department run. Based on the estimated proposed development population of 633 persons, the total projected annual Fire Department responses is 6. The project is located on near Fire Station No. 1 at 42975 Grand River Avenue. Due to the proximity of the fire station, response time is expected to be only a few minutes.

REFUSE AND SOLID WASTE DISPOSAL

Refuse and solid waste will be disposed into an onsite community trash compactor located within each residential building. The commercial portion of the property will be provided with a dumpster adjacent to the building. Offsite private contractors will collect the trash for final disposal. Curb side pickup will not be available.

EDUCATIONAL DEMANDS ON THE PUBLIC SCHOOL SYSTEM

The total 2014/2015 student enrollment in the Novi Community Schools is 6,266. Of this total, 2,020 were of High School Age (9-12th grade), 1,056 attended Middle School (7-8th grade), and 3,190 were enrolled at the elementary school level. Some impact is expected upon the community educational system due to the expected 60 +/- school age children living in the complex. Station 6 at Novi Town Center is located within the Parkview Elementary school district.

ECONOMIC IMPACT

An Economic Impact Statement will be submitted prior to the Planning Commission Meeting.

TRAFFIC STUDY
(Data Sheets available on
request)

Memo

VIA EMAIL

To: Mr. Albert Ludwig
TriCap Holdings, LLC

From: Julie M. Kroll, PE, PTOE
Lindsay M. Sagorski, PE
Fleis & VandenBrink

Date: March 15, 2018

Re: Flint Street Development
City of Novi, Michigan
Traffic Impact Study

Introduction

This memorandum presents the results of the Traffic Impact Study (TIS) for the proposed Flint Street mixed-use development. The project site is located generally in the southwest quadrant of the Novi Road and Grand River Avenue intersection, adjacent to the south side of Flint Street in Novi, Michigan. The proposed development includes 250 apartment units and 6,000 SF of retail space. Site access for the development will be provided via Flint Street with access to Novi Road and a new connection to Grand River Avenue.

Novi Road and Grand River Avenue are under the jurisdiction of the Road Commission for Oakland County (RCOC), and Flint Street is under the jurisdiction of the City of Novi. This TIS has been completed to identify the impacts (if any) of the proposed development on the following study intersections:

- a. Grand River Avenue & Novi Road,
- b. Grand River Avenue & Flint Street,
- c. Grand River Avenue & Crescent Blvd (proposed),
- d. Main Street/Flint Street & Novi Road, and
- e. The proposed site access points.

The scope of the study was developed based on Fleis & VandenBrink's (F&V) knowledge of the study area, understanding of the development program, accepted traffic engineering practice, and methodologies published by the Institute of Transportation Engineers (ITE). Additionally, F&V solicited input regarding the scope of work from the City of Novi and their traffic consultant (AECOM).

Data Collection

The existing weekday turning movement traffic volume data were collected by F&V subconsultant Traffic Data Collection, Inc. (TDC) on Thursday, September 22, 2016. Intersection turning movement counts were collected during the weekday AM (7:00 AM to 9:00 AM) and PM (4:00 PM to 6:00 PM) peak periods at all study intersections. This data was used as a baseline to establish existing traffic conditions without the proposed development. SCATS data from 2018 was used to compare and adjust the 2016 counts to reflect existing traffic conditions. The baseline existing traffic volumes were reviewed and approved for use in this study by AECOM.

The peak hour volumes for each intersection were utilized for this study and the volumes were balanced upward through the study network. At locations where access is provided between study intersections, "dummy" intersections were used to account for sink and source volumes, and through volumes were carried along the main study roadways. At locations where short links are present and the entering/exiting approach volumes at adjacent intersections were relatively equal volumes were balanced upward. Additionally, F&V collected an

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inventory of existing lane use and traffic controls and obtained existing traffic signal timing information from RCOC. The signalized intersections run SCATS, therefore the signal timings were optimized for each scenario studied. The applicable data referenced in this memorandum are attached.

Existing Conditions

Existing peak hour vehicle delays and Levels of Service (LOS) were calculated at the study intersections using Synchro (Version 10) traffic analysis software. This analysis was based on the existing lane use and traffic control shown on the attached Figure 1, the existing peak hour traffic volumes shown on the attached Figure 2, and the methodologies presented in the *Highway Capacity Manual 6th Edition* (HCM). Typically, LOS D is considered acceptable, with LOS A representing minimal delay, and LOS F indicating failing conditions. Additionally, SimTraffic network simulations were reviewed to evaluate network operations and vehicle queues. The existing conditions results are attached and summarized in Table 1.

Table 1: Existing Intersection Operations

Intersection	Control	Approach	AM Peak		PM Peak	
			Delay (s/veh)	LOS	Delay (s/veh)	LOS
1 Novi Road & Grand River Avenue	Signalized	EB	51.0	D	74.8	E
		WB	48.2	D	72.7	E
		NB	81.1	F	91.6	F
		SB	62.9	E	109.9	F
		Overall	61.0	E	86.8	F
2 Novi Road & Flint Street	Signalized	EB	66.6	E	57.0	E
		WB	62.1	E	61.1	E
		NB	20.4	C	23.7	C
		SB	18.2	B	24.0	C
		Overall	24.9	C	28.1	C
3 Flint Street & Grand River Avenue	STOP (Minor)	WBL	11.8	B	12.4	B
		NB	18.6	C	26.8	D

The results of the existing conditions analysis show that all approaches and movements at the unsignalized study intersection currently operate acceptably at a LOS D or better during both peak periods. However, many approaches and movements at the two signalized study intersections currently operate at a LOS E or F during both peak periods; these are summarized below by location.

Novi Road & Grand River Avenue

Eastbound (Grand River Ave.)

- Left turn movement operates at a LOS E and F during the AM and PM peak periods, respectively.
- Shared through/left turn movement operates at a LOS E during the PM peak period.

Westbound (Grand River Ave.)

- Left turn movement operates at a LOS F during both the AM and PM peak periods.
- Through movement operates at a LOS E during the PM peak period.

Northbound (Novi Road)

- Left turn and through movements operate at a LOS F during both the AM and PM peak periods
- Right-turn movement operate at a LOS E and LOS F during the AM and PM peak periods, respectively.

Southbound (Novi Road)

- Left-turn movement operates at a LOS F during both the AM and PM peak periods.
- Shared through/left turn movement operates at a LOS E and F during the AM and PM peak periods, respectively.

Novi Road & Flint Street/Main Street

Eastbound (Flint Street)

- Shared left/through/right turn movement operates at a LOS E during both the AM and PM peak periods.

Westbound (Main Street)

- Left-turn movement operates at a LOS E during both the AM and PM peak periods.
- Through and right turn movements operate at a LOS E during the AM peak period.

A review of network simulations indicated long vehicle queues for the eastbound, westbound, and northbound left-turn movements as well as the southbound through movement at the Novi Road & Grand River Avenue intersection. The long left-turn queues are caused by a couple of factors; there is a high volume of left-turning vehicles and the left-turn phasing is protected-only. At all other study intersections, acceptable traffic operations were observed during both the AM and PM peak hours.

Existing Conditions Improvements

In order to improve traffic operations to a LOS D or better for all intersection approaches and movements, mitigation measures were evaluated, as summarized below.

Novi Road & Grand River Avenue Improvements

The results of the analysis indicate widening all approaches to provide additional capacity at the intersection, especially for all left turn movements and the southbound through movement, would improve operations; however, geometric constraints at the intersection makes widening not feasible.

Existing traffic operations indicate that left-turn movements currently operate at a LOS E or during the peak periods. Therefore, a left turn phasing analysis was conducted at the intersection. The analysis was performed in accordance with the MDOT left turn phasing guidelines and spreadsheet, to determine if protected left turn phasing should be provided.

The results of this analysis indicate that only the westbound approach currently meets the cross-product threshold for the one hour during the PM peak period. An operational analysis was performed to evaluate the impact of changing the left-turn phasing from protected only to permissive protected. The results showed that all left-turning movements should operate with either permissive/protected or protected only left-turn phasing. The eastbound left-turn and the northbound left-turn phasing analysis recommended protected only left-turns. The left-turn phasing analysis is attached.

Further analysis was performed to evaluate the impact of changing all phasing to permissive/protected or having it remain at protected only. The results of the analysis showed that permissive/protected left turn phasing should be considered at this intersection to improve the traffic operations. Therefore, the following improvements are recommended for implementation to improve existing conditions:

- Provide permissive/protected left-turn phasing at all left-turn movements.

Novi Road & Flint Street Main Street Improvements

The following improvements should be implemented to mitigate critical LOS under existing conditions:

- Provide 70-second cycle length to reduce minor street vehicle delays while maintaining coordination with adjacent 140-second cycle length signals, and
- Construct an eastbound exclusive left turn lane.

The existing intersection operations with the proposed mitigation measures are summarized in Table 2.

Table 2: Existing Intersection Operations with Improvements

Intersection	Control	Approach	AM Peak				PM Peak				
			Existing Conditions		Existing w/ Improvements		Existing Conditions		Existing w/ Improvements		
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	
1	Novi Road & Grand River Avenue	Signalized	EB	51.0	D	30.3	C	74.8	E	59.7	E
			WB	48.2	D	30.0	C	72.7	E	56.5	E
			NB	81.1	F	50.6	D	91.6	F	40.8	D
			SB	62.9	E	57.1	E	109.9	F	72.5	E
			Overall	61.0	E	42.3	D	86.8	F	58.0	E
2	Novi Road & Flint Street	Signalized	EB	66.6	E	30.6	C	57.0	E	25.4	C
			WB	62.1	E	31.3	C	61.1	E	29.5	C
			NB	20.4	C	19.6	B	23.7	C	23.6	C
			SB	18.2	B	6.8	A	24.0	C	10.4	B
			Overall	24.9	C	16.0	B	28.1	C	18.6	B

The results of the existing conditions analysis show that all approaches and movements at the study intersection with proposed improvements would operate acceptably at a LOS D or better during both peak periods, except for the following:

Novi Road & Grand River Avenue

- The eastbound left turn movement operates at a LOS E during the PM peak period.
- The westbound through movement operated at a LOS E during the PM peak period.
- The northbound left turn operates at a LOS E during the PM peak period.
- The southbound shared through/left turn movement operated at a LOS E during the AM and PM peak periods.

A review of network simulations indicated improved conditions at the signalized intersection of Novi Road & Grand River Avenue, however, long vehicle queues for the westbound, eastbound and northbound left-turn movements as well as the southbound through movements are present during the PM peak period. At all other study intersections, acceptable traffic operations were observed during both the AM and PM peak hours.

Implementing permissive/protected phasing at all left-turn movements at the intersection of Novi Road & Grand River Avenue is expected to significantly improve intersection operations; however, several factors contribute to a decision to adjust signal phasing. A review of intersection crash history, arterial road speed data, and other relevant operational metrics should be conducted before changing the protected-only left-turn phasing at this location.

Background Conditions

Historical traffic volume data was reviewed in order to determine the applicable growth rate for the existing traffic volumes to the project build-out year of 2021. The historical growth rates for Grand River Avenue and Novi Road were referenced. SEMCOG traffic volume data indicates that between 2012 and 2016, the Average Annual Daily Traffic (AADT) volumes were stagnant or declining on Novi Road and increased by 1.5% on Grand River Avenue. Therefore, a conservative growth rate of 1.5% per year was utilized in this study for the analysis of background conditions **without the proposed development**.

In addition to background growth, it is important to account for traffic that is expected to be generated by approved developments within the vicinity of the study area that have yet to be constructed or are currently under construction.

The Old Novi Expo Site project will be located generally in the northwest quadrant of the Novi Road and Grand River Avenue intersection, adjacent to the north side of the proposed access roadway which will provide access to Grand River Avenue and Novi Road. The access to Grand River Avenue will line up with the proposed access to the Flint Street development at a new signalized intersection, per the City’s current Flint Street realignment plans. The City of Novi and AECOM provided the trip generation and distribution for the proposed project and are attached.

Additionally, it is anticipated that the new access road connection will divert existing trips from the intersection of Novi Road & Grand River Avenue. A percentage of background traffic volumes that are expected to divert to the new connection of Flint Street between Grand River Avenue and Novi Road were redistributed from the intersection of Novi Road & Grand River. The traffic redistribution is shown in Table 3. The redistributed traffic volumes were reviewed and approved by the City of Novi and AECOM for use in this study.

Table 3: Novi & Grand River Intersection Redistribution

Movement	AM/PM Volumes (Redistribution %)
NB to WB	-7/-10 (5%)
SB to WB	-54/-29 (10%)
EB to SB	-10/-13 (10%)
EB to NB	-15/-20 (5%)

Background Operations

Background peak hour vehicle delays and LOS were calculated based on the existing/proposed lane use and traffic control shown on the attached Figure 1, the background traffic volumes shown on the attached Figure 3, and the methodologies presented in the HCM. The results of the background conditions assessment are attached and summarized in Table 4.

Table 4: Background Intersection Operations

Intersection	Control	Approach	AM Peak				PM Peak				
			Existing Conditions		Background Conditions		Existing Conditions		Background Conditions		
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	
1	Novi Road & Grand River Avenue	Signalized	EB	51.0	D	60.9	E	74.8	E	73.8	E
			WB	48.2	D	48.9	D	72.7	E	70.0	E
			NB	81.1	F	83.5	F	91.6	F	95.9	F
			SB	62.9	E	58.6	E	109.9	F	111.5	F
			Overall	61.0	E	63.6	E	86.8	F	87.0	F
2	Novi Road & Flint Street	Signalized	EB	66.6	E	65.3	E	57.0	E	55.2	E
			WB	62.1	E	59.6	E	61.1	E	59.2	E
			NB	20.4	C	20.6	C	23.7	C	25.3	C
			SB	18.2	B	19.7	B	24.0	C	28.4	C
			Overall	24.9	C	25.7	C	28.1	C	30.9	C
3	Flint Street & Grand River Avenue	STOP (Minor)	WBL	11.8	B	11.9	B	12.4	B	12.6	B
			NB	18.6	C	18.9	C	26.8	D	27.5	D
4	Crescent Drive Connection & Grand River Avenue	Signalized	EB			25.1	C			31.7	C
			WB			17.6	B			17.8	B
			NB			30.1	C			29.9	C
			SB			33.4	C			32.6	C
			Overall			23.2	C			25.7	C

The results show that all study intersection approaches and movements are expected to continue to operate in a manner similar to existing conditions during both the AM and PM peak hours. Vehicle delays and LOS as shown in Table 4 are expected to be similar to existing conditions and minor increases will not be discernable. Review of network simulations also indicates traffic operations which are similar to existing conditions. Poor operations continue to be observed at the Novi Road & Grand River Avenue as well as the Novi Road & Flint Street./Main Street. At all other study intersections, acceptable traffic operations were observed during both the AM and PM peak hours.

Background Conditions Improvements

In order to improve traffic operations to a LOS D or better for all intersection approaches and movements under background conditions, mitigation measures that were identified under existing conditions were applied. The results of the background conditions assessment with improvements are attached and summarized in Table 5.

Table 5: Background Intersection Operations with Improvements

Intersection	Control	Approach	AM Peak				PM Peak				
			Background Conditions		Background w/ Improvements		Background Conditions		Background w/ Improvements		
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	
1	Novi Road & Grand River Avenue	Signalized	EB	60.9	E	14.7	B	73.8	E	56.9	E
			WB	48.9	D	32.0	C	70.0	E	57.3	E
			NB	83.5	F	51.9	D	95.9	F	63.1	E
			SB	58.6	E	52.8	D	111.5	F	78.2	E
			Overall	63.6	E	37.1	D	87.0	F	63.7	E
2	Novi Road & Flint Street	Signalized	EB	65.3	E	30.4	C	55.2	E	24.2	C
			WB	59.6	E	30.8	C	59.2	E	29.0	C
			NB	20.6	C	21.2	C	25.3	C	23.4	C
			SB	19.7	B	8.6	A	28.4	C	22.5	C
			Overall	25.7	C	17.7	B	30.9	C	23.5	C

The results of the background conditions analysis show that vehicle delays and LOS are expected to be similar to existing conditions with proposed improvements except the eastbound left turn movement and southbound through/right turn movement will operate at LOS F during the PM peak period and the northbound through/right turn movement will operate at LOS E during the PM peak period.

A review of network simulations indicated improved conditions at the signalized intersection of Novi Road & Grand River Avenue, however, long vehicle queues for the westbound, eastbound and northbound left-turn movements as well as the northbound and southbound through movements are present during the PM peak period. At all other study intersections, acceptable traffic operations were observed during both the AM and PM peak hours.

Site Trip Generation Analysis

The number of AM and PM peak hour vehicle trips that would be generated by the proposed development was forecast based on data published by ITE in the *Trip Generation Manual, 9th Edition*. The site trip generation forecast for the proposed development is summarized in Table 6.

Table 6: Site Trip Generation

Land Use	ITE Code	Amount	Average Daily Traffic (vpd)	PM Peak Hour (vph)			SAT Peak Hour (vph)		
				In	Out	Total	In	Out	Total
Shopping Center	820	6,000 SF	887	51	43	94	37	37	74
<i>Pass-By</i>		34%	302	17	15	32	13	13	25
		New Trips	585	34	28	62	24	24	49
Apartments	220	250 D.U.	1,931	38	96	134	98	68	166
		Total	2,818	89	139	228	135	105	240
		<i>Pass-By</i>	302	17	15	32	13	13	25
		New Trips	2,516	72	124	196	122	92	215

The vehicle trips that would be generated by the proposed development were assigned to the study road network based on existing peak hour traffic patterns and the methodologies published by ITE. This methodology indicates that new trips will return to their direction of origin, while pass-by trips enter and exit the

development in their original direction of travel. The site trip distributions used in the analysis are summarized in Table 7.

Table 7: Site Trip Distribution

New Trips		AM	PM	Pass-By				
To / From	Via			From	To	Via	AM	PM
North	Novi Road	30%	29%	East	West	Grand River Avenue	18%	26%
South	Novi Road	23%	23%	West	East	Grand River Avenue	30%	26%
West	Grand River Avenue	25%	25%	South	North	Novi Road	25%	22%
East	Grand River Avenue	22%	23%	North	South	Novi Road	27%	26%
		100%	100%					

The site-generated vehicle trips were assigned to the study road network based on these trip distribution patterns and are shown on the attached Figure 4. The site-generated trips were added to the background traffic volumes to calculate the future peak hour traffic volumes shown on the attached Figure 5.

Future Conditions

Future peak hour vehicle delays and LOS **with the proposed development** were calculated based on the proposed lane use and traffic control, the future traffic volumes, the proposed site access plan, and the methodologies presented in the HCM. Additionally, SimTraffic simulations were reviewed to evaluate network operations and vehicle queues. The results of the future conditions analysis are attached and are summarized in Table 8.

Table 8: Future Intersection Operations

Intersection	Control	Approach	AM Peak				PM Peak				
			Background Conditions		Future Conditions		Background Conditions		Future Conditions		
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	
1	Novi Road & Grand River Avenue	Signalized	EB	60.9	E	63.6	E	73.8	E	81.1	F
			WB	48.9	D	51.4	D	70.0	E	75.3	E
			NB	83.5	F	84.8	F	95.9	F	114.5	F
			SB	58.6	E	56.1	E	111.5	F	120.7	F
			Overall	63.6	E	65.2	E	87.0	F	96.8	F
2	Novi Road & Flint Street	Signalized	EB	65.3	E	64.6	E	55.2	E	57.8	E
			WB	59.6	E	49.8	D	59.2	E	56.7	E
			NB	20.6	C	20.9	C	25.3	C	26.0	C
			SB	19.7	B	22.2	C	28.4	C	31.9	C
			Overall	25.7	C	28.0	C	30.9	C	33.3	C
3	Flint Street & Grand River Avenue	STOP (Minor)	WBL	11.9	B	12.1	B	12.6	B	12.8	B
			NB	18.9	C	19.4	C	27.5	D	28.4	D
4	Crescent Drive Connection & Grand River Avenue	Signalized	EB	25.1	C	26.2	C	31.7	C	35.0	D
			WB	17.6	B	10.4	B	17.8	B	17.6	B
			NB	30.1	C	32.6	C	29.9	C	29.3	C
			SB	33.4	C	28.9	C	32.6	C	29.5	C
			Overall	23.2	C	21.7	C	25.7	C	27.1	C
5	Crescent Drive Connection & Site Drive 1	STOP (Minor)	WBL			7.3	A			7.4	A
			NB			9.2	A			9.3	A
6	Flint Street & Site Drive 2	STOP (Minor)	WBL			7.3	A			7.4	A
			NB			9.0	A			9.3	A
7	Flint Street & Site Drive 3	STOP (Minor)	WBL			7.4	A			7.4	A
			NB			9.1	A			9.3	A
8	Flint Street & Site Drive 4	STOP (Minor)	WBL			7.4	A			7.4	A
			NB			9.1	A			9.2	A
9	Flint Street & Commercial Site Drive	STOP (Minor)	WBL			7.4	A			7.4	A
			EBL			0.0	A			0.0	A
			NB			9.5	A			9.5	A
			SB			11.3	B			11.2	B

The results show that all study intersection approaches and movements are expected to continue to operate in a manner similar to background conditions during both the AM and PM peak hours. In general, vehicle delays and LOS as shown in Table 8 are expected to be similar to background conditions and minor increases will not be discernable. Review of network simulations also indicates traffic operations which are similar to background conditions. Poor operations continue to be observed at the Novi Road & Grand River Avenue and Novi Road

& Flint Street/Main Street intersections. At all other study intersections, acceptable traffic operations were observed during both the AM and PM peak hours.

Future Conditions Improvements

In order to improve traffic operations to a LOS D or better for all intersection approaches and movements under future conditions, mitigation measures that were identified under existing conditions were applied. The results of the future conditions assessment with improvements are attached and summarized in Table 9.

Table 9: Future Intersection Operations with Improvements

Intersection	Control	Approach	AM Peak				PM Peak				
			Future Conditions		Future w/ Improvements		Future Conditions		Future w/ Improvements		
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	
1	Novi Road & Grand River Avenue	Signalized	EB	63.6	E	28.7	C	81.1	F	61.2	E
			WB	51.4	D	33.5	C	75.3	E	56.4	E
			NB	84.8	F	51.3	D	114.5	F	54.9	D
			SB	56.1	E	52.3	D	120.7	F	74.9	E
			Overall	65.2	E	41.3	D	96.8	F	64.0	E
2	Novi Road & Flint Street	Signalized	EB	64.6	E	29.7	C	57.8	E	23.5	C
			WB	49.8	D	30.1	C	56.7	E	28.7	C
			NB	20.9	C	21.0	C	26.0	C	22.7	C
			SB	22.2	C	9.5	A	31.9	C	16.1	B
			Overall	28.0	C	18.3	B	33.3	C	20.6	C

The results of the future conditions analysis show that vehicle delays and LOS are expected to be similar to existing and background conditions with proposed improvements.

A review of network simulations indicated improved conditions at the signalized intersection of Novi Road & Grand River Avenue, however, long vehicle queues for the eastbound and northbound left-turn movements as well as the southbound through movements are present during the PM peak period. At all other study intersections, acceptable traffic operations were observed during both the AM and PM peak hours.

Auxiliary Lane Analysis

The City of Novi warrants for right and left-turn lanes were evaluated at the site access point to Flint Street. The results of this analysis show that a right-turn deceleration lane/taper or a left turn lane is not warranted at any of the site driveways.

Table 10: Turn Lane Warrant Analysis

Site Driveway	Left Turn Treatment		Right Turn Treatment	
	AM	PM	AM	PM
Site Drive 1	Not Required	Not Required	Radius Only	Radius Only
Site Drive 2	Not Required	Not Required	Radius Only	Radius Only
Site Drive 3	Not Required	Not Required	Radius Only	Radius Only
Site Drive 4	Not Required	Not Required	Radius Only	Radius Only
Commercial Site Drive	Not Required	Not Required	Radius Only	Radius Only

Network simulations were reviewed to evaluate the projected vehicle queueing on Flint Street, both west of Novi Road and south of Grand River Avenue.

Table 11: Flint Street Queueing Analysis

Approach	95th Percentile Queue Length	Storage Length	Exceeds Available Storage
<i>EB Left-turn Lane Flint Street at Novi Road</i>	75 ft (AM) & 66 ft (PM)	85 feet	No
<i>NB Left-turn Lane Flint Street at Grand River Ave.</i>	74 ft (AM) & 58 ft (PM)	n/a	n/a

Based on this analysis, the required queue length at the Novi Road & Flint Street/Main Street intersection can be accommodated in the tangent section of Flint Street. In addition, a 75 ft northbound left-turn lane should be provided on Flint Street at Grand River Avenue.

Flint Street Design Comparative Analysis

The City of Novi requested a comparative analysis of the City’s current Flint street realignment plans and the proposed design for Flint Street as part of this development plan. The analysis will include the following:

- a. Two Lanes with on-street parking,
- b. Three Lanes without on-street parking

Table 12: Proposed Flint Street Design

	City Design	Applicant Design
Intent	Ring Road	Appears to be a residential collector
Number of Lanes	Three Lanes	Two 14 feet lanes
Parking	No parking	ON-street parallel parking on one side
Radii at intersection	50 feet	35 feet
Non-motorized	On-street bike lanes in both directions	8 foot wider sidewalk on one-side, not sufficient for bike and pedestrian
Left & Right Turn Lanes	Longer	Shorter than proposed by City

Future peak hour vehicle delays and LOS for Flint Street with the proposed development calculated based on the City and applicant’s design and the methodologies presented in the HCM. Additionally, SimTraffic simulations were reviewed to evaluate network operations and vehicle queues. The results of the future conditions comparative analysis are attached and are summarized in Table 13.

Table 13: Flint Street LOS Comparative Analysis

Intersection	Control	Approach	AM Peak				PM Peak				
			Applicant Design		City Design		Applicant Design		City Design		
			Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	Delay (s/veh)	LOS	
5	Crescent Drive Connection & Site Drive 1	STOP (Minor)	WBL	7.3	A	7.3	A	7.4	A	7.4	A
			NB	9.2	A	9.2	A	9.3	A	9.3	A
6	Flint Street & Site Drive 2	STOP (Minor)	WBL	7.3	A	7.3	A	7.4	A	7.4	A
			NB	9.0	A	9.0	A	9.3	A	9.3	A
7	Flint Street & Site Drive 3	STOP (Minor)	WBL	7.4	A	7.4	A	7.4	A	7.4	A
			NB	9.1	A	9.1	A	9.3	A	9.3	A
8	Flint Street & Site Drive 4	STOP (Minor)	WBL	7.4	A	7.4	A	7.4	A	7.4	A
			NB	9.1	A	9.1	A	9.2	A	9.2	A
9	Flint Street & Commercial Site Drive	STOP (Minor)	WBL	7.4	A	7.4	A	7.4	A	7.4	A
			EBL	0.0	A	0.0	A	0.0	A	0.0	A
			NB	9.5	A	9.5	A	9.5	A	9.5	A
			SB	11.3	B	11.3	B	11.2	B	11.2	B

The results of the comparative analysis show that all intersection approaches and movements along Flint Street are expected to operate acceptably at a LOS D or better during both peak periods for both designs.

A review of network simulations showed acceptable traffic operations were observed during both peak hours. Minimal queues were observed for left turning vehicles at the site driveways. The result of the queuing analysis are shown in Table 14. The 95th percentile queue of 43-ft (1-2 vehicles) would occur at Site Drive 3 during the AM peak period with the applicant's design. The applicant's proposed design (two-lane roadway with on-street parking) provides adequate operations with the projected traffic volumes on Flint Street.

Table 14: Flint Street Queuing Comparative Analysis

Intersection	Approach	AM Peak 95 th % Queue (ft)		PM Peak 95 th % Queue (ft)		
		Applicant Design	City Design	Applicant Design	City Design	
5	Crescent Drive Connection & Site Drive 1	WBL	32	35	10	6
6	Flint Street & Site Drive 2	WBL	6	4	16	10
7	Flint Street & Site Drive 3	WBL	43	6	9	12
8	Flint Street & Site Drive 4	WBL	6	6	9	8
9	Flint Street & Commercial Site Drive	WBL	18	6	27	28

Conclusions

The conclusions of this Traffic Impact Study are as follows:

1. The results of the existing conditions analysis show that all approaches and movements at the four unsignalized study intersections currently operate acceptably at a LOS D or better during both peak periods. However, many approaches and movements at the two signalized study intersections currently operate at a LOS E or F during both peak periods.
2. In order to improve traffic operations to a LOS D or better for all intersection approaches and movements, the following mitigation measures are recommended under existing conditions:

Novi Road & Grand River Avenue

- Provide permissive/protected left-turn phasing at all left-turn movements.

Novi Road & Flint Street/Main Street

- Provide 70-second cycle length, and
- Construct eastbound left turn lane

3. The results of the analysis indicate widening all approaches to provide additional capacity at the intersection, especially for all left turn movements and the southbound through movement, would improve operations; however, geometric constraints at the intersection makes widening not feasible. Therefore, the recommended improvements at this intersection are limited to signal timing adjustments.
4. The analysis of background conditions *without the proposed development* show operations similar to existing conditions and any increases in delay would not be discernable.
5. The analysis of future conditions *with the proposed development* show that operations would be similar to background conditions. The development is not expected to have a significant impact on the study intersections.
6. If the recommended improvements are implemented, all study intersection approaches and movements are expected to operate at LOS D or better during both peak periods except for the following:

Novi Road & Grand River Avenue

- The eastbound left turn movement operates at a LOS F during the PM peak period.
- The westbound through movement operated at a LOS E during the PM peak period.
- The northbound left turn operates at a LOS E during the PM peak period.
- The southbound shared through/left turn movement operated at a LOS F during the PM peak period.

7. A right turn deceleration lane/ taper or a left turn lane are not warranted at any site access points on Flint Street.
8. The proposed site driveways should be designed in accordance with City of Novi requirements.
9. The required queue length of 85 feet for the eastbound approach of the Novi Road & Flint Street/Main Street intersection can be accommodated in the tangent section (85 ft) of Flint Street. A 75-ft left turn lane is recommended for northbound left turn at the Crescent Drive Connection & Grand River Avenue.
10. The comparative analysis of the City of Novi and the applicant's proposed Flint Street design show that the applicant's proposed design (two-lane roadway with on-street parking) provides adequate operations with the projected traffic volumes on Flint Street. The network simulations showed acceptable traffic operations were observed during both peak hours for both proposed designs. The results of the queuing analysis showed that the 95th percentile queue of 45 feet (1- 2 vehicles) would occur at Site Drive 3 during the AM peak period with the applicant's design

Attached: Figures 1-5
Traffic Volume Data
SEMCOG Data
Synchro / SimTraffic Results
Auxiliary Lane Warrants
Left Turn Phasing Spreadsheet

LMS:jmk

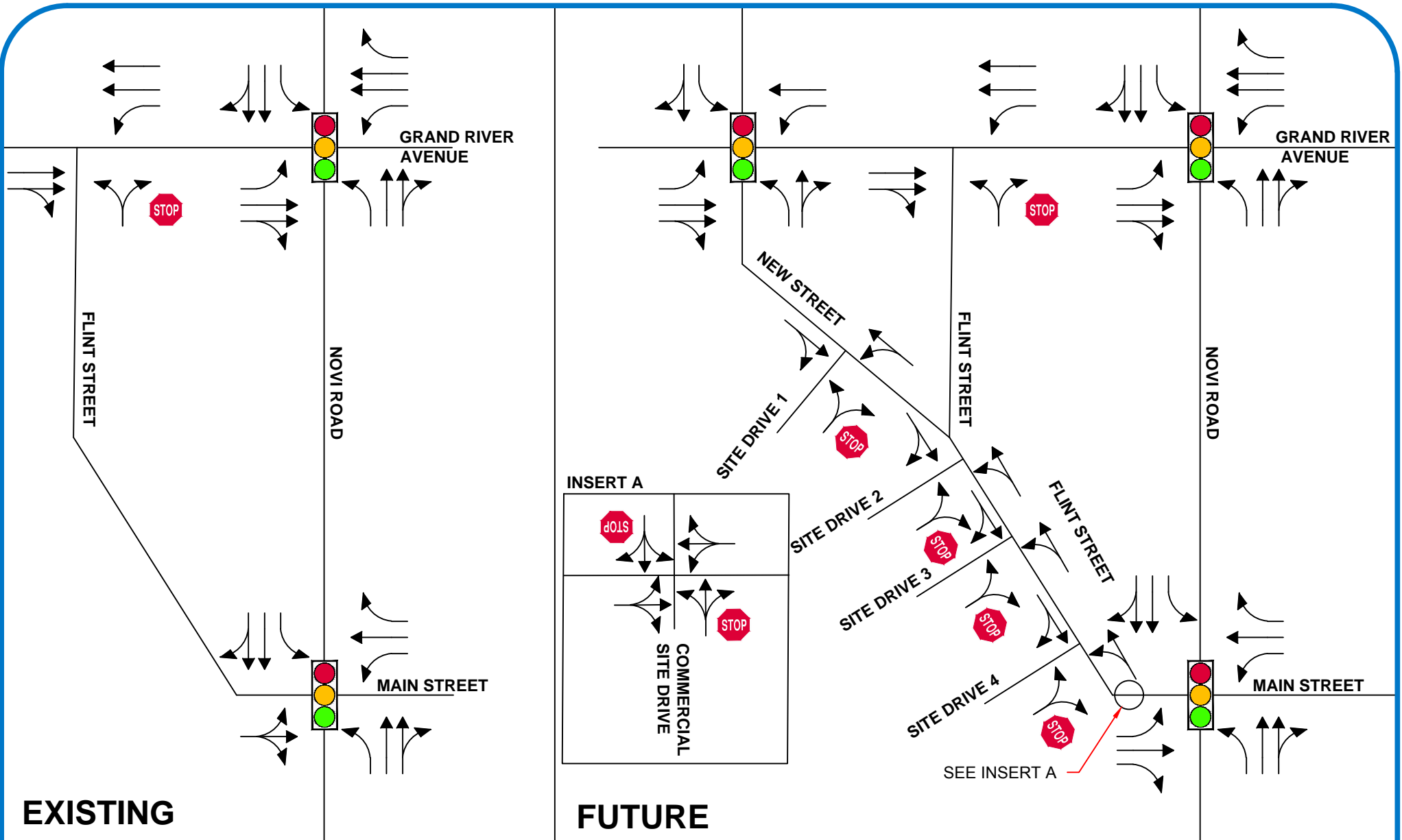


FIGURE 1
LANE USE AND TRAFFIC CONTROL

FLINT STREET DEVELOPMENT - NOVI, MI

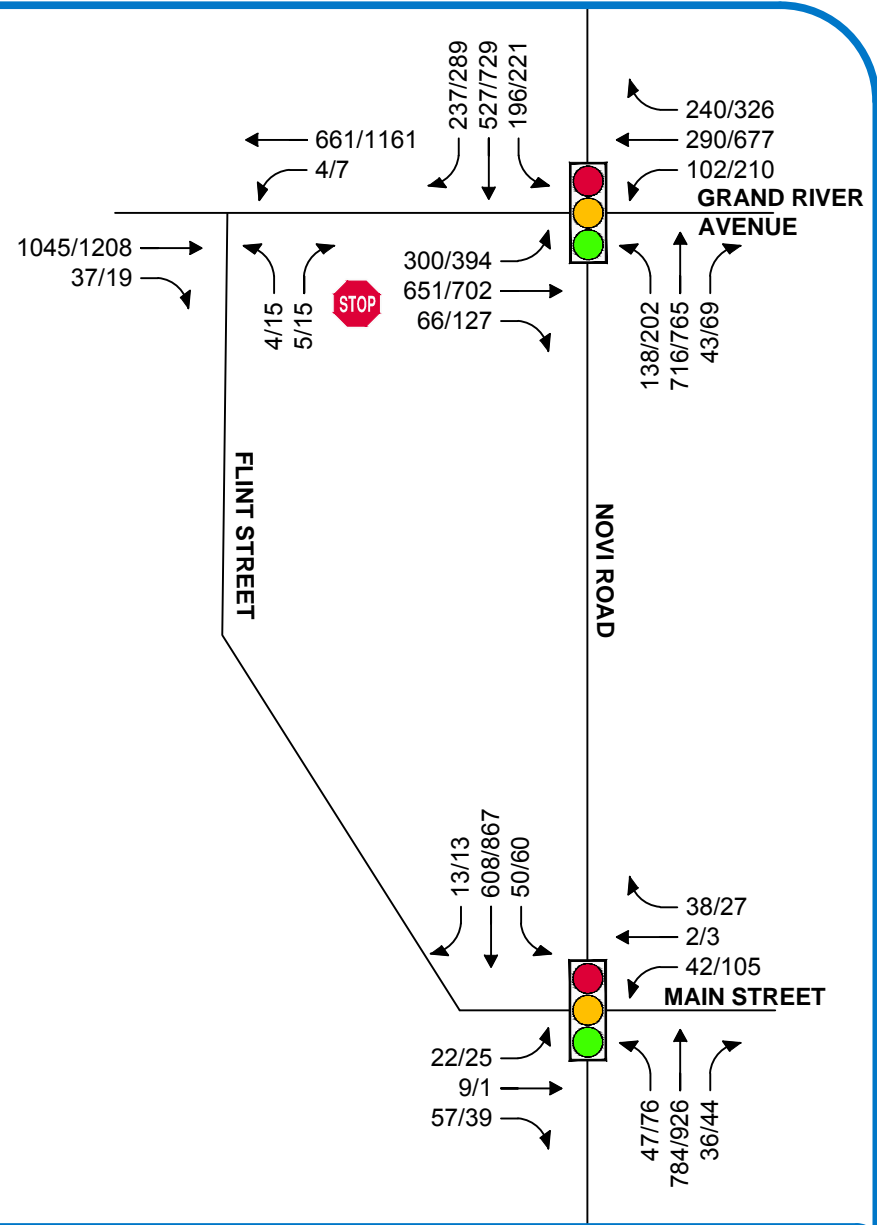






FIGURE 2
EXISTING TRAFFIC VOLUMES

FLINT STREET DEVELOPMENT - NOVI, MI



LEGEND

-  SIGNALIZED INTERSECTION
-  UNSIGNALIZED INTERSECTION
-  TRAFFIC VOLUMES (AM/PM)
-  ROADS



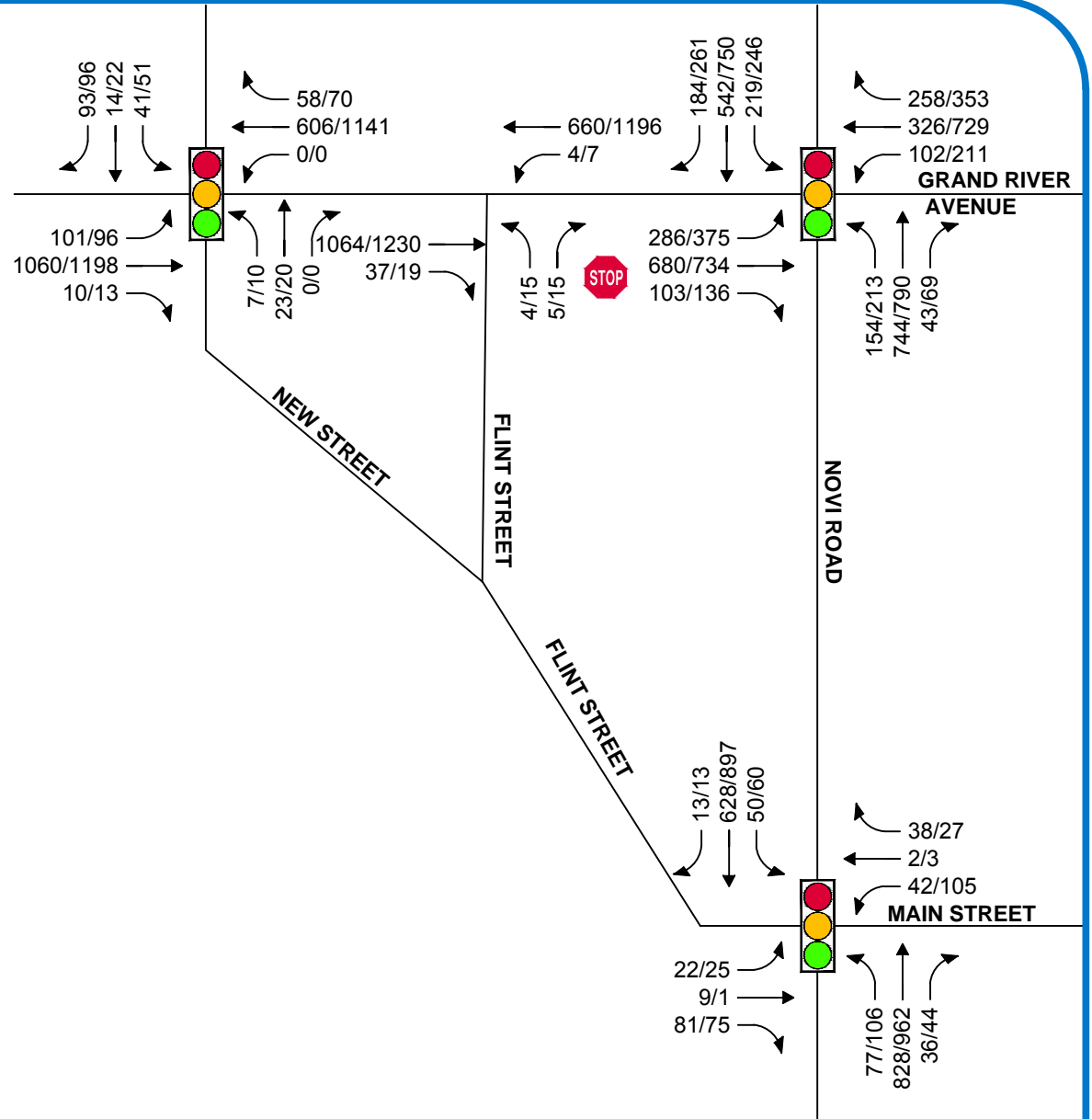






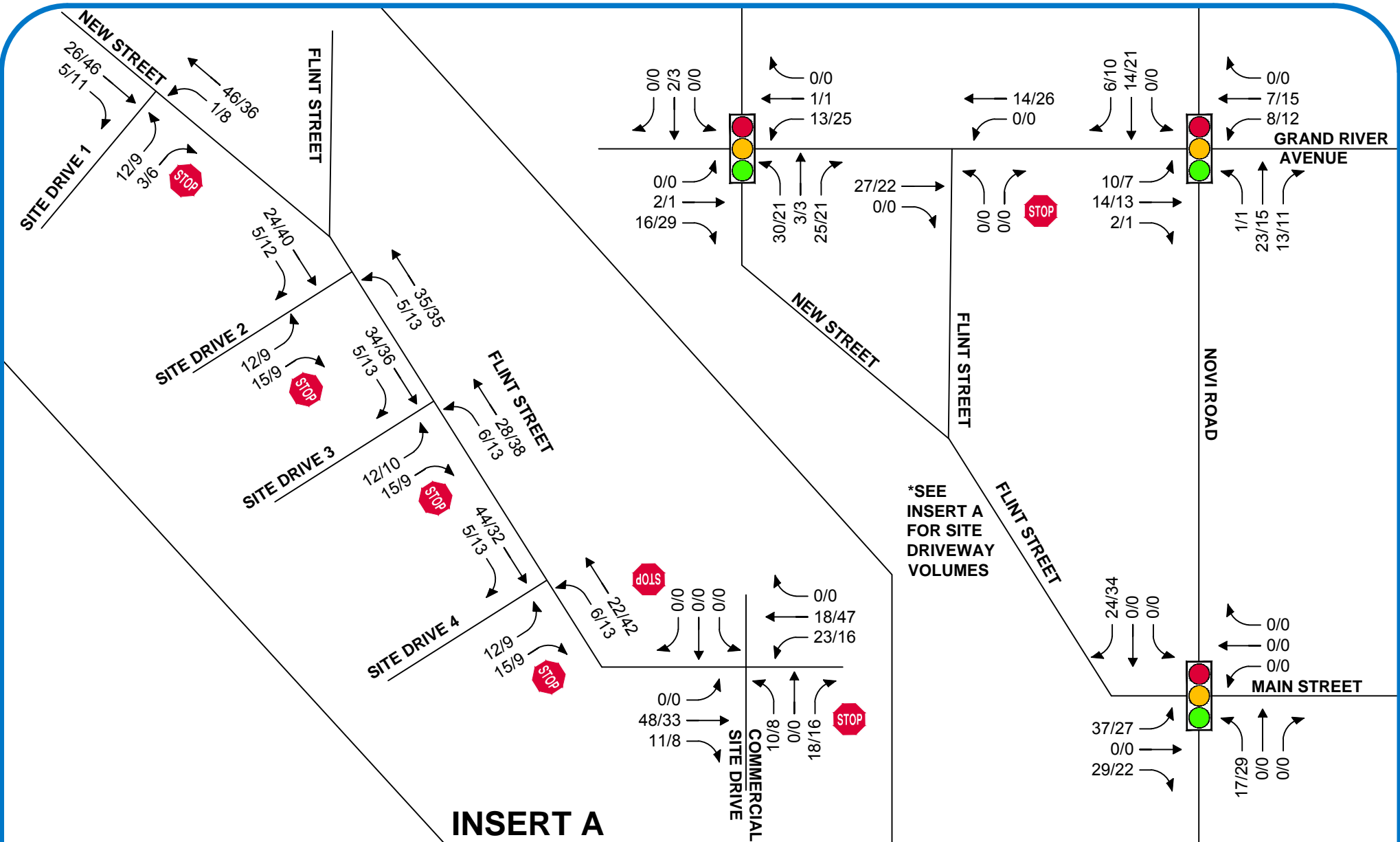
FIGURE 3
BACKGROUND TRAFFIC VOLUMES

FLINT STREET DEVELOPMENT - NOVI, MI

LEGEND

-  SIGNALIZED INTERSECTION
-  UNSIGNALIZED INTERSECTION
-  TRAFFIC VOLUMES (AM/PM)
-  ROADS





INSERT A





***SEE INSERT A FOR SITE DRIVEWAY VOLUMES**



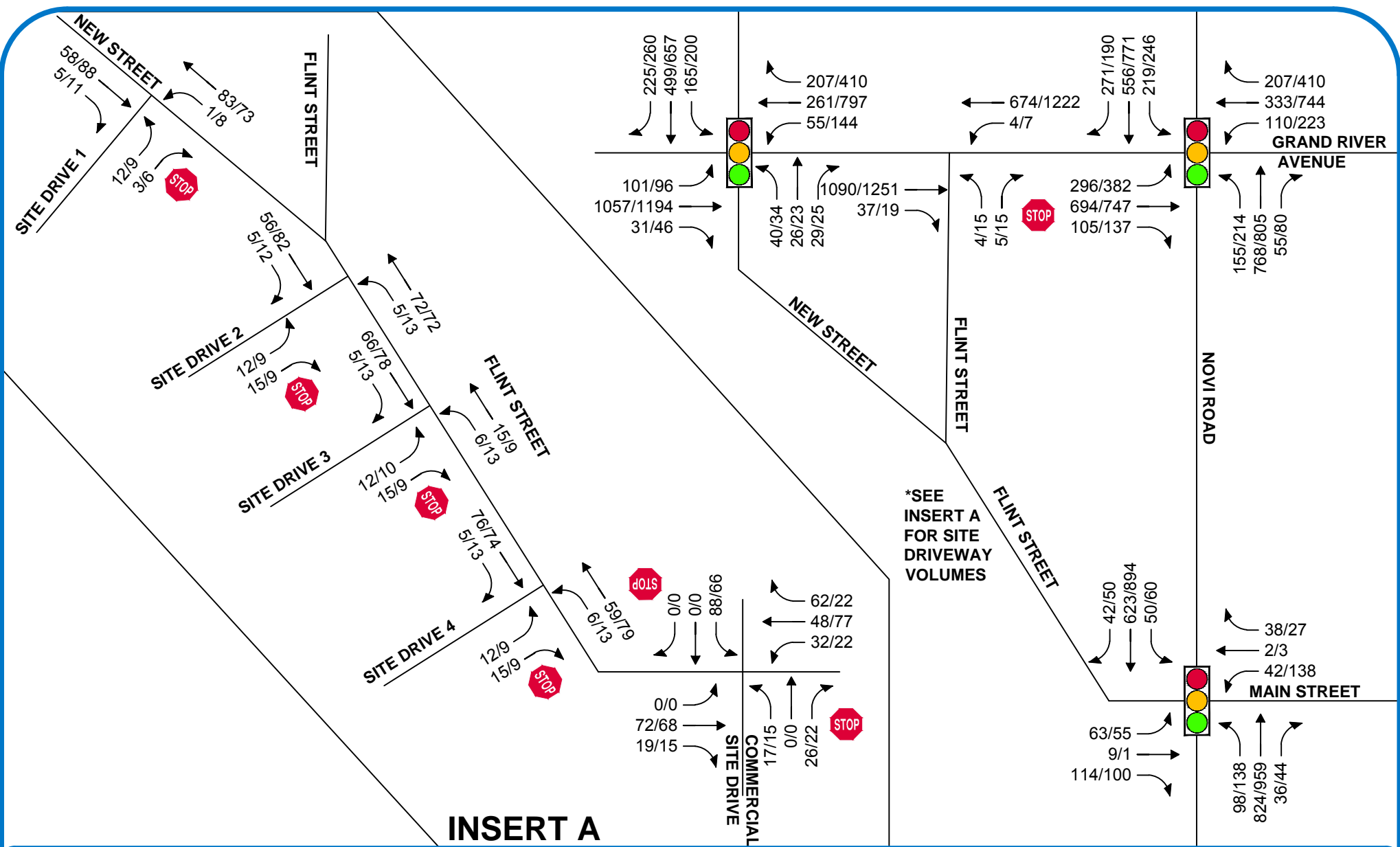
FIGURE 4 SITE-GENERATED TRAFFIC VOLUMES

FLINT STREET DEVELOPMENT - NOVI, MI

LEGEND

-  SIGNALIZED INTERSECTION
-  UNSIGNALIZED INTERSECTION
-  TRAFFIC VOLUMES (AM/PM)
-  ROADS





INSERT A



FIGURE 5 FUTURE TRAFFIC VOLUMES

FLINT STREET DEVELOPMENT - NOVI, MI

LEGEND

- SIGNALIZED INTERSECTION
- UNSIGNALIZED INTERSECTION
- TRAFFIC VOLUMES (AM/PM)
- ROADS



APPLICANT RESPONSE LETTER
Current Prior to Planning Commission Meeting



June 22, 2018

City of Novi
Att: Sri Ravali Komaragiri
Planner
Community Development Department
45175 W. Ten Mile Road
Novi, MI 48375

Re: Petitioner/Applicant for The Bond (fka The District) Second Response to the Plan Center Report (i.e. Planning Review Letter) Dated June 20, 2018 (JSP 18-0010)

Sri:

The following summarizes responses to the Plan Review Center Report dated June 20, 2018.

Project Background

This letter provides categorical responses to the Plan Review Center Report issued by the Planning Department ("Staff"), dated June 20, 2018. However, prior to addressing Staff comments, we felt it was important to revisit discussions that occurred between Staff and Tricap Holdings (Applicant) as part of the planning and pre - development process, which began in early 2017.

The Bond (fkaThe District) proposed mixed - use project includes an approximate 6,000 square foot commercial building on one acre, as well as two four story, luxury mid - rise apartment building encompassing approximately 275,000 square feet, supported by a 270 space, multi - level parking deck that is attached to the building. From the beginning it was the Applicant's intent to introduce and develop a market rate luxury product type that had yet to be built in Novi, in keeping with the spirit of the intent of the Town Center/urban vision and offering a unique, local residential experience within walking distance of all of the Town Center ("TC") District. A project that would readily co-exist, compliment and support the surrounding retail, restaurant and commercial environment.

This concept appeared to be consistent with the type of project the City was looking to attract in the TC District, but it was also acknowledged by all parties that current and even recently enacted residential ordinances may not completely align with the described product design parameters. However, given the desire to introduce a stronger residential component to the Town Center area, combined with the desire to redevelop an area of the City with much potential, yet with numerous challenges, The Bond appeared to be an ideal candidate for the location.

The luxury mid - rise apartment building is not a new residential concept. Thousands of units are built each year, predominantly in core urban cities and many vibrant suburban communities

offering close proximity to retail, restaurant and entertainment districts. However, it is accurate to say this residential concept is somewhat new to Novi and specifically, the Town Center District, which currently consists primarily of retail, restaurant and commercial businesses, with few, if any “urbanesque” residential options.

Residents that choose to live in this style of apartment community are looking for a more robust lifestyle experience than offered in a traditional suburban apartment community. They tend to live and use the entire building and its amenities and not just the apartment unit. The property and entire building becomes their “unit”. The typical amenities and demand drivers in a luxury mid-rise property like The Bond include:

- fitness centers (often more than one)
- business centers
- resident clubrooms (often more than one)
- active and passive outdoor and courtyard gathering spaces with grilling spaces, kitchens, outdoor lounge, exercise and and gaming areas
- active social programming and regularly scheduled resident events
- extensive outdoor pool areas with socially focused seating and gathering spaces
- secure bike repair and storage rooms within the building
- sheltered parking with direct access to the building
- close proximity to restaurants, retail and entertainment venues
- technology with accessible connectivity (i.e. fiber, wi - fi, etc)
- more resident amenities in lieu of larger units

Based on these characteristics, this building type tends to be more efficient with respect to unit size and mix with a greater percentage of common area relative to total unit square footage. Also, while common in many urban cities, this particular building type is much more dense (i.e. DUA) with respect to land coverage. By example, The Bond will offer approximately 25,000 – 30,000 square feet of indoor and outdoor common area and resident gathering spaces that are accessible to tenants 24 hours per day, compared to a more traditional 250 unit suburban style apartment community that might offer a 5,000 – 6,000 square foot clubhouse with perhaps a few other outdoor resident spaces. Programming of these spaces are also quite different.

In addition to discussing a more modern and urban product type the City indicated their desire and need to realign and reconstruct Flint Street as part of the City’s comprehensive plan to connect the southwest loop (i.e. Flint Street) to the TC District. While a new street will provide a mutual benefit to the City and the project, the Applicant is gifting the property necessary to achieve the desired road profile and realignment. This approximate one acre of land provides a benefit for all, but also creates design and ordinance challenges when combined with the already shallow configuration of the parcel. This is more evident as we approach the Planning Review process, particularly as it relates to parking, building setbacks, landscaping requirements, parking setbacks and other site related issues.

From the time first introduced to the City, it was always understood that to get to the outcome that was mutually desired on this challenging parcel, deviations from existing ordinances would be required.

Applicant Response to Planning Review Letter Dated June 20, 2018

Categorical Response to Staff Comments Regarding Conformance with the Zoning Ordinance with Respect to Article 3 (Zoning Districts), Article 4 (Use Standards, Article 5 (Site Standards and any other Applicable Provisions of the Zoning Ordinance are as follows:.

1. Density and Total Number of Rooms:

Code Deviation – The applicant is proposing 627 rooms with a total density of 33 DUA. City Council can approve the increase in the room count (421 allowed, 627 proposed) up to twice the number of rooms allowed and thus the increase in density proposed (23 DUA

approximate allowable, 33 DUA proposed). The master plan for the land use recommends a density of 20 DUA for the subject property.

Response - Consistent with Applicant's comments in the Project Background above, The Bond is a lifestyle oriented urban/suburban designed building offering a desirable mix of resident amenities different from traditional luxury market rate apartment communities. The scale/density of the project is imperative to justify the scope of amenities and legitimize the project's unique yet isolated location in this area of the TC. As a result of The Bond's isolated location on Flint Street it has no notable impact on adjacent property owners or businesses, particularly as it relates to traffic. This appeared to be consistent with the product type sought by the City for this location.

2. **Maximum Percentage of 1 Bedroom Units that Exceeds the Ordinance (Requires ZBA Approval):**

Code Deviation #1 – The maximum percentage of 1 bedroom units is 50% (58% is proposed). Staff recommends revising the unit mix to conform to 50%.

Response - The unit mix of 1, 2 and 3 bedroom units was based on an internal marketing survey and assessment of the Novi luxury rental market that indicates a desired target mix of approximately 60% 1 bedroom units. This was based on a number factors including the target demographic and renter profile, indicated rental rates, residential amenities and experiences that will be offered to residents. The mix is also consistent with similar, recently built and stabilized properties owned by the applicant in similar Michigan markets (i.e. Lansing and Grand Rapids). Other key factors contributing to the unit mix determination:

- A key demographic for the property will include young professionals, both singles and couples, demanding 1 bedroom units under \$1400 per month.
- A unit mix that offers a number of 1 bedroom configurations, covering a range of almost 200 square feet, offering a variety of floor plans and premium locations within the property.
- A limited number of luxury 1 bedroom units currently offered in the Novi market that do not offer like amenities or resident experiences (e.g. private parking deck, active and passive outdoor courtyard experiences, resident club rooms, bike repair centers, walking distance to Novi Town Center).
- The shift to a larger percentage of 1 bedroom units is consistent with the industry overall as it relates to the urban-suburban luxury rate market given the design attributes of the product type.

As noted below, applicant will seek a variance from the Zoning Board of Appeals for an increase from 50% to 58% 1 bedroom units.

3. **Total Parking Required and Proposed** - No deviation is sought as proposed parking of 432 spaces is consistent with TC-1 parking standards for a mixed used development.

4. **Mixed Use Development** – Staff is suggesting a form of agreement and/or financial guarantees acceptable to City that assures the commercial component will be built within a certain time as suggested by applicant and approved by the City.

Response – Applicant is open to entering a form of agreement mutually acceptable to applicant and City to assure the commercial component is built within a certain time.

5. **Photometric Plan (Sec 5.7.1)** – The applicant has submitted a revised photometric plan, following much discussion with Staff, that includes an Avg/Min light level ratio for the entire site that is 4.8:1, which exceeds the maximum allowable ratio of 4:1.

- Additionally, an insignificant area along the south property line has a foot candle value greater than the 1 fc limit.
Response – Applicant will request a ZBA variance for both the increased average minimum light level ratio and increased foot candle value along the impacted areas on the south property line.
6. **Sheet Title** – Sheet No. 3 is referred to as "Right of Way Taking Plan. The City requests this be changed to reflect a voluntary contribution of the land by applicant. Applicant will change the sheet title prior to final site plan approval.
 7. **Planning Review Chart** – As requested, applicant referred to Planning Review Chart to provide responses to minor comments noted in the Planning Chart. Specific responses to identified issues are as follows:
 - a. Phasing (Pg 2 of 19) – Building and parking counts will be added to the Phasing Plan
 - b. Building Height (Pg 3 of 19) – Maximum building height will be added to the Site Plan Data on sheet 2 of the Site Plan
 - c. Commercial Building Setbacks (Pg 4 of 19) – The commercial building is setback greater than the TC-1 ordinance allows (10 ft max allowed, 15 ft proposed). The irregular shape of the parcel makes it difficult to conform to the ordinance and a waiver may be requested if we can't realign the building with the ordinance.
 - d. Commercial Parking Setback (Rear Yard) – Applicant will be requesting a waiver form the parking setback to accommodate cemetery parking and a path to connect the parking to the cemetery.
 - e. Development Amenities (Pg 7 of 19) – The dog park encroaches into the 25 foot watercourse setback. Applicant will be requesting a waiver and note any impact to the site plan.
 - f. Economic Impact (Pg 18 of 19) – The estimated economic impact of the The Bond is as follows:
 - i. Total Construction/Project Value of approximately \$42 million;
 - ii. Approximately 350 temporary construction personnel employed during construction (2 years), with total wages and benefits of approx. \$20 million;
 - iii. Seven full time employees upon completion
 - iv. Total, local annual economic impact of approximately \$1M (e.g. wages, purchases of goods and services).
 8. **Project Phasing** – Applicant provided clarification, as requested in the June 1 Review Letter.
 9. **Street Name Change** – Project and street naming were accepted by Naming and Street Naming Committee, subject to City Council approval. In the interim, applicant will refer to the project as The Bond and the street as Bond Street.
 10. **Exterior Signage** – Applicant will submit a sign application after Site Plan approval.
 11. **Conservation Easements** – to be submitted with Final Site Plan submittal, if required

Identified City Council Waivers/DCS Variances

(Please note: Applicant has worked with Staff with the intent of eliminating or reducing as many deviations as possible).

Ordinance deviations and applicable City Council waivers requested by applicant are as follows:

1. **Maximum Number of Rooms Allowed (Sec 4.82.2a)** - See staff and applicant comments noted above. **Applicant requests a waiver for an increase in the number of rooms allowed.**
2. **Front Yard Building Setback (Sec 3.1.26.D)** – The maximum allowed under the ordinance is 10 ft and the applicant is requesting 15 ft. **Applicant requests a waiver to allow a 15 ft front yard setback due to parcel constraints.**
3. **Parking Setback (Sec 3.1.26.D)** – Applicant requests a variance to reduce the parking setback from 10 ft to 6 ft along the cemetery property line. The required setback area for the commercial portion of the property is 12,181 sf (i.e. 20 ft setback from Bond Street, 20 ft setback from Novi Road, 10 ft setbacks for side and rear yards). Reducing the side yard parking setback to 6 ft will reduce the setback area provided. A property split is proposed between residential and commercial portion of the development along the centerline of the Middle Rouge River. The 10 ft parking setback along this future property line can be increased to a 15 ft setback to provide the required setback area needed for the commercial portion of the property.
4. **Commercial Parking Surface** – Applicant proposes to build six temporary parking spaces for cemetery use until such time that permanent Phase 3 improvements are made at which time a permanent parking surface will be installed. **Accordingly, the following City Council variances are requested:**
 - a. City Council Variance from Sec 11-239(b)(1), (2) of Novi City Code for absence of hard surface for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1;
 - b. City Council Variance from Sec 11-239(b)(1), (2) of Novi City Code for absence of curb and gutter for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1;
 - c. City Council Variance from Sec 11-239(b)(3) of Novi City Code for pavement markings and layout for parking lot and driveway for proposed temporary parking lot of six spaces in Phase 1.
5. **Traffic** – The applicant is requesting approval of the following deviations from City Council, or proposing adjustments to at Final Site Plan to eliminate a deviation and avoid the need for a waiver:
 - a. A deviation was noted for a variance from Design and Construction Standards Sec 11-216(d) for reduction in distance opposite side commercial driveway. As discussed between Staff and Seiber Keast via an email dated June 21, 2018, opposite side spacing does not apply for non-residential collectors operating at 25 MPH. However, same side driveway spacing must be provided to the development on the west side of Bond Street (fka Flint St) and one driveway that is used for emergency access only. The emergency access drive will be blocked by break away bollards, or a gate system for daily use and it will only be used for fire truck access. Starting from the northern most driveway, the spacing between the four active driveways is as follows: 261 feet, 325 feet, 438 feet and 161 feet. **Therefore, the applicant suggests a variance will not be required.**
 - b. There is a current deviation due to the reduction in curb height when not fronting the 17 feet parking spaces (6 inches required, 4 inches proposed). Applicant will revise all curb height to 6 inches, except when the curb is fronting a parking space that is 17 feet in length (4 inches will be provided for overhang of vehicles). **Therefore, a City Council waiver should not be required.**
 - c. There is a current deviation in the proposed site plan for a reduction in sidewalk width to access the bike parking for the Commercial area (6 feet required, 5 feet clear sidewalk in addition to 2 foot overhang). Applicant proposes to modify the site plan by relocating the bicycle parking area to the other end of the building in

the greenspace adjacent to the handicap parking space. The sidewalk leading to the relocated bicycle parking area will be widened to 6 feet, per the ordinance.
Therefore, a City Council waiver will not be required.

6. **Façade Deviations and Waivers** – The Façade Review and Staff recommends approving all façade waivers being requested by applicant. Please see Review Letter dated June 20, 2018 for the eight specific waivers required.
7. **Landscaping Deviations and Waivers** – The Landscape Review and Staff recommends approving all landscaping waivers being requested by applicant. Please see Landscaping Review Letter dated June 19, 2018 and attached response letter by Allen Design LLC, dated June 21, 2018. All waivers listed on the Landscaping Review Letter will be updated on the landscape plan.

Zoning Board of Appeals - Variance Requests

The following is a list of deviations and variances to be requested by the applicant from the Zoning Board of Appeals:

1. **Maximum Allowable Percentage of Units (Sec 4.82.2.e)** – A maximum of 50% of 1 bedroom units is allowed and the applicant has proposed 58%. A variance of 8%, or 20 units will be requested. Please see narrative above supporting the increase in 1 bedroom units from 50% to 58%.
2. **Parking Setbacks (Sec 3.27.1.D)** – Due to the irregular shape of the parcel and inability to engineer all parking in the rear yard, a Zoning Board of Appeals variance from section 3.27.1.D will be requested for
 - a. (1) side yard parking (i.e. commercial – 49 spaces);
 - b. (2) front yard parking (i.e. residential – 38 spaces, or 9% of total parking);
 - c. (3) side yard parking (i.e. residential – east and west side of building – 35 spaces, or 12% of total parking).
3. **Building Setbacks (Sec 4.82.2e)** – Due to the irregular shape of the parcel, deviations exist regarding building setbacks. The applicant will be requesting the following variances from ZBA:
 - a. Variance for reduction of minimum building setback for Building 1 on east side (15 ft required, 12 ft proposed, for a length of 12 ft, or less than 4% of the building width).
 - b. Variance for reduction of minimum building setback for Building 2 on east side (15 ft required, 8 ft proposed, for a length of 16 ft, or less than 6% of the building width).
 - c. Variance for a reduction of minimum building setback for the parking garage along the west property line (15 ft required, 5 ft proposed for entire length of structure).
4. **Lighting and Photometric Plan (Sec 5.7.3.E)** – The applicant will be requesting the following variances from ZBA:
 - a. Variance for the increase in the average minimum light level ration from 4:1 to 4.8:1;
 - b. Variance for the maximum allowed foot candle along the south property line abutting the railroad tracks from 1 FC to approximately 1.7 FC for an insignificant length along the south property line.
5. **Loading Areas (Sec 3.27.1 H and Sec 5.4.2)** – Loading is required to be located in the rear yard and applicant is proposing to locate in side yards. Applicant will be requesting the following variances regarding the ordinance(s) regarding loading areas in TC – 1;

- a. Variance to allow two loading areas in the rear yard for residential;
- b. Variance for a reduction in minimum area required for a loading area for each of the two loading spaces in a residential section (2,830 sq ft required vs 644 proposed).

Both variances are supported by staff given the residential nature of the project.

6. **Sidewalk Width Along Bond (fka Flint Street) (3.27.1.1)** – Sidewalks required along non-residential collector to be 12.5 feet wide. Proposed sidewalk along Flint Street is 8 feet and therefore does not conform.
 - a. ***Applicant will be requesting a variance to reduce the side walk width from 12.5 feet to 8 feet.***

The request is consistent with the City's realignment and reconstruction plans for Flint Street and there are future plans to construct a 10 foot wide path on the opposite side of Flint/Bond Street, effectively doubling the surface area of the sidewalk area along Flint/Bond.

7. **Parking Space Dimensions and Maneuvering Lanes (Sec 5.3.2)** – Applicant will request a ZBA variance for not meeting the minimum depth requirement for parking spaces in a garage/ramp. 19 feet is required and 18 feet is proposed. Consistent with staff support and discussion, a standard pre - cast parking structure is designed to accommodate parking spaces 18 feet deep and 24 foot drive aisles for turning and maneuvering. Modification is deemed costly and unnecessary given the dimensions are industry standard.

Applicant Responses to Other Reviews

Please see responses attached as follows:

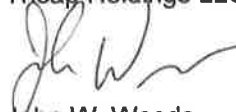
1. Engineering Review, Wetland Review, Traffic Review and Fire Review – Addressed in responses noted above and any remaining items requiring response will be provided prior to Final Site Plan.
2. Landscape Review – Please see attached letter from Allen Design, LLC, dated June 20, 2018 and responses noted above.
3. Woodland Review – No response required. Additional comments to be addressed with Final Site Plan approval.
4. Façade Review – Please see response and waiver request above - Identified City Council Waivers (Item #6) above.

(see signature block on following page)

Should you have any questions or want to discuss specific items noted above please call at your earliest convenience at 248-361-0718.

Sincerely,

DTN Development Group and
Tricap Holdings LLC



John W. Woods
DTN, on behalf of The Bond

Cc: Glenn Cantor, Tricap
Albert Ludwig, Tricap
Raji Uppla, DN
Chuck Holman, DTN

June 21, 2018

Mr. Rick Meader, Landscape Architect
City of Novi Community Development
45175 West 10 Mile
Novi, MI 48375

RE: The Bond

Dear Mr. Meader:


Below are our responses to your review dated June 19, 2018.

Landscape Review

- The waivers listed on the plan will be updated to match those noted in the review.
- We will examine the perimeter tree plantings in the western area to determine if more room can be added for the trees. If this area cannot be enlarged, we will note it in our response letter.
- Planters will be added to the commercial building facing the parking lot to soften the building edge.
- Snow deposit areas will be revised to ensure consistency.
- Face curbs will be provided throughout the development.
- Grade will be adjusted to prevent the dog park area from sheet flowing into the creek.
- The wall along Bond Street will be extended as suggested.
- The greenbelt and perimeter trees that are being double counted will be noted on the revised plan.
- Notes indicating plantings shall not be planted within 4' of the property line will be shown near the property lines.

If you have any questions or comments regarding this response, please contact me at your convenience.

Sincerely,



James C. Allen
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