



## ADELL CENTER PRO JZ18-24 with Rezoning 18.724

### **ADELL CENTER PRO JZ 18-24 AND ZONING MAP AMENDMENT 18.724**

Public hearing at the request of Orville Properties, LLC for a Zoning Map Amendment 18.724 for Planning Commission's recommendation to City Council for a Planned Rezoning Overlay Concept Plan (PRO) associated with a zoning map amendment, to rezone from Expo (EXPO) to TC (Town Center). The subject property is approximately 23-acres and is located at 43700 Expo Center Drive, north of Grand River Avenue and south of I-96 in Section 15. The applicant is proposing to develop the property as a multi-unit commercial development consisting of nine units accessed by a proposed private drive. The current PRO Concept plan includes a request for an Unlisted Use Determination under Section 4.87 of the Zoning Ordinance.

### **Required Action**

Recommendation to the City Council for approval or denial of the rezoning request from Expo (EXPO) to TC (Town Center) with a Planned Rezoning Overlay.

## Motion sheet

### Approval

In the matter of the request of Orville Properties, LLC, for the Adell Center JZ18-24 with Zoning Map Amendment 18.724, motion to **recommend approval** to the City Council to rezone the subject property from EXPO (Exposition) to TC (Town Center) with a Planned Rezoning Overlay.

The recommendation includes the following ordinance deviations for consideration by the City Council:

**[insert all deviations being recommended from the applicants response dated July 03, 2018]**

If the City Council approves the rezoning, the Planning Commission recommends the following conditions be made part of the PRO Agreement:

**[insert any conditions]**

This motion is made because the proposed Town Center zoning district is a reasonable alternative to the Master Plan for Land Use, and because, as stated by the applicant:

**[Select any that apply from applicant's letter]:**

1. *The proposed development will convert a vacant parcel of property at a major intersection and entryway into the City of Novi.*
2. *The development of this property will reduce any chances of crime associated with a vacant parcel by providing new development with continuous movement of people and vehicles throughout the property.*
3. *This development will convert a property that is currently zoned EXPO center into a zoning district that will allow a use that is beneficial to neighborhood businesses and the community in general.*
4. *This development will help produce a more positive image of the City of Novi by the 100,000 plus motorists travelling along Interstate on a weekly basis.*
5. *The approval of this development will bring additional entertainment, overnight stay and dining opportunities to the City that will benefit the City of Novi residents as well as bring in residents and visitors from neighboring communities.*
6. *This development is centrally located to several communities that will serve as a weekend long youth sporting tournaments and weekly events held at the Suburban showplace. These events typically bring in people from all over southeast Michigan and the United states.*
7. *The approval of this development will trigger a sale of proposed units within Adell Center thereby generating an increase in property values in addition to the value of neighboring properties.*
8. *The approval of this development will create 200-300 temporary construction jobs and permanent jobs.*
9. *The approval of this development will increase the tax base within the City of Novi. As reported by the Mayor at a recent City Council meeting, the city desires to increase the tax base to fund additional services such as police, fire and parks departments.*
10. *It is estimated that this development will increase the tax base by over \$3 million annually, plus an additional personal property tax generated from the new businesses.*
11. *The approval of the proposed development will include the improvement of over three acres of existing city regulated woodlands/wetland areas to allow for better access by the public. The approval of this development will include a consistent and*

*cohesive streetscape and signage package throughout. The proposed development includes new public art (pocket parks) locations for placement of community art.*

**--OR--**

**Postpone**

In the matter request of Orville Properties, L.L.C. for the Adell Center, JZ18-24 with Zoning Map Amendment 18.724, a motion to **postpone** making a recommendation on the proposed PRO and Concept Plan to allow the applicant time to provide additional information and to allow the City staff and consultants, and the Planning Commission, to evaluate all aspects of the Concept Plan as proposed. This recommendation is made for the following reasons:

1. Additional information is required regarding parking. The applicant's materials refer to a shared parking study, but no such study has been provided for review by the staff and consultants or the Planning Commission. In addition, at this time, the materials provided by the applicant do not include information regarding the minimum number of spaces that are required by ordinance to be provided, and the number provided per each proposed use or site, so that the City staff and consultants and Planning Commission can determine the nature and extent of the variance or deviation requested as part of the PRO. Information that the City normally would have includes things such as parking counts per use or site based, for example, on the number of hotel rooms and amount of banquet space (for the hotel uses) and/or the number of seats or employees for the restaurants proposed. The materials and documentation provided so far is insufficient for the review required.
2. The staff and the Planning Commission require more information regarding the effect of widening the pavement for the roadway, as recently proposed by the applicant (such as a revised concept plan with updated lot lines, setbacks, greenbelt, conceptual parking lot layout, etc.), from 30 feet to 36 feet, which may result in different/additional variances or deviations as described in the planning staff's memo.
3. If the road is not widened from 30 feet to 36 feet, the City staff and consultants have asked for additional information as described in the planning staff's memo.
4. Information regarding the use of the water tower, if any, as part of the development has not been provided.
5. Additional information is required with regard to the proposed uses for Unit 4; more specifically, if the uses are more intense than simply parking they may require additional improvements (e.g., a turn lane), and additional trip generation information may be required.
6. The City's facade consultant has requested additional information regarding certain of the uses as described in the façade review letter.
7. Additional information is required regarding sign packages for certain of the uses, in particular Carvana and I Fly, which have not been completed and submitted in the required format with all required information.
8. The City's traffic consultant and City Engineer have not resolved the speed limit on the roadway, which may affect the driveway spacing between Units 3 and 4, and between Units 2 and 3.
9. The location and exact description of the 15% open space needs to be clarified; the trails referred to need to be shown, and the effects on woodlands as described in the woodland consultant's letter must also be clarified.
10. The applicant is encouraged to address and/or reduce the number of deviations required and provide information showing how each Zoning Ordinance provision

sought to be deviated would, if the deviation were not granted, prohibit an enhancement of the development that would be in the public interest, and would be consistent with the Master Plan and the surrounding area.

11. The applicant should have the opportunity to clarify if any PRO conditions are being offered under the PRO provisions of the Zoning Ordinance.

--OR--

**Denial**

In the matter of the request of Orville Properties, LLC, JZ18-24 with Zoning Map Amendment 18.724, motion to **recommend denial** to the City Council to rezone the subject property from EXPO (Exposition) to TC (Town Center) with a Planned Rezoning Overlay, *because the proposed zoning is not consistent with the Master Plan for Land Use recommendation for Office Research Development Technology land uses for the subject property; because the applicant has not met its burden under the Planned Rezoning Overlay (PRO) ordinance to establish that the enhancements proposed under the PRO Concept Plan would not be likely to be achieved without utilizing the PRO process, as set forth in the staff and consultant review letters; because the applicant has not established a basis for certain of the proposed deviations as set forth in the staff and consultant review letters; and because: [insert any additional]*



## UNLISTED USE DETERMINATION FOR CARVANA AS 'VENDING MACHINE FULFILLMENT CENTER'

### **UNLISTED USE DETERMINATION FOR CARVANA AS 'VENDING MACHINE FULFILLMENT CENTER'**

Consideration of the request of Carvana for an Unlisted Use Determination under Section 4.87 of the Zoning Ordinance. The applicant is requesting a determination on the appropriateness of a Vending Machine Fulfillment Center as a Special Land Use in the TC, Town Center District.

### **Required Action**

Planning Commission's recommendation to approve, deny or postpone the request for unlisted use determination of Carvana, a Vending Machine Fulfillment Center as a Special Land Uses in the TC (Town Center District).

## **MOTION SHEET**

### **Approval**

In the matter of Unlisted Use Determination, motion to **recommend approval** to City Council to allow Carvana, 'Vending Machine Fulfillment Center' as the described unlisted use, as an appropriate use subject to Special Land Use Conditions in Town Center District based on the following motion:

- a. Carvana is not expressly authorized or contemplated in City of Novi Zoning Ordinance as a as a principal permitted use or a principal permitted use subject to special conditions;
- b. The proposed use is an appropriate use for the requested location at 43700 Expo Center Drive;
- c. A trip generation report or any additional information as required by our City Traffic Consultant, will be submitted at the time of Special Land Use Consideration;
- d. *(additional conditions here if any)*

*(This motion is made because the plan is otherwise in compliance with Article 4, Section 4.87 the Zoning Ordinance and all other applicable provisions of the Ordinance.)*

**- OR -**

### **Denial**

In the matter of Unlisted Use Determination, motion to **recommend denial** to City Council to allow Carvana, 'Vending Machine Fulfillment Center' as the described unlisted use, as an appropriate use subject to Special Land Use Conditions in Town Center District based on the following motion:

- a. Carvana fits the definition of a 'used car salesroom, showroom, or office' which are only permitted in B-3, General Business district in our Zoning Ordinance;
- b. *(additional conditions here if any)*

*(This motion is made because the plan is not otherwise in compliance with Article 4, Section 4.87 the Zoning Ordinance and all other applicable provisions of the Ordinance.)*

**- OR -**

### **Postpone**

In the matter of Unlisted Use Determination, **postpone** the recommendation to City Council to allow Carvana, 'Vending Machine Fulfillment Center' as the described unlisted use, as an appropriate use subject to Special Land Use Conditions in Town Center District based on the following motion:

- a. To allow continued discussion of this item at the same time as action on the proposed Adell Center PRO;
- b. To allow for staff to consider the appropriateness of the proposed use all locations within Town Center District;
- c. To allow for applicant to provide alternate plans to repurpose the building for other uses if the use of 'Vending Machine Fulfillment Center' eventually becomes outdated;
- d. *(additional conditions here if any)*

MAPS

Location

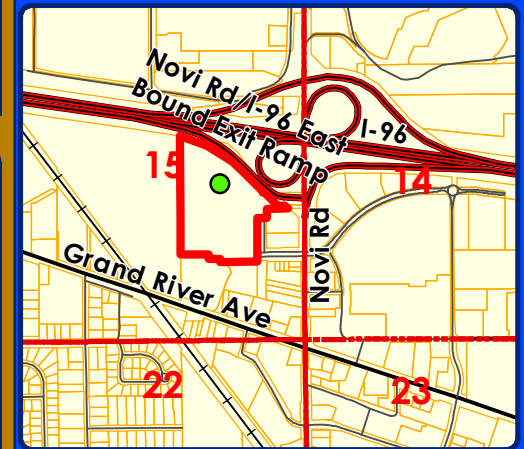
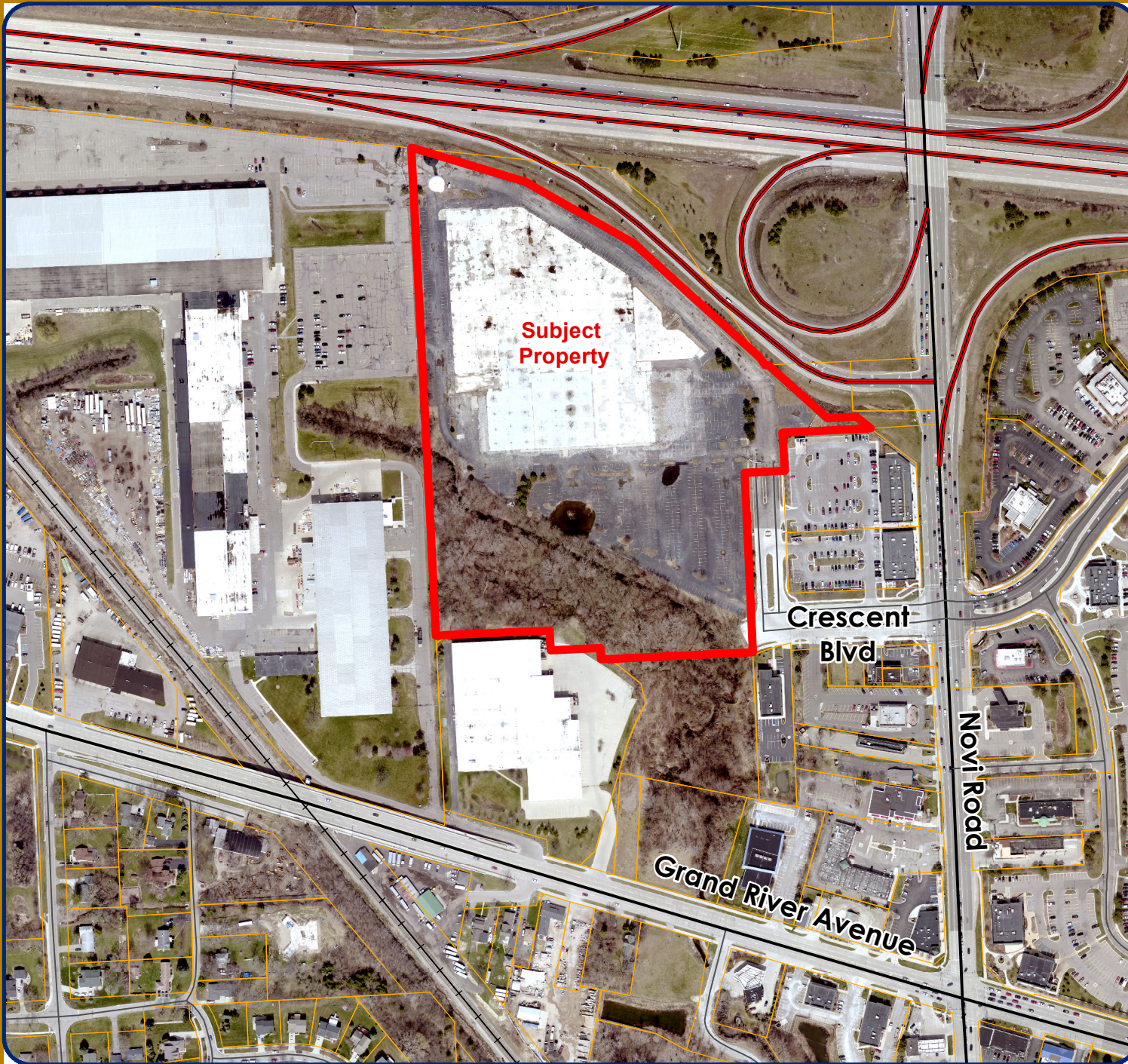
Zoning

Future Land Use

Natural Features

# JZ 18-24 ADELL CENTER PRO

Location



## LEGEND

 Sections



## City of Novi

Dept. of Community Development  
City Hall / Civic Center  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

Map Author: Sri Komaragiri  
Date: 07/06/18  
Project: JZ 18-24 ADELL CENTER PRO  
Version #: 1

0 85 170 340 510 Feet

1 inch = 381 feet



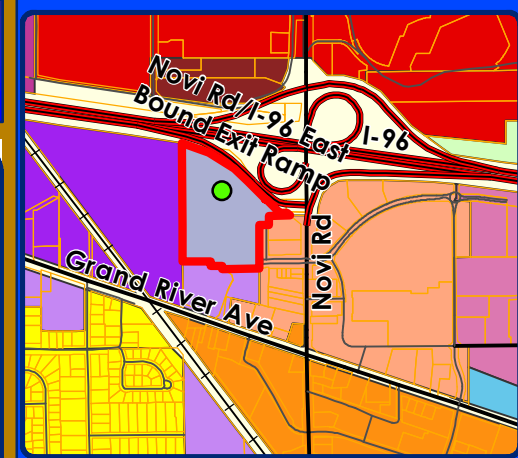
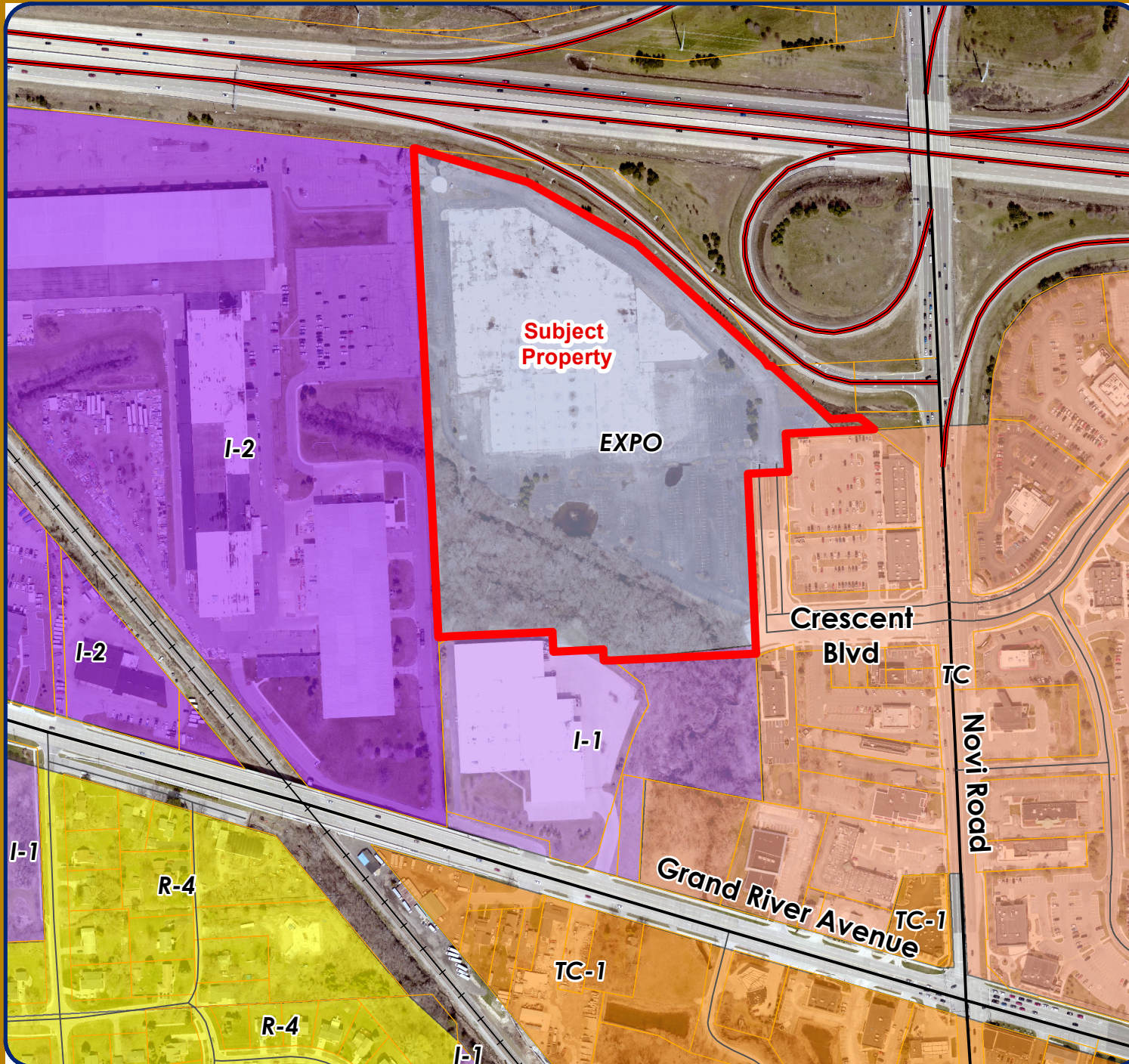
### MAP INTERPRETATION NOTICE

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# JZ 18-24 ADELL CENTER PRO

## Zoning



**LEGEND**

- R-A: Residential Acreage
- R-4: One-Family Residential District
- C: Conference District
- EXPO: EXPO District
- I-1: Light Industrial District
- I-2: General Industrial District
- OS-1: Office Service District
- OSC: Office Service Commercial
- OST: Office Service Technology
- RC: Regional Center District
- TC: Town Center District
- TC-1: Town Center -1 District



**City of Novi**  
 Dept. of Community Development  
 City Hall / Civic Center  
 45175 W Ten Mile Rd  
 Novi, MI 48375  
[cityofnovi.org](http://cityofnovi.org)

Map Author: Sri Komaragiri  
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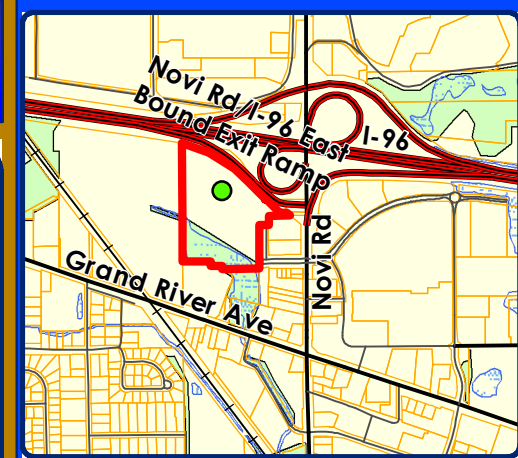
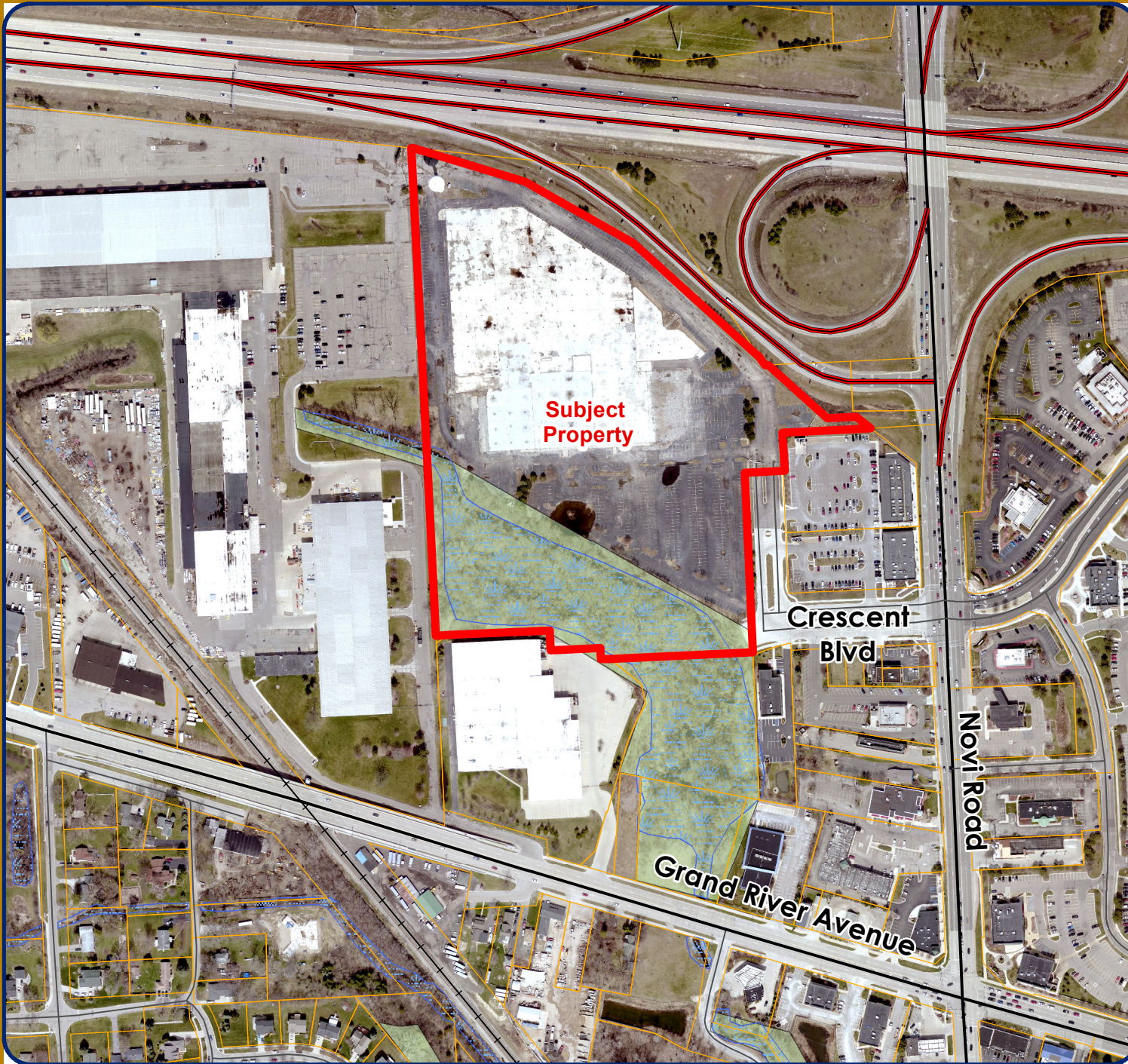
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# JZ 18-24 ADELL CENTER PRO

## Natural Features



- LEGEND**
-  WETLANDS
  -  WOODLANDS



**City of Novi**  
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**PRO CONCEPT PLAN**

**June 04, 2018**

**(Full plan set available for viewing at the Community Development Department.)**



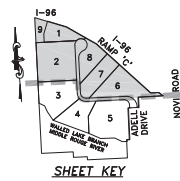
**BENCHMARKS:**

CITY OF NOVI BM 1542  
 "C" ON NORTH RM OF SANITARY MAINLINE,  
 25'± WEST OF BACK OF CURB NOVI ROAD  
 & 45'± SOUTH OF DRIVE #26179 NOVI ROAD  
 ELEVATION: 915.10 (N.A.V.D.88)

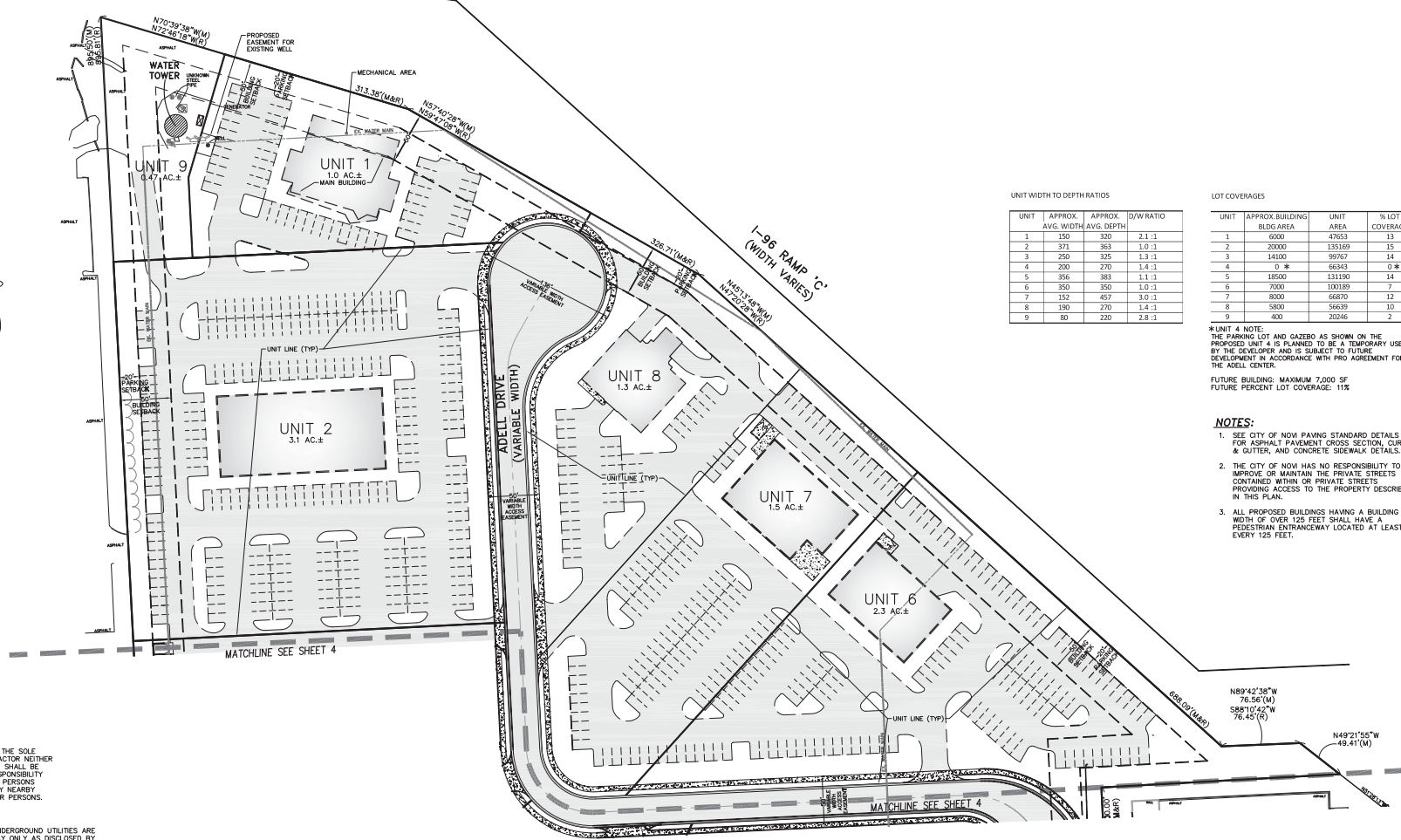
BM #1  
 ARROW ON HYDRANT, SOUTH SIDE OF  
 CRESCENT BOULEVARD, 24'± EAST OF  
 SOUTHEAST PROPERTY CORNER  
 ELEVATION: 915.58 (N.A.V.D.88 DATUM)

BM #2  
 ARROW ON HYDRANT, 260'± WEST OF EXPO  
 CENTER DRIVE, 68'± SOUTH OF BUILDING  
 FOUNDATION  
 ELEVATION: 912.13 (N.A.V.D.88 DATUM)

BM #3  
 ARROW ON HYDRANT NORTH SIDE OF SITE,  
 12.5'± SOUTHWEST OF RIGHT OF WAY FENCE,  
 250'± EAST OF ADELL WATER TOWER.  
 ELEVATION: 914.36 (N.A.V.D.88 DATUM)



I-96  
 (WIDTH VARIES)



UNIT WIDTH TO DEPTH RATIOS

UNIT	APPROX. AVG. WIDTH	APPROX. AVG. DEPTH	D/W RATIO
1	150	320	2.1 : 1
2	371	363	1.0 : 1
3	250	325	1.3 : 1
4	200	270	1.4 : 1
5	356	383	1.1 : 1
6	350	350	1.0 : 1
7	152	457	3.0 : 1
8	190	270	1.4 : 1
9	80	220	2.8 : 1

LOT COVERAGES

UNIT	APPROX. BLDG. AREA	UNIT AREA	% LOT COVERAGE
1	6000	47633	13
2	20000	135169	15
3	141000	90767	14
4	0 *	66343	0 *
5	18500	131190	14
6	7000	100189	7
7	8000	66870	12
8	5800	56639	10
9	400	20246	2

\* UNIT 4 NOTE:  
 THE PARKING LOT AND GAZEBO AS SHOWN ON THE PROPOSED UNIT 4 IS PLANNED TO BE A TEMPORARY USE BY THE DEVELOPER AND IS SUBJECT TO FUTURE DEVELOPMENT IN ACCORDANCE WITH PRO AGREEMENT FOR THE ADELL CENTER.  
 FUTURE BUILDING: MAXIMUM 7,000 SF  
 FUTURE PERCENT LOT COVERAGE: 11%

- NOTES:**
- SEE CITY OF NOVI PAVING STANDARD DETAILS FOR ASPHALT PAVEMENT CROSS SECTION, CURB & GUTTER, AND CONCRETE SIDEWALK DETAILS.
  - THE CITY OF NOVI HAS NO RESPONSIBILITY TO IMPROVE OR MAINTAIN THE PRIVATE STREETS CONTAINED WITHIN OR PRIVATE STREETS PROVIDING ACCESS TO THE PROPERTY DESCRIBED IN THIS PLAN.
  - ALL PROPOSED BUILDINGS HAVING A BUILDING WIDTH OF OVER 125 FEET SHALL HAVE A PEDESTRIAN ENTRANCEWAY LOCATED AT LEAST EVERY 125 FEET.

**NOTICE:**  
 CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OF PERSONS ENGAGED IN THE WORK, OF ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.

**NOTE:**  
 THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AS DISCLOSED BY AVAILABLE UTILITY COMPANY RECORDS AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE COMPANY. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY IF A CONFLICT IS APPARENT.

RESIDENTIAL  
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 LAND SURVEYING  
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**GREENTECH ENGINEERING, INC.**  
 CIVIL ENGINEERS & LAND SURVEYORS

51147 N. Pontiac Trail, Warren, MI 48093  
 Phone: (248) 868-0700 Fax: (248) 668-0701

**811**  
 Know what's below  
 Call before you dig.

CLIENT: ADELL CENTER  
**P.R.O. PLAN (NORTH)**  
 43700 EXPO CENTER DRIVE - PARCEL NO. 22-15-476-045  
 SECTION 15  
 CITY OF NOVI COUNTY OF OAKLAND MICHIGAN

REVISED  
 2018-6-4 PER REVIEW

DATE: 4-30-2018  
 DRAWN BY: RMS  
 CHECKED BY: DJL/JMF

SCALE: 1" = 50 FT  
 17-334

3

**BENCHMARKS:**

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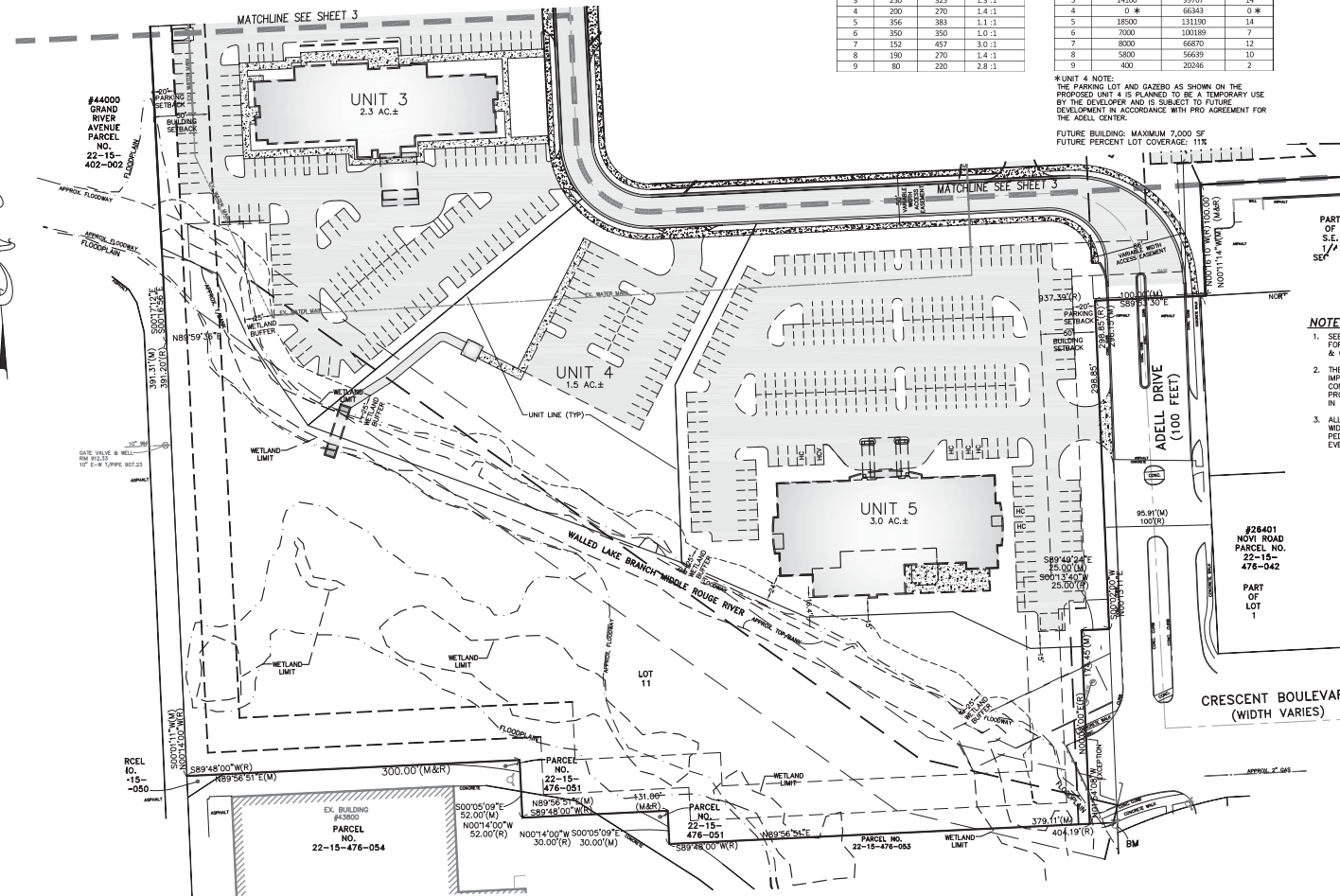
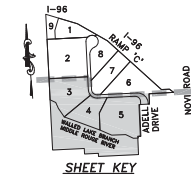
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 Know what's below  
 Call before you dig.

CLIENT: ADELL CENTER  
**P.R.O. PLAN (SOUTH)**  
 43700 EXPO CENTER DRIVE - PARCEL NO. 22-15-476-045  
 TOWNSHIP 15 N RANGE 06 E  
 SECTION 15  
 CITY OF NOVI, MICHIGAN

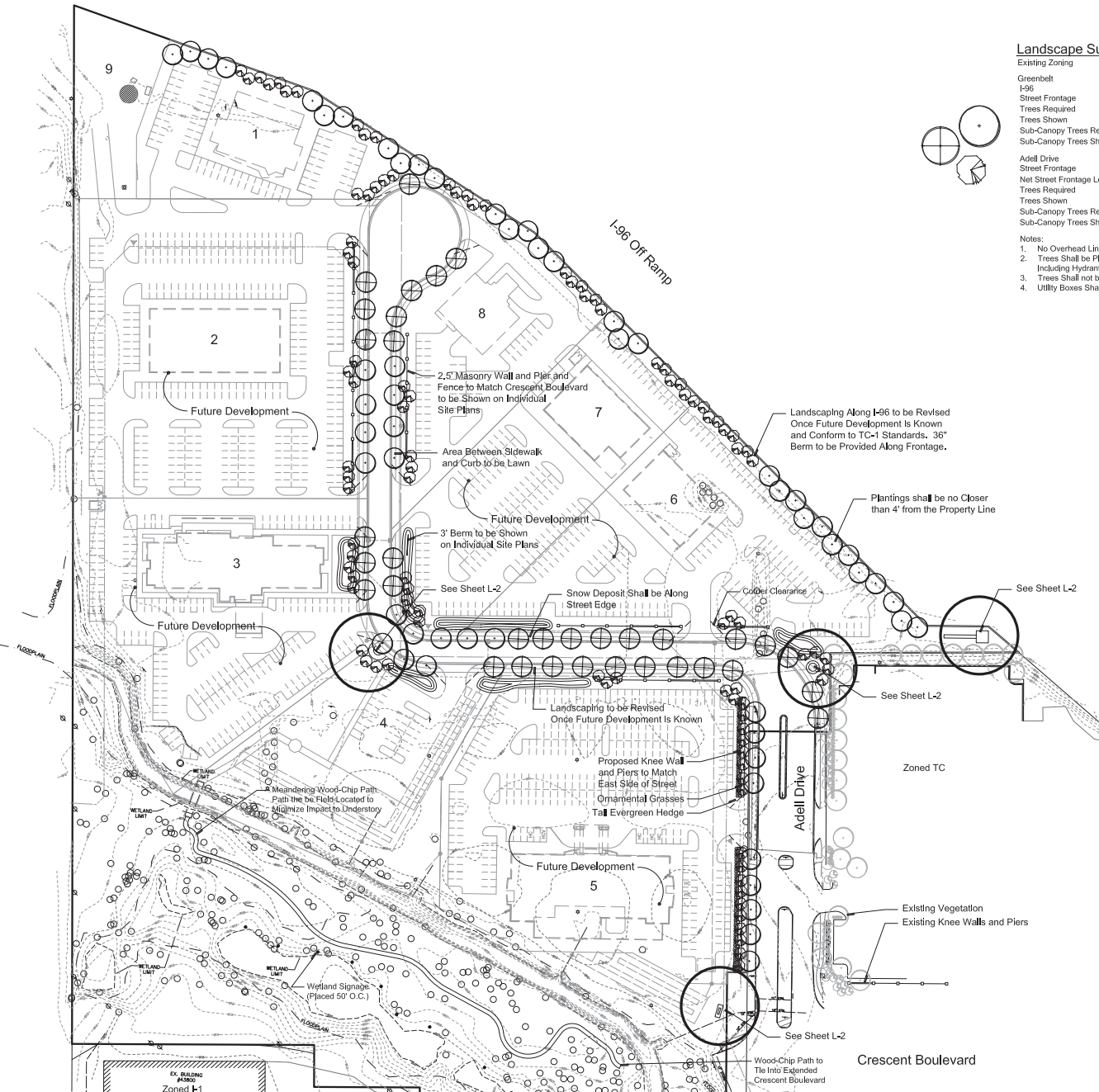
REVISED  
 2018-6-4 PER REVIEW

DATE: 4-30-2018  
 DRAWN BY: RMS  
 CHECKED BY: DJL/JMF

SCALE: HORIZ. 1"=50 FT.  
 VERT. 1"=10 FT.

4

17-334



**Landscape Summary**

Existing Zoning	PRO
Greenbelt I-96	
Street Frontage	1,283 Lf.
Trees Required	32 Trees (1,283 Lf. / 40')
Trees Shown	32 Trees
Sub-Canopy Trees Required	37 Trees (1,283 Lf. / 35')
Sub-Canopy Trees Shown	37 Trees
Adell Drive Street Frontage	2,502 Lf.
Net Street Frontage Less 335' Drives	2,167 Lf.
Trees Required	54 Trees (2,167 Lf. / 40')
Trees Shown	54 Trees
Sub-Canopy Trees Required	62 Trees (2,167 Lf. / 35')
Sub-Canopy Trees Shown	62 Trees

- Notes:
1. No Overhead Lines Exist.
  2. Trees Shall be Planted no Closer than 10' Utility Structure Including Hydrants.
  3. Trees Shall not be Planted within 4' of Property Lines.
  4. Utility Boxes Shall be Screen per Detail on Sheet L-3.



Seal:



Title:  
**Landscape Plan**

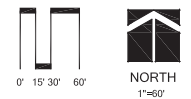
Project:  
**Adell Center  
 Novi, Michigan**

Prepared for:  
 Orville Properties, LLC  
 20733 West Ten Mile  
 Southfield, Michigan 48075

Revision:	Issued:
Submission	April 16, 2018
Revised	April 30, 2018
Revised	May 30, 2018
Revised	June 5, 2018

Job Number:  
 17-058

Drawn By: jca      Checked By: jca



Sheet No.





PLANNING MEMO UPDATE



# MEMORANDUM

**TO:** PLANNING COMMISSION  
**FROM:** SRI RAVALI KOMARAGIRI, PLANNER  
**THRU:** BARBARA MCBETH, AICP, CITY PLANNER  
**SUBJECT:** FOLLOW-UP COMMENTS: PROPOSED ADELL DRIVE DESIGN STANDARDS  
**DATE:** JULY 5, 2018

This memo is provided to clarify Engineering staff and the Traffic Engineering consultant's comments with regard to the proposed Adell Drive private road width, and related access easement for that road. Staff has revisited the Ordinance requirements for private roads within non-residential site condominiums since we last provided our comments in the review letter dated June 29, 2018, and in response to the applicant's comments.

### SUMMARY OF PROPOSED ROAD IMPROVEMENTS

The table below provides a summary of what is proposed and what is required with some additional notes, explained in detail later in this memo.

Item	Required/Staff Recommended	Applicant Proposed	Comments based on Applicants response letter dated July 03, 2018
<b>Roadway Width</b> <i>(Sec. 5.10 of Zoning Ordinance &amp; Sec. 11-198 of City Code)</i>	36 feet	30 feet	The applicant has agreed to revise the road width, but staff has to review a revised plan to identify additional or reduced deviations that may be required due to this change. Refer to notes in the memo.
<b>Access Easement</b> <i>(R.O.W is not applicable as the applicant is proposed Private Roads.</i>	70 feet	50 feet (includes sidewalk)	The applicant has not referred to any revisions to the proposed access easement width in the response letter. See notes in the memo.
<b>Sidewalk Width</b> <i>(Sec. 11-256 of City Code)</i>	6 feet	5 feet	The applicant has agreed to revise the width to meet ordinance standards.
<b>Distance between Sidewalks and back of the curb</b>	15 feet	5 feet	The applicant has not proposed to either revise the plan or request a deviation for this item at this time.

(Section 7.4.2.C of the Engineering Design Manual)			The impacts of widening the road to 36 feet wide are undetermined at this time.
<b>Setback measured from which point</b> (Sec. 6.3.2.B)	Where streets and roadways are private, front yard setbacks shall be measured as if such right-of-way lines existed; the width of such hypothetical right-of-way shall be based upon the function of such street, in this case that of a non-residential collector	Edge of access 50 feet easement	The applicant is now requesting to measure setbacks from the back of the curb. <b>Staff is unable to provide comments as we are not clear of placement of sidewalk location if the road width is revised to 36 feet.</b>

**APPLICABLE CODE FOR ROADWAY WIDTH**

**Section 11-192 and Section 11-198, Design and Construction Standards**

Staff has identified the proposed Adell Drive meets the definition of ‘Non-Residential Collector’ as listed in Section 11-192 of City Code of Ordinances, as the road collects vehicles from commercial areas(all 9 units with mixed uses) and distributes those vehicles to the arterial system (Crescent Boulevard).

As noted earlier, based on the proposed uses and the expected traffic generation, staff has identified that the proposed drive meets the definition of Non-Residential Collector road. As such, applicable standards per Section 11-198 from Design and Constructions Manual would apply. Refer to Table VIII-A Street and Roadway Rights-Of-Way (R.O.W.) Width, Pavement Width and Pavement Thicknesses from 11-198 attached to this memo.

**Zoning Ordinance Section 5.10**

Alternatively, the proposed Adell Drive can also referred to as a ‘Major Drive’ as per the Section 5.10, as it includes a principal cul-de-sac drive that has direct access to an existing Public Road (Expo Center Drive). The proposed drive is subject to Section 5.10 as it is a non-residential project that contains more than one principal building and at least one of the principal buildings is located 900 feet or more from an adjacent public road. Refer to Section 5.10.2. as attached. It should be noted both terms (non-residential collector and major drive) co-exist and one term does not supersede the other.

Per Section 5.10, a private ‘major drive’ despite that designation shall be constructed to ‘local street’ standards of 28 feet wide measured from the back of curb to the back of curb. It should be noted that 28 feet wide is the *minimum* required width for a major drive. However, based on a recommendation from City Engineer or the City’s Traffic Consultant, a wider cross section -- such as one that would be required for a ‘non-residential collector’ -- may be required. Refer to Section 5.10 attached.

**ACCESS EASEMENT AND SITE CONDOMINIUM**

Based on a review of existing road networks as classified on the City of Novi 2016 Master Plan for Land Use, within non-residential developments it appears that most of the existing

industrial/office developments were developed as a site condominium or a platted subdivision with public roads, and were built according to Section 11-198 of the Design and Construction Standards. Most of the roads are classified on the Master Plan as Non-Residential Collectors built to those standards, with a few exceptions for local roads. It appears that most of those local roads (such as Gen-Mar, Vincenti, and Regency etc.) within those developments are constructed to a width from 30 feet to 36 feet, and the ROW width ranges from 60 feet to 70 feet depending on intensity of uses within each development. In this instance, the applicant is proposing a non-residential site condominium with a private road network.

Access easements are typically required to separate public land from private property, and provide a means for the public to travel to and access private property. Access easements also allow for possible future roadway expansion, placement of sidewalks and street trees, and for placement and maintenance of public utilities. Private roads often provide a similar function, but if the private street is not built to public road standards, it would not be able to be accepted as a public road in the future.

### **Zoning Ordinance Section 6.3**

As provided in Novi's Site Condominium standards in Section 6.3.2.B of the Zoning Ordinance, where streets and roadways are private, front yard setbacks shall be measured *as if such right-of-way lines existed*. The ordinance further states that the width of such hypothetical right-of-way shall be based upon the function of such street -- in this case that of a non-residential collector. The applicant is, by contrast, proposing to measure setbacks from the edge of the proposed sidewalk, which does not match the hypothetical right-of-way defined above. This would require an ordinance deviation to allow.

The PRO Concept plan dated June 06, 2018 does not entirely conform to building and parking setbacks as noted in our Planning Review Chart. It should be also noted that the setbacks were proposed to be measured from the edge of proposed 50 feet access easement, which is not the recommended width for access easement for a non-residential collector (70 feet was recommended at that time). All setbacks would be non-compliant if they were measured from the recommended access easement line. Refer to Section 6.3 attached.

### **APPLICANTS RESPONSE AND STAFF'S CONCERNS**

The applicant has recently proposed to increase the roadway width to 36 feet (staff's initial recommendation). While this would address most of the Traffic and Engineering concerns with regard to the *pavement width*, Planning is concerned that it seems likely at this point that changing that width might also impact some *other* aspects of the PRO Concept Plan that are not an issue at the moment. The applicant has stated that the current unit boundaries have been mutually agreed upon with purchasers and we understand from conversations that the applicant is reluctant to make major layout changes. We are not clear how the widening of the road width would affect things like the setbacks and greenbelt width, etc. **Thus, the impacts on other aspects of the plan of widening pavement for the road to 36 feet wide are undetermined at this time.**

## RECOMMENDED NEXT STEP

Based on the items discussed in this memo, staff has determined that a range can be established between these two standards (Sec. 5.10 and Sec. 11-198) to provide the applicant with some flexibility for design standards. **Staff recommends (1) that a hypothetical 60 foot wide access easement as described in the ordinance be shown on a revised PRO Concept Plan to assist in determining whether additional or fewer deviations should be requested, and (2) that the road and the hypothetical easement area be provided as part of the PRO in a manner consistent with the ordinance.**

**In order to confirm the appropriateness of a pavement width of 30 feet as currently indicated on the PRO Concept Plan, the applicant should provide additional supplemental information requested in e-mail dated July 3, 2018.**

1. Provide for each site, individually:
  - a. Loading zone locations
  - b. Size of largest vehicle to access the site/loading zone area
  - c. Truck turning paths into/out of the site to/from the loading zone
2. The applicant should provide truck turning paths along the entirety of Adell Drive including into/out of the site driveways to confirm whether or not truck operations can be accommodated without crossing the centerline along Adell Drive.
3. The potential for land use changes throughout the site such as the determination for the final land use of Unit 4 may need to be reviewed so that left turn lane warrants can be completed. Certain land uses may be restricted if they generate too high of volumes without the inclusion of a left-turn passing lane.
  - a. Currently, none of the sites generate enough left-turning traffic to warrant a left-turn lane.
  - b. The applicant may be asked to make a determination as to whether they want to move forward with a two-lane roadway and possibly restrict the potential for some future land uses, or if they would like to accommodate the left turn lane at the time of original construction and have more flexibility with future land uses.

**If the applicant decides to instead show a pavement width of 36 feet, the applicant should provide a revised Concept Plan so that the staff can review and identify conformance to ordinance standards.**

## Attachments

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1. PRO Concept Plan dated June 04, 2018
2. **Section 5.10** Additional Road Design, Building Setback, and Parking Setback Requirements for One-Family Clustering Option, Two-Family Uses, Multiple-Family Uses, and Certain Non-Residential Uses and Developments
3. **Section 6.3** : Site Condominiums
4. **Sec. 11-192.** - Definitions.
5. **Sec. 11-198.** - Right-of-way performance guarantee. **TABLE VIII-A** Street and Roadway Rights-Of-Way (R.O.W.) Width, Pavement Width and Pavement Thicknesses

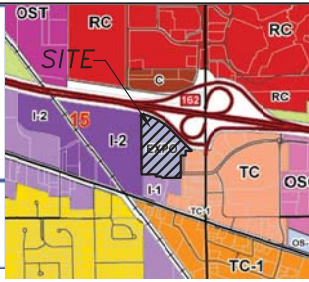
**PRO Concept Plan ( June 04, 2018)**

**Zoning Districts**

- R-1 Residential Single-Family
- R-2 One-Family Residential
- R-3 One-Family Residential
- R-4 One-Family Residential
- RT Two-Family Residential
- RM-1 Low-Density Multiple-Family
- RM-2 High-Density Multiple-Family
- MH Mobile Home
- B-1 Local Business
- B-2 Community Business
- B-3 General Business
- C Conference
- EXPO-1 Expo
- OE Gateway East
- FS Freeway Service
- I-1 Light Industrial
- I-2 General Industrial
- NCC Non-Center Commercial
- OS-1 Office Service
- OS-2 Office Service Commercial
- OS-3 Office Service Technology
- EXO EXO District with EXO Overlay
- OS-4 Office Service
- OS-5 Office Service
- OS-6 Office Service
- OS-7 Office Service
- OS-8 Office Service
- OS-9 Office Service
- OS-10 Office Service
- OS-11 Office Service
- OS-12 Office Service
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- OS-100 Office Service

**Zoning Overlays**

- OS-1 Office Service
- OS-2 Office Service Commercial
- OS-3 Office Service Technology
- EXO EXO District with EXO Overlay
- OS-4 Office Service
- OS-5 Office Service
- OS-6 Office Service
- OS-7 Office Service
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- OS-9 Office Service
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- OS-100 Office Service



**LEGAL DESCRIPTION:**  
FILE NO: 717659

THE LAND REFERRED TO IN THIS COMMITMENT, SITUATED IN THE COUNTY OF OAKLAND, CITY OF NOVI, STATE OF MICHIGAN, IS DESCRIBED AS FOLLOWS:  
PART OF THE NORTHEAST 1/4 OF SOUTHEAST 1/4 OF SECTION 15, TOWN 1 NORTH, RANGE 8 EAST, CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, BEGINNING AT POINT DISTANT SOUTH 00 DEGREES 00 MINUTES 42 SECONDS EAST 1179.04 FEET FROM EAST 1/4 CORNER; THENCE NORTH 87 DEGREES 44 MINUTES 26 SECONDS WEST 220.04 FEET; THENCE NORTH 00 DEGREES 00 MINUTES 09 SECONDS WEST 131.04 FEET; THENCE SOUTH 89 DEGREES 55 MINUTES 51 SECONDS WEST 224.83 FEET; THENCE NORTH 31 DEGREES 20 MINUTES 20 SECONDS WEST 499.91 FEET; THENCE NORTH 48 DEGREES 20 MINUTES 51 SECONDS WEST 186.65 FEET; THENCE NORTH 81 DEGREES 21 MINUTES 39 SECONDS WEST; 524.04 FEET; THENCE SOUTH 00 DEGREES 16 MINUTES 56 SECONDS EAST; 85.91 FEET; THENCE NORTH 89 DEGREES 29 MINUTES 36 SECONDS EAST 337.39 FEET; THENCE NORTH 00 DEGREES 10 MINUTES 10 SECONDS WEST 100 FEET; THENCE NORTH 89 DEGREES 35 MINUTES 39 SECONDS EAST 407.41 FEET; THENCE NORTH 00 DEGREES 00 MINUTES 42 SECONDS WEST 37.67 FEET TO BEGINNING, EXCEPT THAT PART LYING NORTHERLY OF A LINE DESCRIBED AS BEGINNING AT POINT DISTANT NORTH 02 DEGREES 07 MINUTES 02 SECONDS WEST 1321.42 FEET TO BEGINNING; THENCE SOUTH 87 DEGREES 15 MINUTES 42 SECONDS WEST 60.00 FEET FROM SOUTHEAST SECTION CORNER; THENCE NORTH 51 DEGREES 28 MINUTES 13 SECONDS WEST 200.14 FEET; THENCE SOUTH 89 DEGREES 10 MINUTES 42 SECONDS WEST 76.45 FEET; THENCE NORTH 47 DEGREES 20 MINUTES 28 SECONDS WEST 668.09 FEET; THENCE NORTH 09 DEGREES 47 MINUTES 08 SECONDS WEST 326.71 FEET; THENCE NORTH 72 DEGREES 46 MINUTES 18 SECONDS WEST, 313.38 FEET TO POINT OF ENDING.

ALSO PART OF LOT 11 OF "SUPERVISOR'S PLAN NO. 4" BEGINNING AT NORTHWEST LOT CORNER; THENCE NORTH 89 DEGREES 48 MINUTES 00 SECONDS EAST 837.48 FEET; THENCE SOUTH 00 DEGREES 02 MINUTES 00 SECONDS WEST 448.20 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 42 SECONDS WEST 250.00 FEET; THENCE SOUTH 89 DEGREES 48 MINUTES 00 SECONDS WEST 404.19 FEET; THENCE NORTH 00 DEGREES 14 MINUTES 00 SECONDS WEST 30.00 FEET; THENCE SOUTH 89 DEGREES 48 MINUTES 00 SECONDS WEST 300.00 FEET; THENCE NORTH 00 DEGREES 14 MINUTES 00 SECONDS WEST 390.20 FEET TO BEGINNING.

EXCEPT THAT PART TAKEN FOR EXPO CENTER DRIVE DESCRIBED AS: BEGINNING NORTH 89 DEGREES 48 MINUTES 00 SECONDS EAST ALONG THE NORTH LINE OF LOT 11 837.48 FEET AND SOUTH 00 DEGREES 02 MINUTES 00 SECONDS WEST 298.85 FEET FROM THE NORTHWEST CORNER OF LOT 11 OF "SUPERVISOR'S PLAN NO. 4" AS RECORDED IN LIBER 54 A OF PLATS, PAGE 83, OAKLAND COUNTY RECORDS, THENCE CONTINUING SOUTH 00 DEGREES 02 MINUTES 00 SECONDS WEST 149.35 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 42 SECONDS WEST 25.00 FEET; THENCE SOUTH 89 DEGREES 48 MINUTES 00 SECONDS WEST 25.00 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 40 SECONDS EAST 25.14 FEET; THENCE NORTH 00 DEGREES 02 MINUTES 00 SECONDS EAST 149.31 FEET; THENCE SOUTH 89 DEGREES 58 MINUTES 00 SECONDS EAST 25.00 FEET TO THE POINT OF BEGINNING.

EASEMENT PARCELS TOGETHER WITH RIGHTS OF INGRESS AND EGRESS AS CREATED, LIMITED, DEFINED IN INSTRUMENT RECORDED IN LIBER 7245, PAGE 96, AND TOGETHER WITH NON-EXCLUSIVE FOR ROADWAY SIDEWALK AS CREATED, LIMITED AND DEFINED IN INSTRUMENT RECORDED IN LIBER 8286, PAGE 36.

**UNIT WIDTH TO DEPTH RATIOS**

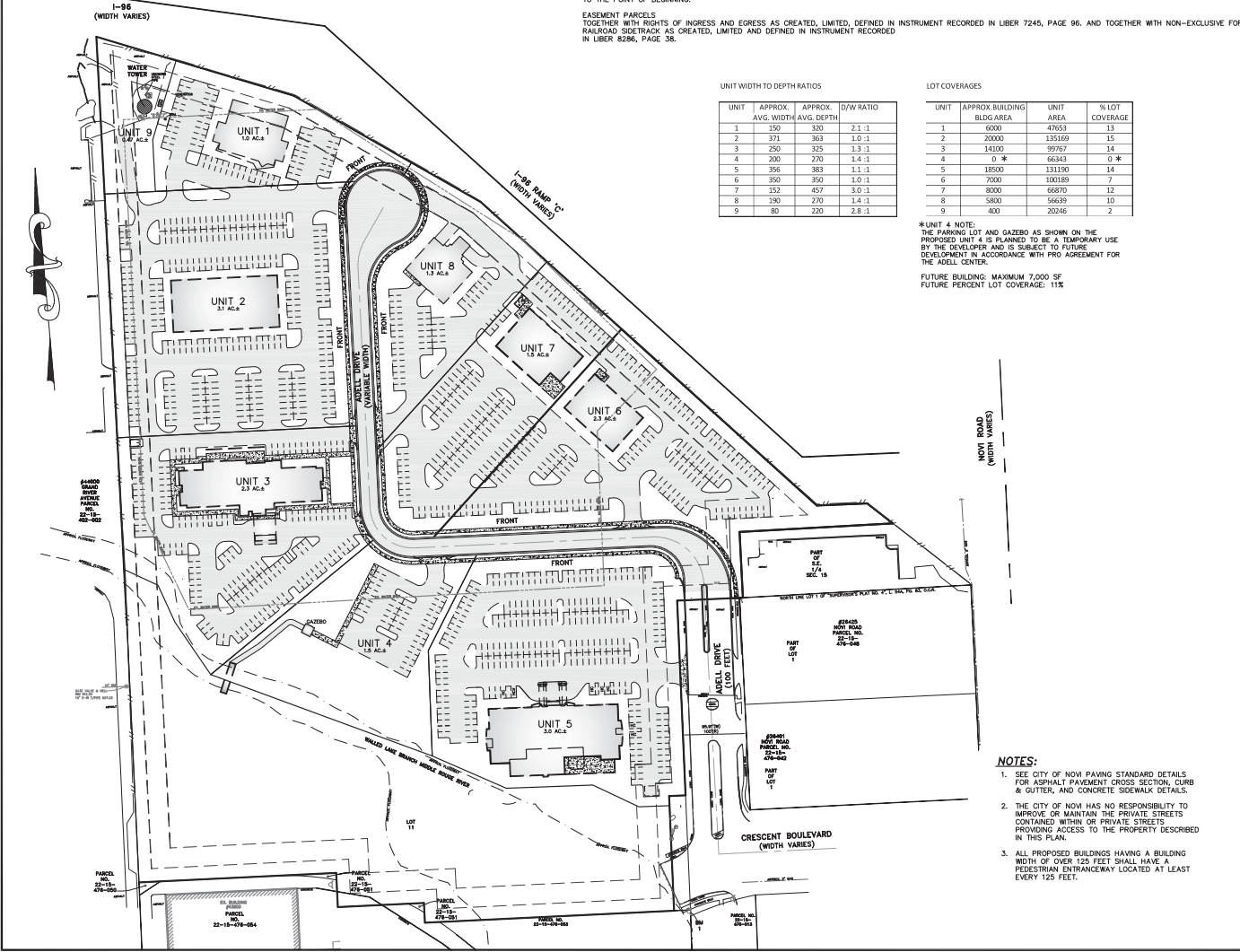
UNIT	APPROX. AVG. WIDTH	APPROX. AVG. DEPTH	D/W RATIO
1	350	320	2:1.1
2	371	363	1:0.1
3	250	325	1:3.1
4	200	200	1:1.1
5	356	383	1:1.1
6	350	350	1:0.1
7	152	457	3:0.1
8	190	270	1:4.1
9	80	220	2:8.1

**LOT COVERAGE**

UNIT	APPROX. BLDG AREA	AREA	% LOT COVERAGE
1	6000	47653	13
2	20000	135169	15
3	14100	99767	14
4	0	6643	0
5	18500	131190	14
6	7000	100189	7
7	8000	66870	12
8	3000	56639	10
9	400	20246	2

\*UNIT 4 NOTE:  
THE PARKING LOT AND GAZERO AS SHOWN ON THE PROPOSED UNIT 4 IS PLANNED TO BE A TEMPORARY USE BY THE DEVELOPER AND IS SUBJECT TO FUTURE DEVELOPMENT IN ACCORDANCE WITH PRO AGREEMENT FOR THE ADELL CENTER.

FUTURE BUILDING MAXIMUM 7,000 SF  
FUTURE PERCENT LOT COVERAGE: 11%



**REQUESTED DEVIATIONS (EXCERPTS FROM PRO AGREEMENT):**

- PLANNING DEVIATION FOR MAXIMUM BUILDING HEIGHT OF STRUCTURES NOT TO EXCEED THE GREATER OF 70' TORES OR 80% OF THE HEIGHT IN HEIGHT. THIS DEVIATION REFERS TO SECTION 3.272.A.II OF THE CITY OF NOV CODE OF ORDINANCES. THIS DEVIATION REQUEST WOULD ALLOW FOR THE BUILDING HEIGHT OF THE PROPOSED DRURY HOTEL TO EXCEED 70' IN HEIGHT.
- PLANNING DEVIATION FOR THE REQUIREMENT FOR FRONTAGE UPON A PUBLIC STREET TO ALLOW FOR THE CREATION OF A SEPARATE CONDOMINIUM UNIT FOR THE EXISTING ON-SITE WATER TOWER.
- THIS DEVIATION REFERS TO SECTION 6.1.2 AND SECTION 6.3.2.A OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION FOR THE REQUIREMENT THAT THE DEVELOPMENT HAS ACCESS TO A MAJOR THOROUGHFARE.
- THIS DEVIATION REFERS TO SECTION 5.1.3 AND SECTION 5.1.3.1 OF THE CITY OF NOV CODE OF ORDINANCES.
- CITY COUNCIL DEVIATION FROM THE ENGINEERING DESIGN STANDARDS REQUIREMENT OF A 60' WIDE ACCESS EASEMENT.
- PLANNING DEVIATION FOR THE REQUIREMENT FOR MINIMUM LOT AREA AND WIDTH TO ALLOW FOR THE CREATION OF A SEPARATE CONDOMINIUM UNIT FOR THE EXISTING ON-SITE WATER TOWER. THIS DEVIATION REFERS TO SECTION 6.3.2.2 OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION FOR THE REQUIREMENT FOR WETLAND/WATER COURSE SETBACK TO ALLOW FOR TEMPORARY WETLAND/WATER COURSE SETBACK IMPACT AS REQUIRED TO REMAIN THE EXISTING PARKING LOT AND STORM SEWER INFRASTRUCTURE AS WELL AS GRADING AND CONSTRUCTION OF THE NEW SANITARY SEWER LINE, WATER MAIN LINE AND TEMPORARY GRADING FOR THE NEW BUILDING. THIS DEVIATION REFERS TO SECTION 3.4.4 AND 5. THIS DEVIATION REFERS TO SECTION 3.6.2.M OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION FOR MODIFICATION OF PARKING SETBACK REQUIREMENTS TO ALLOW FOR A 20 FOOT MINIMUM FRONT YARD SETBACK FROM PROPOSED SIDEWALK EASEMENT AND 0' MINIMUM SIDE YARD SETBACK AREA SETBACKS WITHIN THE ADELL CENTER DEVELOPMENT. THIS DEVIATION REFERS TO SECTION 3.6.2.O OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION TO ALLOW FOR THE RECORDED PROPOSED SITE CONDOMINIUM UNITS 3, 4 AND 5 LYING PARTIALLY WITHIN THE FLOOD PLAN OF THE WALLEE LAKE BRANCH OF THE WOODLIFE RIVER. THIS DEVIATION REFERS TO SECTION 4.03.A OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION FOR PARKING, LOADING, SIGNS, LANDSCAPING, ETC. TO ELIMINATE REQUIREMENT FOR LOADING AREAS FOR THE FOLLOWING UNITS:
  - I. UNIT 1 - IRLY
  - II. UNIT 3 - FAIRFIELD INN
  - III. UNIT 5 - DRURY HOTEL
- PLANNING DEVIATION TO ALLOW FOR MAXIMUM ALLOWABLE WALL MATERIAL PERCENTAGES PER BUILDING FACADE ACCORDING TO THE FOLLOWING UNITS: SEE FACADE PLANS AS LISTED BELOW.
  - I. UNIT 1 - IRLY (SHEETS 1 THROUGH 12)
  - II. UNIT 3 - FAIRFIELD (SHEETS A-101 THROUGH A-202)
  - III. UNIT 5 - DRURY (SHEETS A11 THROUGH A14)
- UNIT 8 - CARAVANA (SHEETS 50-2 & 50-3)
- THIS DEVIATION REFERS TO SECTION 5.15 OF THE CITY OF NOV CODE OF ORDINANCES.
- CITY COUNCIL DEVIATION FROM THE EXISTING SIGN ORDINANCE 17-168 TO ALLOW FOR BUILDING SIGNAGE FOR THE FOLLOWING PROPOSED UNITS:
  - I. UNIT 1 - IRLY (SHEETS 1 THROUGH 12)
  - II. UNIT 5 - DRURY HOTEL (SHEETS 5 THROUGH 8)
  - III. UNIT 8 - CARAVANA (SHEETS 50-2 & 50-3)
- THIS DEVIATION REFERS TO SECTION 3.271.1 OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION FOR MINIMUM NUMBER OF PARKING SPACES PER UNIT AND AS RECOMMENDED BY A SHARED PARKING STUDY AS PREPARED BY THE APPLICANT'S TRAFFIC CONSULTANT AND AGREED UPON BY THE CITY'S TRAFFIC ENGINEER. THIS DEVIATION REFERS TO SECTION 5.2 OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION TO ALLOW FOR A MINIMUM OF FIFTEEN (15) PERCENT OF THE GROSS OVERALL SITE AREA FOR EACH SITE WITHIN THE DEVELOPMENT TO BE INCLUDED AS PART OF THE NON-DEVELOPED AREA IN THE SOUTHERLY PORTION OF THE OVERALL DEVELOPMENT. THIS DEVIATION REFERS TO SECTION 3.271.1.F OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION TO ALLOW FOR FUTURE RENOVATIONS, ALTERATIONS, OR ADDITIONS AS MADE TO THE BUILDINGS WITHIN THIS DEVELOPMENT, THE EXTERIOR BUILDING FACADE OF THE ENTIRE BUILDING SHALL BE BROUGHT INTO COMPLIANCE WITH THE APPROVED PRO AGREEMENT FOR THIS DEVELOPMENT. THIS DEVIATION REFERS TO SECTION 3.271.1.G OF THE CITY OF NOV CODE OF ORDINANCES.
- PLANNING DEVIATION FROM THE REQUIREMENT THAT BUILDINGS BE SET BACK NO LESS THAN 80 FEET, AND NO MORE THAN 137 FEET, AS MEASURED FROM THE CENTERLINE OF THE ARTERIAL. THIS DEVIATION REFERS TO SECTION 3.271.1.H OF THE CITY OF NOV CODE OF ORDINANCES.
- CITY COUNCIL VARIANCE TO ELIMINATE THE REQUIREMENT FOR A TRAFFIC IMPACT STUDY DUE TO THE CITY IS CURRENTLY UNDERTAKING A TRAFFIC STUDY FOR THIS AREA.

IN ADDITION TO THE GENERAL SITE DEVIATIONS REQUESTED ABOVE, THE FOLLOWING ADDITIONAL SITE SPECIFIC DEVIATIONS ARE REQUESTED:

- UNIT 3 - FAIRFIELD INN - PLANNING DEVIATION FROM THE REQUIREMENT FOR NO PARKING STALLS TO BE LOCATED ADJACENT TO A PARKING LOT ENTRANCE FROM A STREET (PUBLIC OR PRIVATE) SHALL BE LOCATED CLOSER THAN TWO (2) FEET FROM THE STREET RIGHT-OF-WAY (ROW) LINE, STREET EASEMENT OR SIDEWALK, WHOEVER IS CLOSER. THIS DEVIATION REQUEST IS TO ALLOW A PARKING STALL TO BE LOCATED A MINIMUM OF 11.9 FEET FROM THE ROADWAY EASEMENT. THIS DEVIATION REFERS TO SECTION 5.3.13 OF THE CITY OF NOV CODE OF ORDINANCES.
- UNIT 3 - FAIRFIELD INN - FRONT PARKING SETBACK FROM THE ROADWAY EASEMENT TO ALLOW FOR A 11.9' SETBACK FROM THE ROADWAY EASEMENT.
- UNIT 3 - FAIRFIELD INN - FRONT BUILDING SETBACK FOR A PROPOSED 53.5 FOOT SETBACK. THIS BUILDING POSITION OFFERS MAXIMUM USE OF THE PROPERTY FOR VEHICULAR PARKING AND CIRCULATION. THE LARGE AREA BETWEEN THE BUILDING LOCATION AND PROPERTY LINE HAS BEEN MAINTAINED THAT WILL BE UTILIZED AS EXPANSIVE LANDSCAPING AND PEDESTRIAN USE.

**3.271.C SETBACK WAIVER FOR DISTANCE FROM I-96 CENTERLINE**

AS PART OF THIS PRO APPROVAL, WE REQUEST A WAIVER FROM THE ABOVE REQUIREMENT THAT BUILDINGS BE SET BACK NO LESS THAN 80 FEET, AND NO GREATER THAN 137 FEET, AS MEASURED FROM THE CENTERLINE OF THE ARTERIAL, SINCE THIS SITE IS LOCATED ADJACENT TO THE I-96/NOV ROAD RAMP. THIS STANDARD IS NOT APPLICABLE TO THIS PROJECT SINCE THIS SITE LACKS SETBACK FRONTAGE ON I-96.

**BUILDING SETBACKS**

PERMETER	50 FT
ADELL DRIVE	30 FT
INTERIOR SIDE	0 FT
INTERIOR REAR	15 FT

**PARKING SETBACKS**

PERMETER	20 FT
ADELL DRIVE	20 FT
INTERIOR SIDE	0 FT
INTERIOR REAR	0 FT

**PERMITTED USES:**

- ANY USE THAT IS ALLOWED IN THE EXPO DISTRICT AS A PRINCIPAL PERMITTED OR SPECIAL LAND USE.
- ANY USE THAT IS ALLOWED IN THE TC, TC-1 TO-1 AS A PRINCIPAL PERMITTED OR SPECIAL LAND USE.
- ANY USE THAT IS ALLOWED IN THE B-1, B-2 AND B-3 DISTRICT AS A PRINCIPAL PERMITTED OR SPECIAL LAND USE.
- WHERES SIMILAR TO A BREW PUB

**NOTE:**  
THE BUILDING AND PARKING LOT LAYOUTS ON EACH OF THE PROPOSED UNITS ARE CONCEPTUAL ONLY. UPON APPROVAL OF THE REZONING AND SITE PLAN FOR THE ADELL DRIVE EASEMENT, THE PURCHASERS OF EACH UNIT WITHIN THIS PROPOSED DEVELOPMENT WILL BE RESPONSIBLE FOR SUBMITTING SITE PLANS TO THE CITY OF NOV FOR REVIEW AND APPROVAL.

**UNIT 4 NOTE:**  
THE PARKING LOT AND GAZERO AS SHOWN ON THE PROPOSED UNIT 4 IS PLANNED TO BE A TEMPORARY USE BY THE DEVELOPER AND IS SUBJECT TO FUTURE DEVELOPMENT IN ACCORDANCE WITH PRO AGREEMENT FOR THE ADELL CENTER.

- NOTES:**
- SEE CITY OF NOV PAVING STANDARD DETAILS FOR ASPHALT PAVEMENT CROSS SECTION, CURB & GUTTER, AND CONCRETE SIDEWALK DETAILS.
  - THE CITY OF NOV HAS NO RESPONSIBILITY TO IMPROVE OR MAINTAIN THE PRIVATE STREETS CONTAINED WITHIN OR PRIVATE STREETS PROVIDING ACCESS TO THE PROPERTY DESCRIBED IN THIS PLAN.
  - ALL PROPOSED BUILDINGS HAVING A BUILDING WIDTH OF OVER 125 FEET SHALL HAVE A PEDESTRIAN ENTRANCEWAY LOCATED AT LEAST EVERY 125 FEET.

**GREENTECH ENGINEERS & LAND SURVEYORS**  
INCORPORATED  
25147 W. Pontiac Trail, Warren, MI 48093  
Phone: (248) 688-0700 Fax: (248) 688-0701

**811**  
Know what's below  
Call before you dig.

**P.R.O. PLAN (OVERALL)**  
43700 EXPO CENTER DRIVE - PARCEL NO. 22-15-476-045  
TOWNSHIP 15 N RANGE 8 E  
SECTION 15  
CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

**CLIENT:** ADELL CENTER

**REVISED:** 2018-6-4 PER REVIEW

**DATE:** 4-30-2018  
**DRAWN BY:** RMS  
**CHECKED BY:** DJL/JMF

**SCALE:** HOR 1"=80 FT  
VER 1"=10 FT

**FBK:** --  
**CHK:** --

**2**

**17-334**

**Section 5.10 Additional Road Design, Building Setback, and Parking  
Setback Requirements for One-Family Clustering Option, Two-Family Uses,  
Multiple-Family Uses, and Certain Non-Residential Uses and Developments**



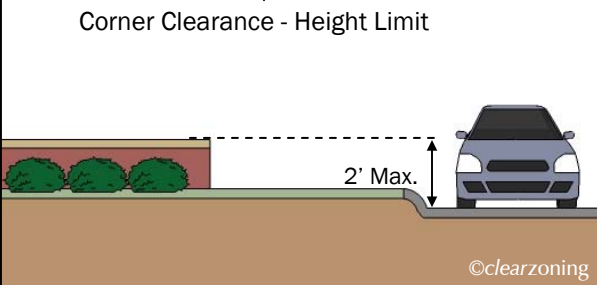
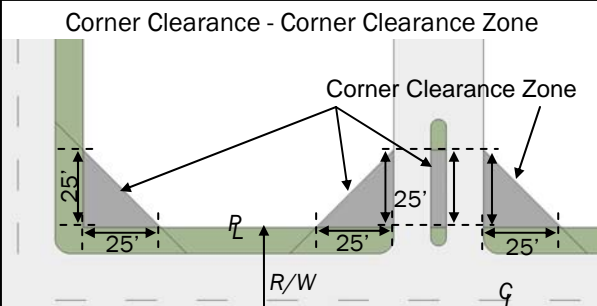
**5.8 RESIDENTIAL ENTRANCEWAY**

In all Residential districts, so called entranceway structures including but not limited to walls, columns and gates marking entrances to single-family subdivisions or multiple housing projects may be permitted and may be located in a required yard, except as provided in **Section 5.9** Corner Clearance, provided that such entranceway structures shall comply to all codes of the Municipality, and shall be approved by the Building Department [Department of Building and Safety] and a permit issued.

**5.9 CORNER CLEARANCE**

Except as otherwise permitted in this Code of Ordinances, no fence, wall, plant material, sign or other obstruction to vision above a height of two (2) feet from the established street grades shall be permitted within the clear view zone which is the triangular area formed at the intersection of any existing public street right-of-way lines by a straight line drawn between said right-of-way lines at a distance along each line of twenty-five (25) feet from their point of intersection. Medians shall be included in this measurement. Height of shrubs, perennials or ornamental grasses is the mature height for the species of plant material. Canopy and sub-canopy trees can be located at the outside edge of the triangle. No evergreen trees are allowed within or at the edge of the clear view zone. Driveways shall have a ten (10) foot clear view zone as measured from the back of curb and edge of drive. Private roads shall meet the same twenty-five (25) foot clear view zone. 🏠

**5.9 Corner Clearance**



No visual obstructions within the corner clearance zone. Obstructions to vision above a height of 2', measured from established street grade, are not allowed. Plant materials are measured at mature height.

**5.10 ADDITIONAL ROAD DESIGN, BUILDING SETBACK, AND PARKING SETBACK REQUIREMENTS FOR ONE-FAMILY CLUSTERING OPTION, TWO-FAMILY USES, MULTIPLE-FAMILY USES, AND CERTAIN NON-RESIDENTIAL USES AND DEVELOPMENTS**

1. The following apply to uses and developments (other than subdivision plats and site condominiums) constructed according to the requirements and standards for the one-family clustering option, two-family uses, multiple-family uses, and non-residential districts meeting the standards in subsection 2 below. Conventional general and site condominium residential developments located in one-family districts shall be designed to meet Ordinance standards as if they were conventional residential platted subdivisions. For general condominiums, theoretical "lot" lines shall be shown on the review plans for the purpose of determining compliance with ordinances, but shall not be shown on the final recorded condominium plans.



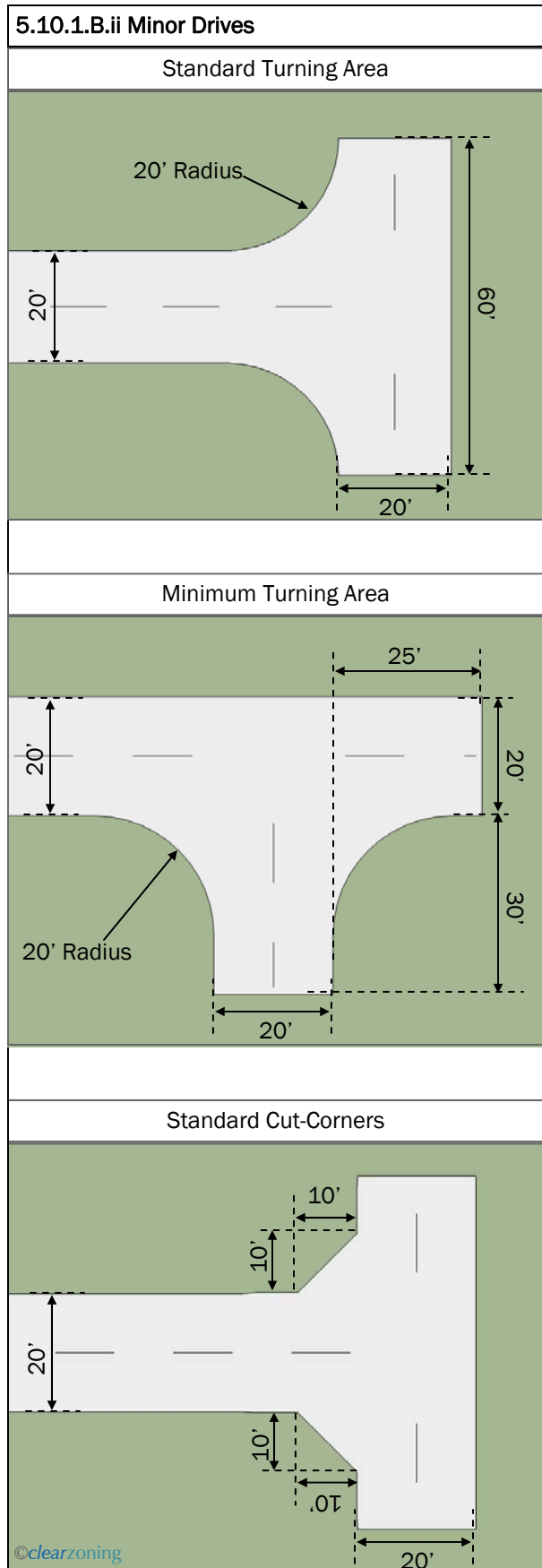
A. All uses and developments shall include a public road network or private drive network. A public road network shall be built to **City of Novi Design and Construction Standards**.

B. A private drive network within a cluster, two-family, multiple-family, or non-residential uses and developments shall be built to **City of Novi Design and Construction Standards for local street standards** (twenty-eight (28) feet back-to-back width, although for industrial, commercial, office, and high tech uses, a wider road cross-section may be required, at the discretion of the approving body, based on a recommendation from the city engineer or traffic consultant) with the following exceptions:

i. If a private drive network is proposed, there shall be a private "major drive" - i.e., a principal internal loop drive or principal cul-de-sac drive that has direct access to an exterior public road (see examples at the end of this Section);

ii. There may be intersecting "minor drives," i.e., individual private drives off a major drive or internal public road that are built according to the City of Novi's parking drive and parking space standards (Section 5.3). For example, a two-way minor drive would require a twenty-four (24) foot aisle with optional adjacent parking on one or both sides. Where on-street parking is proposed, it shall be limited to one side of the minor drive and the drive shall be a minimum width of twenty-eight (28) feet. On-street parking shall not be permitted for non-residential developments unless the minor drive is widened to maintain a minimum twenty-four (24) foot traveled way for two-way traffic. In order to qualify as a minor drive, the maximum length shall not exceed six-hundred (600) feet, measured from the near edge of the intersecting major drive or internal public road to:

- The far edge of pavement (where no turnaround is required),
- The center of the cul-de-sac or
- The far edge of the T-turnaround (at the centerline of the drive)



<b>1</b>	Purpose and Introduction
<b>2</b>	Definitions
<b>3</b>	Zoning Districts
<b>4</b>	Use Standards
<b>5</b>	Site Standards
<b>6</b>	Development Procedures
<b>7</b>	Admin and Enforcement



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All minor drives in excess of one-hundred fifty (150) feet, with only one point of access, shall provide a cul-de-sac meeting City Design and Construction Standards or a T-turnaround, meeting the standard shown above. ✍

- iii. For multiple-family and non-residential projects, parking lots with access from major drives or public streets may also be permitted. Parking lots shall conform with the City of Novi Zoning Ordinance and Design and Construction Standards. ✍
- iv. For major and minor private drives, the minimum centerline radius shall be one-hundred (100) feet, in accordance with the American Association of State Highway and Transportation Officials (AASHTO) minimum local street standards. Adjacent parking and on-street parking shall be limited near curves with less than two-hundred thirty (230) feet of centerline radius, at the discretion of the approving body, in order to provide safe and adequate sight distance. ✍
- v. Building and parking lot setbacks specified in the applicable zoning district shall be measured as follows:
  - a. When abutting a "major drive," measure setbacks from back of curb;
  - b. When abutting a property line, measure from property line;
  - c. When abutting a "minor drive," measure from back of curb;
  - d. Parking lots shall be setback a minimum of ten (10) feet from a major and minor drives and twenty (20) feet from any property line, unless a greater distance is specified for non-residential and multiple-family uses elsewhere in this Ordinance. Angled and perpendicular parking spaces may be accessed directly from a minor drive or parking lot aisle, but not from a major drive; ✍

- e. For non-residential uses, buildings and structures shall be setback a minimum of ten (10) feet from any major drive or minor drive, measured from the back of curb or pavement edge (where no curb is provided). Setbacks from property lines and residential districts shall be as set forth elsewhere in this Ordinance.
- vi. When abutting a parking space, the minimum building setback from the end of a parking stall shall be twenty-five (25) feet in all residential zoning districts and ten (10) feet in non-residential districts.

- 2. Non-residential projects shall be subject to the requirements of this Section if they meet the following criteria:
  - A. The development contains more than one principal building, and
  - B. At least one of the principal buildings is located nine-hundred (900) feet or more from an adjacent public road.

For the purpose of determining application of this subsection, the entire development parcel, including future phases, shall be considered. If the approving body determines that it is reasonable to expect that future development of the balance of the site would permit construction of a principal building nine-hundred (900) feet or more from an adjacent public roadway, then the entire site shall be developed in conformance with this Section.



## Section 6.3 Site Condominiums

- D. Indicate when and where written comments will be received concerning the request.
4. Notice of a public hearing required for the amendment of, or to supplement, this Ordinance shall be given in the same manner as provided under this act for the adoption of the original ordinance, and the planning commission shall give a notice of the proposed rezoning in the same manner as required under paragraph (3), above. Except that, for any group of adjacent properties numbering eleven (11) or more that is proposed for rezoning, the planning commission shall give a notice of the proposed rezoning in the same manner as required under paragraph (3), except that the requirements for mailing and the listing of street addresses do not apply to that group of adjacent properties.

### 6.3 SITE CONDOMINIUMS

1. Approval under this Section shall be required as a condition to the right to construct, expand or convert a site condominium. The approval process shall comply with those procedures for site plan review provided in this Ordinance. The Planning Commission shall conduct its review in accordance with the public hearing requirements set forth in [Section 6.2](#) of this Ordinance. At the time of final site plan review, the developer shall provide a copy of the proposed master deed and any additional documentation to be recorded with the Register of Deeds, for review and approval by legal counsel, with respect to all matters subject to regulation by the City, including, without limitation, ongoing preservation and maintenance of drainage, retention, woodland, wetland and other natural areas and common areas in the project.
2. The following regulations shall apply to site condominium projects:
- A. Each building site shall front on and have direct access to a public street or onto a private street that complies in all respects to the [Design and Construction Standards \(Novi Code Chapter 11\)](#) applicable to public streets and roadways. There shall also be provided concrete pedestrian safety paths (sidewalks) of five (5) feet in width along both sides of all public or

private roads within the site condominium project.

- B. There shall be compliance with all requirements of the Schedule of Regulations, and other provisions of this Ordinance with the understanding that references to "lot" in such regulations shall mean and refer to "building site" as defined in this Section, and references to "building" (meaning principal building) or "structure" (meaning principal structure) shall mean and refer to "building envelope" as defined under this Section. Where site condominium streets and roadways are dedicated to the public, front yard setbacks shall be measured from right-of-way lines. Where streets and roadways are private, front yard setbacks shall be measured as if such right-of-way lines existed; the width of such hypothetical right-of-way shall be based upon the function of such street as a major arterial, arterial, minor arterial, residential collector, nonresidential collector, residential street or nonresidential street, as those terms are used in the Master Plan for the City of Novi and the City of Novi Design and Construction Standards. In the review of a site condominium, it is recognized that it may not be feasible to precisely apply traditional definitions and measures applicable to developments. However, the review of plans submitted under this Section shall be accomplished with the objective and intent of achieving the same results as if the improvements were being proposed pursuant to the [City of Novi Subdivision Ordinance](#), excepting the procedural requirements imposed under that Ordinance. In particular, all street trees and boulevard and island plantings shall be in accordance with the requirements of said ordinance. In those instances where streets are to be dedicated as public streets, street tree planting shall be under the direction of the Director of Public Services or his designee, in the manner provided by said ordinance.
- C. Before the issuance of building permits for units, the developer shall demonstrate approval by city, county, and state entities having jurisdiction with regard to any aspect of the development, including, without limitation, roads, water supply, sewage disposal, storm drainage and other utilities. As to the phase in which the unit is

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located, before the issuance of a building permit, the Building Official shall determine that all improvements such as roads, water supply, sewage disposal, storm drainage and other utilities have been completed in accordance with approved plans and such improvements are determined to be acceptable for use. All site improvements shall comply with the City of Novi Design and Construction standards, Chapter 11, Novi Code of Ordinances. "As-built" drawings shall be submitted in accordance with **Chapter 11, Article XIII of the Novi Code of Ordinances**. Prior to issuance of a building permit within a given phase, the developer shall comply with those requirements for performance guarantees contained in **Chapter 26.5 of the Ordinance Code**, as amended. Land improvement permits shall be required for individual building sites. Notwithstanding the above, the Building Division may issue building permits for model homes numbering not more than the lesser of four (4) or ten (10) percent of the total number of sites prior to completion of all roads, water supply, sewage disposal, storm drainage and other utilities, provided that roads, water supply, sewage disposal, storm drainage and other utilities to service the site used for model homes are completed and determined to be acceptable for use. Certificates of occupancy for such homes shall be limited for model purposes until such time as all such improvements are completed and determined acceptable for use.

- D. Before issuance of temporary or final certificates of occupancy, the developer shall comply with the requirements for performance guarantees contained in Chapter 26.5 of the Ordinance Code, to the extent such guarantees have not already been posted pursuant to subsection 2.C., above.
- E. With respect to each building envelope, within ninety (90) days following final inspection of the improvement, the developer shall submit to the Building Official an "as built" survey that complies with the requirements of Sections 125 and 126 of the Land Division Act, Act 288 of 1967, being MCL 560.125—.126, as amended, including dimensions between each improvement and the boundaries of the building site, and distance of each

improvement from any wetland, floodplain, and/or floodway. Monuments shall be located in the ground in accordance with the requirements MCL 560.125, as amended.

- F. The fees for all reviews shall be established by ordinance and/or resolution adopted by the City Council.
- G. Any proposed amendment of a master deed which would involve any subject matter reviewed or reviewable under this Section shall be reviewed and approved by the Planning Commission prior to recordation.



**Sec. 11-192 Definitions**

## Sec. 11-192. - Definitions.

The following words, terms and phrases, when used in this article, shall have the meanings ascribed to them in this section, except where the context clearly indicates a different meaning:

*Alley* means any roadway affording a secondary means of access to abutting property, and not intended for general traffic circulation.

*Arterial street* means a street designed to provide traffic continuity over long distances as well as to provide access to abutting properties and minor intersecting streets. Such streets also carry traffic from collectors and minor arterials to major activity centers. (See City Thoroughfare Plan current edition).

*Arterial system* means the portion of the street system comprised of major arterial, arterial, and minor arterial streets.

*Boulevard street* means a street of two (2) one-way pavements of two (2) or more lanes each, separated by a grassed or paved island (can be collector or minor street).

*Collector street* means a street which provides both land access and traffic circulation within residential neighborhoods or commercial, industrial or office areas. Such streets collect vehicles from the local streets or commercial/industrial/office areas and distribute them to the arterial system or to a local destination. Such streets are designated as residential collectors or non-residential collectors.

*Cul-de-sac street* means a short minor street having one (1) end permanently terminated by a vehicular turnaround.

*Freeway* means a street designed to handle large volumes of traffic moving at high speeds over long distances or between urban areas.

*Local or minor street* means a street which is of limited continuity used primarily for access to the abutting residential properties.

*Major arterial street* means a street which provides for movement of large volumes of traffic over long distances. On major arterials, service to abutting land is subordinate to this major through movement. Generally, medians are provided to separate traffic flows and access is controlled. (See City Thoroughfare Plan current edition).

*Marginal access road (frontage road)* means a minor street which is parallel to a street which is part of the arterial system and which provides access to abutting properties and protection from through traffic.

*Minor arterial street* means a street which accommodates trips of moderate length at a somewhat lower level of traffic mobility than arterials. (See City Thoroughfare Plan current edition).



*Private road or drive* means undedicated roads, drives or rights-of-way which are not accepted by the city as public. In the case of industrial or commercial developments any road or drive providing access to more than one (1) parcel of land or two (2) or more buildings under separate lease agreements. In the case of all residential developments, any road or drive which provides access to more than one (1) residential unit.

*Roadway* means a private road or drive which provides emergency or other vehicular and/or pedestrian access to buildings, facilities, or developments located upon private lands.

*Street* means a right-of-way dedicated to public use or about to become dedicated for public use, which provides emergency or other vehicular and pedestrian access to adjacent properties, and may be further designated as an alley, arterial street, major arterial street, minor arterial street, minor or local street, freeway, marginal access or frontage road, industrial or commercial street, collector street, cul-de-sac street, U-street, or a boulevard street or however otherwise dedicated. Included shall be the land between the right-of-way lines, whether improved or unimproved, and may comprise pavement, curbs and gutters, shoulders, ditches, sidewalks, parking areas, lawn areas, and other areas within the right-of-way.

*U-street* means a short boulevard street permanently terminated by a half-circle turnaround.

(Ord. No. 86-124, § 9.02, 4-21-86; Ord. No. 93-124.06, Pt. LX, 2-1-93; Ord. No. 96-124.07, Pt. XXXV, 12-16-96)

**Cross reference**— Definitions and rules of construction generally, § 1-2.

Sec. 11-198. - Right-of-way performance guarantee.

For any work proposed within the right-of-way of the arterial system of the city, a performance guarantee shall be posted in accordance with the provisions of section 26.5-41 and section 31-2 herein.

(Ord. No. 93-124.06, Pt. LXXIV, 2-1-93; Ord. No. 04-124.14, § 2, 9-13-04)

TABLE VIII-A  
STREET AND ROADWAY  
RIGHTS-OF-WAY (R.O.W.) WIDTH,  
PAVEMENT WIDTH AND  
PAVEMENT THICKNESSES

The following minimum rights-of-way widths, pavement widths and pavement thicknesses shall be required:

**Section 11-198**

## Sec. 11-198. - Right-of-way performance guarantee.

For any work proposed within the right-of-way of the arterial system of the city, a performance guarantee shall be posted in accordance with the provisions of section 26.5-41 and section 31-2 herein.

(Ord. No. 93-124.06, Pt. LXXIV, 2-1-93; Ord. No. 04-124.14, § 2, 9-13-04)

TABLE VIII-A  
STREET AND ROADWAY  
RIGHTS-OF-WAY (R.O.W.) WIDTH,  
PAVEMENT WIDTH AND  
PAVEMENT THICKNESSES

The following minimum rights-of-way widths, pavement widths and pavement thicknesses shall be required:

Street or Roadway Type	R.O.W. Width (in feet)	Pavement Width (B/Curb to B/Curb in feet)	Pavement Thickness(3)	
			Concrete (in inches) (see below)	Bituminous (in inches)
	Per City Thoroughfare			
Arterial System	Master Plan	Special Design Considerations		
Residential Collector	86	36	9	11
Non-residential Collector	70(1)	36	9	11
Local Street	60	<u>28</u>	7	<u>8.5</u>
Boulevard (Collector)	100	<u>28</u> (2-lane)	9	11

Boulevard (Local)	86	24 (2-lane)	7	<u>8.5</u>
Private (Residential)	N/A	<u>28</u>	7	<u>8.5</u>
Private (Non- residential)	N/A	<u>28</u>	9	11
Marginal Access (Residential)	30(2)	20(5)	7	<u>8.5</u>
Marginal Access (Non- residential)	30(2)	20(5)	9	11
TC-1 Local or Collector Road	70(4)	45(4)	9	11

1. Industrial, commercial and office street R.O.W. widths may be reduced to sixty (60) feet with two (2) five-foot easements, subject to the review and approval of the city.

2. Marginal access road shall be either dedicated as public rights-of-way or an easement which will permit the use of the marginal access road for traffic circulation from one property to another. Such easement shall be in a form acceptable to the city. For two (2) way operation, right-of-way width (or easement) shall be forty (40) feet.

3. A minimum eight-inch 21AA full width aggregate base is required under all concrete roadways. However, alternate designs for pavement cross sections that provide equal or greater structural stability and longevity will be considered by the city engineer if adequate engineering data is furnished for analysis. Thickness shown is based on clay subsoil.

4. Width may vary at intersections where additional lanes are necessary.

5. For two-way operation, pavement width shall be twenty-eight (28) feet.

NOTE: Design requirements may be increased due to existing conditions encountered in the field as determined by the city.

BITUMINOUS PAVING - FULL DEPTH AND COMPOSITE OPTIONS

Required Thickness (inches)	Lift	Full Depth Material Thickness (in.)		Composite Material Thickness	
<u>8.5</u>	1	1100T	1.5	1100T	1.5
	2	1100L	<u>2.5</u>	1100L	<u>2.5</u>
	3	500B	2.25	21AA	8
	4	500B	2.25		
—					
	1	1500T	1.5	1500T	1.5
	2	1500L	1.5	1500L	1.5
	3	1500L	2	1500L	2
11	4	500B	3	21AA	12
	5	500B	3		

## PLANNING REVIEW



# PLAN REVIEW CENTER REPORT

June 29, 2018

## Planning Review

Adell Center PRO

JZ 18-24 with Rezoning 18.724

### PETITIONER

Orville Properties, LLC

### REVIEW TYPE

Rezoning Request from EXPO (Expo) to TC (Town Center) with a Planned Rezoning Overlay (PRO)

### PROPERTY CHARACTERISTICS

<b>Section</b>	15	
<b>Site Location</b>	Address: 43700 Expo Center Drive; Parcel Id: 50-22-15-476-045 north of Grand River Avenue and south of I-96 in Section 15	
<b>Site School</b>	Novi Community School District	
<b>Current Site</b>	Expo: Expo	
<b>Proposed Site</b>	TC: Town Center	
<b>Adjoining Zoning</b>	North	C: Conference (across I-96)
	East	TC: Town Center
	West	I-2: General Industrial
	South	I-1: Light Industrial
<b>Current Site Use</b>	Vacant; Existing unused parking lot	
<b>Adjoining Uses</b>	North	Novi Oaks Hotels
	East	Retail/Restaurants
	West	Industrial Office
	South	Industrial Office
<b>Site Size</b>	23 Acres (Net Site Acreage approximately 19.4 Acres)	
<b>Plan Date</b>	June 6, 2018	

### PROJECT SUMMARY

The applicant is proposing to develop the property as a multi-unit commercial development consisting of nine units accessed by a proposed private drive. The development proposes a mix of two hotels, one fitness center, two restaurants, one indoor recreational facility, off-street parking lot for seasonal events and an unlisted use similar to automobile sales facility. The existing water tower on site is proposed remain on a separate unit. The current PRO Concept Plan includes a request for an Unlisted Use Determination under Section 4.87 of the Zoning Ordinance.

The table below lists the prospective users for each unit based on the information provided by the applicant at the time of Pre-application meeting.

Unit No.	End Users	Proposed Height	Proposed Use Category
Unit 1	I-Fly	58 ft. (2 stories)	Indoor Commercial Recreation Facilities
Unit 2	Planet Fitness	40 ft. to 50 ft. (2 stories)	Indoor Commercial Recreation Facilities

Unit 3	Fairfield Inn & Suites	63 ft. (5 stories)	Hotels
Unit 4	Seasonal Event Space	Not provided	Off-street Parking Lot
Unit 5	Drury Inn & Suites	85 ft. (7 stories)	Hotels
Unit 6	Restaurant	20 ft. -30ft. (1 story)	End user to be determined
Unit 7	Restaurant	20 ft. -30ft. (1 story)	End user to be determined
Unit 8	Carvana	71ft. (7 stories)	Unlisted Use
Unit 9	Water Tower	120 ft. Existing tower	Existing Structure

Note, however, that the application as finally submitted no longer appears to clearly identify the same end users as are set forth in the Table above. The PRO narrative states the use categories but it no longer states the end users for each unit, and the applicant is asked to clarify that. While there are references in certain plan notes to I-fly, Drury, Fairfield, and Carvana, there is also a note on the plan indicating that “permitted uses” could be all the uses in the Expo, TC, TC-1, and B-1, B-2, and B-3 districts. That note must be removed. References to the specific users that are still identified may appear throughout this review.

The applicant is not proposing a phased construction; however, the applicant is proposing to build the roads and the utilities first. Individual users will build within the respective unit boundaries shown on the plan. The applicant submitted a narrative and a Community Impact Statement.

**PROJECT REVIEW HISTORY**

The applicant submitted for a Pre-Application Meeting, which was held on May 14, 2018. Staff indicated that the proposed zoning conflicts the future land use designation and requested additional information to make an informed recommendation to the Planning Commission and the City Council.

**PRO OPTION**

The PRO option creates a “floating district” with a conceptual plan attached to the rezoning of a parcel. As part of the PRO, the underlying zoning is proposed to be changed (in this case from EXPO to TC) and the applicant enters into a PRO agreement with the City, whereby the applicant submits a conceptual plan for development of the site. The City Council reviews the Concept Plan, and if the plan may be acceptable, it directs for preparation of an agreement between the City and the applicant, which also requires City Council approval. Following final approval of the PRO concept plan and PRO agreement, the applicant will submit for Preliminary and Final Site Plan approval under standard site plan review procedures. The PRO runs with the land, so future owners, successors, or assignees are bound by the terms of the agreement, absent modification by the City of Novi. If the development has not begun within two (2) years, the rezoning and PRO concept plan expires and the agreement becomes void.

**RECOMMENDATION**

The proposed rezoning category requested by the applicant is currently not supported by the Future Land Use Map. The applicant has requested to waive the requirement to attend Master Planning and Zoning Committee with a letter dated June 11, 2018. Staff recommends **postponing** the decision to a later date to allow the applicant additional time to address the additional information still needed as discussed below.

**COMMENTS**

It is staff’s opinion that the proposed rezoning district of TC, Town Center may be a reasonable alternative for the subject property, even though it is not supported by future land use map,



however, the application is missing information and there are too many deviations from the ordinance standards for Planning Staff to be able to support the request at this time. Some of the concerns are as follows;

1. As the applicant stated in the submitted narrative, staff agrees that it is highly unlikely that another exposition center will be built on this property since Suburban Collection showplace is well established in City of Novi. **However, as the current EXPO zoning district allows, alternative uses to an exposition facility are currently permitted, and the intent indicates the EXPO district is also designed to promote research, office and light industrial development, and help meet the needs of the City's expected future economy for all types of research, office, light industrial and related uses. In addition to the permitted Exposition facilities uses, the EXPO District also allows professional office buildings, offices and offices sales and service activities, public or private health and fitness facilities and clubs, medical offices, research and development, technical training and design of pilot or experimental products, data processing centers, warehousing, and many other uses as listed in the ordinance. As noted, many of the uses permitted in the I-1 Light Industrial District in Section 3.1.18, except greenhouses and pet boarding facilities are currently permitted as the property is zoned.**
2. The last operating building on the subject property was the Novi Exposition facility which was demolished in 2012. The site has been vacant since then. The subject property is an ideal candidate for redevelopment either under the current zoning, or another zoning district.
3. The subject property is the only undeveloped property located near the edge of the existing Town Center District boundary.
4. The City's Future Land Use map indicates Office Service Technology (OST) which allows most of the uses previously identified such as hotels and motels (when designed to be an integral part of the office development), sit-down restaurants, indoor recreational facilities and Off-street parking lots as permitted uses. One exception to this is Carvana, which requires City Council approval for unlisted use determination. The submitted development plan is not currently proposing any office related uses, therefore the hotel and restaurant uses would currently not fit within the OST District. (Staff does not agree with applicant's interpretation that the OST retail overlay services are applicable to the subject property)

**The proposed uses (except Carvana which is subject to separate City Council approval) and the rezoning category could therefore be acceptable alternative to the current zoning, or to the Town Center zoning district, but the proposed Concept Plan does not meet the intent of Town Center district Ordinance for multiple reasons and is also not conforming to multiple requirements of the Ordinance. Staff believes that the applicant has ample opportunity to modify the plan to meet the intent of TC district and note the following for applicant's consideration:**

1. **TOWN CENTER AREA STUDY:** The property's proximity to the surrounding retail, restaurants and hotels could make the proposed rezoning category appropriate; the applicant should be able to achieve greater compliance with the design guidelines from Town Center Area Study and redesign the site layout to more closely meet the intent of Town Center district. Town Center area study offers the following recommendations for northwest area which is immediately abutting the subject property.
  - a. Use Middle Rouge in site design
  - b. Pedestrian-oriented with small front/side setbacks.
  - c. Shared parking located at rear or side of building.
2. **DESIGN AND LAYOUT CONCERNS:** The current layout is more consistent with a traditional industrial park layout we typically see in Light Industrial districts. The applicant has stated that the current unit boundaries have been mutually agreed upon with purchasers and we understand from conversations that the applicant is reluctant to make major layout changes. The applicant can still consider:

- a. Providing additional amenities within the site such as benches, safety paths, decorative lighting etc.
  - b. Enhancing the site design to use the existing branch of the Middle Rouge River as an amenity or focal point.
  - c. Creating safe and attractive pedestrian connections between the units by creating breaks in the sea of parking.
  - d. Expanding and enhancing the proposed pocket parks.
  - e. Better defining the potential uses and layout for Unit 4.
  - f. Proposing shared parking among the various proposed uses; and thereby providing additional green spaces by reducing the parking spaces.
  - g. Considering revisions to site plan to minimize the number of deviations requested.
3. **PEDESTRIAN ORIENTED DEVELOPMENT:** As stated in Sec. 3.1.25.A., *'The TC, Town Center district is designed and intended to promote the development of a pedestrian accessible, commercial service district in which a variety of retail, commercial, office, civic and residential uses are permitted'*. The proposed uses (with the exception of Carvana) can be classified as commercial/entertainment uses which align with the intent of TC, Town Center district. However, the development does not promote a pedestrian accessible environment.
4. **OFF-STREET PARKING LOTS FOR SEPARATE USES:** *'The TC Town Center district is further designed and intended to discourage the development of separate offstreet parking facilities for each individual use, and to encourage the development of off-street parking facilities designed to accommodate the needs of several individual uses'*. The proposed concept plan depicts each unit as having related parking within their respective unit boundaries, with the exception of the both the restaurants. All the parking lots are mostly connected to each other with shared access drives with the exception of Unit 5 (Drury Hotel). **It is staff's opinion that this development provides an opportunity to reduce parking by proposing shared parking arrangement, supported by a shared parking study, thus leaving additional space for public gathering or usable open space or to reduce deviations. The applicant is currently seeking a deviation for reduction of usable open space.**
5. **UNLISTED USE DETERMINATION:** The intent of the Town Center District recognizes that uses such as new and used motor vehicle sales can have a disruptive effect on the intended pedestrian orientation of the districts. One of the proposed uses, Carvana, is a non-traditional model used for used vehicle sales. It does not have traditional style of larger horizontal parking lots for sale vehicles display. **It is an experimental concept. However there is no guarantee for the long-term viability of the use.** Please refer to Unlisted Use Determination memo provided under a separate packet for more comments on unlisted use determination. **Staff is currently unable to make a full determination on the nature of the use because of the lack of information. The applicant is asked to refer to the attached memo and address the staff's concerns.**
6. **ADELL DRIVE:** Adell Drive as proposed would be considered a non-residential collector road and shall be designed in accordance with the Design and Construction Standards and Section 5.10 of the Zoning Ordinance. The pavement width shall be 36 feet back of curb to back of curb within a 70 foot wide public or private right-of-way or easement. A 60-foot private/public right-of-way width with two (2) five-foot easements is also acceptable. Refer to Traffic and Engineering comments for more comments. **This is obviously a principal deviation issue that the Commission and Council will need to decide; other deviations flow directly from this issue.**
5. **DEVIATIONS:** The applicant has provided a list of deviations in the cover letter that is not complete and accurate. The applicant is asked to revise the list based on staff's comments

provided in this section. Please be specific about the deviation requested and provide a justification as how if each deviation, if not granted, would prohibit an enhancement of the development that would be in the public interest, and that approving the deviation would be consistent with the Master Plan and compatible with the surrounding areas.

6. **FUTURE SITE PLAN REVIEWS:** The proposed development is an ambitious project that would require a carefully laid out implementation plan. The applicant, who is also the current land owner, is proposing to build the roads and the utilities and divide the land into individual condominium units. Each future buyer will then be responsible for getting necessary site plan and other permit approvals, and be responsible for each unit’s construction. There is no tentative timeline indicated for completion of all units. Until all units are completed, the impacts of construction traffic to the surrounding areas/businesses are hard to contemplate. **The applicant should consider adding a tentative completion date as a condition for the PRO agreement.**

Since the development will be tied to PRO plan, when individual site plans are submitted for review, they are expected to conform to the code requirements for all items that are not are regulated by the approved deviations and conditions as part of the PRO Agreement. For these reasons, it is **vital staff to have a clear understanding of what is being proposed at this time in order to provide clarity for future reviews. The applicant should provide the intent to address possible or anticipated future deviations if they are not requested at this time.**

**COMPARISON OF ZONING DISTRICTS**

The following table provides a comparison of the current (EXPO) and proposed (TC) zoning classifications.

	<b>EXPO Zoning (Existing)</b>	<b>TC (Proposed)</b>
<b>Intent</b>	The EXPO Exposition Overlay district is designed to accommodate the development of a planned exposition facility. The EXPO district is also designed to promote research, office and light industrial development, and help meet the needs of the City’s expected future economy for all types of research, office, light industrial and related uses	The TC, Town Center district is designed and intended to promote the development of a pedestrian accessible, commercial service district in which a variety of retail, commercial, office, civic and residential uses are permitted.
<b>Principal Permitted Uses</b>	See attached copy of Section 3.1.14.B for EXPO uses Uses permitted in the I-1 Light Industrial District in Section 3.1.18, except greenhouses and pet boarding facilities. See attached copy Section 3.1.18.B and Sec. 4.77 I-1 uses in EXPO	See attached copy of Section 3.1.25.B Most of the proposed uses are permitted; <u>Carvana is considered an unlisted use and subject to City Councils approval.</u> More comments are provided in this letter
<b>Special Land Uses</b>	See attached copy of Section 3.1.14.C	See attached copy of Section 3.1.25.C
<b>Minimum Lot Size</b>	Section 3.24	Sec. 3.6.2.D determined by lot layout
<b>Maximum Lot Coverage</b>		
<b>Building Height</b>	65 feet or 5 stories	65 feet or 5 stories whichever is less
<b>Building Setbacks</b>	50 ft. or height of building (See section 3.24 for more regulations)	Sec. 3.27.1.C Depends on type of road frontage; Unlike EXPO, buildings are expected to be closer to the street. Proposed street for the current PRO is considered a non-residential collector; Front: 0 ft. minimum; 10 feet maximum Side and rear: 0 feet minimum; no

	EXPO Zoning (Existing)	TC (Proposed)
		maximum
Usable Open Space	Not Applicable	200 sq. ft. Minimum usable open space per dwelling unit 15% gross open space
Minimum Square Footage	Not Applicable Minimum FAR 0,5	Not applicable

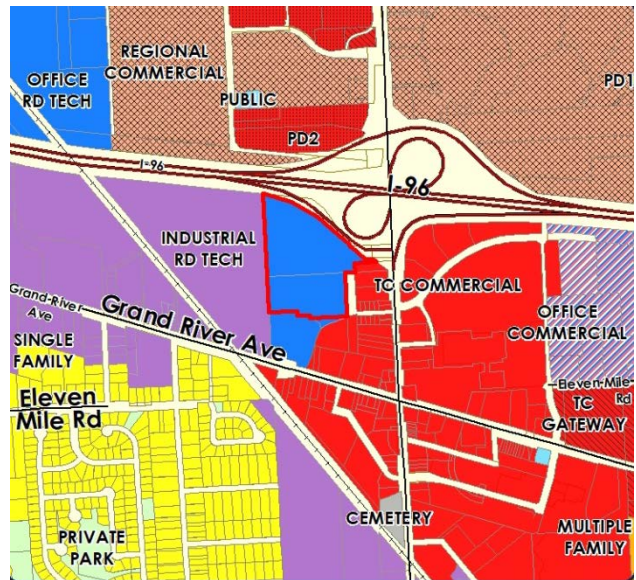
**COMPATIBILITY WITH SURROUNDING LAND USE**

The surrounding land uses are shown in the chart below. The compatibility of the proposed rezoning with the zoning and uses on the adjacent properties should be considered by the Planning Commission in making the recommendation to City Council on the rezoning request. The following table summarizes the zoning and land use status for the subject property and surrounding properties.

	Existing Zoning	Existing Land Use	Master Plan Land Use Designation
Subject Property	Current: EXPO	Vacant/unused parking lot	Office Research Development Technology (uses consistent with OST Zoning District)
Eastern Parcels	TC: Town Center	Retail/Restaurants	TC Commercial (uses consistent with TC Zoning District)
Western Parcels	I-2 General Industrial	Industrial Office	Industrial Research Development Technology (uses consistent with I-1 Zoning Districts)
Northern Parcels	C: Conference (across I-96)	Novi Oaks Hotels	PD2 and Regional Commercial (uses consistent with RC Zoning District)
Southern Parcels	I-1 Light Industrial	Industrial Office	Office Research Development Technology (uses consistent with OST Zoning Districts)



Zoning



Future Land Use

The subject property is tucked in a dead-end corner abutted by interstate to the north and heavily wooded area to the South and strip retail to the east. The site location provides limited to zero connectivity to adjoining properties to north, west and south. The nearest property boundary is approximately 400 feet from Novi Road to the east.

Comau Industries, located to the west, is the only property between the subject property and rail road tracks. It is a well-established industrial automation company. The only connection between the subject property and the Comau site is the secondary emergency access proposed by the applicant. There is no other vehicular or pedestrian connection proposed. It is highly unlikely that Comau property will be redeveloped for a different land use.

There is an existing water tower which is proposed to remain in its own unit as a non-conforming structure and/or use. The purpose of the tower as a part of the new development is not defined at this time. It appears that no changes are proposed to the tower itself.

Existing land use patterns indicate a concentration of retail and restaurants on all sides with some residential to the south of Grand River Avenue and railroad tracks as shown in the image to the right. **The subject property is an ideal candidate for redevelopment.** It is currently zoned as Expo (Expo) and has been vacant since 2012. Suburban Showplace is a successful exposition facility in Novi. The last operating building on the subject property was the Novi Exposition facility which was demolished in 2012. **It is highly unlikely that another exposition facility will be developed in close proximity.** Although significant opportunities exist both as zoned (EXPO uses including I-1 uses except greenhouses and pet boarding facilities) and as master planned (OST uses). It is staffs opinion that the proposed rezoning to Town Center district may be appropriate reasonable alternative to the recommendations of the Master Plan recommendation.



Existing Land Uses in the Vicinity



Approximate Building Heights in the Vicinity

It is evident that the proposed development that includes taller buildings up to 85 feet tall with unique uses and unique architectural styles is going to change the existing streetscape (see image

**below) dramatically along I-96 frontage.** Other buildings along the I-96 frontage range in height from approximately 25 feet to 50 feet in height. The applicant is proposing a unified landscape and hardscape design along the proposed Adell drive to unify the development. The concept plan proposes a 3 feet tall berm with landscaping along I-96 frontage. The image above indicates the approximate heights of existing buildings in the vicinity.



*Existing Streetscape along I-96 frontage*

Refer to Review Summary on Page 8 for potential traffic impacts created by this property.

#### **DEVELOPMENT POTENTIAL**

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The Novi Expo Center was located on the subject property from 1992 till the building was demolished in 2012. The site has been vacant since then. Currently, the only structure on the property is the existing water tower in the northwest corner of the site, the previous building concrete slab and the unused parking lot. Previously, the owner proposed a couple of conceptual ideas for redevelopment, but none of those concepts moved forward.

The current zoning of EXPO District allows hotels, restaurants and recreational facilities as permitted uses, when part of a development that includes an exposition facility, I-1 uses except greenhouses and pet boarding facilities). The site measures approximately 23 acres of which approximately only 4 acres are covered by regulated wetlands and woodlands. This leaves about 19 acres of contiguous land for development. **The redevelopment potential for the site using the current zoning is entirely possible, given the flexibility that the EXPO District affords.**

The Future Land Use map recommends Office Service Technology (OST) uses of the site. The OST District allows most of the uses such as hotels and motels (when designed to be an integral part of the overall OST Office development), sit-down restaurants (when part of an office building) and indoor recreational facilities, as well as Off-street parking lots, as permitted uses. The primary exception to that appears to be Carvana. The current development is not proposing any office related uses. The recommended rezoning category of TC may not allow all the proposed uses.

**Due to its proximity to the surrounding retail, restaurants and hotels, the proposed rezoning to TC, Town Center may be appropriate, however, the applicant should be able to achieve greater compliance with the design guidelines from similar areas within the Town Center Area Study, and redesign the site layout to more closely meet the intent of Town Center district. The current layout is more consistent with a traditional industrial park layout we typically see in Light Industrial districts.**

#### **REVIEW CONCERNS**

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**ENGINEERING:** The requested rezoning to Town Center will result in utility demands that are approximately equal to the utility demand if the property were to be redeveloped under the current EXPO zoning. The conceptual storm water management plan indicates underground

storage in three locations sized for bankfull volume. The PRO plan is currently not meeting some of the general requirements of Chapter 11 of the Code of Ordinances, the Storm Water Management Ordinance and/or the Engineering Design Manual. **Please refer to Engineering review letter for more details.**

**TRAFFIC:** Based on the initial results of a preliminary analysis that was done to assess roadway capacity impacts of the proposed Adell Center development, the City's consultants identified that the intersection of Novi Road and Crescent Boulevard is expected to be able to accommodate the additional traffic during the AM and PM peak periods. The intersection of Novi Road and Grand River currently operates under congested conditions which may worsen with the added traffic demand. It should be noted that the construction of Crescent Boulevard from Adell Drive to Grand River Avenue is could alleviate some of the pressure of the Novi Road and Grand River intersection. The Traffic review also supports the suggestion that the applicant should provide a shared parking study to review the potential for reducing impervious surfaces, adding other site elements and reducing the needs for as many deviations. Last but not the least, Traffic supports the Engineering Division's recommendation for the need for a wider road than proposed (28 feet proposed, 36 feet recommended) based on the intensity and mix of uses proposed within the development. **Please refer to Traffic review letter for more details.**

**WOODLANDS:** Based on the Woodland Summary information on the Tree Inventory Plan (Sheet 21), there appear to be a total of 312 surveyed trees on the subject property. Of these, 29 of the trees are not located within the Regulated Woodland Boundary leaving a total of 283 Regulated Woodland Trees. The applicant indicates that the *proposed development will include the improvement of over three acres of existing City regulated woodlands/wetland areas to allow for better access by the public as part of the benefits offered to Public.* **The plans do not clearly indicate the extent of improvement or impacts to the existing regulated wetland/woodlands area to clearly identify the benefits.**

**WETLANDS:** The southern portion of the site (south of the existing asphalt parking lot) contains the Walled Lake Branch of the Middle Rouge River, wetlands, floodplains and trees. This area (approximately 7 acres), contains the areas of City-regulated wetlands. Our wetland consultant is unable to identify the impacts to wetlands or buffers accurately. It appears some buffer impacts may be required for proposed parking lot improvements for Units 3, 4 and 5. The site plan proposes a pedestrian connection over the Middle Rouge River. **Additional comments and concerns are detailed in wetland review letter.**

**RING ROAD/PROPERTY LEGAL DESCRIPTION:** Both the submitted lans and the legal description should be updated to reflect the City-owned Right-of-Way for the Ring Road improvements.

**FIRE SECONDARY EMERGENCY ACCESS:** Plans should clearly identify the location and details about the secondary emergency access to the development.



*Regulated Woodlands and Wetlands*

## 2016 MASTER PLAN FOR LAND USE: GOALS AND OBJECTIVES

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The proposed development could be said to follow some of the objectives listed in the 2016 Master Plan for Land Use update (adopted by Planning Commission on July 26, 2017) as listed below. Staff comments are in **bold**.

### 1. COMMUNITY IDENTITY

- a. Maintain quality architecture and design throughout the City. **The development proposes various buildings with different architectural styles. However, the applicant is proposing a consistent entryway wall and landscape along the proposed private drive that may serve to unify the development.**

### 2. ECONOMIC DEVELOPMENT

- a. Retain and support the growth of existing businesses and attract new businesses to the City of Novi. **The property is positioned to accomplish this goal with any appropriate development.**

### 3. ECONOMIC DEVELOPMENT / COMMUNITY IDENTITY

- a. I-96/Novi Road Study Area. Develop the I-96/Novi Road Study Area in a manner that reflects the importance of this important gateway to the City in terms of its location, visibility, and economic generation. Mitigate impacts to the City's infrastructure. **The subject property falls in that study area and is located at an important gateway to the City. Impacts to city's infrastructure and mitigation required are yet to be determined.**

### 4. ENVIRONMENTAL STEWARDSHIP

- a. *Protect and maintain the City's woodlands, wetlands, water features and open space.* **The proposed concept plan is not proposing to impact regulated wetlands. It is unclear whether the applicant is proposing to preserve the site's remaining wetlands and woodlands by way of a conservation easement.**

## MAJOR CONDITIONS OF PLANNED REZONING OVERLAY AGREEMENT

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The Planned Rezoning Overlay process involves a PRO concept plan and specific PRO conditions in conjunction with a rezoning request. The submittal requirements and the process are codified under the PRO ordinance (Section 7.13.2). Within the process, which is initiated by the applicant, the applicant and City Council can agree on a series of conditions to be included as part of the approval which must be reflected in the Concept Plan and or the PRO agreement.

The PRO conditions must be in material respects, more strict or limiting than the regulations that would apply to the land under the proposed new zoning district. *Development and use of the property shall be subject to the more restrictive requirements shown or specified on the PRO Plan, and/or in the PRO Conditions imposed, and/or in other conditions and provisions set forth in the PRO Agreement.* **The applicant should submit a list of conditions that they are seeking to include with the PRO agreement. The applicant's narrative does not specifically list any such PRO conditions at this time.**

## ORDINANCE DEVIATIONS

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Section 7.13.2.D.i.c(2) permits deviations from the strict interpretation of the Zoning Ordinance within a PRO agreement. These deviations must be accompanied by a finding by City Council that *"each Zoning Ordinance provision sought to be deviated would, if the deviation were not granted, prohibit an enhancement of the development that would be in the public interest, and that approving the deviation would be consistent with the Master Plan and compatible with the surrounding areas."* Such deviations must be considered by City Council, who will make a finding of whether to include those deviations in a proposed PRO agreement. A proposed PRO agreement would be considered by City Council only after tentative approval of the proposed concept plan and rezoning.



The Concept Plan submitted with an application for a rezoning with a PRO is not required to contain the same level of detail as a preliminary site plan. Staff has reviewed the applicant's Concept Plan in as much detail as possible to determine what deviations from the Zoning Ordinance are currently shown. The applicant may choose to revise the concept plan to better comply with the standards of the Zoning Ordinance, or may proceed with the plan as submitted with the understanding that those deviations would have to be approved by City Council in a proposed PRO agreement. The following are deviations from the Zoning Ordinance and other applicable ordinances shown on the concept plan. The applicant has submitted a narrative describing the requested deviations.

**The applicant has provided a list of deviations in the cover letter that is not complete or accurate. The applicant is asked to revise the list based on staff's comments provided in this letter and the other review letters. The applicant is asked to be specific about the deviations requested and provide a justification to explain how if each deviation "...were not granted, [it would] prohibit an enhancement of the development that would be in the public interest, and that approving the deviation would be consistent with the Master Plan and compatible with the surrounding areas."**

#### **Planning Deviations:**

Following is the list of deviations based on the Planning review of PRO Concept Plan provided.

- 1. Minimum Parking Required Per Use (Sec. 5.2.12):** At the time of Pre-application submittal, the applicant has submitted preliminary parking calculations for the uses proposed. Per those calculations, they have estimated that a minimum of 807 spaces are required and 911 spaces are proposed. Staff has provided the minimum requirements per Ordinance and requested additional information to establish the minimum required and proposed parking counts. At that time, staff also suggested that revisions are required to parking lot layout to conform to the code requirements such as end islands, parking stall setbacks, location of loading areas and dumpsters etc. These revisions may or may not eliminate some of the required parking spaces. Staff recommends that this development is a viable candidate for shared parking between different uses. The applicant is asked to consider a shared parking study. **The applicant has requested a deviation for minimum number of parking spaces per unit and as recommended by a shared parking study as prepared by the applicant's traffic consultant and agreed upon by the City's Traffic Engineer.**

**With the current Concept Plan submittal, the parking calculations have been eliminated.** A reference to shared parking study has been made under requested deviations, but a study has not been provided. The study requires City Council approval prior to PRO approval. ***Due to this missing information, staff is unable to make any determination with regards to the deviation requested.***

- 2. Side Lot lines:** The current unit layout does not conform to the Section 4.02.B of Subdivision Ordinance. Side lot lines for units 1, 6, 7 and 8 are not at right angles or radial to the street lines. **The applicant did not specifically request or address this deviation at this time.**
- 3. Frontage on a Public Street (Sec. 5.12):** Each of the proposed lots has access from the proposed private drive. Unit 9 does not have any frontage on any drive. It is considered a landlocked parcel with no frontage. Access is proposed to be provided by a private access easement. This access easement also allows a secondary emergency access for the entire development. **The applicant has requested this deviation.**
- 4. Open Space Area (Sec. 3.27.1.F):** An Open space plan (sheet 22) is provided. It indicates a total of 4 acres (17.79%) of open space which includes regulated wetlands and woodlands area. This is not allowed because the code requires the Open space to be usable such as pedestrian plazas or permanently landscape areas. Please revise the plan to conform to the

requirement. **The applicant has requested this deviation.**

5. **Building Height** (Sec.3.1.26.D): TC allows a maximum building height of 65 ft. or 5 stories whichever is less. Unit 5 Drury Hotel (85 ft., 7 stories) and Unit 8 Carvana (71 ft., 7 stories) both exceed the maximum height and number of stories allowed. **The applicant has requested a deviation for Drury Hotel, but not Carvana.**
6. **Exterior Side Yard Building Setbacks** (Sec 3.1.25 D): Unit 1 does not meet the minimum required building setback for the exterior side yard fronting I-96. A minimum of 50 ft. is required, approximately 35 ft. is proposed. **The applicant did not specifically request or address this deviation at this time.**
7. **Front Parking Setback** (Sec 3.1.25.D): TC requires a minimum front yard parking setback of 20 ft. from the access easement. Unit 1 (approximately 0 ft. proposed), Unit 3 (approximately 1ft. to 2 ft. proposed) and Unit 4 (approximately 14 ft. proposed) do not provide the minimum required. **The applicant requests a deviation to allow 0 ft. setback for all side yards. Staff recommends identifying the approximate minimums for each unit instead of 0 setbacks for overall site. The applicant did not request these specific deviations at this time.**
8. **Side Parking Setback** (Sec 3.1.25.D): TC requires a minimum side yard parking setback of 20 ft. from the access easement. Proposed setbacks are listed below :
  - a. **Unit 1:** 14 ft. approximately along West
  - b. **Unit 2:** 15 ft. approximately along South
  - c. **Unit 3:** 5 ft. approximately along South
  - d. **Unit 4:** 5 ft. approximately along East
  - e. **Unit 5:** 5 ft. approximately along East
  - f. **Unit 6:** 0 ft. approximately along West
  - g. **Unit 7:** 0 ft. approximately along East and West
  - h. **Unit 8:** 0 ft. approximately along East**The applicant requests a deviation to allow 0 ft. setback for side yards. Staff recommends identifying the approximate minimums for each unit instead of 0 setbacks for overall site.**
9. **Water Tower:** The applicant is proposing that the water tower is to remain on its own site. **This is not a principal permitted use of a site. It is also not considered an accessory use, since its proposed use is not detailed. The creation of a new, separate legal parcel of limited size for the purpose of housing the tower on its own is therefore a required deviation that will need to be addressed in the PRO Agreement (e.g., what happens to the property if the owner determines to remove it, access, etc.).**

**Note:** The applicant is also asked to clarify the actual setback distance for each of these lots. The above provided numbers are just approximations.

### Planning Deviations That May Be Required

Following is the list of deviations that may or may not be required. The applicant is asked to provide clarification whether it is their intent to meet the Ordinance requirements at the time of Preliminary site plan submittal or whether any of those deviations are requested at this time. If any deviations are requested at this time, the applicant is asked to submit additional information for review. Staff does not recommend blanket deviations with many unknown factors.

1. **Minimum Bike Parking Required Per Use** (Sec. 5.16): Refer to Planning chart for requirements
2. **Dumpster** (Sec 4.19.2.F): Dumpsters shall be located in the rear yard. Refer to the attached Planning chart for more requirements. **A deviation is required if a dumpster is proposed in any other location**

3. **Loading Space Location** (Sec. 5.4.2): Within TC zoning, loading space shall be provided in the rear yard (or in the interior side yard beyond the side yard setback for double frontage lots). **A deviation is required if it is proposed in any other location.**
4. **Loading Space Area** (Sec. 5.4.2): Loading area should be a minimum ratio of 10 sq. ft. per front foot of building. **A deviation is required if it does not provided the minimum square footage.**
5. **Lighting and Photometric Plan** (Sec. 5.7): A Photometric plan and additional information is typically required at the time of Final Site Plan when the site is not abutting a residential district. However, given that the proposed unit lines through the parking lot and proximity of parking spaces to Adell drive, staff anticipates that there may be certain deviations from the Ordinance requirements. **Those deviations should be identified and included as part of the PRO agreement.**
6. **Side Building Setbacks** (Sec 3.1.25 D): Units 6 and 7 do not appear to meet the minimum 15 ft. side yard building setback. **The applicant is asked to clarify the distance in order to determine whether this deviation is required.**

#### **Façade Deviations:**

**Section 9 Waivers:** The applicant has provided building elevations for I-fly, Carvana, Planet Fitness, Fairfield Inn and Suites and Drury Inn.

- a. **Unit 1 I-fly:** Elevations provided. Section 9 waiver supported contingent on some revisions made.
  - i. Underage of brick (30% minimum required, 0% proposed on all sides)
  - ii. Underage of combined brick and stone (50% minimum required, 0% proposed on all sides)
  - iii. Overage of painted concrete (0% allowed, 59 % on front, 70% on right, 70% on left and 52% on Rear proposed)
  - iv. Overage of EIFS (25% maximum allowed, 34% on front, 30% on right, 30% on left and 41% on Rear proposed)
- b. **Unit 2 Planet Fitness:** Elevations provided. Incomplete submittal. Deviations identified. Section 9 waiver not supported.
- c. **Unit 3 Fairfield Inn:** Elevations provided. Incomplete submittal. Deviations identified. Section 9 waiver not supported.
  - i. Under of brick (30% minimum required, 14% on front, 16% on right, 16% on left and 23% on Rear proposed)
  - ii. Underage of combined brick and stone (50% minimum required, 15% on front, 19% on right, 19% on left and 25% on Rear proposed)
  - iii. Overage of EIFS (25% maximum allowed, 67% on front, 34% on right, 34% on left and 55% on Rear proposed)
  - iv. Overage of Phenolic simulated wood (25% maximum allowed, 44% on right and 44% on left proposed)
- d. **Unit 5 Drury Inn:** Elevations provided. Section 9 waiver supported.
  - i. Overage of EIFS (25% maximum allowed, 43% on front, 47% on right, 47% on left and 58% on Rear proposed)
- e. **Unit 8 Carvana:** Elevations provided. Section 9 waiver supported.
  - ii. Underage of brick (30% minimum required, 7% proposed on front side)
  - iii. Underage of combined brick and stone (50% minimum required, 7% on front, 30% on right, 30% on left and 39% on Rear proposed)
  - iv. Overage of display glass (25% maximum allowed, 80% on front, 63% on right, 63% on left and 57% on Rear proposed)

The applicant requested a deviation to allow

**Please refer to Façade review for additional comments and revisions recommended. Any monument signs, accessory structures over 200 square feet, rooftop appurtenances and dumpster enclosures are subject to Façade Ordinance requirements and are required to**

conform to the Ordinance requirements if deviations are not sought prior to PRO concept plan approval.

#### Engineering Deviations:

1. **Street and Roadway Rights-Of-Way** (Sec. 6.3.2. B and (Sec. 3.27.1 N) The Proposed road is considered a non-residential collector and our City's Traffic Engineer recommends a 36 feet wide cross-section with a 60 feet Right-of-way and additional 5 feet access easement on either side. The applicant is only proposing a 28 feet wide road with a 50 feet access easement that includes the sidewalk and the road. All setbacks must be calculation from the edge of access easement. Refer to Traffic review for more comments. **The applicant has requested a deviation from this standard. The deviation is relevant to the overall layout of the site.**
2. **Sidewalk width:** Six feet wide sidewalks are required on either side. Five feet walks are proposed at this time. **The applicant did not request this deviation at this time.**
3. **Sidewalk placement:** The outside edge of the sidewalk should be located a minimum of 15 feet from the back of curb, and shall not be placed closer than five (5) feet from the back of curb. The proposed road cross section shows 10 feet from back of curb to outside edge of the five (5) foot sidewalk where fifteen (15) feet is required. **The applicant did not request this deviation at this time.**

#### Landscape Deviations:

1. **End Islands** (Sec. 5.3.12): **A landscape island is required every 15 spaces.** Units 2, 4, 7 have parking bays greater than 15 contiguous space. **This deviation is not supported by staff as revisions can be made so that the deviations can be avoided.**

Please note that this review is just based on the plans submitted for the overall development, as no landscape plans for the individual units were provided except for the greenbelt plantings. As such, it is assumed that each unit's other landscaping ((parking lot interior and perimeter), building foundation, and loading zone screening) will meet all landscaping requirements. If any landscape waivers are requested for a unit, that unit's site plans will need to be submitted for consideration by the Planning Commission prior to PRO Concept plan approval.

#### TRAFFIC DEVIATIONS:

1. **Traffic Impact Study:** Lack of traffic study as the site falls under the study boundaries for Comprehensive Traffic study, which is ongoing.
2. **Parking stall located adjacent to a parking lot entrance** (public or private)(Sec. 5.3.13): *A minimum of 25 ft. is required between the edge of a parking stall and the street right-of-way (ROW) line or street easement or sidewalk, whichever is closer.* **It appears that at least unit 1 and 3 do not meet the requirement at the entrance. The applicant is asked to provide the distance proposed to clearly identify the deviation. It is staff's recommendation that the layout is revised to conform to the code due to Traffic safety concerns.**  
**The following are the Traffic deviations that may be required once additional information is received from the applicant.**
3. Section 11-194(a)(7) indicates a maximum cul-de-sac street length of 800 feet for all developments except for R-A zoned properties. If Adell Drive is in excess of 800 feet, the applicant may be required to seek a City Council variance for exceeding the maximum street length.
4. Should the minimum same side driveway spacing requirements not be met, the applicant may be required to seek a deviation.
5. Should the proposed number of drive approaches and/or the drive approach system not comply with the guidance in the ordinance, the applicant may be requested/required to provide justification and/or apply for deviations.
6. The applicant should provide additional trash receptacle locations to meet the needs of the eight (8) sites within the development, or request a deviation.
7. A waiver is required if the applicant moves forward with painted islands.

### Sign Deviations:

The application has provided information about signage for I-fly, Drury and Carvana as part of the PRO Concept plan submittal. However, formal sign permit applications were provided for Drury, Adell Center and Carvana. Our permit reviewers have identified multiple deviations for the proposed signage. At the same time, they have requested additional information to further verify conformance to other sign permit requirements. Please refer to their comments provided under separate cover for more details. It is unclear whether signage deviations (City Code) are appropriate as part of the PRO process (Zoning Ordinance), also staff is unable to identify all the deviations that are required at this time. As mentioned earlier, staff do not recommend a blanket approval for deviations without reviewing the complete submittal or without knowing the extent of deviations sought. **The applicant has requested a deviation to allow for building signage for I fly, Drury and Carvana. A deviation request for Adell Center signage has not been made. All monument signs for individual units are subject to sign ordinance requirements. Any signage, wherein the deviations are not approved as part of the current PRO plan approval, should conform to the code requirements at a later time.**

### Deviations Requested by the Applicant, but No Longer Appear to be Required

The following deviations no longer are required

1. Access to Major Thoroughfare as there are no single family residential districts on the other side of the access road.
2. Setback deviation from I-96 centerline as the proposed rezoning category is Town Center.
3. Wetland buffer impacts as the impacts to wetland buffer are reviewed as part of wetland permit review
4. Permitted Uses listed on Sheet 2-PRO Plan are not applicable to this project. This should be eliminated from the sheet notes. Please refer to section 3.1.25.B and 3.1.25.C for permitted uses.

### APPLICANT'S BURDEN UNDER PRO ORDINANCE

The Planned Rezoning Overlay ordinance (PRO) requires the applicant to demonstrate that certain requirements and standards are met. The applicant should be prepared to discuss these items, especially in number 1 below, where the ordinance suggests that the enhancement under the PRO request would be unlikely to be achieved or would not be assured without utilizing the Planned Rezoning Overlay. Section 7.13.2.D.ii states the following:

1. (Sec. 7.13.2.D.ii.a) *Approval of the application shall accomplish, among other things, and as determined in the discretion of the City Council, the integration of the proposed land development project with the characteristics of the project area, and result in an enhancement of the project area as compared to the existing zoning, and such enhancement would be unlikely to be achieved or would not be assured in the absence of the use of a Planned Rezoning Overlay.*
2. (Sec. 7.13.2.D.ii.b) *Sufficient conditions shall be included on and in the PRO Plan and PRO Agreement on the basis of which the City Council concludes, in its discretion, that, as compared to the existing zoning and considering the site specific land use proposed by the applicant, it would be in the public interest to grant the Rezoning with Planned Rezoning Overlay; provided, in determining whether approval of a proposed application would be in the public interest, the benefits which would reasonably be expected to accrue from the proposal shall be balanced against, and be found to clearly outweigh the reasonably foreseeable detriments thereof, taking into consideration reasonably accepted planning, engineering, environmental and other principles, as presented to the City Council, following recommendation by the Planning Commission, and also taking into consideration the special knowledge and understanding of the City by the City Council and Planning Commission.*

## PUBLIC INTEREST/ BENEFITS TO PUBLIC UNDER PRO ORDINANCE

Section 7.13.2.D.ii states that the City Council must determine that the proposed PRO rezoning would be in the public interest and the benefits to public of the proposed PRO rezoning would clearly outweigh the detriments. The following are being suggested by the applicant (as listed in their narrative) as benefits resulting from the project. Because staff is indicating that additional information about aspects of the project is needed, our comments are minimal at this time:

1. *The proposed development will convert a vacant parcel of property at a major intersection and entryway into the City of Novi.*
2. *The development of this property will reduce any chances of crime associated with a vacant parcel by providing new development with continuous movement of people and vehicles throughout the property.*
3. *This development will convert a property that is currently zoned EXPO center into a zoning district that will allow a use that is beneficial to neighborhood businesses and the community in general.*
4. *This development will help produce a more positive image of the City of Novi by the 100,000 plus motorists travelling along Interstate on a weekly basis.*
5. *The approval of this development will bring additional entertainment, overnight stay and dining opportunities to the City that will benefit the City of Novi residents as well as bring in residents and visitors from neighboring communities.*
6. *This development is centrally located to several communities that will serve as a weekend long youth sporting tournaments and weekly events held at the Suburban showplace. These events typically bring in people from all over southeast Michigan and the United states.*
7. *The approval of this development will trigger a sale of proposed units within Adell Center thereby generating an increase in property values in addition to the value of neighboring properties.*
8. *The approval of this development will create 200-300 temporary construction jobs and permanent jobs.*
9. *The approval of this development will increase the tax base within the City of Novi. As reported by the Mayor at a recent City Council meeting, the city desires to increase the tax base to fund additional services such as police, fire and parks departments. **Items 1 through 9 may be accurate statements, but the applicant might want to address whether it requires a PRO rezoning process (as opposed, for example, to a "straight" rezoning to a district like TC) to accomplish them.***
10. *It is estimated that this development will increase the tax base by over \$3 million annually, plus an additional personal property tax generated from the new businesses. **Staff cannot comment on the accuracy of this figure.***
11. *The approval of the proposed development will include the improvement of over three acres of existing city regulated woodlands/wetland areas to allow for better access by the public. **This may be accurate, but the City would need more detail to understand the extent of benefits to the public.***
12. *The approval of this development will include a consistent and cohesive streetscape and signage package throughout. **This is a determination for the Planning Commission and Council to make.***
13. *The proposed development includes new public art (pocket parks) locations for placement of community art. **More information on the art being referred to is required to evaluate this.***

## SUMMARY OF OTHER REVIEWS

1. Engineering Review (dated 06-27-18): It does not meet the general requirements on Chapter 11, Storm water management ordinance or Engineering design manual. Additional comments to be addressed with revised concept plan submittal. Engineering is currently **not** recommending approval.
2. Landscape Review (dated 06-25-18): Landscape review has identified deviations that may be required. Staff supports only a few. Refer to review letter for more comments. Landscape

- recommends approval provided individual site plan conform to the code at the time of site plan approval.
3. Wetland Review (dated 06-29-18): An authorization to encroach into 25 foot buffer setback is required for this site plan at the time of Preliminary Site Plan review. Wetlands recommend approval.
  4. Woodland Review (dated 06-29-18): A City of Novi woodland permit is not required for the proposed plan. Additional comments to be addressed at the time of Preliminary Site Plan review.
  5. Traffic Review (dated 06-29-18): Traffic requested additional information to verify conformance and identify additional deviations. Additional Comments to be addressed with the revised concept submittal. Traffic is currently **not** recommending approval.
  6. Facade Review (dated 06-24-18): There appear to be significant deviations on the proposed elevations. Façade review was unable to make a determination as to the degree of compliance with the Façade Ordinance due to a lack of information. Façade is currently **not** recommending approval for some of the building elevations.
  7. Fire Review (dated 06-13-18): Fire has provided additional comments and questions that would require clarification.

#### **NEXT STEP: PLANNING COMMISSION**

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The applicant has requested to waive the requirement to attend Master Planning and Zoning Committee with a letter dated June 11, 2018.

**Some of the reviews are currently not recommending approval at this time. Staff does not believe that some aspects of the plans are fully completed. There are a number of items that still need to be clarified and further information is requested for additional review.** However, the PRO Concept Plan is scheduled to go before Planning Commission for a public hearing on July 11, 2018 based on applicant's request. Please provide the following by 10 am **July 3, 2018**. **Staff reserves the right to make additional comments as this expedited review continues.**

1. Concept Plan submittal in PDF format. Staff has received this item with the initial submittal
2. A response letter addressing ALL the comments from ALL the review letters and primarily a **request for waivers as you see fit based on the reviews.**
3. A color rendering of the Site Plan, if any to be used for presentation purposes.
4. Façade boards as requested by Façade review letter. If you want to bring the board to the meeting, please send a picture of the façade board by July 3<sup>rd</sup> to include in the PC packet.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or [skomaragiri@cityofnovi.org](mailto:skomaragiri@cityofnovi.org)



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Sri Ravali Komaragiri – Planner

**Attachments:** Planning Review Chart  
Section 3.1.14.B – EXPO Permitted Uses  
Section 3.1.14.C - EXPO Special Land Uses

Section 3.1.18.B – I-1 permitted uses  
Section 3.1.25.B – TC Permitted Uses  
Section 3.1.25C – TC Special Land Uses  
Sec. 4.77. I-1 uses in EXPO district

# EXPO Exposition District

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## A. INTENT

The EXPO, Exposition district is designed to accommodate the development of a planned exposition facility including exhibit halls, display floor area, meeting rooms, conference rooms, eating and lodging facilities, banquet, concession and catering uses, museums, theaters, recreational facilities, space for school and civic affairs, warehousing accessory to the preceding uses, and off-street parking and loading/unloading space.

The Exposition district is intended to encourage an exposition facility which is accessible from thoroughfares and freeways and has adequate site area. To permit quality design and sound economic value for the City, any exposition facility should have unified architectural and functional design and provide an overall master plan of all phases for the complete development.




The City also recognizes that in order to conserve the value of land and buildings in the district and adjacent districts, and to protect the City's tax revenue, a need to provide for alternative uses in the district may be warranted should the demand for an exposition facility in the district diminish due to changing conditions in the City. Therefore, the district is also designed to promote research, office and light industrial development which is free from danger of fire, explosions, toxic and noxious matter, radiation, and other hazards, and from offensive noise, vibration, smoke, odor and other objectionable influences and to help meet the needs of the City's expected future economy for all types of research, office, light industrial and related uses.

 **User Note:** For uses listed in **bold blue**, refer to Article 4, or click on use, for use-specific standards


## B. PRINCIPAL PERMITTED USES

- i. Exposition facilities including display floor space, exhibition halls, meeting rooms, conference rooms, banquet rooms, pre-function space, offices and catering uses

The following uses, when part of a development which includes an exposition facility:

- ii. Hotels  and motels 
- iii. Offices and office buildings
- iv. Museums
- v. Theaters
- vi. Restaurants (sit-down) , but not Fast food sit-down
- vii. Recreational facilities
- viii. **Retail sale of products or services occurring as part of a scheduled exposition function** §4.76

The following uses are permitted subject to **Section 4.77:**

- ix. Professional office buildings, offices and office sales and service activities
- x. **Accessory buildings, structures and uses**  §4.19 customarily incident to the above permitted uses
- xi. Publicly owned and operated parks, parkways and outdoor recreational facilities
- xii. **Public or private health and fitness facilities and clubs** §4.34
- xiii. Medical offices, including laboratories and clinics

## B. PRINCIPAL PERMITTED USES (continued)

The following uses are permitted subject to **Section 4.45 and Section 4.77.**

- xiv. Research and development, technical training and design of pilot or experimental products
- xv. Data processing and computer centers
- xvi. **Warehousing and wholesale establishments** §4.43
- xvii. **Manufacturing**  §4.43
- xviii. **Industrial office sales, service and industrial office related uses** §4.44
- xix. Trade or industrial schools
- xx. **Laboratories; experimental, film or testing** §4.43
- xxi. Public utility buildings, telephone exchange buildings, electrical transformer stations and substations, and gas regulator stations, other than outside storage and service yards
- xxii. Public or private indoor recreation facilities
- xxiii. Private outdoor recreational facilities
- xxiv. **Veterinary hospitals**  **or clinics**  §4.31
- xxv. **Motion picture, television, radio and photographic production facilities** §4.47
- xxvi. Other uses of a similar and no more objectionable character to the above uses
- xxvii. **Accessory buildings, structures and uses**  §4.19 customarily incident to any of the above permitted uses

## C. SPECIAL LAND USES

- i. Reserved





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**4.74 PARKING FOR SALE OF NEW, UNLICENSED MOTOR VEHICLES AND PARKING OF LICENSED RENTAL AND LOANER MOTOR VEHICLES**

In the P-1 district, parking for sale of new, unlicensed motor vehicles and parking of licensed rental and loaner motor vehicles, but not including junk or inoperable motor vehicles, partially dismantled or damaged motor vehicles, are permitted as a special land use without time limitation subject to the following conditions:

1. Motor vehicles parked pursuant to this section shall be limited to passenger vehicles (cars, vans, pick-up trucks and sports utility vehicles), and shall not include semi-trucks or trailers, step-vans or other commercial vehicles.
2. No car hauler delivery operations shall be allowed.
3. A landscaped berm shall be provided around all sides of a P-1 zoned site abutting a residential zoned district which shall comply with standards at [Section 3.14.5.E](#).
4. No dumpster or trash storage facility shall be permitted.
5. Night lighting shall be shielded from all adjacent residential zoned districts.
6. A noise impact statement is required subject to the standards of [Section 5.14.10.B](#).

**4.75 CONFERENCE CENTERS**

In the C district, conference centers, including meeting rooms and halls, conference rooms, banquet rooms, pre-function space, and catering uses comprising contiguous space are permitted as a special land use.

**4.76 RETAIL SALE OF PRODUCTS OR SERVICES OCCURRING AS PART OF A SCHEDULED EXPOSITION FUNCTION**

1. In the EXPO district, retail sale of products or services occurring as part of a scheduled exposition function are a permitted use, provided, a scheduled exposition function that involves the sale of products shall not be repeated more often than six (6) times within a calendar year.
2. In the EXO district, retail sale of products or services are permitted as a special land use when occurring as part of a scheduled exposition function occurring in an exposition facility .

**4.77 I-1 USES IN THE EXPO DISTRICT**

The listed uses in Section 3.1.14.B.xvii through xxvii are permitted uses in the EXPO district subject to the following:

1. All usage as an exposition facility ceases in the district;
2. Subject to the conditions required in [Section 3.14.1 through Section 3.14.5](#).

**4.78 RETAIL USES, SERVICE USES AND RESTAURANT USES**

1. In order to provide a limited amount of retail and personal service establishments to serve the employees of and visitors to the nearby office use areas, the following additional uses shall be permitted by the Planning Commission on a limited number of properties located in the OST district and EXO Overlay district as a Retail Service Overlay.
  - A. Retail uses, service uses and restaurant uses are permitted as a special land use subject to the following:
    - i. Generally recognized retail businesses and personal service establishments, as permitted in the B-1 district, [Section 3.1.10.B.i](#) and [3.1.10.B.ii](#), and other uses similar to the identified uses, subject to the limitations of [Section 3.19.5 and 3.19.7](#).
    - ii. Restaurants, including sit down, banquet facilities or other places serving food or beverage, except those having the character of a drive-in or having a drive-through window, subject to the limitations of [Section 3.19.7](#).
    - iii. Fast food drive-through restaurants shall be permitted only on properties meeting the requirements of subsection B, below subject to the limitations of subsection B.ii.d and [Section 3.19.7](#).

- B. These above uses shall only be permitted on properties:
  - i. Located within the areas designated "Office, Research, Development & Technology with Retail Service Overlay" on the Future Land Use Map in the City of Novi's Master Plan for Land Use; or



# I-1 Light Industrial District

3.1.18



**User Note:** For uses listed in **bold blue**, refer to Article 4, or click on use, for use-specific standards

## B. PRINCIPAL PERMITTED USES

- i. Professional office buildings, offices and office sales and service activities
- ii. **Accessory buildings, structures and uses** §4.19 customarily incident to the above permitted uses
- iii. Publicly owned and operated parks, parkways and outdoor recreational facilities
- iv. **Public or private health and fitness facilities and clubs** §4.34
- v. Medical offices, including laboratories and clinics

The following uses are subject to **Section 4.45:**

- vi. Research and development, technical training and design of pilot or experimental products
- vii. Data processing and computer centers
- viii. **Warehousing and wholesale establishments** §4.43
- ix. **Manufacturing** §4.43
- x. **Industrial office sales, service and industrial office related uses** §4.44
- xi. Trade or industrial schools
- xii. **Laboratories experimental, film or testing** §4.43
- xiii. Greenhouses
- xiv. Public utility buildings, telephone exchange buildings, electrical transformer stations and substations, and gas regulator stations, other than outside storage and service yards
- xv. Public or private indoor recreation facilities
- xvi. Private outdoor recreational facilities
- xvii. **Pet boarding facilities** §4.46
- xviii. **Veterinary hospitals** **or clinics** §4.31
- xix. **Motion picture, television, radio and photographic production facilities** §4.47
- xx. Other uses of a similar and no more objectionable character to the above uses
- xxi. **Accessory buildings, structures and uses** §4.19 customarily incident to any of the above permitted uses

## C. SPECIAL LAND USES

The following uses shall be permitted where the proposed site does not abut a residentially zoned district:

- i. **Metal plating, buffing, polishing and molded rubber products** §4.48
- ii. **Uses which serve the limited needs of an industrial district (subject to Section 4.43),** as follows:
  - a. Financial institutions, unions, union halls, and industrial trade schools or industrial clinics
  - b. Industrial tool and equipment sales, service, storage and distribution
  - c. **Eating and drinking establishments and motels** §4.49
- iii. **Automobile service establishment** §4.50
- iv. **Self-storage facilities** §4.51
- v. **Retail sales activities** §4.52
- vi. **Central dry cleaning plants or laundries** §4.53
- vii. **Railroad transfer, classification and storage yards** §4.43
- viii. **Tool, die, gauge and machine shops** §4.43
- ix. **Storage facilities for building materials, sand, gravel, stone, lumber, storage of contractor's equipment and supplies** §4.54
- x. **Municipal uses** §4.43
- xi. **Motion picture, television, radio and photographic production facilities** §4.47
- xii. **Outdoor space for parking of licensed rental motor vehicles** §4.90
- xiii. **Accessory buildings, structures and uses** customarily incident to any of the above permitted uses

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### A. INTENT

The TC, Town Center district is designed and intended to promote the development of a pedestrian accessible, commercial service district in which a variety of retail, commercial, office, civic and residential uses are permitted. Each use shall be complementary to the stated function and purpose of the district and shall not have adverse impact upon adjacent street capacity and safety, utilities, and other City services.





The TC Town Center district is further designed and intended to discourage the development of separate off-street parking facilities for each individual use, and to encourage the development of off-street parking facilities designed to accommodate the needs of several individual uses. Furthermore, it is recognized that uses which have as their principal function the sale or servicing of motor vehicles, such as automobile service establishments, car washes, or new and used motor vehicle sales or service establishments, and drive-in restaurants and restaurants with drive-through facilities, have a disruptive effect on the intended pedestrian orientation of the districts

**i** **User Note:** For uses listed in **bold blue**, refer to Article 4, or click on use, for use-specific standards

### B. PRINCIPAL PERMITTED USES


- i. **Retail businesses use**  §4.78.3
- ii. Retail business service uses 
- iii. **Dry cleaning establishments, or pick-up stations, dealing directly with the consumer** §4.24
- iv. Business establishments which perform services on the premises
- v. Professional services 
- vi. Post office and similar governmental office buildings, serving persons living in the adjacent residential area
- vii. Off-street parking lots 
- viii. Private clubs , fraternal organizations and lodge halls
- ix. **Places of worship** §4.10
- x. **Retail business**  §4.27
- xi. **Service establishments of and office showroom or workshop nature** §4.27
- xii. **Restaurants (sit-down), banquet facilities or other places serving food or beverage** §4.27
- xiii. **Theaters, assembly halls, concert halls, museums or similar places of assembly** §4.27
- xiv. **Business schools and colleges or private schools operated for profit** §4.27
- xv. Offices and office buildings
- xvi. Municipal uses
- xvii. Indoor commercial recreation facilities
- xviii. Outdoor theaters, plazas, parks, public gathering places, including those along a river walk, and like public facilities

### B. PRINCIPAL PERMITTED USES (continued)

- xix. Hotels 
- xx. **Financial institutions** §4.81
- xxi. **Residential dwellings** §4.82
- xxii. **Day care centers**  and **adult day care centers**  §4.12.2
- xxiii. Instructional centers
- xxiv. Other uses similar to the above uses subject to conditions noted
- xxv. **Accessory structures and uses**  §4.19 customarily incidental to the above permitted uses

### C. SPECIAL LAND USES

The following uses shall be permitted by the City Council, following review and recommendation of the Planning Commission.

- i. **Open air business uses** §4.80.1
- ii. **Sale of produce and seasonal plant materials outdoors** §4.30
- iii. **Veterinary hospitals**  or **clinics**  §4.31
- iv. **Microbreweries**  §4.35
- v. **Brewpubs**  §4.35





**PLANNING REVIEW CHART: TC - Town Center District with a Planned Rezoning Overlay(PRO)**

**Review Date:** June 26, 2018  
**Review Type:** PRO Concept Plan  
**Project Name:** **JZ 18-24 ADELL CENTER (18.724)**  
**Plan Date:** April 30, 2018 (Plan sets Received May 02, 2018)  
**Prepared by:** Sri Ravali Komaragiri, Planner  
**E-mail:** [skomaragiri@cityofnovi.org](mailto:skomaragiri@cityofnovi.org) **Phone:** 248.735.5607

- **Bold:** Items that need to be addressed by the applicant prior to the approval of the PRO Concept Plan
- Underlined: Items that need to be addressed prior to the approval of the Preliminary Site Plan
- Blue and underline: Items in are items that do not currently conform to the Zoning Ordinance and may be considered as a deviation

Item	Required Code	Proposed	Meets Code	Comments
<b>Zoning and Use Requirements</b>				
<b>Master Plan</b> <i>(adopted July 26, 2017)</i>	Office Research Development Technology	Restaurants, recreational facilities, hotels, off-street parking and a unlisted use	No	<b>The subject property should be rezoned to TC to permit most of the uses proposed</b>
<b>Town Center Area Study</b>	This site is in close proximity to study area boundary for Town Center Area study adopted in 2014	The applicant is requesting to rezone to TC.	No?	<b>Given that the proposed rezoning would be an extension of existing TC boundary, the applicant should consider recommended design guidelines with regards to pedestrian circulation, amenities and plazas, etc and try to incorporate them into the current design</b>
<b>Zoning</b> <i>(Effective Dec. 25, 2013)</i>	EXPO	TC: Town Center	No	
<b>The applicant has provided the prospective users information at the time of Pre-application meeting. It is not included in the current submittal. Information below is provided as a reference based on previous submittal. Please include the corresponding user information for each unit on the PRO Concept plan. The applicant is asked to limit the type of uses as shown on the PRO concept plan as a condition of the PRO agreement.</b>				
<b>Uses Permitted</b> (Sec 3.1.26.B & C) Sec. 3.1.25.B. - Principal Uses Permitted. Sec. 3.1.25.C. – Special Land Uses Permitted.  <b>Sec. 4.87 Unlisted Use Determinations:</b> Where a proposed use of land or use of a building is not contemplated or specified by this	<b>Unit 1: I-Fly</b> Indoor Commercial Recreation Facilities	Yes	<i>Permitted Use</i>	
	<b>Unit 2: Planet Fitness</b> Indoor Commercial Recreation Facilities	Yes	<i>Permitted Use</i>	
	<b>Unit 3: Fairfield Inn &amp; Suites</b>	Yes	<i>Permitted Use</i>	

Item	Required Code	Proposed	Meets Code	Comments
<p>Ordinance, or where the Planning Division has a question as to the appropriateness of a use that involves other features that were not contemplated or specified by this Ordinance, the Planning Division shall request a determination from the City Council, after review and recommendation from the Planning Commission, as to what district or districts, if any, in which the proposed use may be appropriate as a special land use. In acting upon the request, the City Council shall take into consideration the spirit, purpose and intent of the Ordinance and the Master Plan for Land Use.</p> <p>If the City Council determines that:</p> <ol style="list-style-type: none"> <li>Such use does not appear to be expressly authorized in the zoning ordinance as a principal permitted use or a principal use permitted subject to special conditions,</li> <li>Such use does not appear to have been contemplated by this Ordinance as a principal permitted use or a principal permitted use subject to special conditions, or</li> <li>Such use involves features which do not appear to have been contemplated by the zoning ordinance as features of a principal permitted use or a principal permitted use subject to special conditions, the City Council shall specify what district or districts, if any, in which the proposed use may be appropriate as a special land use.</li> </ol> <p>Following such a determination, a party authorized to do so may file an application pursuant to Section 6.2 for approval of the use as a special land use in a district in which the City Council has determined the use may be appropriate as a special land use.</p>		Hotels		
		<p>Unit 4: <b>Seasonal Event Space</b>                      Off-street Parking Lot;                       Future building of up to 7,000 sq.ft.</p>	No?	<p><i>Permitted Use</i>  <b>Additional information is required with regards to type of events, frequency of events and estimated attendance</b></p> <p><b>A note on sheet 2 indicates that a future building of up to 7,000 sq. ft. will be built at a later time. Use is not specified. Any future development for this development which is different from the current PRO concept plan would require an amendment to the PRO approval.</b></p>
		Unit 5: <b>Drury Inn &amp; Suites</b> Hotels	Yes	<i>Permitted Use</i>
		Unit 6: <b>Restaurant</b> End user to be determined	Yes?	<i>Permitted Use if a sit-down restaurant. More information is requested.</i>
		Unit 7: <b>Restaurant</b> End user to be determined	Yes?	<i>Permitted Use if a sit-down restaurant. More information is requested.</i>
		Unit 8: <b>Carvana</b> Unlisted Use	No?	<p><b><u>This appears to be an unlisted use and is subject to conditions of Section 4.87</u></b>  <b>Unlisted use determination.</b></p> <p><b>Refer to Planning Review letter for more comments and review of unlisted use determination request.</b></p>
		Unit 9: <b>Water Tower</b>	Yes?	<p><b>The existing water tower which is proposed to remain in its own unit as a non-conforming structure and/or use. The purpose of the tower as a part of the new development is not defined at this time. It appears that no changes are proposed to the tower itself.</b></p>
Density	Not Applicable	Residential	NA	

Item	Required Code	Proposed	Meets Code	Comments
<i>Future Land Use Map(adopted July 26, 2017)</i>		development not proposed		
<b>Phasing</b>	Show proposed phasing lines on site plan. Describe scope of work for each phase. Each phase should be able to stand on its own with regards to utilities and parking	Phasing is not proposed	NA?	<b>Phasing is not proposed.</b>

**6.3 SITE CONDOMINIUMS**

The applicant proposes to build roads and utilities and divide the site as individual condominium units based on the lot lines indicated on the PRO concept plan. Each individual user is responsible for site plan review and approvals and construction of each unit at respective schedules. There is no tentative timeline indicated for completion of all the units.

Please refer to Section 6.3 Site Condominiums and Section 6.1.E. for requirements for Roads and Utilities plan.

Any deviations from Site Condominium plan standards should also be included in the PRO agreement. Based on the concept plan provided, listed below are our initial comments that do not conform to the code.

Additional comments will be provided at the time of concept plan review, once a revised plan is submitted.

1. Each building site shall front on and have direct access to a public street or onto a private street that complies in all respects to the Design and Construction Standards (Novi Code Chapter 11) applicable to public streets and roadways.
2. The lot size, width, depth and shape in any subdivision shall be appropriate
3. Excessive lot depth in relation to width shall be avoided. A depth-to-width ratio of 3 to 1 shall normally be considered a maximum.
4. Side lot lines shall be at right angles or radial to the street lines, or as nearly as possible thereto. The current unit layout does not conform to this code. Side lot lines for units 1, 6, 7 and 8 are not at right angles or radial to the street lines.

**PRO Concept Plan Submittal: Additional requirements**

<b>Written Statement</b> <i>(Site Development Manual)</i>  <i>The statement should describe the items listed to the right</i>	Potential development under the proposed zoning and current zoning	The applicant has addressed this item in the narrative.	Yes	<b>Staff agrees that the Town Center maybe a reasonable alternative to the existing zoning.</b>
	Identified benefit(s) of the development	Public benefits are proposed at this time.	No?	<b>Please refer to Plan Review letter for more comments</b>
	Conditions proposed for inclusion in the PRO Agreement (i.e., Zoning Ordinance deviations, limitation on total units, etc.)	List of deviations are included in the narrative	No?	<b>List of deviations is not comprehensive. Please include a comprehensive list of deviations based on information provided in the review letters.</b>  <b>The applicant is asked to consider the additional</b>

Item	Required Code	Proposed	Meets Code	Comments
				conditions as suggested in the Planning review letter.
<b>Sign Location Plan</b> (Page 23,SDM)	Installed within 15 days prior to public hearing Located along all road frontages	Provided and approved; Signs are installed on site	Yes	
<b>Traffic Impact Study</b> (Site development manual)	A Traffic Impact Study as required by the City of Novi Site Plan and Development Manual.	The site falls under the study boundaries for Comprehensive Traffic study which is ongoing	Yes?	<b>Refer to Traffic review letter for more comments</b>
<b>Community Impact Statement (CIS)</b> (Sec. 2.2)	<ul style="list-style-type: none"> <li>- Over 30 acres for permitted non-residential projects</li> <li>- Over 10 acres in size for a special land use</li> <li>- All residential projects with more than 150 units</li> <li>- A mixed-use development, staff shall determine</li> </ul>	It appears to be a mixed-use development, based on the number of different uses.  A CIS is provided	Yes?	<b>Refer to Planning Review letter for more comments.</b>
<b>Height, bulk, density and area limitations</b>				
<b>Frontage on a Public Street</b> (Sec. 5.12) (Sec. 6.3.2.A) Each building site shall front on and have direct access to a public street or onto a private street that complies in all respects to the Design and Construction Standards (Novi Code Chapter 11) applicable to public streets and roadways	Frontage upon a public street is required	Current concept plan proposed a site condominium. Each of the proposed lots has access from the proposed private drive. Unit 9 does not have any frontage on any drive.	No	<a href="#"><u>One deviation for all units fronting on private drive</u></a>  <a href="#"><u>One deviation for lack of frontage for Unit 9.</u></a>  <a href="#"><u>Easements for access to public street will be required</u></a>
<b>Access To Major Thoroughfare</b> (Sec. 5.13)	Access to major thoroughfare is required, unless the property directly across the street between the driveway and major thoroughfare is either multi-family or non-residential	Master site has access to Crescent Boulevard, individual parcels have access to internal private drive; No single family residential zoning in the vicinity	NA	

Item	Required Code	Proposed	Meets Code	Comments
<b>Open Space Area</b> (Sec. 3.27.1.F)	15% (permanently landscaped open areas and pedestrian plazas).  <u>Open space can be calculated for either each individual unit or for the entire development</u>	An Open space plan (sheet 22) is provided. It indicates a total of 4 acres (17.79%) of open space.  Easements include land on the south side of the site. The plan indicates a pedestrian connection over the wetlands to that area. No other improvements are indicated on the plan.	No?	<b>This is not allowed because the code requires the Open space to be usable such as pedestrian plazas or permanently landscape areas. Please revise the plan to conform to the requirement.</b>  <u><a href="#">This will be considered a deviation if the minimum 15 percent is not met</a></u>
<b>Maximum % of Lot Area Covered</b> (By All Buildings) (Sec. 3.6.2 D)	No Maximum	Appears to meet requirements. Lot coverage appears to range from 2% to 15% for each individual units	Yes	
<b>Building Height</b> (Sec.3.1.26.D)	5 stories or 65 ft, whichever is less  Provisions for additional height only applies for TC-1, not TC district	Unit 1:58ft (2 stories)	Yes	The building is taller for 2 stories
		Unit 2: 40 ft. to 50 ft. (2 stories)	Yes	
		Unit 3: 63 ft. (5 stories)	Yes	
		Unit 4: Building not proposed at this time	No?	<b>Provide more information about gazebo height</b>
		Unit 5: 85 ft. (7 stories)	No	<u><a href="#">It exceeds the maximum allowed. It is considered a deviation</a></u>
		Unit 6: 20 ft. -30ft. (1 story)	Yes	
		Unit 7: 20 ft. -30ft. (1 story)	Yes	
		Unit 8: 71ft. (7 stories)	No	<u><a href="#">It exceeds the maximum allowed. It is considered a deviation</a></u>
		Unit 9:120 ft. Existing tower (Non-conforming existing structure)	No?	

**Building Setbacks** (Sec 3.1.26 D) and (Sec. 3.27.1.C)

**Non-residential collectors and Local Streets**  
 Additional setbacks may also be required by Planning Commission or City Council if deemed necessary for better design or functionality.  
**Proposed Adell drive is considered a non-residential collector road. Refer to Traffic review for more detail.**



Item	Required Code	Proposed	Meets Code	Comments
<p><b>NOTE REGARDING SETBACKS:</b>            The current submittal indicates the front lot lines at the edge of proposed curb. Under the list of deviations, the applicant is proposing to build a private road built to City standards and including the road as a common element.</p> <p><b>Proposed road is considered a non-residential collector and would require a 70 feet access easement. <u>All setbacks must be calculation from the edge of access easement. However, the applicant is only proposing a 50 ft. Access easement that includes the sidewalk and the road.</u></b></p> <p><b>ROW WIDTH</b>  <i>(Sec. 6.3.2.B) Where streets and roadways are private, front yard setbacks shall be measured as if such right-of-way lines existed; the width of such hypothetical right-of-way shall be based upon the function of such street as a major arterial, arterial, minor arterial, residential collector, nonresidential collector, residential street or nonresidential street, as those terms are used in the Master Plan for the City of Novi and the City of Novi Design and Construction Standards. Private roads are allowed, but they should be built to City standards and should be placed under an access easement equal to the width of typical Right of way.</i></p>				
<p><b>Front</b>            50 feet minimum from all lot lines for exterior lot             15 feet minimum for front side, for interior lot lines             15 feet between separate buildings on same side</p>	<p>All proposed units must have frontage on Adell drive. A minimum of 15 ft. is required.</p>	<p>15 ft. minimum is provided for all units. Water tower is its own unit and does not have frontage on the Adell drive</p>	<p>Yes</p>	<p>The applicant is proposing a 50 feet access easement. Setbacks should be measure from edge of access easement.             Setback dimensions for each lot are not shown.             Label the dimension of setback from edge of access easement for each side for each lot.</p>
<p><b>Exterior Side Yard</b>            50 feet exterior            15 feet interior             Exterior: lot lines located abutting non-TC district lots.             Interior: lot lines abutting TC district lots.</p>	<p>Unit 1: I-96 Exterior: 50 ft.            Unit 2: NA            Unit 3: NA            Unit 4: NA            Unit 5: Adell Interior: 15 ft.            Unit 6: I-96 Exterior: 50 ft.            Unit 7: I-96 Exterior: 50 ft.            Unit 8: I-96 Exterior: 50 ft.            Unit 9: I-96 Exterior: 50 ft.</p>	<p><b>Approximately 35 ft. proposed</b>             NA             15 ft. min.             50 ft. minimum             Existing: 50 ft. minimum</p>	<p>No             NA             Yes</p>	<p><a href="#">This will be considered a deviation if the plans are not revised to meet the maximum setback</a>             Setback dimensions for each lot are not shown.             Label the dimension of setback from edge of access easement for each side for each lot.</p>
<p><b>Side Yard</b>            50 feet exterior            15 feet interior             Exterior: lot lines</p>	<p>All units require a minimum of 15 ft. from side lot lines</p>	<p>All units appear to have a minimum of 15 ft. except for Unit 6 and 7. Staff is unable to determine.</p>	<p>No ??</p>	<p>Setback dimensions for each lot are not shown.             Label the dimension of setback from edge of</p>

Item	Required Code	Proposed	Meets Code	Comments
located abutting non-TC district lots.  Interior: lot lines abutting TC district lots.	Unit 9: Existing	Existing		access easement for each side for each lot.  Please confirm if Unit 6 and 7 meet the ordinance requirement
<b>Rear Yard</b> 50 feet exterior 15 feet interior  Exterior: lot lines located abutting non-TC district lots.  Interior: lot lines abutting TC district lots.	Unit 1: 15 ft. interior	Appears to meet the minimum	Yes??	Setback dimensions for each lot are not shown.  Label the dimension of setback from edge of access easement for each side for each lot.
	Unit 2: 50 ft. exterior			
	Unit 3: 50 ft. exterior			
	Unit 4: 15 ft. interior	Building not proposed at this time		
	Unit 5: 15 ft. interior	Unit 5: Not provided		
	Unit 6: NA (double frontage)	NA		
	Unit 7: NA (double frontage)			
	Unit 8: NA (double frontage)			
Unit 9: NA				
<b>Parking Setback (Sec 3.1.25.D)</b>				
<b>Front</b>	20 ft. from ROW or (access easement for private roads)	<b>Unit 1: A minimum of 0 ft. is provided</b>	No?	The applicant is proposing a 50 feet access easement. Setbacks should be measure from edge of access easement.  Setback dimensions for each lot are not proposed. A table has been provided which indicates  20 ft. along Perimeter 20 ft. from Adell drive; <u>This is incorrect. Unit 1, 3 and 4 do not have 20 ft. minimum from 50 ft. access easement.</u> 0 ft. for side and rear lot lines
		Unit 2: 20 ft.		
		<b>Unit 3: 1 to 2 ft.</b>		
		<b>Unit 4: 14 ft.</b>		
		Unit 5: 20 ft.		
		Unit 6: 20 ft.		
		Unit 7: 20 ft.		
		Unit 8: 20 ft.		
Unit 9: Not applicable; No parking				
<b>Exterior Side Yard</b>	20 ft. from ROW or (access easement for private roads)	Unit 1: 20 ft.	Yes	The applicant is proposing a 50 feet access easement. Setbacks should be measured from edge of access easement.
		Unit 2: NA		
		Unit 3: NA		
		Unit 4: NA		

Item	Required Code	Proposed	Meets Code	Comments
		Unit 5: 20 ft. Unit 6: 20 ft. Unit 7: 20 ft. Unit 8: 20 ft. Unit 9: NA		Setback dimensions for each lot are not shown.  Label the dimension of setback from edge of access easement for each side for each lot.
Side Yard	20 ft. from ROW or (access easement for private roads)	Unit 1: East: 20 ft. min. West: 14 ft. approx. Unit 2: North: 20 ft. min. South: 15 ft. approx. Unit 3: North: appears to be 20 ft. approx. South: 5 ft. Unit 4: West: appears to be 20 ft. approx. East: 5 ft. approx. Unit 5: West: appears to be 20 ft. approx. East: 5 ft. approx. Unit 6: West 0 ft. East: 20 ft. Unit 7: West: 0 ft. East: 0 ft. Unit 8: East: 0 ft. Unit 9: Not Applicable	No	The applicant is proposing a 50 feet access easement. Setbacks should be measured from edge of access easement.  Setback dimensions for each lot are not shown.  Label the dimension of setback from edge of access easement for each side for each lot.  <a href="#">The applicant requests a deviation to allow 0 ft. setback for side yards. Staff recommends identifying the approximate minimums for each unit instead of blanket deviation to allow for 0 setbacks for overall site.</a>
Rear Yard	10 ft. from ROW or (access easement for private roads)	Unit 1: 20 ft. Unit 2: 20 ft. Unit 3: 20 ft. Unit 4: 20 ft. Unit 5: 20 ft. Unit 6: NA (double frontage) Unit 7: NA (double frontage)	Yes?	The applicant is proposing a 50 feet access easement. Setbacks should be measured from edge of access easement.  Setback dimensions for each lot are not shown.  Label the dimension of setback from edge of access easement for each

Item	Required Code	Proposed	Meets Code	Comments
		Unit 8: NA (double frontage)		side for each lot.
		Unit 9: NA (double frontage)		
<b>Note To District Standards (Sec 3.6.2)</b>				
<b>Exterior Side Yard Abutting a Street</b> (Sec 3.6.2.C)	All exterior side yards abutting a street shall be provided with a setback equal to front yard.	Units 1, 5, 6, 7 and 8 have an exterior side yard	No?	
<b>Minimum lot area and width</b> (Sec 3.6.2.D)	Except where otherwise provided in this ordinance, the minimum lot area and width, maximum percentage of lot coverage shall be determined by the requirements set forth.	It is unclear whether each unit meets ordinance standards for setback, landscaping, parking, loading and open space etc. Refer to all reviews for other notes	No?	<b>Unit 9 does not have any frontage on proposed private drive</b> <a href="#">This is considered a deviation if it is not revised to conform to the code</a>
<b>Yard setbacks</b> (Sec 3.6.2.H&L)	If site abuts a residential zone, buildings must be set back at least 3' for each 1' of building height, but in no case can be less than 20' setback	Does not abut residential zoning	NA	
<b>Wetland/Watercourse Setback</b> (Sec 3.6.2.M)	A setback of 25 ft. from wetlands and from high watermark course shall be maintained	Wetland setbacks are shown on Sheet 4. It appears that there may be disturbance to the buffer;  A wetland crossing is proposed from a pathway from Unit 4. Additional information is not provided	No?	<b>Refer to wetlands review letter for more detail.</b>
<b>Parking setback screening</b> (Sec 3.6.2.P)	Required parking setback area shall be landscaped per sec 5.5.3.	<b>Unable to determine</b>	No?	<b>Refer to Landscape review for more details</b>
<b>Modification of parking setback requirements</b> (Sec 3.6.2.Q)	The Planning Commission may modify parking setback requirements based on its determination	Site plan does not conform with front and side yard parking setback requirements	No?	

Item	Required Code	Proposed	Meets Code	Comments
	according to Sec 3.6.2.Q.			
<b>TC-1 District Required Conditions (Sec 3.27)</b>				
<b>Site Plans</b> (Sec. 3.27.1.A.)	Site area under 5 acres: Requires Planning Commission approval; Site area over 5 acres: Requires City Council approval upon Planning Commission recommendation	The parent parcel is over 5 acres. Individual lots are less than 5 acres	No?	<b>Site plan for roads and utilities plan and site condominium, requires City Council approval upon Planning Commission recommendation.</b>  <b>Site plan approval for individual lots less than require Planning Commission approval, unless Council reserves the right to approval site plans as part of PRO approval</b>
<b>Parking Setbacks and Screening</b> (3.27.1 D)	20 ft. from ROW (access easement for private roads)	This is incorrect. Unit 1, 3 and 4 do not have 20 ft. minimum from 50 ft. access easement.	No	<a href="#"><u>This is considered a deviation if it is not revised to conform to the code</u></a>
	Surface parking areas must be screened by either a 2.5 ft. brick wall, semitransparent screening or a landscaped berm from all public ROW (access easement for private roads)	A combination of brick wall and a semi-transparent screening is provided on both side of proposed Adell drive	Yes	
	For TC-1, No front yard or side yard parking on any non-residential collector.	Not applicable	NA	
<b>Architecture/Pedestrian Orientation</b> (3.27.1 E)  <b>No maximum length of buildings for TC</b>	No building in the TC-1 district shall be in excess of one-hundred twenty-five (125) feet in width, <b>unless pedestrian entranceways are provided at least every one-hundred twenty-five (125) feet of frontage.</b>	Not applicable	NA	
<b>Façade materials</b> (Sec. 3.27.1 G)	All sides of the building and accessory buildings must have the same	<b>Unit 1 I fly:</b> Elevations provided. Section 9 waiver is supported.	Yes?	<b>Please refer to Façade review for more details and missing information.</b>

Item	Required Code	Proposed	Meets Code	Comments
	materials. Façade materials may deviate from brick or stone with PC approval.	<b>Unit 2 Planet Fitness:</b> Elevations provided. Incomplete submittal. Deviations identified. Section 9 waiver is not supported	No?	If deviations are not identified/ requested at this time, the elevations are expected to conform to the code at the time of Preliminary Site Plan approval.  Section 9 waivers are required for Units 1, 2, 3, 5 and 8.  It appears that no changes are proposed to the water tower at this time.
		<b>Unit 3 Fairfield:</b> Elevations provided. Incomplete submittal. Deviations identified.	No?	
		<b>Unit 4 Temporary parking Lot:</b> Building is not proposed at this time		
		<b>Unit 5 Drury Inn:</b> Elevations provided. Section 9 waiver is supported.	Yes?	
		<b>Unit 6 Restaurant:</b> Elevations not provided		
		<b>Unit 7 Restaurant:</b> Elevations not provided		
		<b>Unit 8 Carvana:</b> Elevations provided. Section 9 waiver is supported.		
		<b>Unit 9 Water Tower:</b> Existing non-conforming structure		
<b>Parking, Loading, Signs, Landscaping, Lighting, Etc</b> (Sec. 3.27.1 H)	All loading in TC-1 shall be in rear yards.	Requested rezoning category is TC	NA	<u>Refer to comments on loading areas on page 16 for more detail.</u>
	Off-street parking counts can be reduced by the number of on-street parking adjacent to a use	On-street parking is not proposed	NA	
	PC may allow parking requirement reduction when parking areas serve dual functions.	The current concept plan appears to provide sufficient parking as noted on page 13.	Yes?	The applicant is asked to consider shared parking in order to reduce the parking proposed to eliminate or reduce some of the deviations such as setbacks and loading areas as listed in the letter.

Item	Required Code	Proposed	Meets Code	Comments
	Special assessment district for structured park	Not proposed	NA	
<b>Sidewalks required</b> (Sec. 3.27.1 I)	For TC-1 only, Sidewalks required along non-residential collector to be 12.5 ft. wide.	Not Applicable	NA	
	Direct pedestrian access between all buildings and adjacent areas	Pedestrian access is not indicated in the current set of plans	No	<b>Site Plan is expected to conform to the code at the time of individual site plan reviews.</b>
<b>Bicycle Paths</b> (Sec. 3.27.1 J)	Bike paths required to connect to adjacent residential & non-residential areas.	None provided	NA?	
<b>Development amenities</b> (Sec. 3.27.1 L)	All sites must incorporate amenities such as exterior lighting, outdoor furniture, and safety paths in accordance with Town Center Study Area.	Minimal amenities are proposed ;  Four focal areas are provided with enhanced landscaping, but amenities such as benches, plazas, lighting, safety path etc are not provided	No?	<b>The applicant is asked to propose more amenities to meet the intent of destination entertainment center as indicated in the narrative.</b>  <b>The design has more potential for providing more defined public gathering spaces, especially with Unit 4.</b>
<b>Combination of use groups within a single structure</b> (Sec. 3.27.1 M) (Sec.3.27.2.B)	- Additional regulations per Sec. 3.27.1.M and 3.27.2.B apply if combination of uses proposed in same building	Each building stands on its own with a single use	NA	
<b>Street and Roadway Rights-Of-Way</b> (Sec. 3.27.1 N)	Nonresidential collector and local streets shall provide ROWs consistent with DCS standards  Roadway width: 36 feet ROW/Access Easement: 70 feet	Roadway width: 30 feet Access Easement: 50 feet to edge of sidewalk	No	Proposed private drive does not meet the DCS standards; <b>Refer to Traffic and Engineering comments for more information</b>

**Parking, Handicap Parking and Bike Requirements**

**Note: Parking calculations were provided at the time of Pre-application meeting. Staff provided the following comments at that time. Parking calculation is eliminated with the current concept plan submittal. A reference to shared parking study has been made under requested deviations, but a study is not provided.**

Item	Required Code	Proposed	Meets Code	Comments
<b>Staff is unable to make a determination based on the missing information. Information provided below is what was provided at the time of Pre-application meeting.</b>				
<p><b>Required Parking Calculation</b>                      (Sec. 5.2.12)                      (Sec. 4.82.2)</p> <p><b>See Individual requirements below</b></p>	Unit 1: Per Owner : 38 spaces	Unit 1: 52 Spaces	No?	Please provide additional information how 30 spaces are determined to be adequate by the owner.
	Unit 2: For 1000 memberships= 182 spaces	Unit 2: 216 Spaces	Yes	
	Unit 3: Per applicant, 138 Spaces	Unit 3: 134 Spaces	No?	Provide information such as number of rooms, employees and accessory uses such as banquet halls, if proposed. This determines the minimum required parking.
	Unit 4: Per applicant, 38 Spaces	Unit 4: 38 Spaces	No?	Provide additional information with regards to type of uses and frequency of uses and estimated visitors for seasonal events
	Unit 5: Per applicant, 181 Spaces	Unit 5: 201 Spaces	No?	Provide information such as number of rooms, employees and accessory uses such as banquet halls, if proposed. This determines the minimum required parking.
	Unit 6: For 7,000 SF per applicant: 100 spaces	Unit 6: 89 Spaces	No	Unit 6 does not have minimum required spaces on its own, but has the required spaces for both restaurants together. If Unit 6 built is built prior to Unit 7, it will not have sufficient parking unless all parking is built. Please provide clarification
	Unit 7: For 7,000 SF per applicant: 100 spaces	Unit 7: 141 Spaces	Yes	
	Unit 8: Per Owner: 30 Spaces	Unit 8: 40 Spaces	No?	Please provide additional information how 30 spaces are determined to be adequate by the owner.
	Unit 9: 0	Unit 9: 0 Spaces	Yes?	
	Total Required: 807 Spaces	TOTAL PROPOSED: 911 Spaces		
<b>Minimum Parking Required Per Use (Sec. 5.2.12)</b>				



Item	Required Code	Proposed	Meets Code	Comments
<u>Unit 1: I FLY: Indoor commercial recreation facilities</u> 1 for 2 people allowed under maximum occupancy (??)	<u>Unit 2: PLANET FITNESS: Indoor commercial recreation facilities</u> 30,000 SF or less: 1 for each 5.5 memberships Greater than 30,000 SF: 1 for each 9 memberships (family or individual)	<u>Unit 3: FAIRFIELD: Hotel</u> <u>Unit 5: Drury: Hotel</u> 1 for room + 1 per employee + as needed for accessory uses + Banquet Halls (if any) 1 per 3 people @ max. occupancy		
<u>Unit 4: Seasonal Events: Undefined</u>	<u>Unit 6 and Unit 7: Sit-down Restaurant</u> 1 per 70 GLA or 1 per 2 employees + 1 per customer max capacity including waiting areas	<u>Unit 8: Carvana: Undefined Use</u>		<u>Unit 9: Water Tower</u>
<b>Barrier Free Spaces</b> <i>Barrier Free Code</i>  <i>*No deviations since this is a Michigan Building Code requirement</i>	Unit 1: 2 Regular, 1 Van accessible Unit 2: 6 Regular, 1 Van accessible Unit 3: 4 Regular, 1 Van accessible Unit 4: 1 Regular, 1 Van accessible Unit 5: 6 Regular, 1 Van accessible Unit 6: 3 Regular, 1 Van accessible Unit 7: 3 Regular, 1 Van accessible Unit 8: 1 Regular, 1 Van accessible Unit 9: 0 Spaces	Unit 1: 2 Spaces Unit 2: 0 Spaces Unit 3: 2 Spaces Unit 4: 0 Spaces Unit 5: 7 Spaces Unit 6: 0 Spaces Unit 7: 0 Spaces Unit 8: 0 Spaces Unit 9: 0 Spaces	No No No No Yes No No No No	<b>Minimum required barrier free spaces should be provided</b>
<b>Minimum number of Bicycle Parking</b> <i>(Sec. 5.16.1)</i>	Unit 1: 8 spaces Unit 2: 8-22 spaces Unit 3: 4 Spaces Unit 4: TBD Unit 5: 4 spaces Unit 6: 2 – 5 spaces Unit 7: 2-5 spaces Unit 8: TBD	Unit 1: 0 Unit 2: 0 Unit 3: 0 Unit 4: 0 Unit 5: 0 Unit 6: 0 Unit 7: 0 Unit 8: 0	No No No No No No No	<b>Provide information to verify conformance</b>

Item	Required Code	Proposed	Meets Code	Comments
	Unit 9: NA	Unit 9:0	No	
<b>Minimum Bike Parking Required Per Use (Sec. 5.16)</b>				
<b>Unit 1: I FLY</b> <b>Unit 2: PLANET FITNESS:</b> <b>Indoor commercial recreation facilities</b> 10 % of required/provided car parking, minimum of 8 spaces		<b>Unit 3: FAIRFIELD: Hotel</b> <b>Unit 5: Drury: Hotel</b> Hotel, minimum of 4 spaces		<b>Unit 4: Seasonal Events: Undefined</b> Undefined
<b>Unit 6 and Unit 7: Sit-down Restaurant</b> 5 % of required/provided car parking, minimum of 2 spaces		<b>Unit 8: Carvana: Undefined Use</b>		<b>Unit 9: Water Tower</b>
<b>Parking Lot Design Requirements (Sec. 5.3.2.)</b>				
<b>Parking Space Dimensions and Maneuvering Lanes</b> (Sec. 5.3.2)	- 90° Parking: 9 ft. x 19 ft. - 24 ft. two way drives - 9 ft. x 17 ft. parking spaces allowed as long as detail indicates a 4" curb at these locations - 60° 9 ft. x 18 ft.	All appear to be 9 ft. x 19 ft.	No	The applicant can consider the possibility of reducing the parking spaces and adding more green space if a shared parking agreement is proposed
<b>Parking lot entrance offset</b> (Sec. 5.3.6)	Parking lot entrances must be set back 25' from any single-family residential district.	Subject property does not abut single-family residential district.	NA	
<b>End Islands</b> (Sec. 5.3.12)	- End Islands with landscaping and raised curbs are required at the end of all parking bays that abut traffic circulation aisles. - The end islands shall generally be at least 8 ft. wide, have an outside radius of 15 ft., and be constructed 3 ft. shorter than the adjacent parking stall	End islands are indicated as required. Unable to determine the compliance with the requirements	No	Provide information to verify conformance. A landscape island is required every 15 spaces This information can be provided with Preliminary site plan if it conforms to the code
<b>Parking stall located adjacent to a parking lot entrance</b> (public or private) (Sec. 5.3.13)	- Shall not be located closer than twenty-five (25) feet from the street right-of-way (ROW) line, street easement or sidewalk, whichever is closer	Unit 1: No conforming Unit 2: Unit 3: Not conforming Unit 4: Unit 5: Unit 6:	No   No	Dimensions are not shown on the plan. Please indicate the distance. It appears that at least unit 1 and 3 do not meet the requirement at the entrance. This is considered a

Item	Required Code	Proposed	Meets Code	Comments
		Unit 7: Unit 8: Unit 9:		<a href="#"><u>deviation if it is not revised to conform to the code</u></a>
<b>Barrier Free Space Dimensions</b> <i>Barrier Free Code</i>	- 8' wide with an 8' wide access aisle for van accessible spaces - 8' wide with a 5' wide access aisle for regular accessible spaces	Not indicated	No	<u>This information can be provided with Preliminary site plan if it conforms to the code</u>
<b>Barrier Free Signs</b> <i>Barrier Free Code</i>	One sign for each accessible parking space.	Not indicated	No	<u>This information can be provided with Preliminary site plan if it conforms to the code</u>
<b>Bicycle Parking General requirements</b> (Sec. 5.16)	- No farther than 120 ft. from the entrance being served - When 4 or more spaces are required for a building with multiple entrances, the spaces shall be provided in multiple locations - Spaces to be paved and the bike rack shall be inverted "U" design - Shall be accessible via 6 ft. paved sidewalk	Not indicated	No	<u>This information can be provided with Preliminary site plan if it conforms to the code</u>  <b>A general location should be indicated on the concept plan</b>
<b>Bicycle Parking Lot layout</b> (Sec 5.16.6)	Parking space width: 6 ft. One tier width: 10 ft. Two tier width: 16 ft. Maneuvering lane width: 4 ft. Parking space depth: 2 ft. single, 2 ½ ft. double	Not indicated	No	<u>This information can be provided with Preliminary site plan if it conforms to the code</u>
<b>Loading Space Area</b> (Sec. 5.4.2)	Within TC zoning, loading space shall be provided in the rear yard (or in the interior side yard beyond the side yard setback for double frontage lots) in the ratio of 10 sq. ft. per front foot of building.	Loading spaces are not indicated on the plan.  Unit 1 (I-fly); Unit 3 (Fairfield) and Unit 5 (Drury Hotel) are requesting deviations for lack of loading space	<b>No</b>	<b>All units are expected to meet the code at the time of Preliminary site plan, if a deviation is not required at this time.</b>  <b>Unit 9 does not require a loading space and should request a deviation.</b>

Item	Required Code	Proposed	Meets Code	Comments
<b>Loading Space Screening</b> (Sec. 5.4.2 B)	Loading area must be screened from view from adjoining properties and from the street.	Loading spaces are not indicated on the plan	No?	Staff anticipates that there will be reduction of parking spaces to accommodate loading area. <u>Deviations may be required if the proposed area and location do not conform to the code.</u>  Reasonable justification is not provided to support lack of loading space for hotels. Loading and unloading operations are typically expected for hotels for food and services.
<b>Dumpster</b> Sec 4.19.2.F	<ul style="list-style-type: none"> <li>- Located in rear yard</li> <li>- Attached to the building or no closer than 10 ft. from building if not attached</li> <li>- Not located in parking setback</li> <li>- Rear lot abuts ROW, 50 ft. setback required.</li> <li>- Away from Barrier free Spaces</li> </ul>	Unit 1: Not shown	No?	<p>Show dumpster locations for all sites to verify conformance</p> <p>If a dumpster if not provided, indicate the means of proposed trash removal</p> <p><a href="#">This is considered a deviation if it is not revised to conform to the code</a></p>
		Unit 2: Not shown	No?	
		Unit 3: Rear yard	Yes	
		Unit 4: Not shown	Yes/No	
		<b>Unit 5: Appears to be in exterior side yard; Does not meet the setback requirement</b>	No	
		Unit 6: Rear yard		
		Unit 7: interior side yard	No?	
		Unit 8: Not shown	No?	
		Unit 9: Not shown	No?	
<b>Dumpster Enclosure</b> Sec. 21-145. (c) Chapter 21 of City Code of Ordinances	<ul style="list-style-type: none"> <li>- Screened from public view</li> <li>- A wall or fence 1 ft. higher than height of refuse bin</li> <li>- And no less than 5 ft. on three sides</li> <li>- Posts or bumpers to protect the screening</li> <li>- Hard surface pad.</li> <li>- Screening Materials: Masonry, wood or evergreen shrubbery</li> </ul>	Not indicated	No?	<u>This information can be provided with Preliminary site plan if it conforms to the code</u>

Lighting and Photometric Plan (Sec. 5.7)				
<p><b>STAFF COMMENT:</b> Photometric plan and additional information is typically required at the time of Final Site Plan when the site is not abutting a residential district.</p> <p><u>However, given that the proposed unit lines through the parking lot and proximity of parking spaces to Adell drive, staff anticipates that there may be certain deviations from the Ordinance requirements. Those deviations should be identified and included as part of the PRO agreement.</u></p>				
Intent (Sec. 5.7.1)	Establish appropriate minimum levels, prevent unnecessary glare, reduce spill-over onto adjacent properties & reduce unnecessary transmission of light into the night sky	A plan is provided which indicates street lighting along Adell drive. Lighting and photometric information for rest of the site is not included in the current submittal.	No	
Lighting Plan (Sec. 5.7.2 A.i)	Site plan showing location of all existing & proposed buildings, landscaping, streets, drives, parking areas & exterior lighting fixtures			
Building Lighting (Sec. 5.7.2.A.iii)	Relevant building elevation drawings showing all fixtures, the portions of the walls to be illuminated, illuminance levels of walls and the aiming points of any remote fixtures.			
Lighting Plan (Sec.5.7.2 A.ii)	Specifications for all proposed & existing lighting fixtures			
	Photometric data			
	Fixture height			
	Mounting & design			
	Glare control devices			
	Type & color rendition of lamps			
	Hours of operation			
	Photometric plan illustrating all light sources that impact the subject site, including spill-over information from neighboring properties			

Item	Required Code	Proposed	Meets Code	Comments
<b>Required Conditions</b> (Sec. 5.7.3.A)	Light pole height not to exceed maximum height of zoning district (65 ft. for TC)			
<b>Required Conditions</b> (Sec. 5.7.3.B&G)	<ul style="list-style-type: none"> <li>- Electrical service to light fixtures shall be placed underground</li> <li>- Flashing light shall not be permitted</li> <li>- Only necessary lighting for security purposes &amp; limited operations shall be permitted after a site's hours of operation</li> </ul>			
<b>Security Lighting</b> (Sec. 5.7.3.H)  Lighting for security purposes shall be directed only onto the area to be secured.	<ul style="list-style-type: none"> <li>- All fixtures shall be located, shielded, and aimed at the areas to be secured.</li> <li>- Fixtures mounted on the building and designed to illuminate the facade are preferred.</li> </ul>			
<b>Required Conditions</b> (Sec.5.7.3.E)	Average light level of the surface being lit to the lowest light of the surface being lit shall not exceed 4:1			
<b>Required Conditions</b> (Sec. 5.7.3.F)	Use of true color rendering lamps such as metal halide is preferred over high & low pressure sodium lamps			
<b>Min. Illumination</b> (Sec. 5.7.3.K)	Parking areas: 0.2 min			
	Loading & unloading areas: 0.4 min			
	Walkways: 0.2 min			
	Building entrances, frequent use: 1.0 min			
	Building entrances, infrequent use: 0.2 min			
<b>Max. Illumination adjacent to Non-Residential</b>	When site abuts a non-residential district, maximum illumination at			

Item	Required Code	Proposed	Meets Code	Comments
(Sec. 5.7.3.K)	the property line shall not exceed 1 foot candle			
<b>Cut off Angles</b> (Sec. 5.7.3.L)	When adjacent to residential districts: - All cut off angles of fixtures must be 90° - maximum illumination at the property line shall not exceed 0.5 foot candle			
<b>Building Code and Other Requirements</b>				
<b>Accessory Structures</b> (Sec. 4.19)	- Each accessory building shall meet all setback requirements for the zoning district in which the property is situated - Shall meet the façade ordinance standards	A gazebo is proposed on Unit 4. Additional information is not proposed at this time.	No?	<b><u>If Gazebo exceeds 200 square feet, which it appears to exceed, then it should comply with Façade requirements or seek necessary Façade deviations.</u></b>  Other structures such as flag poles, generators, smoke shelters, carports etc are subject to accessory structures regulations. Accessory structures other than flag poles should be located in rear yard only.
<b>Exterior Building Wall Façade Materials</b> (Sec. 5.15) (Sec. 3.27.1.G)	Façade Region: 1	Elevation drawings submitted for some of the units	Yes/No	<b>See Façade review for additional comments and further detail</b>
<b>Roof top equipment and wall mounted utility equipment</b> Sec. 4.19.2.E.ii	All roof top equipment must be screened and all wall mounted utility equipment must be enclosed and integrated into the design and color of the building	Elevations are not provided for all units	No	<u>This information can be provided at the time of Preliminary site plan that conforms to the code</u>
<b>Building Code</b>	Building exits must be connected to sidewalk system or parking lot.	Sidewalks not shown on the plans	No	<u>This information can be provided at the time of Preliminary site plan that conforms to the code</u>
<b>Design and Construction</b>	Land description, Sidwell number (metes and	Provided	Yes	

Item	Required Code	Proposed	Meets Code	Comments
<b>Standards Manual</b>	bounds for acreage parcel, lot number(s), Liber, and page for subdivisions).			
<b>General layout and dimension of proposed physical improvements</b>	Location of all existing and proposed buildings, proposed building heights, building layouts, (floor area in square feet), location of proposed parking and parking layout, streets and drives, and indicate square footage of pavement area (indicate public or private).	Not provided; Submittal is not complete	No	<b>Provide additional information as requested in all reviews to verify conformance and identify deviations.</b>
<b>Economic Impact</b>	<ul style="list-style-type: none"> <li>- Total cost of the proposed building &amp; site improvements</li> <li>- Number of anticipated jobs created (during construction &amp; after building is occupied, if known)</li> </ul>	200-300 temporary or permanent jobs  \$125 million development	Yes	Included in the cover letter under benefits to public
<b>Signage</b>  <b>See link below (Chapter 28, Code of Ordinances)</b>	<ul style="list-style-type: none"> <li>- Signage if proposed requires a permit.</li> <li>- <u>Signage is not regulated by the Planning Commission or Planning Division.</u></li> </ul>	The current site plan drawings indicate signage on some of the elevations provided	Yes/No	<b>Refer to comments provided by our Ordinance department on a separate packet</b>
<b>Property Address</b>	The applicant should contact the Building Division for an address prior to applying for a building permit.	One is not required at this time. Individual lot address would require separate addresses at a later time	<b>No</b>	<u>Submit address application after Final Site Plan approval.</u>
<b>Project and Street Naming Committee</b>	Some projects may need approval from the Street and Project Naming Committee.	The applicant requested a name change for Expo drive to Adell drive. It was approved by the Committee, but still would require City Council approval	Yes?	<b>The project name 'Adell Center' would require the Committee approval as well. Please submit an application for the project name approval.</b>  <b>Contact Hannah Smith at 248-347-0579 for more information on application and process</b>



Item	Required Code	Proposed	Meets Code	Comments
<b>Master Deed</b>	Master Deed should be approved for site condominiums prior to stamping set approval	The applicant is proposing to develop the property as a Site Condominium	No	<b>A site condominium approval is required prior to start working on a Master Deed</b>
<b>Easements</b>	- A 60 feet ROW with additional 10 feet access easement or 70 feet access easement is required for proposed Adell drive	Proposed lot line abuts the edge of sidewalk. A 50 feet access easement is provided	No	<b>Please show easements that conform to the code on the plans</b>

**NOTES:**

1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.
2. The section of the applicable ordinance or standard is indicated in parenthesis. Please refer to those sections in Article 3, 4, and 5 of the zoning ordinance for further details.
3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.

## ENGINEERING REVIEW



# PLAN REVIEW CENTER REPORT

June 27, 2018

## Engineering Review

Adell Center  
JZ 18-0024

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### Applicant

Orville Properties LLC

### Review Type

PRO Concept Plan

### Property Characteristics

- Site Location: West of Novi Road, North of Crescent Drive
- Site Size: 22.48 acres
- Plan Date: 06/04/2018
- Design Engineer: Greentech Engineering, Inc.

### Project Summary

- Construction of roads and utilities to serve multiple commercial developments on the site.
- Water service would be provided by two connections to existing City water main south of site at Crescent Boulevard, and west of the development for a looped system.
- Sanitary sewer service would be provided by an 8-inch extension sanitary sewer from an existing manhole located at the northeast end of Expo Center Drive, and by leads to existing 15-inch sanitary sewer southwest of the site.
- Storm water would be collected by a single storm sewer collection system. The site is located within the drainage area for the C&O Regional detention basin. Restricted discharge via the Walled Lake Branch of the Middle Rouge is proposed with bankfull storage provided in a proposed underground detention system.

### Recommendation

Approval of the PRO Concept Plan is NOT recommended, as described in item 1 of this review letter.

**Comments:**

The PRO Concept plan does not meet the general requirements of Chapter 11 of the Code of Ordinances, the Storm Water Management Ordinance and/or the Engineering Design Manual. The following items must be addressed prior to resubmittal:

1. Adell Drive as proposed would be a non-residential collector road and shall be designed in accordance with the Design and Construction Standards and Section 5.10 of the Zoning Ordinance. The pavement width shall be 36 feet back of curb to back of curb within a 70 foot wide public or private right-of-way or easement. A 60-foot private/public right-of-way width with two (2) five-foot easements is also acceptable. Staff does not support the requested deviation for a 50-foot easement with a roadway width of 30 feet back of curb to back of curb. Refer to the traffic review letter for additional details.

**Additional Comments (regarding PRO Concept deviations):**

2. Sidewalk width along a non-residential collector road shall be six (6) feet. Five (5) foot sidewalks as shown in the plans would require a variance from Design and Construction Standards.
3. As described in Section 7.4.2.C of the Engineering Design Manual, sidewalks shall be placed 1 foot inside the right-of-way/easement line, or a minimum of fifteen (15) feet shall be provided from back of curb to outside edge of sidewalk on private streets. The proposed road cross section shows 10 feet from back of curb to outside edge of the five (5) foot sidewalk where fifteen (15) feet is required.

**Additional Comments (to be addressed upon Preliminary Site Plan submittal):**

4. A right-of-way permit will be required from the City of Novi for work in the Crescent Boulevard and Expo Center Drive right-of-way.
5. Refer to Section 26.5-35 for requirements for private roadways:
  - a. A private maintenance covenant is required for any private street.
  - b. Per Section 26.5-35(h), a statement is required on any plan containing a private street with the following language: "City of Novi has no responsibility to improve or maintain the private streets contained within or private streets providing access to the property described in this plan".
6. Soil borings shall be provided for a preliminary review of the constructability of the proposed development (roads, underground detention, etc.). Borings identifying soil types, and groundwater elevation should be provided at the time of Preliminary Site plan.
7. A letter from either the applicant or the applicant's engineer must be submitted with the Preliminary Site Plan submittal highlighting the changes made to the plans addressing each of the comments in this review.

Utilities

8. Existing water main west of the site is 10-inch in size in City GIS records, not 12-inch as called out on the plans.
9. Minimum water main size on the site shall be 12-inch to serve the development.
10. Fire hydrants shall be provided as required by the Fire Marshal, generally at no more than five hundred (500) foot intervals and such that no part of a building is more than three hundred (300) feet of hose length from a hydrant.
11. Valves shall be provided to limit pipe runs to a maximum of eight hundred (800) feet between valves.
12. Remove existing water main shown to be removed during demolition from the utility plan sheets.
13. Confirm size and location of sanitary sewer and sewer easement to the southwest to determine if any off site easements are needed for proposed sanitary sewer leads to existing main.
14. Each building is required to have a unique sanitary sewer monitoring manhole, within a dedicated 20-foot wide access easement to the monitoring manhole from the public right-of-way (rather than a public sanitary sewer easement).
15. If required, proposed Unit 4 sewer lead crossing the Walled Lake Branch of the Middle Rouge would require MDEQ approval and permitting.
16. Existing 15-inch sanitary sewer extending south from the existing manhole southwest of Crescent Boulevard and Expo Center Drive should be shown on the plans.

Paving & Grading & Floodplain

17. Provide existing topography and 2-foot contours extending at least 100 feet past the site boundary. Any off-site drainage entering this site shall be identified.
18. Site grading shall be limited to 1V:4H (25-percent), excluding landscaping berms.
19. Provide spot grades along property lines adjacent to perimeter curb line to demonstrate that site drainage is self-contained.
20. Show the limits of the 100-year floodplain and floodway and Base Flood Elevations for the Walled Lake Branch of the Middle Rouge River.
21. A City of Novi floodplain use permit may be required for any proposed floodplain impact. An MDEQ floodplain use permit may also be required for discharge to the Middle Rouge. The applicant will need to confirm any required MDEQ permitting.

### Storm Sewer

22. A minimum cover depth of 3 feet shall be maintained over all storm sewers.
23. An easement is required over the storm sewer accepting and conveying off-site drainage.

### Storm Water Management Plan

24. The Storm Water Management Plan (SWMP) shall comply with the Storm Water Ordinance and Chapter 5 of the Engineering Design Manual (refer to the runoff coefficients, 1V:4H allowable basin slopes, etc.).
25. The conceptual storm water management plan indicates underground storage in three locations sized for bankfull volume. Provide a cross-section of each underground detention system showing critical elevations (low water, and bankfull high water, and pavement elevation). Ensure at least 1 ft. of freeboard between the high water elevation and the subgrade elevation under the pavement. Also provide explanation and clarification of how the underground detention storage and discharge works relative to the adjacent floodway and flood plain.
26. Label the 100 year flood way and flood plain and show the Base Flood Elevations from the current Flood Insurance Rate Map panel on the plan set.
27. Indicate the proposed location of each first flush storm water quality treatment unit for each building unit and the roadway. Each unit will require its own Storm Drain Facility Maintenance Easement Agreement. Clarify where and how the first flush runoff from the roadway is treated.
28. Provide supporting calculations for runoff coefficient determination. A runoff coefficient of 0.35 shall be used for all turf grass lawns (mowed lawns).
29. Identify the location of each underground detention outlet control structure and indicate the invert elevation where discharging to the Walled Lake Branch of the Middle Rouge. MDEQ permitting will be required for any new outlet locations.
30. An adequate maintenance access route to the outlet structures and any other pretreatment structures shall be provided (15 feet wide, maximum slope of 1V:5H, and able to withstand the passage of heavy equipment). The access route(s) must not conflict with proposed landscaping.
31. A Storm Drain Facility Maintenance Easement Agreement and access easement the outlet structures will be required for the underground detention units.
32. Indicate the overland routing or storm sewer bypass designed for the event that the underground system cannot accept flow.
33. Provide a soil boring in the vicinity of the proposed underground detention system to determine bearing capacity and the high water elevation of the groundwater table.
34. The underground detention system(s) shall be kept outside the influence of any planting areas.

Off-Site Easements

35. Any required off-site easements must be executed prior to final approval of the plans. Drafts shall be submitted at the time of the Preliminary Site Plan submittal.

**The following must be provided at the time of Preliminary Site Plan submittal:**

36. A letter from either the applicant or the applicant's engineer must be submitted with the Preliminary Site Plan highlighting the changes made to the plans addressing each of the comments listed above and indicating the revised sheets involved.

**The following must be submitted at the time of Final Site Plan submittal:**

37. A letter from either the applicant or the applicant's engineer must be submitted with the Final Site Plan highlighting the changes made to the plans addressing each of the comments listed above and indicating the revised sheets involved.
38. An itemized construction cost estimate must be submitted to the Community Development Department at the time of Final Site Plan submittal for the determination of plan review and construction inspection fees. This estimate should only include the civil site work and not any costs associated with construction of the building or any demolition work. **The cost estimate must be itemized** for each utility (water, sanitary, storm sewer), on-site paving, right-of-way paving (including proposed right-of-way), grading, and the storm water basin (basin construction, control structure, pretreatment structure and restoration).
39. Draft copies of any off-site utility easements, a recent title search, and legal escrow funds must be submitted to the Community Development Department for review and approved by the Engineering Division and the City Attorney prior to being executed.

**The following must be submitted at the time of Stamping Set submittal:**

40. A draft copy of the maintenance agreement for the storm water facilities, as outlined in the Storm Water Management Ordinance, must be submitted to the Community Development Department. Once the form of the agreement is approved, this agreement must be approved by City Council and shall be recorded in the office of the Oakland County Register of Deeds.
41. A draft copy of the 20-foot wide easement for the water main to be constructed on the site must be submitted to the Community Development Department.
42. A draft copy of the 20-foot wide easement for the sanitary sewer to be constructed on the site must be submitted to the Community Development Department.
43. A 20-foot wide drainage easement where off-site drainage is conveyed via storm sewer within the development.
44. Executed copies of reviewed and approved off-site easements, if applicable.

To the extent this review letter addresses items and requirements that require the approval of or a permit from an agency or entity other than the City, this review shall not be considered an indication or statement that such approvals or permits will be issued.

Please contact Darcy Rechten at (248) 735-5695 with any questions.

  
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Darcy N. Rechten, P.E.

cc: Sri Komaragiri, Community Development  
Theresa Bridges, Engineering  
George Melistas, Engineering



# MEMORANDUM



**TO:** BARBARA MCBETH, CITY PLANNER  
**FROM:** DARCY RECHTIEN, PLAN REVIEW ENGINEER *DR*  
**SUBJECT:** REVIEW OF REZONING IMPACT ON PUBLIC UTILITIES  
REZONING 18.724, ADELL CENTER  
**DATE:** JUNE 28, 2018

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The Engineering Division has reviewed a rezoning request for the 22.48 acres located south of Interstate 96, west of Expo Center Drive, north and west of Crescent Boulevard. The applicant is requesting to rezone 22.48 acres to Town Center from the existing zoning of EXPO. The Master Plan for Land Use indicates a future land use of Office Research Technology. It should be noted that the site is within the C&O Regional Detention drainage area, with only bankfull storm water detention storage required on the site.

#### Utility Demands

A residential equivalent unit (REU) equates to the utility demand from one single family home. The future land use zoning of Office Research Technology would result in roughly 4.0 REUs per acre for office development with associated bankfull storm water detention storage.

If the area were redeveloped under the current EXPO zoning, demand on the utilities for the site could range between 1.8 REUs for retail to 4.0 REUs per acre for office/convention center development, and as high as 11.0 REUs per acre for sit-down restaurants.

The proposed Town Center zoning permits similar uses as the EXPO zoning, with similar utility demands. The applicant intends to propose a mixed development including hotels and restaurants along with recreation and entertainment venues. The development is projected to result in utility demands at or near 11.0 REUs per acre, similar to what could be developed with the current EXPO zoning.

#### Water System

The site is located within the Intermediate Pressure District. Water service is currently available from a ten-inch water main west of the site, and existing eight-inch water main along the south side of Crescent Boulevard. The proposed rezoning would have minimal impact on available capacity, pressure and flows in the City's water distribution system.

#### Sanitary Sewer

The site is located within the West Oaks Sewer District. Sanitary service is available by connection to an existing eight-inch sanitary sewer in Expo Center Drive, and existing fifteen-inch sewer running along the south and west boundaries of the proposed development. The proposed rezoning is not anticipated to have an apparent impact on the capacity of the downstream sanitary sewer within the City's infrastructure.

Summary

The requested rezoning to Town Center will result in utility demands that are approximately equal to the utility demand if the property were to be redeveloped under the current EXPO zoning. The requested rezoning and associated development projects utility demands that are higher than those anticipated with the future land use zone of Office Research Technology, but very similar utility demands to the current zoning on the parcel. Therefore, we conclude that the rezoning would have a minimal impact on the public utilities. It should be noted, however, that any increase in sanitary flow may require the acquisition of additional capacity downstream of Eight Mile Road at the time of build-out.

cc: George Melistas; Engineering Senior Manager  
Ben Croy, P.E.; Water & Sewer Senior Manager

## LANDSCAPE REVIEW



# PLAN REVIEW CENTER REPORT

June 25, 2018

## PRO Concept Site Plan

Adell Center

### Review Type

Rezoning Concept Plan Landscape Review

### Job #

JZ 18-0024

### Property Characteristics:

- Site Location: Northwest of Novi Road/Crescent Drive.
- Site Zoning: Expo – Proposed rezoning to TC with PRO
- Adjacent Zoning: North: I-96, East: TC, South: TC/I-1, West: I-2
- Plan Date: April 30, 2018

### Recommendation:

This concept plan is **recommended for approval**. The landscaping along Adell Drive has adopted some of the recommendation from the Town Center Study to provide a link with adjacent sites in the Town Center District. This is appreciated. Some revisions are necessary to meet all ordinance requirements, but most don't need to be considered as deviations. They can be handled as part of the site plan approval process.

Please note that this approval is just based on the plans submitted for the overall development, as no landscape plans for the individual units were provided except for the greenbelt plantings. As such, it is assumed that each unit's other landscaping ((parking lot interior and perimeter), building foundation, and loading zone screening) will meet all landscaping requirements. If any landscape waivers are requested for a unit, that unit's site plans will need to be submitted for consideration by the Planning Commission.

### **LANDSCAPE DEVIATIONS:**

Units 2, 4, 7 have parking bays greater than 15 contiguous spaces. *This deviation is not supported by staff.* If islands 10 feet across (at back of curb) and 200sf in area minimum are added to decrease the bays to no more than 15 contiguous spaces, and at least 1 deciduous canopy tree is planted in each of those islands, the deviations can be avoided.

### Ordinance Considerations:

This project was reviewed for conformance with Chapter 37: Woodland Protection, Zoning Article 5.5 Landscape Standards, the Landscape Design Manual and any other applicable provisions of the Zoning Ordinance. Items in **bold** below and on the accompanying Landscape Chart must be addressed and incorporated as part of the Preliminary Site Plan submittal.

Underlined items must be addressed and incorporated as part of the Final Site Plan submittal.

Please follow guidelines of the Zoning Ordinance and Landscape Design Guidelines. This review is a summary and not intended to substitute for any Ordinance.

Existing Soils (Preliminary Site Plan checklist #10, #17)

Provided

Existing and proposed overhead and underground utilities, including hydrants.(LDM 2.e.(4))

Provided

Existing Trees (Sec 37 Woodland Protection, Preliminary Site Plan checklist #17 and LDM 2.3 (2))

1. A tree survey is provided.
2. It appears that all non-regulated trees, north of the stream, will be removed. No trees within the regulated woodland are shown as being removed.

Proposed topography. 2' contour minimum (LDM 2.e.(1))

1. Proposed elevations for Adell Drive are provided, and some conceptual berms along Adell Drive are shown on the landscape plans.
2. A note indicates that a 36" berm shall be provided along the I-96 frontage. **That berm should undulate in height, with a minimum height of 36"**. No berm is required for Unit 1, where the building fronts directly on the I-96 right-of-way or in front of the sign at the east end of the site.

Street Tree Requirements (Zoning Sec. 5.5.3.E.i.c and LDM 1.d.)

1. Street trees are not required along I-96, or in the TC district.
2. If the area between the curb and sidewalk could be widened from 5 feet to 8 feet, it would provide better growing conditions for the trees, and would allow larger trees to be planted there.
3. Some of the required greenbelt trees along Adell Drive are proposed as street trees. This is appreciated. **As the area provided between the curb and sidewalk is only 5 feet, the guidelines in the Landscape Design Manual, Section 2.a.(1) should be followed, even though it is not a residential subdivision, so appropriate trees that can perform better in the restricted space provided, and provide a nice aesthetic for the development.**

Adjacent to Public Rights-of-Way – Berm (Wall) & Buffer (Zoning Sec. 5.5.3.B.ii and iii)

I-96.

1. A 36" berm is noted as being proposed. That berm should be provided along all parking and vehicular access areas and have undulations with a minimum height of 36".
2. The required 20 foot minimum greenbelt for areas adjacent to parking is provided along the entire I-96 frontage.
3. An acceptable number of canopy and subcanopy trees is provided. See the landscape chart for calculations.

Adell Drive.

1. A mix of berm, 2.5' tall brick wall and 2.5' brick pilasters and ornamental fencing, as requested in the Town Center Study, is provided along both sides of Adell Drive.
2. The required 20 foot minimum greenbelt for areas adjacent to parking is provided along the entire I-96 frontage. The greenbelt starts at the back edge of the sidewalk, however, as the unit lines are drawn to the center of the road, not 1' behind the sidewalk as is typically the case.
3. An acceptable number of canopy and subcanopy trees is provided. See the landscape chart for calculations.

Corner Clearance (Zoning Sec 5.9)

Provided.

Parking Lot Landscaping – interior and perimeter (Zoning Sec. 5.5.3.C.)

1. No landscape plans for the units are provided.
2. **The site plans for each unit must conform to the ordinance requirements or the unit's site plans must be taken to the Planning Commission for whatever landscape waivers are requested.**

Snow Deposit (LDM.2.q.)

1. Snow deposit areas are shown on the site, along with a note that snow will be deposited along the edge of the road.

2. As the area between the curb and the sidewalk is only 5 feet, snow should not be deposited there as the trees planted there may be damaged by the plows, piled snow or salt. **Please remove that note and add one stating that the snow should not be deposited in the landscape strip along the drive.**

Building Foundation Landscape Requirements (Sect 5.5.3.D)

No building foundation landscaping or landscape areas are clearly indicated for any of the buildings. The landscaping must comply with the ordinance or the unit(s) with non-compliant foundation landscaping will need to go to the Planning Commission to seek whatever landscape waivers are required.

Transformer/Utility Box Screening (Zoning Sec 5.5.3.D.)

**When utility box locations are provided, required screening should be added to plan and plant list.**

Storm Basin Landscape (Zoning Sec 5.5.3.E.iv and LDM 1.d.(3))

1. As only underground storm water detention is proposed, no detention landscaping is required.
2. If any surface level detention is required/proposed, the required detention basin landscaping must be provided.

Plant List (LDM 2.h. and t.)

1. Not provided, but not required for a concept plan unless species that don't conform to the woodland replacement chart or prohibited species are proposed.
2. Since no list is proposed, it is assumed that the species will conform to city requirements.

Planting Notations and Details (LDM)

Provided

Irrigation (LDM 1.a.(1)(e) and 2.s)

Irrigation plan for landscaped areas or an alternative plan for ensuring that plants get the water required for establishment and long-term survival is required for Final Site Plans.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5621 or rmeader [rmeader@cityofnovi.org](mailto:rmeader@cityofnovi.org).



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Rick Meader – Landscape Architect

## LANDSCAPE REVIEW SUMMARY CHART – PRO Concept Plan

**Review Date:** June 25, 2018  
**Project Name:** JZ 18-24: Adell Center  
**Plan Date:** April 30, 2018  
**Prepared by:** Rick Meader, Landscape Architect E-mail: [rmeader@cityofnovi.org](mailto:rmeader@cityofnovi.org);  
 Phone: (248) 735-5621

Items in **Bold** need to be addressed by the applicant before approval of the Preliminary Site Plan. Underlined items need to be addressed for Final Site Plan.

NOTE: THE COMMENTS BELOW PERTAIN TO THE OVERALL DEVELOPMENT AND THE CENTRAL DRIVE. FINAL REVIEWS OF EACH UNIT WILL BE REQUIRED WHEN THEIR LANDSCAPE PLANS ARE PROVIDED.

### DEVIATIONS FROM REQUIREMENTS NOTED:

Units 2, 4, 7 have parking bays with more than 15 contiguous spaces shown on the overall concept plan.

### LANDSCAPE INFORMATION NOT PROVIDED FOR ANY OF THE INDIVIDUAL UNITS:

1. Parking lot landscaping calculations and plantings.
2. Building Foundation landscaping calculations and plantings.
3. Loading zones and required screening for them.
4. Utility box screening.

Since this information is not provided, and the applicant is requesting approval for the entire site at this time, each individual unit must meet all landscaping requirements applicable to the zoning and use. If any landscape waivers are required by a unit's landscape plans, that project will have to be taken to the Planning Commission for approval of those waivers.

Item	Required	Proposed	Meets Code	Comments
<b>Landscape Plan Requirements (LDM (2))</b>				
<b>Landscape Plan</b> <i>(Zoning Sec 5.5.2, LDM 2.e.)</i>	<ul style="list-style-type: none"> <li>▪ New commercial or residential developments</li> <li>▪ Addition to existing building greater than 25% increase in overall footage or 400 SF whichever is less.</li> <li>▪ 1"=20' minimum with proper North. Variations from this scale can be approved by LA</li> <li>▪ Consistent with plans throughout set</li> </ul>	Overall plan: 1"=60' Focal areas: 1"=20'	Yes	<ol style="list-style-type: none"> <li>1. Scale 1"=60'</li> <li>2. <b>Please use a 1"=20', minimum scale for the building foundation planting designs when they are provided.</b></li> </ol>
<b>Project Information</b> <i>(LDM 2.d.)</i>	Name and Address	Yes	Yes	
<b>Owner/Developer Contact Information</b> <i>(LDM 2.a.)</i>	Name, address and telephone number of the owner and developer or	Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
	association			
<b>Landscape Architect contact information</b> (LDM 2.b.)	Name, Address and telephone number of RLA	Yes	Yes	
<b>Sealed by LA.</b> (LDM 2.g.)	Requires original signature	Yes	Yes	<u>Need for Final Site Plans</u>
<b>Miss Dig Note</b> (800) 482-7171 (LDM.3.a.(8))	Show on all plan sheets	Yes	Yes	
<b>Zoning</b> (LDM 2.f.)	Include all adjacent zoning	<u>Parcel:</u> EXPO Rezone to TC w/PRO <u>North:</u> I-96 <u>East:</u> TC <u>South:</u> TC/I-1 <u>West:</u> I-2	Yes	
<b>Survey information</b> (LDM 2.c.)	<ul style="list-style-type: none"> <li>▪ Legal description or boundary line survey</li> <li>▪ Existing topography</li> </ul>	Sheets 6-9	Yes	
<b>Existing plant material</b> <b>Existing woodlands or wetlands</b> (LDM 2.e.(2))	<ul style="list-style-type: none"> <li>▪ Show location type and size. Label to be saved or removed.</li> <li>▪ Plan shall state if none exists.</li> </ul>	Sheets 20-21	Yes	No regulated trees are shown as being removed.
<b>Soil types</b> (LDM.2.r.)	<ul style="list-style-type: none"> <li>▪ As determined by Soils survey of Oakland county</li> <li>▪ Show types, boundaries</li> </ul>	Sheet 5	Yes	
<b>Existing and proposed improvements</b> (LDM 2.e.(4))	Existing and proposed buildings, easements, parking spaces, vehicular use areas, and R.O.W	Yes	Yes	
<b>Existing and proposed utilities</b> (LDM 2.e.(4))	Overhead and underground utilities, including hydrants	Proposed storm sewer, water and sanitary are provided.	Yes	<b>Please clearly show and call out any existing overhead lines on landscape plan or add note stating that there will be no overhead utilities on the site.</b>
<b>Proposed grading. 2' contour minimum</b> (LDM 2.e.(1))	Provide proposed contours at 2' interval	<ul style="list-style-type: none"> <li>• Spot elevations on Sheet 14.</li> <li>• Some berm contours showing a 3' berm are provided along Adell Drive.</li> <li>• Note indicates a 36" berm will be</li> </ul>	Yes	<ol style="list-style-type: none"> <li>1. No berms are required along Adell Drive so that height can be less than 36" if desired.</li> <li>2. The berm along I-96 needs to undulate in height, with a 36" minimum height to screen the vehicles</li> </ol>



Item	Required	Proposed	Meets Code	Comments
		provided along I-96 frontage.		from view of I-96.
<b>Snow deposit</b> (LDM.2.q.)	Show snow deposit areas on plan	Yes	Yes/No	<ol style="list-style-type: none"> <li>1. Please indicate snow deposit areas that won't harm landscaping.</li> <li>2. Some areas indicated are good, but with only a 5' wide strip between the curb and sidewalk, and trees placed midway in that area, snow will damage the trees planted there so it shouldn't be used for snow deposits.</li> <li>3. The snow should be piled in locations where the landscaping won't be damaged.</li> </ol>
<b>LANDSCAPING REQUIREMENTS</b>				
<b>Parking Area Landscape Requirements LDM 1.c. &amp; Calculations (LDM 2.o.)</b>				
<b>General requirements</b> (LDM 1.c.)	<ul style="list-style-type: none"> <li>▪ Clear sight distance within parking islands</li> <li>▪ No evergreen trees</li> </ul>	No landscaping shown yet	TBD	
<b>Name, type and number of ground cover</b> (LDM 1.c.(5))	As proposed on planting islands	Lawn is indicated to cove area between sidewalk and curb.	Yes/No	Please indicate proposed ground covers on all areas of plan.
<b>General (Zoning Sec 5.5.3.C.ii)</b>				
<b>Parking lot Islands</b> (a, b. i)	<ul style="list-style-type: none"> <li>▪ A minimum of 200 SF to qualify</li> <li>▪ A minimum of 200sf unpaved area per tree planted in an island</li> <li>▪ 6" curbs</li> <li>▪ Islands minimum width 10' BOC to BOC</li> </ul>	Conceptual parking lot islands are shown on overall plan, but individual lot plans were not provided to verify their sizes.	TBD	<ol style="list-style-type: none"> <li>1. Please label SF of individual islands' unpaved area (should not include sidewalks) on lot landscape plans.</li> <li>2. Please dimension widths of islands on those plans.</li> <li>3. Please increase widths and/or areas of islands as necessary to meet the requirements.</li> </ol>
<b>Curbs and Parking stall reduction</b> (c)	Parking stall can be reduced to 17' and the curb to 4" adjacent to a	Conceptual parking lot islands are shown on	TBD	Please dimension parking spaces on lots' plans.

Item	Required	Proposed	Meets Code	Comments
	sidewalk of minimum 7 ft.	overall plan, but individual lot plans were not provided to verify their sizes.		
<b>Contiguous space limit (i)</b>	Maximum of 15 contiguous spaces	Several units have bays greater than 15 spaces long (Units 2, 4, 7)	No	<ol style="list-style-type: none"> <li>1. Please add interior islands to break up long bays and enlarge endcap islands as necessary to support at least 1 canopy tree.</li> <li>2. Each endcap island and interior island needs to have at least 1 deciduous canopy tree planted in it.</li> </ol>
<b>Plantings around Fire Hydrant (d)</b>	No plantings with matured height greater than 12' within 10 ft. of fire hydrants or utility structures, or 5' from underground utility lines.	Some proposed trees appear to be closer than 10' from a utility structure.	No	<ol style="list-style-type: none"> <li>1. Please provide proper spacing from all utility lines and structures.</li> <li>2. To assist contractors, please add a note to the plans stating that all trees are to be at least 10 feet from hydrants or utility structures.</li> <li>3. Please lay out utilities to remove conflicts with trees to be planted in interior islands, and parking lot perimeters.</li> </ol>
<b>Landscaped area (g)</b>	Areas not dedicated to parking use or driveways exceeding 100 sq. ft. shall be landscaped	TBD	TBD	This will be verified when the individual units' landscape plans are provided.
<b>Clear Zones (LDM 2.3.(5))</b>	25 ft corner clearance required. Refer to Zoning Section 5.5.9	Clear zones are provided and trees are not located within them.	Yes	
<b>Category 1: For OS-1, OS-2, OSC, OST, B-1, B-2, B-3, NCC, EXPO, FS, TC, TC-1, RC, Special Land Use or non-residential use in any R district (Zoning Sec 5.5.3.C.iii)</b>				
A = Total square footage of vehicular use areas up to 50,000sf x 7.5%	<ul style="list-style-type: none"> <li>• <math>A = x \text{ sf} * 7.5\% = A \text{ sf}</math></li> <li>• <math>x * 7.5\% = A \text{ sf}</math></li> </ul>	TBD	TBD	Please show parking lot Vehicular Use Areas and areas for each unit on their landscape plans.
B = Total square footage of additional	<ul style="list-style-type: none"> <li>• <math>B = x \text{ sf} * 1\% = B \text{ sf}</math></li> <li>• <math>(xxx - 50000) * 1\% = B</math></li> </ul>	TBD	TBD	Please show parking lot Vehicular Use Areas

Item	Required	Proposed	Meets Code	Comments
paved vehicular use areas (not including A or B) over 50,000 SF) x 1 %	sf			and areas for each unit on their landscape plans.
<b>Category 2: For: I-1 and I-2 (Zoning Sec 5.5.3.C.iii)</b>				
A. = Total square footage of vehicular use area up to 50,000 sf x 5%	$A = x \text{ sf} * 5\% = A \text{ sf}$	NA		
B = Total square footage of additional paved vehicular use areas over 50,000 SF x 0.5%	$B = 0.5\% \times 0 \text{ sf} = B \text{ SF}$	NA		
<b>All Categories</b>				
C = A+B Total square footage of landscaped islands	$xxx + xxx = xx \text{ SF}$	xxx sf	TBD	<ol style="list-style-type: none"> <li>1. Please show calculations for each unit on their plans.</li> <li>2. Please provide required landscape islands for each unit on their plans.</li> <li>3. Please label each island with its area in SF.</li> </ol>
D = C/200 Number of canopy trees required	$xx/200 = xx \text{ Trees}$	No trees	TBD	<ol style="list-style-type: none"> <li>1. Please show calculations for each unit on their plans.</li> <li>2. Please provide required trees for each unit.</li> <li>3. Please uniquely label each tree as a parking lot tree.</li> </ol>
<b>Perimeter Green space</b>	1 Canopy tree per 35 lf	No perimeter trees are proposed except along the I-96 frontage.	TBD	<ol style="list-style-type: none"> <li>1. Please show calculations for each unit on their landscape plans.</li> <li>2. Please provide required perimeter trees for each unit.</li> <li>3. Please label each tree as a parking lot tree.</li> <li>4. Please indicate which trees are being double-counted as perimeter and greenbelt canopy</li> </ol>

Item	Required	Proposed	Meets Code	Comments
				trees.
<b>Parking land banked</b>	NA	None		
<b>Berms, Walls and ROW Planting Requirements</b>				
<b>Berms</b>				
<ul style="list-style-type: none"> <li>▪ All berms shall have a maximum slope of 33%. Gradual slopes are encouraged. Show 1ft. contours</li> <li>▪ Berm should be located on lot line except in conflict with utilities.</li> <li>▪ Berms should be constructed with 6" of top soil.</li> </ul>				
<b>Residential Adjacent to Non-residential (Sec 5.5.3.A) &amp; (LDM 1.a)</b>				
<b>Berm requirements (Zoning Sec 5.5.A)</b>	Site does not abut residential so no berm is required for this purpose.	None	Yes	
<b>Planting requirements (LDM 1.a.)</b>	LDM Novi Street Tree List	NA		
<b>Adjacent to Public Rights-of-Way (Sec 5.5.B) and (LDM 1.b)</b>				
<b>Berm requirements (Zoning Sec 3.27.1.D and 5.5.3.B.(5), LDM 1.b)</b>	<ul style="list-style-type: none"> <li>▪ <b>Surface parking lots within the TC-1 district need to be screened from the right-of-way line by either:</b> <ul style="list-style-type: none"> <li>○ a 2.5' ornamental brick wall OR</li> <li>○ semi-transparent screening such as a brick pilaster with metal decorative fence,OR</li> <li>○ a landscaped berm.</li> </ul> </li> <li>▪ <b>In addition, the Landscape Design Manual 1.b (2)(c) requires that sites adjacent to freeways achieve substantial aesthetic enhancement and diminution of paving and parking views along these corridors.</b></li> </ul>	<ul style="list-style-type: none"> <li>▪ A 36" berm with a mix of canopy and subcanopy trees is proposed in a note along I-96.</li> <li>▪ A 2.5' masonry wall and pier and fence to match the Crescent Boulevard fence/wall is proposed along most of Adell Boulevard.</li> <li>▪ A berm is proposed for areas where a wall/fence is not proposed. Landscaping on the berms is not indicated.</li> </ul>	Yes	Please provide conceptual landscaping for what would be on the berms along Adell Drive.
<b>Cross-Section of Berms (LDM 2.j)</b>				
Slope, height and width	<ul style="list-style-type: none"> <li>▪ Label contour lines</li> <li>▪ Maximum 33%</li> <li>▪ Min. 3 feet flat horizontal area</li> <li>▪ Minimum 3 feet high</li> <li>▪ Constructed of loam with 6' top layer of</li> </ul>	Detail for Adell Drive is provided.	Yes/No	Please provide berm cross section details for the I-96 berm.

Item	Required	Proposed	Meets Code	Comments
	topsoil.			
Type of Ground Cover		NA		
Setbacks from Utilities	Overhead utility lines and 15 ft. setback from edge of utility or 20 ft. setback from closest pole	NA		<ol style="list-style-type: none"> <li>1. Please clearly indicate any overhead lines to remain on the site.</li> <li>2. If there are none, please add a note to that effect.</li> </ol>
<b>Walls (LDM 2.k &amp; Zoning Sec 5.5.3.vi)</b>				
Material, height and type of construction footing	Freestanding walls should have brick or stone exterior with masonry or concrete interior		TBD	Please provide wall elevations and provide construction details.
Walls greater than 3 ½ ft. should be designed and sealed by an Engineer		No details provided	TBD	
<b>ROW Landscape Screening Requirements (Sec 5.5.3.B. ii) – USE TC Requirements</b>				
Greenbelt width (2)(3) (5)	Adjacent to Prking: 20 ft. Not adjacent to Pkg: 0 ft	<ul style="list-style-type: none"> <li>▪ 20 ft along I-96 border</li> <li>▪ 20 ft from back of sidewalk along Adell Drive</li> </ul>	TBD	
Min. berm crest width	3 ft along I-96		TBD	
Minimum berm ht (9)	3 ft along I-96	Note indicates that 36" berm will be provided.	TBD	Minimum height of undulating berm should be 36".
3' wall	(4)(7)	Sign walls		
Canopy deciduous or large evergreen trees Notes (1) (10)	<ul style="list-style-type: none"> <li>▪ Parking: 1 tree per 25 lf</li> <li>▪ Not adj to Pkg: 1 per 30 ft</li> </ul> <p><b><u>I-96:</u></b>                      Adj to pkg: 1030 lf/25 = 41 trees                      Not adj to pkg: 420 lf/30 = 14 trees                      Total: 55 trees</p> <p><b><u>Adell Drive:</u></b>                      Adj to pkg: 2087 lf/25 = 83 trees                      Not adj to pkg: 80 lf/30 = 3 trees  <ul style="list-style-type: none"> <li>▪ Total: 86 trees</li> </ul> </p>	<p><b><u>I-96:</u></b>                      31 trees</p> <p><b><u>Adell Dr:</u></b>                      59 trees</p>	When totaled with the proposed subcanopy trees, the required landscaping is met.	<ol style="list-style-type: none"> <li>1. In TC, either the large tree or subcanopy tree requirement must be met but not both.</li> <li>2. The screening along I-96 may have to be denser to achieve the objective of screening the parking from the ROW.</li> </ol>
Sub-canopy deciduous trees Notes (2)(10)	<b><u>I-96:</u></b> Adj to pkg: 1030 lf/15 = 69 trees	<b><u>I-96:</u></b> 35 trees <b><u>Adell Dr:</u></b>		1. In TC, either the large tree or subcanopy tree requirement

Item	Required	Proposed	Meets Code	Comments
	Not adj to pkg: 420 lf/20 = 21 trees Total: 90 trees <u>Adell Drive:</u> Adj to pkg: 2087 lf/15 = 83 trees Not adj to pkg: 80 lf/20 = 3 trees Total: 86 trees	67 trees		must be met but not both. 2. The screening along I-96 may have to be denser to achieve the objective of screening the parking from the ROW.
<b>Canopy deciduous trees in area between sidewalk and curb</b> <i>(Novi Street Tree List)</i> <i>(Zoning Sec 6.3 Site Condominiums, LDM2.)</i>	In the TC district, street trees are not required.	59 greenbelt trees are located along Adell Drive as street trees.	Yes/No	1. Please widen space between sidewalk and back of curb from 5 ft to 8 ft, per the Landscape Design Manual recommendations for large trees, to provide better chances for growth and survival for the trees. 2. Even though it is not a residential subdivision, if the area is not widened, the guidelines for street tree plantings in Landscape Design Manual Section 2.a.(1) should be followed to give the street trees a better chance at long-term, healthy survival.
<b>Non-Residential Zoning Sec 5.5.3.E.iii &amp; LDM 1.d (2)</b> Refer to Planting in ROW, building foundation landscape, parking lot landscaping and LDM				
<b>Screening of outdoor storage, loading/unloading</b> <i>(Zoning Sec. 3.14, 3.15, 4.55, 4.56, 5.5)</i>		Loading zones and loading zone screening is not proposed.	No	1. Please show loading zones for each unit on their plans. 2. Please provide required screening for each unit.
<b>Transformers/Utility boxes</b> <i>(LDM 1.e from 1 through 5)</i>	<ul style="list-style-type: none"> <li>▪ A minimum of 2ft. separation between box and the plants</li> <li>▪ Ground cover below 4" is allowed up to pad.</li> <li>▪ No plant materials within 8 ft. from the doors</li> </ul>	No	No	1. When transformer locations are finalized, screening shrubs per standard detail are required. 2. Please add detail to plans.

Item	Required	Proposed	Meets Code	Comments
<b>Building Foundation Landscape Requirements (Sec 5.5.3.D)</b>				
<b>Interior site landscaping SF</b>	<ul style="list-style-type: none"> <li>▪ Equals to entire perimeter of the building x 8 with a minimum width of 4 ft.</li> <li>▪ At least 75% of building should be landscaped – ideally all but paved points of entry will be landscaped.</li> <li>▪ Patios are to be landscaped.</li> </ul>	None	TBD	<ol style="list-style-type: none"> <li>1. Please show calculations for each unit on their plans.</li> <li>2. Please provide required plantings for each unit.</li> </ol>
<i>Zoning Sec 5.5.3.D.ii. All items from (b) to (e)</i>	If visible from public street a minimum of 60% of the exterior building perimeter facing Adell Drive and/or I-96 should be covered in green space.	None	TBD	<b>Foundation landscaping to be reviewed for each building when landscape plans are submitted for that unit.</b>
<b>Detention/Retention Basin Requirements (Sec. 5.5.3.E.iv)</b>				
<b>Planting requirements (Sec. 5.5.3.E.iv)</b>	<ul style="list-style-type: none"> <li>▪ Clusters of large native shrubs shall cover 70-75% of the basin rim area</li> <li>▪ 10" to 14" tall grass along sides of basin</li> <li>▪ Refer to wetland for basin mix</li> </ul>	None – only underground detention is proposed.		If any above-ground detention is required, it shall be landscaped per the requirement.
<b>Phragmites Control (Sec 5.5.6.C)</b>	<ul style="list-style-type: none"> <li>▪ Any and all populations of <i>Phragmites australis</i> on site shall be included on tree survey.</li> <li>▪ Treat populations per MDEQ guidelines and requirements to eradicate the weed from the site.</li> </ul>	None indicated	TBD	<ol style="list-style-type: none"> <li>1. Please survey the site for any populations of <i>Phragmites australis</i> and submit plans for its complete removal.</li> <li>2. If none is found, please indicate that on the survey.</li> </ol>
<b>LANDSCAPING NOTES, DETAILS AND GENERAL REQUIREMENTS</b>				
<b>Landscape Notes – Utilize City of Novi Standard Notes</b>				
<b>Installation date (LDM 2.i. &amp; Zoning Sec 5.5.5.B)</b>	Provide intended date	Between Mar 15 and Nov 15, 2019	Yes	
<b>Maintenance &amp; Statement of intent (LDM 2.m &amp; Zoning Sec 5.5.6)</b>	<ul style="list-style-type: none"> <li>▪ Include statement of intent to install and guarantee all materials for 2 years.</li> <li>▪ Include a minimum</li> </ul>	Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
	one cultivation in June, July and August for the 2-year warranty period.			
<b>Plant source</b> (LDM 2.n & LDM 3.a.(2))	Shall be northern nursery grown, No.1 grade.	Yes	Yes	
<b>Irrigation plan</b> (LDM 2.s.)	A fully automatic irrigation system or a method of providing sufficient water for plant establishment and survival is required on Final Site Plans.	No		1. <u>Please add irrigation plan or information as to how plants will be watered sufficiently for establishment and long-term survival.</u> 2. <u>If xeriscaping is used, please provide information about plantings included.</u>
<b>Other information</b> (LDM 2.u)	Required by Planning Commission	NA		
<b>Establishment period</b> (Zoning Sec 5.5.6.B)	<b>2 yr. Guarantee</b>	Yes	Yes	
<b>Approval of substitutions.</b> (Zoning Sec 5.5.5.E)	City must approve any substitutions in writing prior to installation.	Yes	Yes	
<b>Plant List (LDM 2.h.) – Include all cost estimates</b>				
Quantities and sizes	Refer to LDM suggested plant list as well as requirements for planting diversity and prohibited species.	No	No	<b>Please add plant lists for all units and overall site, preferably on Preliminary Site Plan</b>
Root type		No	No	<b>See above</b>
Botanical and common names		No	No	<b>See above</b>
Type and amount of lawn		No	No	<u>Please add areas for each unit each in cost table.</u>
Cost estimate (LDM 2.t)	For all new plantings, mulch and sod as listed on the plan	No	No	<u>Please add cost estimates for each unit and overall project to final site plan.</u>
<b>Planting Details/Info (LDM 2.i) – Utilize City of Novi Standard Details</b>				
Canopy Deciduous Tree	Refer to LDM for detail drawings	Yes	Yes	
Evergreen Tree		Yes	Yes	
Multi-stem Tree		No	No	<b>Please add to plan if multi-stem trees are included in plan.</b>
Shrub		Yes	Yes	



Item	Required	Proposed	Meets Code	Comments
Perennial/ Ground Cover		No	No	<b>Please add to plan</b>
Tree stakes and guys. (Wood stakes, fabric guys)		Yes	Yes	
Tree protection fencing	Located at Critical Root Zone (1' outside of dripline)	No	No	<ol style="list-style-type: none"> <li>1. If any trees are being preserved in the development area, please add detail to plan.</li> <li>2. If any trees are being preserved in the development area please show fencing line on Demolition Plans drawn outside of trees' driplines.</li> </ol>
<b>Other Plant Material Requirements (LDM 3)</b>				
<b>General Conditions</b> (LDM 3.a)	Plant materials shall not be planted within 4 ft. of property line	Yes	Yes	
<b>Plant Materials &amp; Existing Plant Material</b> (LDM 3.b)	Clearly show trees to be removed and trees to be saved.	Sheets 10 and 11	Yes	
<b>Landscape tree credit</b> (LDM3.b.(d))	Substitutions to landscape standards for preserved canopy trees outside woodlands/ wetlands should be approved by LA. Refer to Landscape tree Credit Chart in LDM	No		
<b>Plant Sizes for ROW, Woodland replacement and others</b> (LDM 3.c)	2.5" canopy trees 6' evergreen trees		TBD	
<b>Plant size credit</b> (LDM3.c.(2))	NA	No		
<b>Prohibited Plants</b> (LDM 3.d)	No plants on City Invasive Species List		TBD	
<b>Recommended trees for planting under overhead utilities</b> (LDM 3.e)	Label the distance from the overhead utilities		TBD	<ol style="list-style-type: none"> <li>1. Please clearly indicate and label any overhead lines to remain on site.</li> <li>2. If none will be on the site, please add a note to that effect.</li> </ol>
<b>Collected or Transplanted trees</b> (LDM 3.f)		No		

Item	Required	Proposed	Meets Code	Comments
<b>Nonliving Durable Material: Mulch (LDM 4)</b>	<ul style="list-style-type: none"> <li>▪ Trees shall be mulched to 3" depth and shrubs, groundcovers to 2" depth</li> <li>▪ Specify natural color, finely shredded hardwood bark mulch. Include in cost estimate.</li> <li>▪ Refer to section for additional information</li> </ul>	<p style="text-align: center;">Yes</p>	<p style="text-align: center;">Yes</p>	<p><b>Please revise Landscape Note #8 to use compost instead of peat.</b></p>

**NOTES:**

1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.
2. The section of the applicable ordinance or standard is indicated in parenthesis. For the landscape requirements, please see the Zoning Ordinance landscape section 5.5 and the Landscape Design Manual for the appropriate items under the applicable zoning classification.
3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.

WETLANDS REVIEW



ECT Project No. 180408-0100

June 29, 2018

Ms. Barbara McBeth, AICP  
City Planner  
Community Development Department  
City of Novi  
45175 W. Ten Mile Road  
Novi, Michigan 48375

Re: Adell Center (JZ18-0024)  
Wetland Review of the PRO Concept Plan

Dear Ms. McBeth:

Environmental Consulting & Technology, Inc. (ECT) has reviewed the PRO Concept Plan for the proposed Adell Center project prepared by Greentech Engineering, Inc. dated June 4, 2018 and stamped "Received" by the City of Novi Community Development Department on June 5, 2018 (Plan). The Plan was reviewed for conformance with the City of Novi Wetland and Watercourse Protection Ordinance and the natural features setback provisions in the Zoning Ordinance.

**ECT currently recommends approval of the PRO Concept Plan for Wetlands. The Applicant shall address the items noted in the *Wetland Comments* Section of this letter prior to receiving Wetland approval of the Preliminary Site Plan.**

The following wetland related items are required for this project:

Item	Required/Not Required/Not Applicable
Wetland Permit (specify Non-Minor or Minor)	Likely Required
Wetland Mitigation	Not Required
Wetland Buffer Authorization	Likely Required
MDEQ Permit	To Be Determined. It is the applicant's responsibility to contact the MDEQ in order to determine the need for a wetland use permit.
Wetland Conservation Easement	Required

The Plan includes the construction of a mixed-use district with several proposed building sites, associated parking, utilities and underground stormwater detention systems. The current Plan indicates a total of nine (9) building units, with Unit 4 being a parking lot and gazebo. The Plan notes that this unit is planned to be a temporary use by the developer and is subject to future development in accordance with the PRO agreement for the Adell Center.

**Wetland Evaluation**

Environmental Consulting & Technology, Inc. (ECT) conducted a wetland evaluation for the proposed Adell Center project (hotel and entertainment center) at 43700 Expo Center Drive (Parcel ID 50-22-15-

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Ann Arbor, MI  
48105

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769-3164

476-045) on May 15, 2018. ECT met with the applicant's current wetland consultant, King & MacGregor Environmental, Inc. for the wetland boundary verification. The subject site is located south of I-96 and north of Grand River Avenue, west of Novi Road in Section 15. (Parcel ID number 50-22-15-476-045). ECT also reviewed the *Old Novi Expo: Wetland Delineation and Determination of Jurisdiction* report prepared by BWA Consulting dated October 5, 2017 (i.e., Report). This Report was received by the City by the applicant on May 1, 2018 as part of a wetland boundary verification request for the property.

The site is the home of the Novi Expo Center which closed and the building was demolished in 2012. Currently, the only structure on the property is the existing water tower in the northwest corner of the site. Remnants of the former Novi Expo Center remain including the concrete building slab and the asphalt parking lot. The southern portion of the site (south of the existing asphalt parking lot) contains the Walled Lake Branch of the Middle Rouge River, wetlands, floodplains and trees. This area (approximately 7 acres), contains the areas of City-regulated wetlands as mentioned above.

ECT's in-office review of available materials included the City of Novi Regulated Wetland and Watercourse map (see Figure 1), USGS topographic quadrangle map, NRCS soils map, USFWS National Wetland Inventory map, and historical aerial photographs (from Oakland County). Based on our review of this information the overall proposed project parcel contains areas mapped as City-Regulated Wetlands/Watercourses. The site appears to contain wetland/watercourse areas that are regulated by the City of Novi as well as the Michigan Department of Environmental Quality (MDEQ).

The focus of the site inspection was to review site conditions in order to determine whether City-regulated wetlands are found on-site. BWA completed a wetland delineation on the site on September 22, 2017. Pink wetland boundary flagging was in place at the time of this site inspection. ECT reviewed the flagging and agrees that the wetland boundaries were accurately flagged in the field. It should be noted that the applicant has provided a wetland flagging map that indicates the approximate locations of the wetland flagging/staking on site (see Figure 2). Based on the existing vegetation and topography, it is ECT's assessment that the on-site wetlands have been accurately delineated at this time.

The BWA Report notes that a total of six (6) wetlands were identified. The following is a brief description of the on-site wetland features (see Figure 2 provided by BWA):

Wetland 1 – Forested, scrub shrub, and emergent wetland contiguous to the Creek (Walled Lake Branch of the Middle Rouge River). BWA notes that the dominant wetland vegetation includes reed canary grass (*Phalaris arundinacea*), American elm (*Ulmus americana*), and orange jewelweed (*Impatiens capensis*). Wetland hydrology is indicated by saturated soils adjacent to the stream. Hydric (i.e., wetland) soils were identified within the wetland boundaries. This wetland area is essentially confined to the upper banks of the Creek.

Wetlands 2, 3, and 5 – These are isolated emergent depressions within the floodplain (according to FEMA FIRM Panel #0626) of the Walled Lake Branch of the Middle Rouge River. Dominant wetland vegetation includes reed canary grass, orange jewelweed, and wood nettle (*Laportea canadensis*). Wetland hydrology is indicated by water stained leaves and drift lines. Hydric soils were identified within the wetland boundaries. These wetland areas are located south of Wetland 1.

Wetland 4 – This is an area of isolated, forested and emergent depression. Dominant wetland vegetation includes common reed (*Phragmites australis*), American elm, and wood nettle. Wetland hydrology is indicated

by water stained leaves. Hydric soils were identified within the wetland boundaries. This wetland area is located in the southwest corner of the site.

Wetland 6 – This area is an emergent swale associated with the Walled Lake Branch of the Middle Rouge River. Dominant vegetation includes reed canary grass, wood nettle, and orange jewelweed. Wetland hydrology is indicated by saturated soils near the surface. Hydric soils were identified within the wetland boundaries. This wetland is located in the southeast corner of the site.

BWA noted in the Report that in their opinion all six (6) on-site wetlands are subject to regulation by the City of Novi as well as MDEQ and that permits would be required for any work proposed within these wetlands.

#### **Regulatory Status - MDEQ**

ECT has evaluated the on-site wetlands and believes that they are all considered to be essential/regulated by the City of Novi as they meet one or more of the essentiality criteria (i.e., functions and values) outlined in the City of Novi Wetland and Watercourse Protection Ordinance and regulated by the MDEQ. As noted, the wetlands appear to accurately flagged in the field and appear to be generally indicated accurately on the *Wetland Sketch* provided by BWA (Figure 2, attached).

The Michigan Department of Environmental Quality (MDEQ) generally regulates wetlands that are within 500 feet of an inland lake, pond, or stream, or within 1,000 feet of a Great Lake, Lake St. Clair, the St. Clair River, or the Detroit River. Isolated wetlands five (5) acres in size or greater are also regulated. The MDEQ may also exert regulatory control over isolated wetlands less than five acres in size "...if the department determines that protection of the area is essential to the preservation of the natural resources of the state from pollution, impairment, or destruction and the department has notified the owner". BWA states that in their opinion, all six (6) of the on-site wetland areas are subject to regulation by the MDEQ, as they are either contiguous to, or are within 500 feet of the on-site stream. It is the applicant's responsibility to contact MDEQ in order to confirm the regulatory authority with respect to the on-site wetland areas.

#### **Regulatory Status – City of Novi**

The City of Novi Wetland and Watercourse Protection Ordinance (City of Novi Code of Ordinances, Part II, Chapter 12, Article V.; Division 2.) describes the regulatory criteria for wetlands and review standards for wetland permit applications. The City of Novi regulates wetlands that are: (1) contiguous to a lake, pond, river or stream, as defined in Administrative Rule 281.921; (2) two (2) acres in size or greater; or (3) less than two (2) acres in size but deemed essential to the preservation of the natural resources of the city under the criteria set forth in subsection 12-174(b). Wetlands deemed regulated by the City of Novi require the approval of a use permit for any proposed impacts to the wetland.

All six (6) of the on-site wetlands appear to be located within the area depicted as regulated wetland on the City of Novi Regulated Wetland and Watercourse Map (Figure 2). ECT has evaluated each wetland and believes that each wetland is regulated by the City's Wetland and Watercourse Protection Ordinance because all on-site wetlands are located within 500-feet of the Walled Lake Branch of the Middle Rouge River.

The applicant shall provide information on subsequent plans that clearly indicates the areas (square feet and/or acres) of all of the existing on-site wetlands and their 25-foot setbacks/buffers. The plans shall also clearly indicate the area (square feet or acres) of all wetland and wetland buffer impacts (both permanent and temporary, if applicable) and the volume (cubic yards) of all wetland impacts. It is not clear from the

utility plans whether impacts are proposed within the wetlands or 25-foot wetland setback for the purpose of stormwater outfall construction.

It should be noted that in those cases where an activity results in the impact to wetland areas of 0.25-acre or greater that are deemed essential under City of Novi Ordinance subsection 12-174(b) mitigation shall be required. The applicant shall submit a mitigation plan which provides for the establishment of replacement wetlands at a ratio of 1:1 through 2:1 times the area of the natural wetland impaired or destroyed, if impacts meet or exceed the 0.25-acre threshold. In general, the MDEQ's threshold for the requirement of wetland mitigation is 0.3-acre of wetland impacts. Mitigation does not appear to be a requirement of the current Plan.

As noted above, any proposed use of the wetlands will require a City of Novi *Wetland Use Permit* as well as an *Authorization to Encroach the 25-Foot Natural Features Setback* for any proposed impacts to the 25-foot wetland buffers. The applicant is urged to minimize impacts to on-site wetlands and wetland setbacks to the greatest extent practicable. The City regulates wetland buffers/setbacks. Article 24, Schedule of Regulations, of the Zoning Ordinance states that:

*“There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to require a minimum setback from wetlands and watercourses”.*

#### **Proposed Wetland/Watercourse Impacts**

It is unclear if the Plan currently proposes impacts to wetlands or 25-foot natural features setback buffers. It appears as though the proposed site improvements (parking area for Unit 3 and the Unit 5 building, for example) are proposed directly adjacent to the 25-foot wetland/watercourse setback. In addition, there appears to be a proposed bridge spanning the Walled Lake Branch of the Middle Rouge River (near/on Unit 4). It is ECT's understanding that a nature or walking trail may be proposed as part of the overall site plan. The applicant has noted that the approval of the proposed development will include the improvement of over three (3) acres of existing City-regulated woodlands/wetland areas to allow for better access by the public. Without details for the proposed improvements for public benefit provided on the Plan, ECT cannot provide further comment. Detailed information with regard to this trail shall be provided on subsequent site plan submittals in order to ensure that any proposed impacts to wetlands, wetland buffers or regulated trees are minimized to the greatest extent practicable.

It should also be noted that the utility plans appear to indicate six (6) stormwater outfalls to the Walled Lake Branch of the Middle Rouge River. The Plan appears to indicate that these outfalls may all be existing. Five (5) of the outfalls appear to be located within the 25-foot wetland/watercourse setback and one (1) of the outfalls appears to be located within Wetland 1. Subsequent site plan submittals shall clarify and quantify all proposed impacts within wetlands or 25-foot wetland setbacks.

The applicant is urged to minimize impacts to all wetlands and 25-foot wetland setback areas to the greatest extent practicable. The City regulates wetland and watercourse buffers/setbacks. Article 24, Schedule of Regulations, of the Zoning Ordinance states that:

*“There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to require a minimum setback from wetlands and watercourses”.*

The plans shall clearly indicate the area (square feet or acres) of all wetland and 25-foot wetland setback impacts (both permanent and temporary, if applicable) and the volume (cubic yards) of all proposed direct impacts to wetlands, if applicable.

**Wetland and Watercourse Comments**

ECT recommends that the Applicant address the items noted below in subsequent site plan submittals:

1. ECT encourages the Applicant to minimize impacts to on-site wetlands and 25-foot wetland setbacks to the greatest extent practicable. Currently, it is unclear if the six (6) stormwater outfalls shown on the utility plans are existing or proposed. The applicant should clarify what (if any) work/grading associated with these stormwater outfalls is proposed within both the 25-foot wetland/watercourse setback and within Wetland 1 (i.e., associated with the Walled Lake Branch of the Middle Rouge River).
2. There appears to be a proposed bridge spanning the Walled Lake Branch of the Middle Rouge River (near/on Unit 4). It is ECT's understanding that a nature or walking trail may be proposed as part of the overall site plan. Detailed information with regard to this trail shall be provided on subsequent site plan submittals in order to ensure that any proposed impacts to wetlands, wetland buffers or regulated trees are minimized to the greatest extent practicable.
3. As noted above, the current Plan does not specifically indicate (label) any direct impacts to wetlands or wetland buffers. The applicant shall indicate, quantify and label all proposed impacts to these natural features on subsequent plan submittals, if applicable. The areas of the on-site wetlands and associated 25-foot wetland setback areas shall be indicated and quantified on subsequent site plan submittals.
4. It appears as though a MDEQ Wetland Permit and a City of Novi *Wetland Use Permit* would be required for any proposed impacts to on-site wetlands, if applicable. A City of Novi *Authorization to Encroach the 25-Foot Natural Features Setback* would be required for any proposed impacts to on-site 25-foot wetland or watercourse buffers.
5. It should be noted that it is the Applicant's responsibility to confirm the need for a Permit from the MDEQ for any proposed wetland or floodplain impacts. Final determination as to the regulatory status of any on-site wetlands (if applicable) shall be made by MDEQ. The Applicant should provide a copy of the MDEQ Wetland Use Permit application to the City (and our office) for review and a copy of the approved permit upon issuance. A City of Novi Wetland Permit cannot be issued prior to receiving this information.
6. The Plan should address how any temporary impacts to wetland or 25-foot wetland buffers shall be restored, if applicable. Subsequent Plan submittals shall include specifications for any proposed seed mixes proposed for use within these areas.
7. The applicant should ensure that any proposed snow storage areas are located such that any runoff will not directly affect any on-site wetlands (if applicable).
8. ECT suggests that any proposed stormwater management plan be reviewed by the City of Novi Engineering Department to ensure that they meet the City of Novi design requirements. Specifically, the Plan appears to propose underground stormwater detention systems. It is not immediately clear if



these systems will include stormwater pre-treatment structures in conjunction with the storage. The stormwater shall receive pre-treatment prior to being outlet to wetlands.

**Wetland Conclusion**

The project site appears to contain wetlands/watercourse that are regulated by both the City of Novi and the MDEQ. The current Plan does not specifically indicate (label) any direct impacts to wetlands, wetland buffers, or floodplains. Any proposed impacts to on-site wetlands will require a permit from the MDEQ, a City of Novi *Wetland and Watercourse Use Permit*, and an *Authorization to Encroach the 25-Foot Natural Features Setback* for any proposed impacts to the 25-foot wetland buffers. Subsequent site plan submittals shall clearly indicate all proposed impacts (permanent or temporary) to the existing wetlands/watercourse or associated 25-foot wetland setbacks.

**Recommendation**

ECT currently recommends approval of the PRO Concept Plan for Wetlands. The Applicant shall address the items noted in the *Wetland Comments* Section of this letter prior to receiving Wetland approval of the Preliminary Site Plan.

If you have any questions regarding the contents of this letter, please contact us.

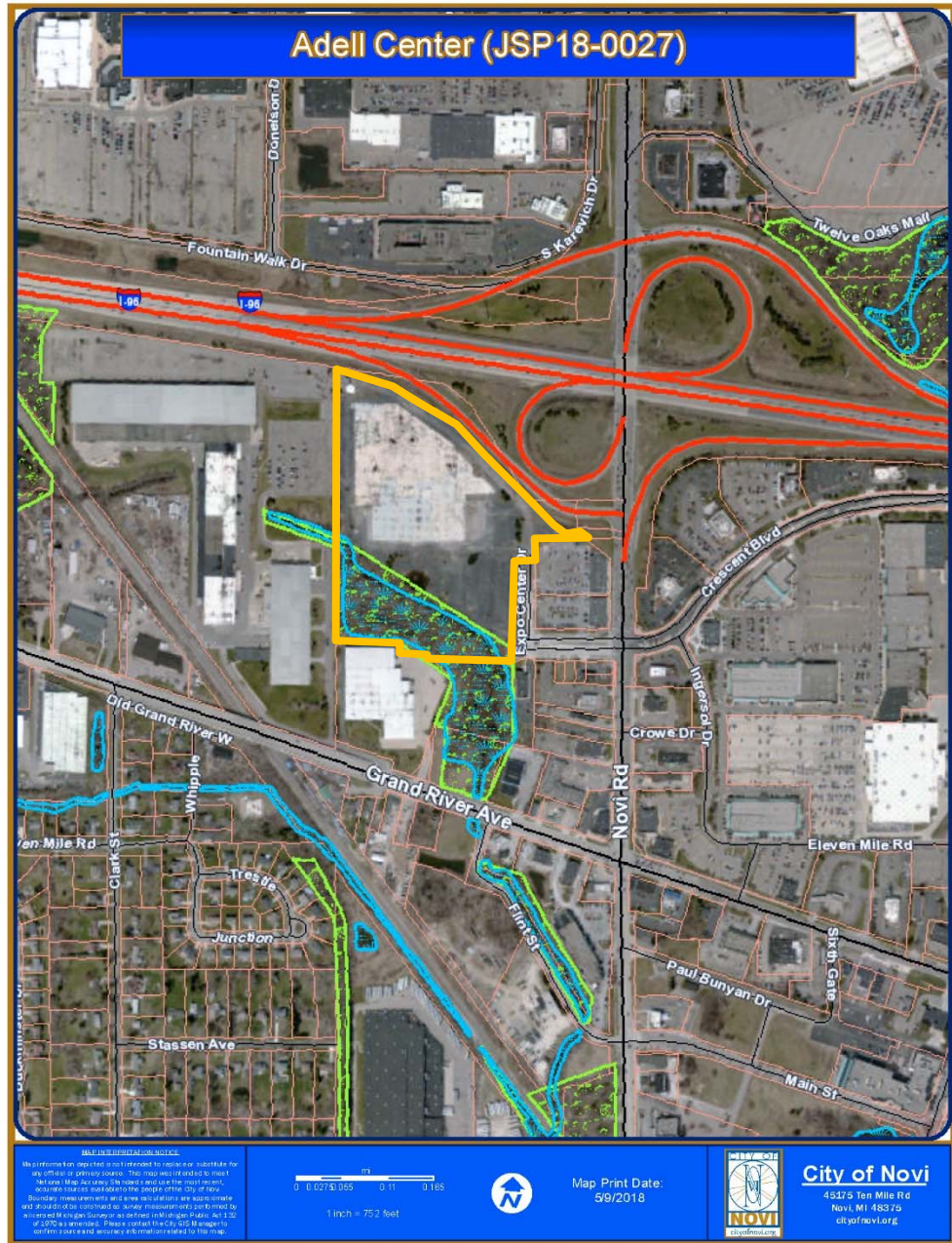
Respectfully submitted,  
**ENVIRONMENTAL CONSULTING & TECHNOLOGY, INC.**



Pete Hill, P.E.  
Senior Associate Engineer

cc: Lindsay Bell, City of Novi Planner  
Sri Komaragiri, City of Novi Planner  
Rick Meader, City of Novi Landscape Architect  
Hannah Smith, City of Novi Planning Assistant

Attachments: Figure 1 – City of Novi Regulated Wetland and Woodland Map  
Figure 2 – Wetland Sketch  
Site Photos



**Figure 1.** City of Novi Regulated Wetland & Woodland Map (approximate parcel boundary shown in orange). Regulated Woodland areas are shown in green and regulated Wetland areas are shown in blue.

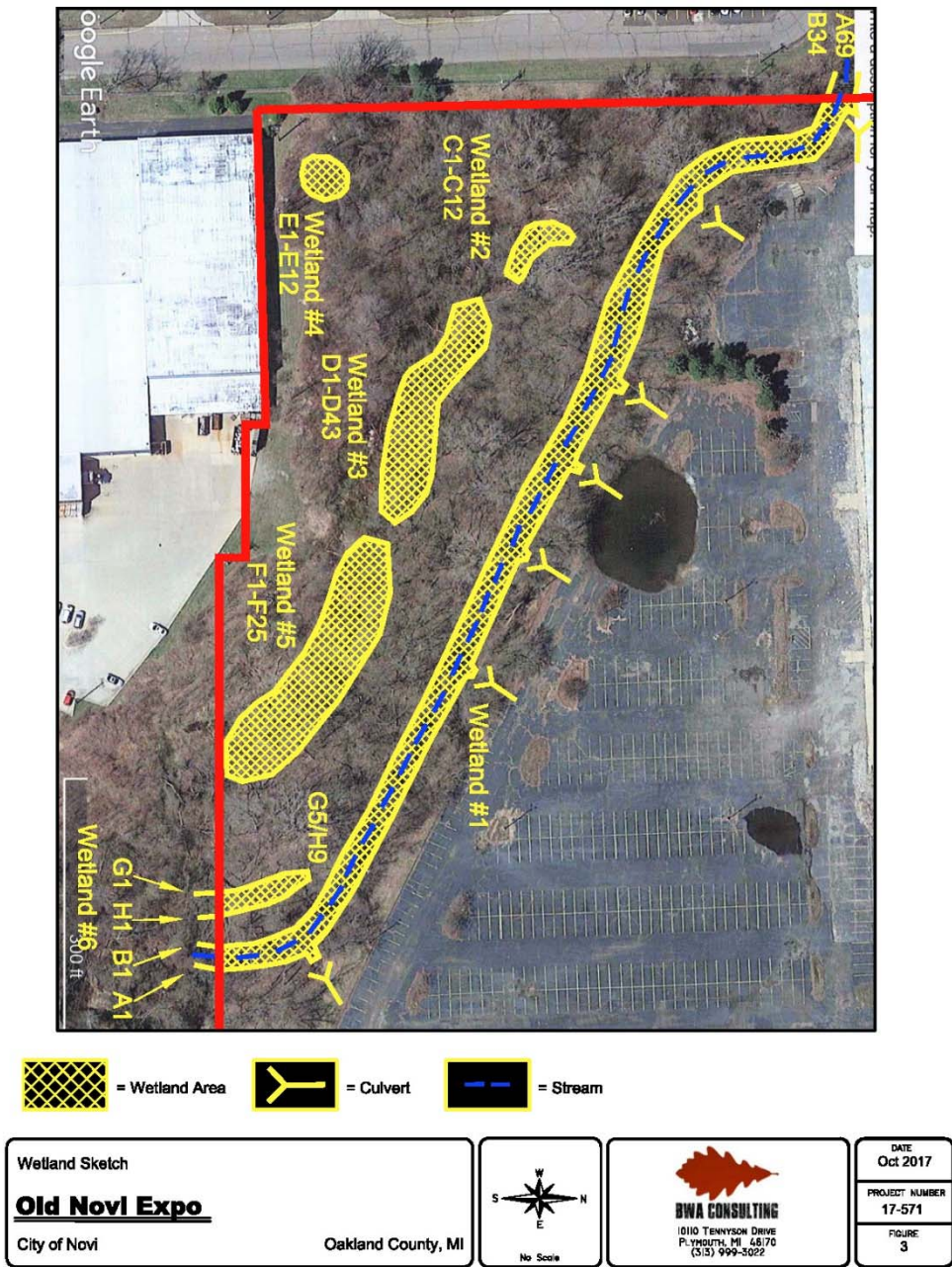


Figure 2. Wetland Sketch (provided by BWA).

*Site Photos*



**Photo 1.** Looking northeast along the Walled Lake Branch of the Middle Rouge River (ECT June 19, 2018).



**Photo 2.** Looking west at Wetland D in the southern portion of the project property (ECT, June 19, 2018).

WOODLANDS REVIEW



ECT Project No. 180408-0200

June 29, 2018

Ms. Barbara McBeth, AICP  
City Planner  
Community Development Department  
City of Novi  
45175 W. Ten Mile Road  
Novi, Michigan 48375

Re: Adell Center (JZ18-0024)  
Woodland Review of the PRO Concept Plan

Dear Ms. McBeth:

Environmental Consulting & Technology, Inc. (ECT) has reviewed the Preliminary Site Plan for the proposed Adell Center project prepared by Greentech Engineering, Inc. dated June 4, 2018 and stamped "Received" by the City of Novi Community Development Department on June 5, 2018 (Plan). The Plan was reviewed for conformance with the City of Novi Woodland Protection Ordinance Chapter 37.

**ECT currently recommends approval of the PRO Concept Plan for Woodlands. The Applicant shall address the items noted in the *Woodland Comments* Section of this letter prior to receiving Woodland approval of the Preliminary Site Plan.**

The following woodland related items are required for this project:

Item	Required/Not Required/Not Applicable
Woodland Permit	Not Currently Required
Woodland Fence	Likely Required
Woodland Conservation Easement	Not Required

The proposed development is located north of Grand River Avenue and west of Novi Road in Section 15. The Plan includes the construction of a mixed-use district with several proposed building sites, associated parking, utilities and underground stormwater detention systems. The current Plan indicates a total of nine (9) building units, with Unit 4 being a parking lot and gazebo. The Plan notes that this unit is planned to be a temporary use by the developer and is subject to future development in accordance with the PRO agreement for the Adell Center.

The site is the home of the Novi Expo Center which closed and the building was demolished in 2012. Currently, the only structure on the property is the existing water tower in the northwest corner of the site. Remnants of the former Novi Expo Center remain including the concrete building slab and the asphalt parking lot. The southern portion of the site (south of the existing asphalt parking lot) contains the Walled Lake Branch of the Middle Rouge River, wetlands, floodplains and trees. This area (approximately 7 acres), contains the areas of City-regulated wetlands and City-regulated woodlands.

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Based on our review of the Plan, Novi aerial photos, Novi GIS, City of Novi Official Wetlands and Woodlands Map (see Figure 1), and on-site evaluation this proposed project site contains areas indicated as Regulated Woodlands as well as Regulated Wetlands. The area of regulated woodland encompasses approximately the southern portion of the project property (south of the Walled Lake Branch of the Middle Rouge River).

It should be noted that the purpose of the City of Novi Woodland Protection Ordinance (Chapter 37) is to:

- *Provide for the protection, preservation, replacement, proper maintenance and use of trees and woodlands located in the city in order to minimize disturbance to them and to prevent damage from erosion and siltation, a loss of wildlife and vegetation, and/or from the destruction of the natural habitat. In this regard, it is the intent of this chapter to protect the integrity of woodland areas as a whole, in recognition that woodlands serve as part of an ecosystem, and to place priority on the preservation of woodlands, trees, similar woody vegetation, and related natural resources over development when there are no location alternatives;*
- *Protect the woodlands, including trees and other forms of vegetation, of the city for their economic support of local property values when allowed to remain uncleared and/or unharvested and for their natural beauty, wilderness character of geological, ecological, or historical significance; and*
- *Provide for the paramount public concern for these natural resources in the interest of health, safety and general welfare of the residents of the city.*

#### **On-Site Woodland Evaluation**

ECT has reviewed the City of Novi Official Woodlands Map and completed an onsite Woodland Evaluation on June 19, 2018. ECT's in-office review of available materials included the City of Novi Regulated Woodland map and other available mapping. As noted above, the southern portion of the subject property is mapped as City of Novi Regulated Woodlands on the official City of Novi Regulated Wetland and Watercourse Map (see Figure 1). The proposed limits of disturbance do not include areas mapped as City-Regulated Woodlands.

An existing tree list is included on Sheet 21 (*Tree Inventory Plan*). This Plan identifies tree tag numbers, diameter-at-breast-height (DBH), common name, condition, and removal status. The tree survey is included on Sheets 20 (*Woodland Plan*). The surveyed trees have been marked with aluminum tree tags allowing ECT to compare the tree diameters reported on the Plan with the existing trees in the field. ECT found that the Plan appears to accurately depict the location, species composition and the size of the existing trees. ECT took a sample of diameter-at-breast-height (DBH) measurements and found that the data provided on the Plan was consistent with the field measurements.

As noted above, the area north of the Walled Lake Branch of the Middle Rouge River is not mapped as City of Novi Regulated Woodland. In general, the majority of the on-site trees are box elder (*Acer negundo*) and eastern cottonwood (*Populus deltoides*). The site also includes Austrian pine (*Pinus nigra*), green spruce (*Picea pungens*), sugar maple (*Acer saccharum*), black walnut (*Juglans nigra*), American elm (*Ulmus americana*), and several other species. In terms of habitat quality and diversity of tree species, the overall subject site consists of fair to good quality trees. In terms of a scenic asset, wildlife habitat, windblock, noise buffer or other environmental asset, the forested area located on the subject site is considered to be of fair quality.

**City of Novi Woodland Review Standards & Woodland Permit Requirements**

Based on Section 37-29 (*Application Review Standards*) of the City of Novi Woodland Ordinance, the following standards shall govern the grant or denial of an application for a use permit required by this article:

*No application shall be denied solely on the basis that some trees are growing on the property under consideration. However, the protection and conservation of irreplaceable natural resources from pollution, impairment, or destruction is of paramount concern. Therefore, the preservation of woodlands, trees, similar woody vegetation, and related natural resources shall have priority over development when there are location alternatives.*

In addition,

*“The removal or relocation of trees shall be limited to those instances when necessary for the location of a structure or site improvements and when no feasible and prudent alternative location for the structure or improvements can be had without causing undue hardship”.*

The City of Novi regulates all trees 8-inches diameter-at-breast-height (DBH) and greater that are located within the areas delineated as regulated woodlands on the City-Regulated Woodlands Map. The City also regulates any individual tree greater than or equal to 36-inches DBH, irrespective of whether such tree is within a regulated woodland. Proposed woodland impacts will require a Woodland Permit and the regulated trees shall be relocated or replaced by the permit grantee.

**Proposed Woodland Impacts and Replacements**

Based on the *Woodland Summary* information on the *Tree Inventory Plan* (Sheet 21), there appear to be a total of 312 surveyed trees on the subject property. Of these, 29 of the trees are not located within the Regulated Woodland Boundary leaving a total of 283 Regulated Woodland Trees.

The Plan does not currently propose the removal of City-Regulated Trees and therefore does not require any Woodland Replacement credits.

It is ECT’s understanding that a nature or walking trail may be proposed as part of the overall site plan. The applicant has noted that the approval of the proposed development will include the improvement of over three (3) acres of existing City-regulated woodlands/wetland areas to allow for better access by the public. Without details for the proposed improvements for public benefit provided on the Plan, ECT cannot provide further comment. Detailed information with regard to this trail shall be provided on subsequent site plan submittals in order to ensure that any proposed impacts to wetlands, wetland buffers or regulated trees are minimized to the greatest extent practicable.

**Woodland Comments**

Please consider the following comments when preparing subsequent site plan submittals:

1. ECT encourages the Applicant to minimize impacts to on-site woodlands to the greatest extent practicable. Currently, the Plan does not propose impacts to Regulated Woodlands. However, as noted above, it is ECT’s understanding that a nature or walking trail may be proposed as part of the overall site plan. Detailed information with regard to this trail shall be provided on subsequent site plan submittals in order to ensure that any proposed impacts to wetlands, wetland buffers or regulated trees are minimized to the greatest extent practicable.



2. A Woodland Permit from the City of Novi would be required for proposed impacts to any trees 8-inch diameter-at-breast-height (DBH) or greater and located within an area designated as City Regulated Woodland, or any tree 36-inches DBH regardless of location on the site. Such trees shall be relocated or replaced by the permit grantee. The current Plan does not indicate impacts to City-Regulated Trees.
3. If Woodland Replacement credits are required on subsequent Plan submittals, ECT recommends that the applicant take all steps feasible in order to provide as many of the required Woodland Replacement credits through the planting of on-site replacement trees. If on-site Woodland Replacement planting is proposed, all deciduous replacement trees shall be two and one-half (2 ½) inches caliper or greater and count at a 1 replacement tree-to-1 credit replacement ratio. All coniferous replacement trees shall be six (6) feet in height (minimum) and count at a 1.5 replacement tree-to-1 credit replacement ratio. All Woodland Replacement trees shall be species that are listed on the City's Woodland Tree Replacement Chart (attached).
4. If applicable, a Woodland Replacement Performance financial guarantee for the planting of replacement trees will be required. This financial guarantee will be based on the number of on-site woodland replacement trees (credits) being provided at a per tree value of \$400. Based on a successful inspection of the installed on-site Woodland Replacement trees, the original Woodland Financial Guarantee shall be returned to the Applicant. Twenty-five percent (25%) of the value of the Woodland Replacement material shall be kept for a period of 2-years after the successful inspection of the tree replacement installation as a *Woodland Maintenance and Guarantee Bond*.
5. If applicable, Woodland Replacement material should not be located 1) within 10' of built structures or the edges of utility easements and 2) over underground structures/utilities or within their associated easements. In addition, replacement tree spacing should follow the *Plant Material Spacing Relationship Chart for Landscape Purposes* found in the City of Novi *Landscape Design Manual*.
6. If applicable, the Applicant will be required to pay the City of Novi Tree Fund at a value of \$400/credit for any Woodland Replacement tree credits that cannot be placed on-site. If no Woodland Replacement Trees are proposed on-site, the required payment to the City of Novi Tree Fund will be \$8,000 (20 Credits Required x \$400/Credit).
7. The Applicant shall provide preservation/conservation easements as directed by the City of Novi Community Development Department for any areas of woodland replacement trees (if applicable). The applicant shall demonstrate that the all proposed woodland replacement trees will be guaranteed to be preserved as planted with a conservation easement or landscape easement to be granted to the city. This language shall be submitted to the City Attorney for review. The executed easement must be returned to the City Attorney within 60 days of the issuance of the City of Novi Woodland permit.

Adell Center (JZ18-0024)  
Woodland Review of the Preliminary Site Plan  
June 29, 2018\_REV.1  
Page 5 of 8

**Recommendation**

ECT currently recommends approval of the PRO Concept Plan for Woodlands. The Applicant shall address the items noted in the *Woodland Comments* Section of this letter prior to receiving Woodland approval of the Preliminary Site Plan.

If you have any questions regarding the contents of this letter, please contact us.

Respectfully submitted,

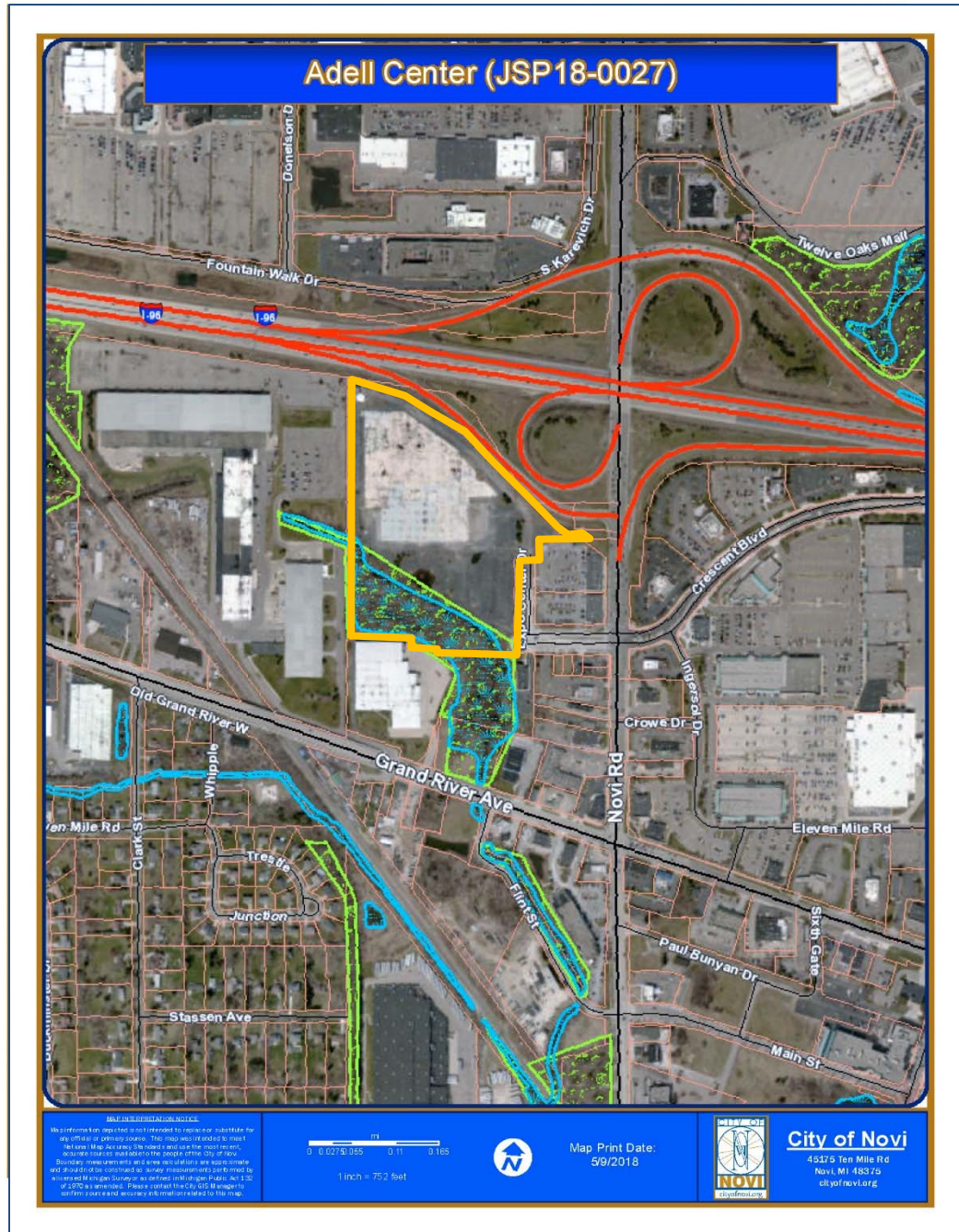
**ENVIRONMENTAL CONSULTING & TECHNOLOGY, INC.**



Pete Hill, P.E.  
Senior Associate Engineer

cc: Lindsay Bell, City of Novi Planner ([lbell@cityofnovi.org](mailto:lbell@cityofnovi.org))  
Sri Komaragiri, City of Novi Planner ([skomaragiri@cityofnovi.org](mailto:skomaragiri@cityofnovi.org))  
Rick Meader, City of Novi Landscape Architect ([rmeader@cityofnovi.org](mailto:rmeader@cityofnovi.org))  
Hannah Smith, City of Novi Planning Assistant ([hsmith@cityofnovi.org](mailto:hsmith@cityofnovi.org))

Attachments: Figure 1 – City of Novi Regulated Wetland and Woodland Map  
Woodland Tree Replacement Chart  
Site Photos



**Figure 1.** City of Novi Regulated Wetland & Woodland Map (approximate parcel boundary shown in orange). Regulated Woodland areas are shown in green and regulated Wetland areas are shown in blue.

### Woodland Tree Replacement Chart

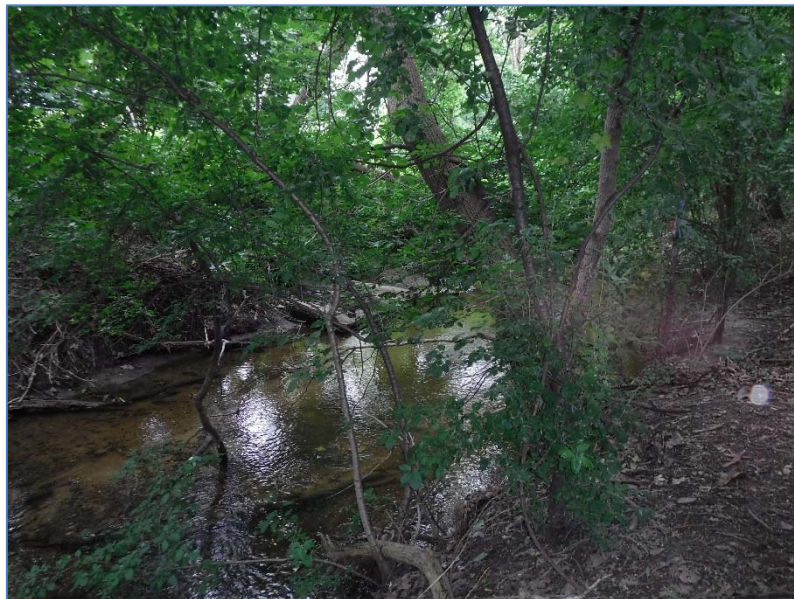
(from Chapter 37 Woodlands Protection)  
 (All canopy trees to be 2.5" cal or larger, evergreens as listed)

Common Name	Botanical Name
Black Maple	Acer nigrum
Striped Maple	Acer pennsylvanicum
Red Maple	Acer rubrum
Sugar Maple	Acer saccharum
Mountain Maple	Acer spicatum
Ohio Buckeye	Aesculus glabra
Downy Serviceberry	Amelanchier arborea
Smooth Shadbush	Amelanchier laevis
Yellow Birch	Betula alleghaniensis
Paper Birch	Betula papyrifera
American Hornbeam	Carpinus caroliniana
Bitternut Hickory	Carya cordiformis
Pignut Hickory	Carya glabra
Shagbark Hickory	Carya ovata
Northern Hackberry	Celtis occidentalis
Eastern Redbud	Cercis canadensis
Pagoda Dogwood	Cornus alternifolia
Flowering Dogwood	Cornus florida
American Beech	Fagus grandifolia
Thornless Honeylocust	Gleditsia triacanthos inermis
Kentucky Coffeetree	Gymnocladus dioica
Walnut	Juglans nigra or Juglans cinerea
Eastern Larch	Larix laricina
Tuliptree	Liriodendron tulipifera
Tupelo	Nyssa sylvatica
American Hophornbeam	Ostrya virginiana
White Spruce_(1.5:1 ratio) (6' ht.)	Picea glauca
Black Spruce_(1.5:1 ratio) (6' ht.)	Picea mariana
Red Pine_(1.5:1 ratio) (6' ht.)	Pinus resinosa
White Pine_(1.5:1 ratio) (6' ht.)	Pinus strobus
American Sycamore	Platanus occidentalis
Black Cherry	Prunus serotina
White Oak	Quercus alba
Swamp White Oak	Quercus bicolor
Scarlet Oak	Quercus coccinea
Shingle Oak	Quercus imbricaria
Burr Oak	Quercus macrocarpa
Chinkapin Oak	Quercus muehlenbergii
Red Oak	Quercus rubra
Black Oak	Quercus velutina
American Basswood	Tilia americana

*Site Photos*



**Photo 1.** Looking northwest across the north end of the proposed development site. This section of the site does not contain areas mapped as City-Regulated Woodlands (ECT, June 19, 2018).



**Photo 2.** Looking east at area of City-Regulated Woodland located south of the Walled Lake Branch of the Middle Rouge River (ECT, June 19, 2018).

TRAFFIC REVIEW



AECOM  
27777 Franklin Road  
Southfield  
MI, 48034  
USA  
aecom.com

**Project name:**  
JZ18-24 – Adell Center PRO Concept Traffic  
Review

**From:**  
AECOM

**Date:**  
June 29, 2018

**To:**  
Barbara McBeth, AICP  
City of Novi  
45175 10 Mile Road  
Novi, Michigan 48375

**CC:**  
Sri Komaragiri, Lindsay Bell, George Melistas,  
Theresa Bridges, Darcy Rechten, Hannah Smith

# Memo

**Subject:** JSP18-0027 Adell Center PRO Concept Traffic Review

The PRO concept site plan was reviewed to the level of detail provided and AECOM **recommends denial** for the applicant to move forward based on several undetermined site items including the final proposed width of Adell Drive, the results of a shared parking study and/or other missing site elements, as detailed below.

## GENERAL COMMENTS

1. The applicant, Orville Properties, LLC, is proposing a multi-use development located on the west side of Novi Road, south of I-96, with one point of access to Crescent Blvd. The applicant is proposing eight (8) individual units within the project:
  - a. IFLY indoor skydiving
  - b. Planet Fitness
  - c. Fairfield hotel
  - d. Off-street parking
  - e. Drury hotel
  - f. Sit-down restaurant
  - g. Sit-down restaurant
  - h. Carvana
2. Crescent Blvd is under the jurisdiction of the City of Novi.
3. The parcel is currently zoned EXPO, and the applicant is proposing to rezone to TC (Town Center District) with a Planned Rezoning Overlay (PRO).
4. Summary of traffic-related waivers/variances:
  - a. At the time of the pre-application site plan, the applicant is requesting the following traffic-related waivers or variances. Notes (*in italics*) following each proposed deviation include AECOM's agreement or disagreement with the deviation.
    - i. Planning Deviation for the requirement that the development has access to a major thoroughfare. This deviation refers to section 5.13 of the City of Novi Code of Ordinances. *This deviation is not applicable for this site, based on Section 5.13 because there are no single-family residential.*
    - ii. City Council Deviation from the Engineering Design Standards requirement of a 60' access easement. *The City has classified Adell Drive as a Non-residential collector, per the definition in Section 11-192 of the City's Code of Ordinances, whereby a "Collector street means a street which provides both land access and traffic circulation within residential neighborhoods or commercial, industrial or office areas. Such streets collect vehicles from the local streets or commercial/industrial/office areas and distribute them to the arterial system or to a local*

*destination.” As such, a minimum 36 foot roadway should be provided and a minimum 70 foot access easement is required. Furthermore, the circulation plan provided on sheet 13 indicates truck turning paths crossing into oncoming traffic lanes near the two 90 degree roadway curves. An increased width, with center left turn lane, could reduce the impact to oncoming traffic.*

- 1. Additionally, the proposed sidewalks shall be a minimum of six (6) feet wide along collector streets.*
  - 2. Additionally, the outside edge of the sidewalk should be located a minimum of 15 feet from the back of curb, and shall not be placed closer than five (5) feet from the back of curb.*
- iii. Planning Deviation for Modification of Parking Setback Requirements to allow for a 20 foot minimum front yard setback from proposed sidewalk easement and 0’ minimum side yard parking area setbacks within the Adell Center Development. *Due to the multi-development use of the property, the shared parking up to neighboring side yards and across parcels may be acceptable from a traffic perspective, such that the parking and pedestrian facilities provided are compliant with section 5.2.3. (Also see additional comments in vii and viii below for Unit 3 parking setback deviations.)*
- iv. Planning Deviation for Parking, Loading, Signs, Landscaping, etc. to eliminate requirement for loading areas for the following Units.
1. Unit 1 -IFLY - Ifly has no regularly schedule deliveries that would require a designated loading/unloading area.
  2. Unit 3 - Fairfield Inn -Per Fairfield Inn representatives, their operational requirements do not necessitate the need for a designated loading space.
  3. Unit 5 -Drury Hotel-Drury's deliveries are all made during the daytime non-peak parking times when the regular parking spaces are not in use.

This deviation refers to section 3.27.1 of the City of Novi code of ordinances.

*AECOM would not support the deviation to eliminate the requirement for loading areas for Unit 3 or Unit 5. Hotel facilities often receive food and supply deliveries and laundering services, which would necessitate loading/unloading activities. The statement for Unit 5 that “deliveries are made during daytime non-peak parking times when regular parking spaces are not in use” does not justify the lack of a loading zone. Loading activities should be facilitated in close proximity to the building and there is no guarantee that those spaces will be available at the time of deliveries; thereby increasing the potential for delivery vehicles to park in access aisles and diminish site accessibility and operations. Furthermore, the lack of a loading zone at Unit 1 could prove to be problematic given the potential for future land use changes.*

- v. Planning Deviation for minimum number of parking spaces per unit and as recommended by a shared parking study as prepared by the applicant's traffic consultant and agreed upon by the city's traffic engineer. This deviation refers to section 5.2 of the City of Novi code of ordinances. This deviation request would allow the unit owners to share parking with their neighboring unit owners based on peak hour parking needs as outlined in the shared parking study. *The applicant has not provided a shared parking study at the time of PRO concept submittal. AECOM supports that the applicant provide a shared parking study to review the potential for including other site elements and reducing the need for as many deviations. Further information is included in the Planning letter regarding parking.*
- vi. City Council variance for lack of a traffic impact study due to the city is currently undertaking a traffic study for this area. *The applicant has provided trip generation information for the development that will be incorporated into the region-wide traffic impact study. AECOM supports the variance for lack of a full traffic impact study as part of the plan review process such that the applicant understands that they may be requested to provide additional traffic-related data and information during the review at the City's discretion. The applicant should also confirm understanding that they may be subject to certain off-site and/or on-site mitigation measures as a result of the region-wide traffic impact study at the City's discretion.*
- vii. Planning Deviation for Unit 3 (Fairfield Inn) from the requirement for no parking stalls to be located adjacent to a parking lot entrance from a street (public or private) shall be located closer than two (2) feet from the street right-of-way (ROW) line, street easement or sidewalk, whichever is closer. This deviation refers to section 5.3.13 of the City of Novi code of ordinances. *The calculations stated in this deviation request should be updated to reference the distance to the proposed access*



*easement. AECOM would not support the deviation for parking stalls to be located up to two (2) feet from the street right-of-way (ROW) line, street easement or sidewalk. In some cases, the parked vehicle may pose a sight distance issue and operational concern with completing parking maneuvers within such a close proximity to the driveway.*

- viii. Front Parking Setback from the roadway easement to allow for a 11.9' setback from the roadway easement for Unit 3 (Fairfield Inn). *AECOM would not support the deviation for a reduces parking setback from the roadway easement to allow for 11.9' setback from the roadway easement because in the case of Unit 3, the proposed parking is located near the proposed driveway and may pose a sight distance issue and operational concern with completing parking maneuvers within such a close proximity to the driveway.*
- b. Once additional information is received from the applicant, further review will be performed and additional required deviations may be identified. Items that may require additional deviations include:
  - i. Section 11-194(a)(7) indicates a maximum cul-de-sac street length of 800 feet for all developments except for R-A zoned properties. If Adell Drive is in excess of 800 feet, the applicant may be required to seek a City Council variance for exceeding the maximum street length.
  - ii. Should the minimum same side driveway spacing requirements not be met, the applicant may be required to seek a deviation.
  - iii. Should the proposed number of drive approaches and/or the drive approach system not comply with the guidance in the ordinance, the applicant may be requested/required to provide justification and/or apply for deviations.
  - iv. The applicant should provide additional trash receptacle locations to meet the needs of the eight (8) sites within the development, or request a deviation.
  - v. A waiver is required if the applicant moves forward with painted islands.
  - vi. The applicant should revise the plans to include landscape islands every 15 parking spaces or request a variance.
  - vii. If Adell Drive is classified as a collector street, the sidewalk widths should be updated to six (6) feet or the applicant may request a variance.
  - viii. The City's Engineering Design Manual requires that the outside edge of the sidewalk along a private street should be located 15 feet from the back of curb. The applicant should update the site plan accordingly or request a variance.

## TRAFFIC IMPACTS

1. The applicant provided a trip generation analysis, prepared by Bergmann, which outlines the anticipated number of daily, AM peak and PM peak trips that each of the various land uses may be expected to generate. AECOM reviewed the trips generation estimates and accepts the calculations as provided.
2. The number of new trips expected to be generated by the entire development are as follows:

Trip Generation Summary				
	Estimated Trips	Estimated Peak-Direction Trips	City of Novi Threshold	Above Threshold?
<b>AM Peak-Hour Trips</b>	188	102	100	Yes
<b>PM Peak-Hour Trips</b>	334	186	100	Yes
<b>Daily (One-Directional) Trips</b>	3,988	N/A	750	Yes

- a. The development trip generation estimates exceed the City's threshold of more than 750 trips per day or 100 trips per either the AM or PM peak hour. The applicant has requested a variance for the completion of a traffic impact study because the development will be included in the region-wide TIS that is underway by AECOM. Reference item 4.a.vi under General Comments for further potential conditions related to a traffic impact study and/or mitigation measures that may be required.
- b. Additionally, AECOM performed a preliminary analysis to assess roadway capacity impacts of the proposed Adell Center development. The initial results of that analysis indicate that the intersection of Novi Road and Crescent Boulevard is expected to be able to accommodate the additional traffic during the AM and PM peak periods. The intersection of Novi Road and Grand River operates under existing congested conditions and may worsen with the added traffic demand of the development, specifically the eastbound and southbound left turn movements. It should be noted that the construction of Crescent Boulevard from Adell Drive to Grand River Avenue is expected to alleviate some of the pressure of the Novi Road and Grand River intersection.

## EXTERNAL SITE ACCESS AND OPERATIONS

*The following comments relate to the external interface between the proposed development and the surrounding roadway(s). It should be noted that each commercial driveway interface with Adell Drive is also considered an external access point for purposes of this review letter.*

1. The applicant has proposed the site roadway to be Nonresidential collector or local road. Adell Drive should be classified as a Nonresidential collector based on the definition provided in Section 11-192 of the City's Code of Ordinances, whereby a "Collector street means a street which provides both land access and traffic circulation within residential neighborhoods or commercial, industrial or office areas. Such streets collect vehicles from the local streets or commercial/industrial/office areas and distribute them to the arterial system or to a local destination", and in accordance within the Zoning Ordinance Section 5.10.1.B which states "A private drive network within a cluster, two-family, multiple-family, or non-residential uses and developments shall be built to City of Novi Design and Construction Standards for local street standards (twenty-eight (28) feet back-to-back width, although for industrial, commercial, office, and high tech uses, a wider road cross-section may be required, at the discretion of the approving body, based on a recommendation from the city engineer or traffic consultant)".
  - a. Table VIII-A identifies a minimum roadway width of 36 feet for Nonresidential collectors with a 70 foot right-of-way or access easement. The applicant has requested a deviation, but should revise the plans to meet the standards in accordance with Staff comments (see comment 4.a.ii under General Comments).
  - c. Some traffic operational concerns exist with the proposed 30' wide roadway as it relates to truck circulation. The circulation plan provided on sheet 13 indicates fire truck turning paths crossing into oncoming traffic lanes near the two 90 degree roadway curves. An increased width, with center left turn lane, could reduce the impact to oncoming traffic.
  - d. It is not anticipated that any of the sites would warrant left turn passing lanes, but the inclusion of a center left turn lane would likely have positive impacts on traffic flow throughout the development.
2. The typical on plan sheet 12 shows a 30' width. The applicant should review Table VIII-A to ensure the typical cross-section proposed is in accordance with City standards for a Nonresidential collector and update the plan sheets accordingly.
3. The applicant is proposing that the roadways and drives within the development be private. As such, the development is subject to the requirements of Section 5.10 of the Zoning Ordinance. If the development is approved to be a private drive network, then there shall be a "major drive" (in this case, Adell Drive) – i.e., a principal internal loop drive or principal cul-de-sac drive that has direct access to an exterior public road (Section 5.10.B.i).
  - a. The applicant has updated the plans to include a cul-de-sac at the northern end of Adell Drive.
    - i. The applicant should indicate the length of Adell Drive. Section 11-194(a)(7) indicates a maximum cul-de-sac street length of 800 feet for all developments except for R-A zoned properties. If Adell Drive is in excess of 800 feet, the applicant may be required to seek a City Council variance for exceeding the maximum street length.

- ii. Additionally, Section 11-194(a)(7) states a required outside pavement radius of 54 feet for the cul-de-sac. The developer is proposing back-of-curb radius of 58 feet.
4. The applicant has proposed a secondary point of access to the site at the water tower with a gate per the Fire Department.
5. The applicant should indicate the proposed speed along Adell Drive so that additional criteria and requirements can be determined for the development.
6. The applicant shall provide a clear vision area for all drive approaches to Adell Drive in accordance with Section 216(b).
7. The applicant is encouraged to provide a joint drive approach system throughout the development. If each parcel within the development will be independently owned, the applicant may be required to execute ingress/egress easements, as applicable.
8. The applicant shall indicate same side driveway spacing for commercial driveways proposed along Adell Drive. Commercial drive approaches must be spaced according to the minimum requirements indicated in Section 11-216(d)(1)d. Should the minimum driveway spacing requirements not be met, the applicant may be required to seek a deviation.
9. The applicant should review the driveway placement with relation to the sharp horizontal curves along Adell Drive and consider alternative placement for purposes of creating safe and effective traffic operations throughout the development.
10. The applicant should review Section 216(d)(1)a-c to review driveway spacing and number of driveways provided per parcel. Should the proposed drive approach system not comply with the guidance in the ordinance, the applicant may be requested/required to provide justification and/or apply for deviations.
11. The applicant shall provide dimensions and details for the proposed commercial driveways to confirm compliance with City standards as presented in Figures IX.1 – IX.11, as applicable.
12. The applicant should consider pedestrian activity and connections across Adell Drive and to the various parcels throughout the site in an effort to provide a more walkable district. Any such pedestrian facilities shall be designed in accordance with City standards and the requirements of the American Disability Act.

## INTERNAL SITE OPERATIONS

*The following comments relate to the on-site design and traffic flow operations.*

### General Traffic Flow

1. Updated circulation plans shown on sheet 13 indicate that truck access at the proposed driveway locations may require trucks to enter oncoming traffic lanes in order to adequately complete turns to/from driveways at select locations, particularly near the 90 degree turns along Adell Drive.
2. The applicant should provide additional details for each site to indicate any unique traffic operations that may occur within the site. For example, if the Carvana has unique drop-off/pick-up operations, those should be noted and considered with the layout.
3. The applicant should provide turning radii and aisle widths throughout the entire development to confirm that (a) passenger vehicle operations can be accommodated, (b) fire and emergency vehicle operations can be accommodated, and (c) heavy vehicles and other delivery vehicles can be accommodated within the appropriate spaces. A specific example would be fire access to and from the west side of Unit 1 with the proposed geometry.
4. The applicant has requested a deviation for providing loading zones for units 1, 3 and 5 within the development. TC district requires that 10 square feet of loading zone be provided for each front foot of building. The applicant has provided additional information to build the case for not providing loading zones; however, there are concerns with not providing adequate loading zones for each site, especially if loading activities have the potential to impact parking and maneuvering aisle access, as may be suggested in the hotel parcels. The potential for future land use changes to occur should also be considered when determining whether or not to support a waiver for requiring loading zones for each unit. The applicant should indicate the proposed loading zones for each unit.
5. The applicant has proposed four (4) trash receptacle locations throughout the development.

- a. The applicant should provide additional trash receptacle locations to meet the needs of the eight (8) sites within the development, or request a deviation.
- b. The applicant should review the locations of the trash receptacles and make note that they may be positioned in locations that block parking spaces and may disrupt the flow of traffic during times of trash collections. If alternative locations that reduce the impact to parking spaces or traffic flow are available, it is recommended that they be relocated.

## Parking Facilities

1. The applicant has requested several parking deviations, as discussed in the “General Comments” section of this letter.
2. The applicant should reference the Planning review letter for information on parking quantity requirements. It should be noted that parking calculations were not provided with the PRO concept submittal, but reference to a Shared Parking Study was made. The applicant has not provided a shared parking study at the time of the PRO concept submittal; however, based on the results of deviation approvals and other City requirements regarding parking counts, landscape island requirements every 15 spaces, bicycle parking, loading zones, trash receptacles, a shared parking study may be beneficial, if the proposed sites are not expected to have overlapping parking needs throughout the day.
3. The applicant has included bays of parking with more than 15 spaces, which is not compliant with Section 5.5.3.C.ii.i of the Zoning Ordinance. The applicant should revise the plans to include landscape islands every 15 parking spaces or request a variance.
4. The applicant should provide dimensions for parking spaces and maneuvering aisles throughout the development, in accordance with Section 5.3.2 of the Zoning Ordinance.
  - a. The applicant may provide 19 foot long parking spaces (abutting a six inch curb, where applicable) or may provide 17 foot long parking spaces and provide a four inch curb with two foot clear overhang area in front of the parking space.
5. The applicant should provide dimensions for the landscaped areas throughout the development including length, width, radii, curb height, etc. to review conformance with Section 5.3.12 of the Zoning Ordinance.
  - a. The applicant references painted islands on sheet 12. The applicant should identify which islands are proposed to be painted and a justification as to why they cannot be in compliance either Section 5.3.12 which requires raised islands. A waiver is required if the applicant moves forward with painted islands.
6. The applicant has requested a deviation for the required 25 foot buffer between parking spaces and the street right-of-way line, street easement or sidewalk, whichever is closer. Because of the potential for parking maneuver conflicts with driveway access and in an effort to maintain a clear view area at the driveway access points, this deviation is discouraged.
7. The applicant should include bicycle parking throughout the development in accordance with Section 5.16 of the Zoning Ordinance, and provide quantities, locations and layouts for all proposed bicycle parking.

## Sidewalk Requirements

1. The applicant is generally proposing sidewalk along both sides of Adell Drive, except along the west side of Adell Drive near the approach to Crescent Blvd. The sidewalk should be extended along the west side of Adell Drive to provide sidewalks along all site frontages and to provide connectivity to Crescent Boulevard.
2. The location of the sidewalk along Adell Drive is proposed to be located such that the outside edge is 10 feet from the back of curb. The City's Engineering Design Manual requires that the outside edge of the sidewalk along a private street should be located 15 feet from the back of curb. The applicant should update the site plan accordingly or request a variance.
3. The applicant is proposing a sidewalk width of five (5) feet along Adell Drive. The City's Master Plan for Bicycle and Pedestrian Paths indicates that sidewalks should be a minimum of six (6) wide along collector streets. If Adell Drive is classified as a collector street, the sidewalk widths should be updated to six (6) feet or the applicant may request a variance.

4. The applicant should include any sidewalk ramp/detectable warning surface locations and details in future submittals.
5. The applicant should review the walkability of the development and provide additional non-motorized connections as applicable.

## SIGNING AND STRIPING

1. All on-site signing and pavement markings shall be in compliance with the Michigan Manual on Uniform Traffic Control Devices (MMUTCD). The following is a discussion of the proposed signing and striping.
  - a. The applicant should provide a signing quantities table and additional details (MMUTCD designation and proposed size) in future submittals.
  - b. The applicant should include proposed signing locations throughout the development.
  - c. The applicant should refer to the City of Novi Traffic Control Sign Standards for any proposed street name signs.
  - d. The applicant should update applicable details and provide the following notes and details on future site plans.
    - i. All roadside signs should be installed two feet from the face of the curb to the near edge of the sign.
    - ii. Single signs with nominal dimensions of 12" x 18" or smaller in size shall be mounted on a galvanized 2 lb. U-channel post. Multiple signs and/or signs with nominal dimension greater than 12" x 18" shall be mounted on a galvanized 3 lb. or greater U-channel post as dictated by the weight of the proposed signs.
    - iii. Traffic control signs shall use the Federal Highway Administration (FHWA) Standard Alphabet series.
    - iv. Traffic control signs shall have High Intensity Prismatic (HIP) sheeting to meet FHWA retroreflectivity requirements.
  - e. The applicant should provide notes and details related to proposed pavement markings.
    - i. Blue may be used for accessible parking spaces. Where a standard parking space is located next to an accessible parking space a white line shall about a blue line.
    - ii. The international symbol for accessibility shall be white or white with a blue background and white border. The applicant should provide a detail.
    - iii. Provide details for any proposed pavement marking arrows.


Should the City or applicant have questions regarding this review, they should contact AECOM for further clarification.

Sincerely,

**AECOM**



Maureen N. Peters, PE  
Senior Traffic/ITS Engineer



Paula K. Johnson, PE  
Senior Transportation Engineer



## FAÇADE REVIEW



June 24, 2018

*Façade Review Status Summary:*  
**See Summary on Page 5**

City of Novi Planning Department  
 45175 W. 10 Mile Rd.  
 Novi, MI 48375- 3024

Re: **FACADE ORDINANCE REVIEW**  
**Adell Center, JZ18-24**

Façade Region: 1, All Buildings, Zoning District: Rezoning to TC-1  
 25225365

The following is the façade review of the buildings proposed for the above referenced project. All buildings are required to comply with the Façade Ordinance Section 5.15. This Ordinance requires a minimum of 30% percentage Brick on all facades. All buildings must also comply with a higher standard as described in the Town Center (TC) Ordinance, Section 3.27.G, which requires that all facades be constructed primarily (greater than 50%) of Brick and Stone.

### **Drury Inn & Suites**

<b>Drury Inn &amp; Suites</b>	Front	Right	Left	Rear	Ordinance Maximum (Minimum)
Brick	50%	46%	46%	36%	100% (30% Min.)
Brick and Stone Combined	50%	46%	46%	36%	50% Min (TC Ord.)
Masonry 1 (Split Faced CMU 8x16)	2%	2%	2%	2%	10%
Masonry 2 (Burnished CMU 8x16)	5%	5%	5%	4%	10% (Footnote 2)
<b>EIFS</b>	<b>43%</b>	<b>47%</b>	<b>47%</b>	<b>58%</b>	25%

Our review of Drury Inn and Suites is based on drawings dated 4/12/18 by DCC Design of St. Louis Missouri. As shown above the percentage of EIFS on all facades exceeds the maximum amount allowed by the Ordinance. All other materials are in full compliance. The design uses significant percentages of Brick and EIFS resting on a simulated “stone base” which is comprised of Split Faced and Burnished Concrete Masonry Units (CMU). The EIFS is used on curved areas of the façade, window surrounds and the upper story and is generally framed by Brick and the simulated stone base. We believe that overall appearance of the building will meet the intent of the TC Ordinance which requires that the facades be constructed primarily of Brick and Stone, and that in this case the overall composition of the façades would not be significantly improved by reducing the amount of EIFS. Therefore it is our recommendation that a Section 9 Waiver be granted for the overage of EIFS on all facades.



## IFLY Indoor Skydiving

<b>IFLY Indoor Skydiving</b>	Front	Right	Left	Rear	Ordinance Maximum (Minimum)
Brick	<b>0%</b>	<b>0%</b>	<b>0%</b>	<b>0%</b>	100% (30% Min.)
Brick and Stone Combined	<b>0%</b>	<b>0%</b>	<b>0%</b>	<b>0%</b>	50% Min (TC Ord.)
Concrete, Painted	<b>59%</b>	<b>70%</b>	<b>70%</b>	<b>52%</b>	0%
EIFS	<b>34%</b>	<b>30%</b>	<b>30%</b>	<b>41%</b>	25%
Flat Metal	7%	0%	0%	7%	50%

Our review of IFLY was based on drawings dated 6/4/18 by Vibe-Stantec of Boulder, Colorado. As shown above the minimum percentage of Brick (30%) and minimum combined percentage of Brick and Stone (50%) is not provided, and the proposed percentage of Painted Concrete and EIFS exceed the maximum amounts allowed by the Ordinance on all facades. The applicant has provided a narrative that describes the reasons for these deviations. In the case of the concrete flowpath portion of the building, the applicant states that issues relating to weight and vibration make the installation of cladding untenable. A Section 9 Waiver may be justified for this portion of the building based on this consideration. However, the same condition does not apply to the office portion of the building which is currently proposed to be 100% EIFS. The EIFS consists of multiple colors that appear to be intended as corporate imaging. In general, the Façade Ordinance prohibits the use of façade materials to for the background of a sign so as to increase the visual presence of the building for the purpose of advertising. In this case however, the proposed colors are subdued and harmonize well with the overall façade. We find that there is no significant practical difficulty in providing Brick and/or Stone to the lower portion of both the office and flowpath portions of the building. We would suggest that the applicant consider using Brick and / or Stone on the lower 10' of these areas. With this change we believe that the building would meet the intent and purpose of the Façade Ordinance and that a Section 9 Waiver for the underage of Brick and Stone, overage of Painted Concrete and EIFS would therefore be justified.

## Fairfield Inn & Suites

<b>Fairfield Inn &amp; Suites</b>	South (Front)	East (Right)	West (Left)	North (Rear)	Ordinance Maximum (Minimum)
Brick	<b>14%</b>	<b>16%</b>	<b>16%</b>	<b>23%</b>	100% (30% Min.)
Cultured Stone	1%	3%	3%	2%	50%
Brick and Stone Combined	<b>15%</b>	<b>19%</b>	<b>19%</b>	<b>25%</b>	50% Min (TC Ord.)
EIFS	<b>67%</b>	<b>34%</b>	<b>34%</b>	<b>55%</b>	25%
Phenolic Simulated Wood	14%	<b>44%</b>	<b>44%</b>	16%	25%
Flat Metal, Painted	1%	2%	2%	1%	50%
Molded Cornice	3%	1%	1%	3%	15%

Our review of Fairfield Inn & Suites is based on drawings dated 4/24/18 by TSA Hospitality of Detroit, Michigan. As shown above the minimum percentage of Brick and Stone are not provided on all elevations, the percentage of EIFS exceeds the maximum amount allowed by the Ordinance on all elevations and the percentage of Simulated Wood exceeds the maximum amount allowed by the Ordinance on the right and left side elevations. The precise type of Simulated Wood is not clearly indicated on the drawings. A sample board should be provided to clearly identify the type, texture and color of all faced materials.

In the case of Fairfield In and Suites we are unable to recommend approval due to the above referenced deviations from the Façade and TC Ordinances; the approximately 30% underage in the combined percentage of Brick and Stone being the most significant. We believe the overall design could be brought into closer compliance with the Ordinance by incorporating greater amounts of Brick and/or Stone without compromise in the overall design. The drawings also indicate Fibrous Cement Siding as an “option”; this material is not permitted in Façade Region 1. The overage of Simulated Wood may be justified depending on the precise type, color and texture to be used. A determination about this material will be made after submittal of the sample board.

## Carvana

<b>Carvana</b>	Front (northeast)	Right (northwest)	Left (southeast)	Rear (southwest)	Ordinance Maximum (Minimum)
Brick	<b>7%</b>	30%	30%	39%	100% (30% Min)
Brick and Stone Combined	<b>7%</b>	<b>30%</b>	<b>30%</b>	<b>39%</b>	50% Min. (TC Ord.)
Spandrel Glass, Black	3%	2%	<b>2%</b>	2%	50%
Flat Metal Panels	10%	5%	<b>5%</b>	2%	50%
Display Glass	<b>80%</b>	<b>63%</b>	<b>63%</b>	<b>57%</b>	25%

Our review of Carvana is based on drawings dated 5/25/18, by WHN Architects of Charlotte North Carolina. As shown above the minimum percentage of Brick is not provided on the front façade, the minimum combined percentage of Brick and Stone is not provided on all facades and the percentage of Display Glass exceeds the maximum amount allowed by the Ordinance on all facades. The Façade Ordinance defines Display Glass as areas of vision glass that are intended through exposure to thoroughfares and pedestrian ways and lighting methods to strongly emphasize the items displayed within the building. Based on this definition the Carvana tower would be considered Display Glass. Similar to the Town Center bell tower, we believe that the Carvana tower will represent an iconic element within this project and the Novi shopping district at large. We believe this justifies a deviation from strict interpretation of the Façade Ordinance with respect to Display Glass. The applicant has added significant amounts of Brick in response to pre-application meeting comments. With this revision the underage of Brick and Stone have been reduced to the extent practice and do not significantly diminish the overall quality of the design. Although the rear (southwest) elevation faces to the interior of the Adell Center this elevation has 30% minimum Brick and the overall design is consistent with other facades. Therefore, it is our recommendation that the design is consistent with the intent and purpose of the Façade and TC Ordinances, and that a Section 9 Waiver be granted for the underage of Brick on the front facade, the underage of combined Brick and Stone on all facades, and overage of Display Glass on all facades.

## Planet Fitness

Planet Fitness	Front	Right (Not Ptovided)	Left (Not Ptovided)	Rear	Ordinance Maximum (Minimum)
Brick	39%	N/P	N/P	<b>20%</b>	100% (30% Min.)
Brick and Stone Combined	<b>39%</b>	N/P	N/P	<b>20%</b>	50% Min (TC Ord.)
CMU (Type T.B.D)	16%	N/P	N/P	<b>60%</b>	10% (Footnote 2)
EIFS	<b>37%</b>	N/P	N/P	14%	25%
Flat Metal	4%	N/P	N/P	4%	50%

Our review of Planet Fitness is based on drawings dated 6/23/15 by Moda4 Design of Dayton, Ohio. Our review is based on pectoral representation of materials because the types of materials were not called out with text on the drawings. Also, the side elevations were not provided. The applicant should submit updated drawings and a sample board clearly indicating the types of all façade materials for all facades. As shown above the percentage of Brick and Stone are below the minimum amount allowed by the Ordinance on the front and rear elevations and the percentage of CMU exceeds the maximum amount allowed by the Ordinance on the rear facades. The extent of CMU deviation will depend on the type proposed. As noted earlier the side elevations were not provided. At this time we are unable to recommend approval of the Planet Fitness facades due to significant underage of Brick and Stone on the rear façade, and overage of EIFS on the front façade and the aforementioned missing information in general (Type if CMU and side elevations).

**Development Marquee Sign** – A development marquee sign is proposed that the west terminus of Crescent Boulevard. This signs is subject to the Façade Ordinance to the same extent as the buildings above. Based in the small insert rendering on sheet 13 the sign appears to be constructed primarily of Brick and Stone and therefore appears to generally comply with the Façade Ordinance. More detailed drawings with material callouts will be required at a later date. It should be noted that other monument signs such as those for each unit are subject to the same requirements.

### General Façade Requirements Pertaining to all Buildings;

1. It should be noted that revisions after approval may require reapplication as described in Section 5.15.10 of the Ordinance. Except for the specific Section 9 Waivers granted, all building must comply with the Façade Ordinance are the time of Preliminary Site Plan application.
2. All roof top equipment must be screened from view from all on-site and off-site vantage points using compliant materials consistent with the building design. In this case the elevated views from the nearby highway overpass would be included.
3. Dumpster enclosures (excluding doors) are required to be constructed of brick or stone matching the primary buildings.
4. Monument signs, guard houses, gated entrance pedestals and other structures, if any are required to comply with the Façade Ordinance.

5. Inspections – The Façade Ordinance requires inspection(s) for all projects. Materials displayed on the approved sample board will be compared to materials delivered to the site. It is the applicant’s responsibility to request the inspection of each façade material at the appropriate time. Inspections may be requested using the Novi Building Department’s Online Inspection Portal with the following link. Please click on “Click here to Request an Inspection” under “Contractors”, then click “Façade”. <http://www.cityofnovi.org/Services/CommDev/OnlineInspectionPortal.asp>.

## **Façade Review Summary;**

**Drury Inn & Suites** – Section 9 Waiver recommended for the overage of EIFS on all facades.

**IFLY Indoor Sky Diving** – Section 9 Waiver recommended for the underage of Brick and Stone, and the overage of Painted Concrete and EIFS, contingent upon the addition of Brick or Stone on the lower 10’ of the office and tower structures on all facades.

**Fairfield Inn & Suites** – Not Approved due to underage of Brick and Stone. The applicant should consider adding Brick or Stone, for example for the full height of the façade above the entrance canopy (approximately 40’ wide x 48’ high).

**Carvana** - Section 9 Waiver recommended for the underage of Brick on the front facade, the underage of combined Brick and Stone on all facades, and the overage of Display Glass on all facades.

**Planet Fitness** – Approval not recommended due to various deviations and missing information.

If you have any questions regarding this matter please contact the City of Novi Department of Planning and Community Development.

Sincerely,  
DRN & Architects PC



Douglas R. Necci, AIA  
Novi Façade Consultant

## FIRE REVIEW



June 13, 2018

TO: Barbara McBeth- City Planner  
Sri Ravali Komaragiri- Plan Review Center  
Lindsay Bell-Plan Review Center  
Hannah Smith-Planning Assistant

**CITY COUNCIL**

**Mayor**  
Bob Gatt

**Mayor Pro Tem**  
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**Director of EMS/Fire Operations**  
Jeffery R. Johnson

**Assistant Chief of Police**  
Erick W. Zinser

**Assistant Chief of Police**  
Scott R. Baetens

RE: **Adell Center** (Old Expo Property), **43700 Expo Center Drive**

**JZ # 18-0024**

JSP# 18-27

PSP# 18-0065

**Project Description:**

Large commercial entertainment development, multi-use, and multi-buildings. Demolishing existing open vacant parking lot and re-development with 8 new commercial buildings. Redevelopment of main access driveway into new complex.

**Comments:**

- 1. Site plan shall provide more than one point of external access to the site. A boulevard entranceway shall not be considered as providing multiple points of access. Multiple access points shall be as remote from one another as is feasible. The requirement for secondary access may be satisfied by access through adjacent property where an easement for such access is provided. Secondary access drive MUST be added to the site plans for review. IFC 503.1.2. Access lane MUST be at least 20' wide.**
- 2. The minimum width of a posted fire lane is 20 feet. The minimum height of a posted fire lane is 14 feet. (Fire Prevention Ord.)**
- 3. Fire apparatus access drives to and from buildings through parking lots shall have a minimum fifty (50) feet outside turning radius and designed to support a minimum of thirty-five (35) tons. (D.C.S. Sec 11-239(b)(5)) - Plans show turning radii measured at 44', this will need to be re-designed for 50' outside and 30' inside turning. On the south east corner of Unit 6, from parking lot Units 7&8 by match line plan 3 going to the north and by Unit 1 west side of structure. On plan 4, in parking lot in front of unit 4 and on the south side of Unit 3 doesn't meet city standards.**
- 4. Fire access road MUST not exceed 150' in length. If the access road is longer than 150' you MUST provide some type of turn around. By Unit 5 on the east side.**

Novi Public Safety Administration  
45125 Ten Mile Road  
Novi, Michigan 48375  
248.348.7100  
248.347.0590 fax

cityofnovi.org

5. The distribution system in all developments requiring more than eight hundred (800) feet of water main shall have a minimum of two (2) connections to a source of supply and shall be a looped system. (D.C.S. Sec. 11-68(a))
6. For interior fire protection systems a separate fire protection line shall be provided in addition to a domestic service for each building. Individual shutoff valves for interior fire protection shall be by post indicator valve (P.I.V.) or by valve in well and shall be provided within a public water main easement. (D.C.S. Sec.11-68(a)(9))
7. Hydrants shall be installed in a manner to be in compliance with the City of Novi "Design and Construction Standards", Chapter 11 of the Code of Ordinances. Hydrant spacing is 300' from fire hydrant to fire hydrant. Not as the crow flies. 11-68(F)(1)c.
8. Fire hydrants location per the International Fire Code 2012 (IFC), you MUST have a fire hydrant within 600' from ALL portions of the exterior of the ground floor of the structure. (Not as the crow flies). IFC 507.5.1.
9. Hydrant outlets shall be eighteen (18) inches above final grade, measured from final grade to bottom of outlet. (D.C.S. Sec. 11-68 (f)(1)c.4)
10. No parking shall be allowed within fifteen (15) feet of a hydrant. (D.C.S. Sec. 11-68 (f)(1)c.4)
11. Additional hydrants may be required, depending on the specific hazard or use, to protect the structure. (D.C.S. Sec. 11-68 (f)(1)c.6)
12. All hydrants shall have two 2-1/2 inch male outlets and one 4-1/2 inch male steamer connection. Threads shall be National Standard. (D.C.S. Sec. 11-68 (f)(2))
13. Fire department connections shall be located on the street side of buildings, fully visible and recognizable from the street or nearest point of fire department vehicle access or as otherwise approved by the code official. (International Fire Code)
14. Immediate access to fire department connections shall be maintained at all times and without obstruction by fences, bushes, trees, walls or any other object for a minimum of 3 feet (914 mm). (International Fire Code)

15. Proximity to hydrant: In any building or structure required to be equipped with a fire department connection, the connection shall be located within one hundred (100) feet of a fire hydrant. (Fire Prevention Ord. Sec. 15-17)
16. Maximum Building heights will need to be addresses, as the Novi Fire Department has only 1 - 100' aerial apparatus, and is limited to approx.. 55' height Emergency Access. Proposed buildings in access of 55' may need to conform to the 2015 International Building Code standards for High-Rise (Type I or Type II) construction.

**GENERAL:**

To facilitate fire protection during site preparation and construction of buildings, the following are required:

17. Water mains and fire hydrants shall be installed prior to construction above the foundation. Note this on all plans.
18. The building address is to be posted facing the street throughout construction. The address is to be at least 3 inches high on a contrasting background. Note this on all plans.
19. Street names on suitable poles shall be established and installed prior to construction above the foundation. Note this on all plans.
20. Prior to construction above the foundation of non-residential buildings, an all-weather access road capable of supporting 35 tons shall be provided. Note this on all plans.
21. Free access (unobstructed) from the street to fire hydrants and to outside connections for standpipes, sprinklers or other fire suppression equipment, whether permanent or temporary, shall be provided and maintained at all times.
22. Fire prevention practice during construction shall be in accordance with the adopted Building Code and Fire Prevention Code
23. The installation of security gates across a fire apparatus access road shall be approved by the fire marshal. Where security gates are installed, they shall have an approved means of emergency operation. The security gates and the emergency operation shall be maintained operational at all times. Electric gate operators, where provided, shall be listed in accordance with UL 325. Gates intended for automatic operation shall be designed, constructed and installed to comply with the requirements of ASTM F 2200



**Recommendation:**  
APPROVAL WITH CONDITIONS

Sincerely,

A handwritten signature in black ink, appearing to read 'KSP', with a long horizontal flourish extending to the right.

Kevin S. Pierce-Fire Marshal  
City of Novi – Fire Dept.

cc: file

**SIGN PERMIT REVIEW**



# ORDINANCE SIGN REVIEW REPORT

June 22, 2018

## Sign Permit Review

ADELL CENTER

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**APPLICANT: ADELL / ORVILLE PROPERTIES LLC**  
**SITE LOCATION: 96 / NOVI ROAD**

### 1. TYPE OF SIGN: BUSINESS PARK IDENTIFICATION SIGN – Expressway frontage

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**Code requirement:** 28-1 Business Center Sign

**Proposed:** One (1) business center sign

**Staff Comments:** The property does not meet the definition of a business center ie; retail, industrial, office... If the applicant instead requests the 'Expressway Frontage' sign as a 'ground sign' then the following comments apply

### 2. NUMBER OF SIGNS:

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**Code requirement:** 28-5 (a) one ground sign and one wall sign are permitted

**Proposed:** 1 ground sign

**Staff Comments:** permitted

### 3. SIGN LOCATION:

---

**Code requirement:** 28-5(f)(1) Ground sign placement

**Proposed:** Expressway Frontage sign placement was not indicated on the plan

**Staff Comments:** A ground sign must be at least 3 feet from right-of-way (see remaining code section for more detail)

### 4. SIGN AREA:

---

**Code requirement:** 28-5(b)(2)a. Ground signs shall not exceed thirty (30) sq. ft. OR one (1) sq.ft. of sign area for each 2 feet of setback from the nearest street centerline – Maximum size 100 sq. ft.

**Proposed:** 54.25 square feet – calculations not correct on plan

**Staff Comments:** 28-5(b)(2)a. the sign 'area' allowance cannot be calculated because the distance from nearest adjacent thoroughfare was not indicated on the plan.

28-1 - The diagram for the sign measurement is inaccurate because the entire sign was not measured in accordance with 28-1 'Area of sign'. The whole sign including background must be included in the calculation. See diagram in 28-1.

### 5. SIGN HEIGHT:

---

**Code requirement:** 28-5(a) - Six (6) feet maximum height

**Proposed:** 15'

**Staff Comments:** a variance of 9 feet would be required

### 6. OTHER:

---

**Code requirement:** 28-4

**Proposed:**

**Staff Comments:** No sign shall be located within, project into, or overhang any public right-of-way .....

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or [munderhill@cityofnovi.org](mailto:munderhill@cityofnovi.org).

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Maureen Underhill, Code Compliance Officer



# ORDINANCE SIGN REVIEW REPORT

June 22, 2018

Sign Permit Review

ADELL CENTER

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**APPLICANT: ADELL / ORVILLE PROPERTIES LLC**

**SITE LOCATION: 96 / Novi Road**

## 1. TYPE OF SIGN: BUSINESS PARK ENTRANCE SIGN

---

**Code requirement:** 28-1 SIGN (5); 28-5(d)(11); 28-5(f)(3)

**Proposed:** 2 ground signs depending on the distance between ADELL & CENTER (no greater than 2 feet distance between the signs is permitted)

**Staff Comments:** One entranceway sign is permitted at each entrance. One (1) entrance is proposed

## 2. NUMBER OF SIGNS:

---

**Code requirement:** 28-5(a)

**Proposed:** 1 ground sign

**Staff Comments:** 1 ground sign is permitted

## 3. SIGN LOCATION:

---

**Code requirement:** 28-1 SIGN (5); 28-5(d)(11); 28-5(f)(3)

**Proposed:** 1 sign at entrance of planned development

**Staff Comments:** The sign shall be placed not less than 10 feet from any street right-of-way. The distance from the nearest street right-of-way was not indicated on the plan.

## 4. SIGN AREA:

---

**Code requirement:** 28-1 & 28-5(b)(2)a.

**Proposed:** 32.66 square feet

**Staff Comments:** The diagram for the sign measurement is inaccurate because the entire sign was not measured in accordance with 28-1 'Area of sign'. The whole sign including background must be included in the calculation. See diagram in 28-1.

28-5(b)(2)(a) - ground signs shall not exceed 30 square feet OR one (1) square foot of sign area for every two(2) feet of setback from the nearest street centerline.

## 5. SIGN HEIGHT:

---

**Code requirement:** 28-5(a) – Six (6) feet maximum height

**Proposed:** 15 feet

**Staff Comments:** a variance of 9 feet would be required

## 6. OTHER:

---

**Code requirement:**

**Proposed:**

**Staff Comments:**

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or [munderhill@cityofnovi.org](mailto:munderhill@cityofnovi.org).

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Maureen Underhill, Code Compliance Officer



# ORDINANCE SIGN REVIEW REPORT

June, 22 2018

Sign Permit Review

ADELL CENTER

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**APPLICANT: DRURY INN**  
**SITE LOCATION: 96/ NOVI ROAD**

**1. TYPE OF SIGN: DRURY INN WALL SIGN – NORTH ELEVATION on building**

**Code requirement:** 28-5(d)(8) – two (2) wall signs are permitted as the building will be over 40,00 square feet in size

**Proposed:** 3 wall signs

**Staff Comments:** a variance will be required for more than 2 wall signs

**2. NUMBER OF SIGNS:**

**Code requirement:** 28-5(d)(8) – two (2) wall signs are permitted as the building will be over 40,00 square feet in size

**Proposed:** 3 wall signs

**Staff Comments:** a variance would be required for more than 2 wall signs

**3. SIGN LOCATION:**

**Code requirement:**

**Proposed:** North elevation

**Staff Comments:**

**4. SIGN AREA:**

**Code requirement:** 28-5(b)(1)b. A wall sign displayed on a building occupied by one business shall not exceed one (1) square foot of signage for every 2 feet of setback from nearest adjacent thoroughfare..... no greater than 15% of the frontage of the building and not greater than 250 square feet

**Proposed:** 299.84 square feet

**Staff Comments:** based on 235 feet from centerline of adjacent private road a sign of 117.5 square feet would be permitted. A variance of 182.34 square feet would be required for the sign as proposed

**5. SIGN HEIGHT:**

**Code requirement:** 28-1(17) Wall sign ..... the sign must not extend beyond the height of the wall

**Proposed:** not above height of the wall

**Staff Comments:** approvable

**6. OTHER:**

**Code requirement:**

**Proposed:**

**Staff Comments:**

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or [munderhill@cityofnovi.org](mailto:munderhill@cityofnovi.org).



# ORDINANCE SIGN REVIEW REPORT

June, 22 2018

Sign Permit Review

ADELL CENTER

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**APPLICANT: DRURY INN**  
**SITE LOCATION: 96/ NOVI ROAD**

**1. TYPE OF SIGN: DRURY INN WALL SIGN – SOUTH ELEVATION on building**

**Code requirement:** 28-5(d)(8) – two (2) wall signs are permitted as the building will be over 40,00 square feet in size

**Proposed:** 3 wall signs

**Staff Comments:** a variance will be required for more than 2 wall signs

**2. NUMBER OF SIGNS:**

**Code requirement:** 28-5(d)(8) – two (2) wall signs are permitted as the building will be over 40,00 square feet in size

**Proposed:** 3 wall signs

**Staff Comments:** a variance would be required for more than 2 wall signs

**3. SIGN LOCATION:**

**Code requirement:**

**Proposed:** South elevation

**Staff Comments:**

**4. SIGN AREA:**

**Code requirement:** 28-5(b)(1)b. A wall sign displayed on a building occupied by one business shall not exceed one (1) square foot of signage for every 2 feet of setback from nearest adjacent thoroughfare..... no greater than 15% of the frontage of the building and not greater than 250 square feet

**Proposed:** 299.84 square feet

**Staff Comments:** based on 235 feet from centerline of adjacent private road a sign of 117.5 square feet would be permitted. A variance of 182.34 square feet would be required for the sign as proposed

**5. SIGN HEIGHT:**

**Code requirement:** 28-1(17) Wall sign ..... the sign must not extend beyond the height of the wall

**Proposed:** not above height of the wall

**Staff Comments:** approvable

**6. OTHER:**

**Code requirement:**

**Proposed:**

**Staff Comments:**

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or [munderhill@cityofnovi.org](mailto:munderhill@cityofnovi.org).





# ORDINANCE SIGN REVIEW REPORT

June 22, 2018

## Sign Permit Review

ADELL CENTER

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**APPLICANT:** DRURY INN

**SITE LOCATION:** 96 / NOVI ROAD

**1. TYPE OF SIGN:** DRURY INN WALL SIGN – EAST ELEVATION on building

**Code requirement:** 28-5(d)(8) – two (2) wall signs are permitted as the building will be over 40,00 square feet in size

**Proposed:** 3 wall signs

**Staff Comments:** 2 signs are permitted - a variance would be required for a third sign

**2. NUMBER OF SIGNS:**

**Code requirement:** 28-5 (d)(8) – two (2) wall signs are permitted as the building will be over 40,00 square feet in size

**Proposed:** 3 wall signs

**Staff Comments:** a variance would be required for a third sign

**3. SIGN LOCATION:**

**Code requirement:**

**Proposed:** East Elevation

**Staff Comments:**

**4. SIGN SIZE:**

**Code requirement:** 28-5(b)(1)b. A wall sign displayed on a building occupied by one business shall not exceed one (1) square foot of signage for every 2 feet of setback from nearest adjacent thoroughfare..... no greater than 15% of the frontage of the building and not greater than 250 square feet

**Proposed:** 186.47 square feet

**Staff Comments:** based on 165 feet from centerline of adjacent private road a sign of 82.5 square feet would be permitted. A variance of 103.97 square feet would be required for the sign as proposed

**5. SIGN HEIGHT:**

**Code requirement:** 28-1(17) Wall sign ..... the sign must not extend beyond the height of the wall

**Proposed:** not above height of the wall

**Staff Comments:** approvable

**6. OTHER:**

**Code requirement:**

**Proposed:**

**Staff Comments:**

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or [munderhill@cityofnovi.org](mailto:munderhill@cityofnovi.org).

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Maureen Underhill, Code Compliance Officer

**SUPPLEMENTAL INFORMATION PROVIDED WITH THE PRO CONCEPT PLAN SUBMITTAL**

- 1. PRO Narrative**
- 2. Benefits to Public**
- 3. Request to Waive MPZ meeting**
- 4. Existing Zoning v Proposed Zoning**
- 5. Community Impact Statement**
- 6. I-Fly Narrative and Follow-Response to Façade Comments**
- 7. Drury Follow-Response to Façade Comments**

**PRO NARRATIVE**  
**June 05, 2018**

June 5, 2018

Ms. Barbara McBeth, AICP  
City Planner  
City of Novi – Planning Department  
47175 10 Mile Road  
Novi, MI 48375

Subject: **Adell Center Rezoning, EXP, Exposition District to TC, Town Center District with a Planned Rezoning Overlay (PRO)**  
43700 Expo Center Drive, Novi  
Parcel ID: 22-15-476-045

Dear Ms. McBeth:

Please find the attached application, site plans and fees for consideration by the city to re-zone the above referenced parcel of land from EXPO, Exposition District, to TC, Town Center District with a Planned Rezoning Overlay (PRO). We are excited by the opportunities this project presents for the City of Novi and signature image it will create from I-96. The following is a brief description of the property history and narrative supporting the requested rezoning and why it is the most appropriate development for this site.

***Property History***

The subject parcel of land is located on the South side of I-96, between I-96 and Grand River Avenue, West of Novi Road, in the City of Novi, Michigan. This property has been in Kevin Adell's family since 1965, when his father purchased the property and built his auto parts company, Adell Industries. Afterward, the same building housed the Mohawk Liqueur Corporation. In 1992, the property became home to the Novi Expo Center, which ultimately closed, and the building was demolished in 2012.

***Current Site Conditions***

The only structure remaining on the property at the present time is the existing water tower. Some remnants of the former Novi Expo Center still remain, including the concrete building slab, asphalt parking lot and site entrance. The approximate seven (7) acres of land on the south end of the property contains an existing creek, wetlands, floodplain and several trees. Access to and from the site is very limited with only one access connection to Crescent Boulevard, located near the southeast corner of the property. Crescent Boulevard extends to the property from Novi Road, where a signalized intersection is located. It is understood that the City of Novi will be extending Crescent Drive from the current terminus to Grand River Avenue.

***Overall Development Objective***

As a way to carry on his family legacy, Kevin Adell wishes to develop this property in a manner that creates an attractive and exciting hotel and entertainment center. Mr. Adell's goals are consistent with what is believed to be the city's goals. The Novi City Council adopted in the 2015 Economic Development Goals and Strategies in 2015, Article 3, Section 3.2 which reads, "Market

redevelopment of Adell site, using enhanced services and the MEDC Redevelopment Ready Communities Program.” More specifically we are creating a horizontal mixed-use district that is in keeping with the RRC Best Practice 2.1.

Prior to meeting with and/or making any submittals to the City of Novi, Mr. Adell prepared a conceptual layout for the property and presented his plan, visions and strategy to the market. To date, all of the sites have committed purchasers including very unique and national companies such as Drury Hotels, I-FLY Indoor Skydiving, Planet Fitness, Texas Roadhouse and Carvana. In recent weeks, much media excitement has been generated by the proposed uses. Two of the building sites (Units 6 & 7) have tentative commitments and are in the process of finalizing their conceptual layout plans. Mr. Adell is intending to keep the last remaining building site (Unit 4) for purposes of small hosting seasonal events and for overflow parking if needed.

### ***Rezoning Request***

Throughout the planning process, our planning and design team has remained focused on meeting the site requirements of the committed purchasers, while creating a development to be consistent with the city ordinances and master plan. Due to the uniqueness of some of the intended purchasers, there is no single zoning classification that applies to this creative proposed development. Upon reviewing the development objectives and reviewing staff comments from the predevelopment meeting, it was determined that the proposed TC, Town Center District with a Planned Rezoning Overlay (PRO) would be the best option. Also due to the uniqueness of the site and uses being proposed, a handful of ordinance deviations are required, using the intended flexibility of the PRO. Some of the benefits of this PRO rezoning request include:

1. The Improvement and unified redevelopment of an existing property that presents a poor image of the community;
2. Creation of an attractive entryway into the City of Novi, along the highly visible I-96 ramp, by fronting front quality buildings and uses that are unique to this area;
3. The introduction of higher-quality uses under the TC, Town Center District than might be possible under the current EXPO, Exposition District;
4. Enhanced, coordinated site design that will greatly surpass that of individual developments under the current EXPO District;
5. Creating a cumulative attraction destination for visitors by clustering indoor recreation, hotel, restaurant and destination sales uses together in a coordinated fashion; and
6. Implementation of a horizontal, mixed-use development that meets the standards of the MEDC Redevelopment Ready Communities (RRC) Program and also matches the intent of the City of Novi Master Plan.

The subject property is currently zoned EXPO, Exposition District, from when the site was home to the Novi Expo Center. Now that the Suburban Showplace Collection is located farther to the west on Grand River Avenue, it is highly unlikely, and even unwanted per the City of Novi Master Plan, that another expo center be built. Some of the proposed uses under this application, such as hotels and restaurants, are only allowed as part of an exposition facility in the EXPO District. Still other uses, such as warehousing, public utility buildings, and manufacturing with outdoor storage and distribution, might not be the best transitional uses from the abutting TC, Town Center District to the east. The mixing of extensive tractor trailer traffic with passenger vehicle traffic might prove problematic and negatively impact the business of the commercial uses fronting Novi Road. Nor

would the more industrial-type uses provide an upscale image for a key entryway to the City of Novi, since the site abuts the I-96 ramp and can be seen by thousands of motorists each day.

The proposed development would compliment the abutting uses to the east and utilize the road network already planned for the area. The uses would generate less tractor trailer traffic than some of the above-mentioned permitted uses in the current EXPO District and also present an attractive development at one of the city's primary entryways. Moreover, the PRO option combined with the TC, Town Center District, will help provide the flexibility necessary to accommodate some valued and innovative uses in the city. The architecture for two of the uses (Carvana and I-FLY Indoor Skydiving) will be unique to the area and create visual interest for drivers passing by and exiting onto Novi Road. Since these are new uses to the City of Novi the flexibility of the PRO District will help accommodate the developments while still meeting the intent of ordinance standards.

#### ***Compliance with City of Novi Master Plan***

The Future Land Use (FLU) designation for the subject site is Office, Research, Development and Technology. The FLU description in the master plan indicates that "This land use is designated for a variety of medium-scale and large-scale general and medical office buildings or complexes and research, development and technology facilities, with or without related manufacturing or warehouse facilities. The area may also include facilities for office, research and development support services, human care, *hotels*, motels, higher education and *indoor or outdoor recreation*. In addition, this designation incorporates the former Office, Research, Development, and Technology with Retail Service Overlay (see text that follows) that may allow a limited amount of retail services in appropriate locations to serve the employees and visitors of these use areas, including but not limited to fuel stations, car washes, *restaurants (including drive-through)* and convenience stores as implemented through the Zoning Ordinance.

As shown on the PRO Plan, each of the proposed uses match the above FLU designations including hotels, indoor recreation and restaurants. The only use that does not specifically fall under this FLU description is the proposed Carvana facility, which is a relatively new use. It will certainly serve employee and visitors of the above uses and would fall under the phrases "retail uses" and "including but not limited to" which is intended to provide the City with discretion for such uses that could not be anticipated. Moreover, this use requires a highly visible location from I-96 and is similarly no more objectionable or impacting than the uses specifically identified above. In fact, it will likely have less impact on the infrastructure by generating less vehicle trips and related parking. We recognize that an Unlisted Use Determination, per Section 4.87 of the ordinance, will be needed for Carvana and is addressed in a separate letter.

In summary, the proposed PRO development does meet the Future Land Use designation for the site, as detailed in the 2016 City of Novi Master Plan Update. The uses not only meet the descriptions in the Office, Research, Development and Technology district, but also present a quality re-use of the former Novi Expo site. In addition, the proposed uses provide an appropriate transition between the retail uses to the east and industrial uses to the west, recognizing that the site is oddly-shaped and fronts one of the ramps to I-96.

*Requested Deviations: (which ones still apply under the revised plan)*

As indicated above, no single City of Novi zoning classification allows all of the proposed uses. In order to accommodate all of the committed purchasers, the following deviations from the City of Novi zoning ordinances are requested as part of the proposed PRO development.

- Planning Deviation for maximum building height of structures not to exceed the greater of seven (7) stories or eighty-five feet in height which is consistent and compatible to the buildings of the proposed unit owners facilities nationally. This deviation refers to section 3.27.2.A.i of the City of Novi code of ordinances. This deviation request would allow for the building height of the proposed Drury Hotel which is 84'-5" in height. Based on discussions with Drury, it is understood that the proposed hotel will be built to category 1 standard. Based on discussion at the pre-application meeting, we understand that the fire department has no objection to the height of the Drury building if it is constructed to category 1 standards. The proposed IFLY building is under 58' in height. The existing water tower is in excess of the maximum building height, however it is an existing non-conforming use that is not occupied.
- Planning Deviation for the requirement for frontage upon a public street to allow for the creation of a separate condominium unit for the existing on-site water tower. It is specifically intended that the proposed unit for the water tower is tucked away from the common access drive to minimize the desire of the general public to approach the water tower. Access to the proposed unit that includes the existing water tower will be via a proposed private easement across the adjacent unit. Unit 9 will be subject to all of the rules and restrictions of the master deed, but will not be part of the common open space (general common element) so that the cost of maintaining the water tower does not become the responsibility of the condominium association. This deviation refers to section 5.12 and section 6.3.2A of the City of Novi code of ordinances.
- Planning Deviation for the requirement that the development has access to a major thoroughfare. The general location of this development has previously been a significant traffic generator from the previous Novi Expo center. We feel that with the major infrastructure improvements that were constructed at the end of the tenure of the Novi Expo including roadway and boulevard improvements as well as the more recent improvements that were constructed with the adjacent retail center, the existing infrastructure has the capacity to accommodate the off-site impacts of this development. Per our traffic consultant, the on-site private Adell Drive improvements also have the capacity to handle the proposed improvements. This deviation refers to section 5.13 of the City of Novi code of ordinances.
- City Council deviation from the Engineering Design Standards requirement of a 60' wide access easement. Our proposal includes a 30' area of general common element which coincides with the proposed 30' wide (back of curb to back of curb width). The proposed condominium unit lines will extend to the line of general common element. An access easement will extend beyond the general common element for an additional 10 feet to accommodate a roadside greenbelt and sidewalk area.
- Planning Deviation for the requirement for Minimum lot area and width to allow for the creation of a separate condominium unit for the existing on-site water tower. It is specifically intended that the proposed unit for the water tower is tucked away from the common access drive to minimize the desire of the general public to approach the water tower. Access to the proposed unit that includes the existing water tower will be via a proposed private easement



across the adjacent unit. Unit 9 will be subject to all of the rules and restrictions of the master deed, but will not be part of the common open space (general common element) so that the cost of maintaining the water tower does not become the responsibility of the condominium association. This deviation refers to section 6.3.2.D of the City of Novi code of ordinances.

- Planning Deviation for the requirement for Wetland/Water Course Setback to allow for temporary Wetland/Water Course setback impact as required to remove the existing parking lot and storm sewer infrastructure as well as grading and construction of the new sanitary sewer line, water main line and temporary grading for the new buildings to be located on proposed Units 3, 4 and 5. This deviation refers to section 3.6.2.M of the City of Novi code of ordinances.
- Planning Deviation for Modification of Parking Setback Requirements to allow for a 20 foot minimum front yard setback from proposed sidewalk easement and 0' minimum side yard parking area setbacks within the Adell Center development. A deviation is being requested for a reduction in the width of the Adell Drive roadway easement (see deviation request 4 above). The landscaped areas between the Adell Drive and adjacent parking areas shall comply with the landscape standards set forth in Section 5.5. This deviation refers to section 3.6.2.Q of the City of Novi code of ordinances.
- Planning Deviation to allow for the recording of proposed site condominium units 3, 4 and 5 lying partially within the flood plain of the Walled Lake Branch of the Middle Rouge River. As part of the site plan, there will be no proposed impacts/alterations to the existing flood plain from any units 3 and 5. There will be a pedestrian bridge and walking path constructed across Unit 4 that will be within the flood plain area. This deviation refers to section 4.03.A of the City of Novi code of ordinances.
- Planning Deviation for Parking, Loading, Signs, Landscaping, etc. to eliminate requirement for loading areas for the following Units.
  - i. Unit 1 – IFLY – Ifly has no regularly schedule deliveries that would require a designated loading/unloading area.
  - ii. Unit 3 – Fairfield Inn – Per Fairfield Inn representatives, their operational requirements do not necessitate the need for a designated loading space.
  - iii. Unit 5 – Drury Hotel – Drury’s deliveries are all made during the daytime non-peak parking times when the regular parking spaces are not in use.This deviation refers to section 3.27.1 of the City of Novi code of ordinances.
- Planning Deviation to allow for Maximum allowable wall material percentages per building facade according to the following Units.
  - i. Unit 1 – IFLY – See sheet plan sheets submitted for percentages
  - ii. Unit 3 – Fairfield – See sheet plan sheets submitted for percentages
  - iii. Unit 5 – Drury – See sheet plan sheets submitted for percentages
  - iv. Unit 8 – Carvana – See sheet plan sheets submitted for percentagesThis deviation refers to section 5.15 of the City of Novi code of ordinances.
- City Council Deviation from the existing sign ordinance 17-188 to allow for building signage for the following proposed units.
  - i. Unit 1 – IFLY – See sheet plan sheets submitted for percentages
  - ii. Unit 5 – Drury Hotel – See sheet plan sheets submitted for percentages

iii. Unit 8 – Carvana – See sheet plan sheets submitted for percentages

This deviation refers to section 3.27.1 of the City of Novi code of ordinances.

- Planning Deviation for minimum number of parking spaces per unit and as recommended by a shared parking study as prepared by the applicants traffic consultant and agreed upon by the city's traffic engineer. This deviation refers to section 5.2 of the City of Novi code of ordinances. This deviation request would allow the unit owners to share parking with their neighboring unit owners based on peak hour parking needs as outlined in the shared parking study.
- Planning Deviation to allow for a minimum of fifteen (15) percent of the gross overall site area for each site within the development to be included as part of the non-developed area in the southerly portion of the overall development. The benefit of accommodating all of the required open space in a general common element area is that the association will be responsible for maintenance rather than relying on each unit owner to maintain the required open space areas. This deviation refers to section 3.27.1.F of the City of Novi code of ordinances.
- Planning Deviation to allow for future renovations, alterations, or additions are made to the buildings within this development, the exterior building facades of the entire building shall be brought into compliance with the approved PRO agreement for this development. This deviation refers to section 3.27.1.G. This deviation request is to ensure future building modifications are in compliance with approved PRO agreement
- City Council variance to eliminate the requirement for a traffic impact study due to the city is currently undertaking a traffic study for this area.

In addition to the general site deviations requested above, the following additional site specific deviations are requested.

- Unit 3 – Fairfield Inn - Planning Deviation from the requirement for no parking stalls to be located adjacent to a parking lot entrance from a street (public or private) shall be located closer than two (2) feet from the street right-of-way (ROW) line, street easement or sidewalk, whichever is closer. This deviation request is to allow a parking stall to be located a minimum of 11.9 feet from the roadway easement. This deviation refers to section 5.3.13 of the City of Novi code of ordinances.
- Unit 3 – Fairfield Inn – Front Parking Setback from the roadway easement to allow for a 11.9' setback from the roadway easement.
- Unit 3 – Fairfield Inn – Front building setback for a proposed 53.5 foot setback. This building position offers maximum use of the property for vehicular parking and circulation. The large area between the building location and property line has been maintained that will be utilized as expansive landscaping and pedestrian use.

Ms. Barbara McBeth, AICP, City Planner  
City of Novi – Planning Department  
June 5, 2018  
Page 7 of 7

On behalf of the applicant and based on the above description and attachments, we kindly request positive consideration by the City of Novi on this matter. The applicant is aware that a site plan approval will be required for any proposed use on the property.

If you have any questions, please do not hesitate to contact me.

Sincerely,  
GreenTech Engineering, Inc.



Daniel J. LeClair, PE, PS  
President

Attachments

June 5, 2018

Ms. Barbara McBeth, AICP  
City Planner  
City of Novi – Planning Department  
47175 10 Mile Road  
Novi, MI 48375

Subject: Proposed Development under Proposed Zoning and Current Zoning Districts  
Proposed Adell Center Development

Dear Ms. McBeth:

Please find this statement describing the proposed development under the proposed zoning and the current zoning districts. The proposed development site is currently zoned EXPO District. Our proposal is to re-zone the property to Town Center District (TC).

Our review of the current City of Novi zoning ordinance finds that several of the proposed uses are not allowed in the current EXPO zoning district. In addition, some of the proposed buildings would not be allowed in the current zoning district without the request of variances. As a result of the proposed development and upon discussion with city staff, it is understood that the proposed use could be accomplished with city council approval of a Planned Rezoning Overlay (PRO). It is also understood that a PRO can only be accomplished with a re-zoning of the property. The closest zoning district classification that can accommodate the proposed use is the Town Center District (TC).

With the above said, the rezoning of the property is the only way to successfully develop this property is by way of re-zoning the property to Town Center (TC) utilizing the Planned Rezoning Overlay (PRO) option. With City Council approval of the proposed re-zoning, our client Kevin Adell is very confident that this development will be a very visible and successful project that all of the residents of the City of Novi can be proud of.

If you have any questions, please do not hesitate to contact me.

Sincerely,  
GreenTech Engineering, Inc.



Daniel J. LeClair, PE, PS  
President

**BENEFITS TO PUBLIC**

June 5, 2018

Ms. Barbara McBeth, AICP  
City Planner  
City of Novi – Planning Department  
47175 10 Mile Road  
Novi, MI 48375

**Subject: Adell Center Rezoning, EXP, Exposition District to TC, Town Center District  
with a Planned Rezoning Overlay (PRO)**  
43700 Expo Center Drive, Novi  
Parcel ID: 22-15-476-045

Dear Ms. McBeth:

Per Kevin Adell, the following is our list of public benefits, relating to the above referenced project:

This proposed \$125M development provides public benefit to local residences and businesses in the following ways:

- 1) The proposed development will convert a vacant parcel of property at a major intersection and entryway into the City of Novi.
- 2) The development of this property will reduce any chances of crime associated with a vacant parcel by providing new development with continuous movement of people and vehicles throughout the property.
- 3) This development will convert a property that is currently zoned EXPO Center into a zoning district that will allow a use that is beneficial to neighborhood businesses and the community in general.
- 4) This development will help produce a more positive image of the City of Novi by the 100,000 + motorists traveling along Interstate 96 on a weekly basis.
- 5) The approval of this development will bring additional entertainment, overnight stay and dining opportunities to the city that will benefit the City of Novi residents as well as bring in residents and visitors from neighboring communities.
- 6) This development is centrally located to several communities that will serve as weekend long youth sporting tournaments and weekly events held at the Suburban showplace. These events typically bring in people from all over southeast Michigan and the United States.
- 7) The approval of this development will trigger a sale of the proposed units within the Adell Center thereby generating an increase in property values in addition to the value of neighboring properties.
- 8) The approval of this development will create 200-300 temporary construction jobs and permanent jobs.

- 9) The approval of this development will increase the tax base within the City of Novi. As reported by the Mayor at a recent City Council meeting, the city desires to increase the tax base to fund additional services such as police, fire and parks departments.
- 10) It is estimated that this development will increase the tax base by over \$3M annually, plus an additional personal property tax generated from the new businesses.
- 11) The approval of the proposed development will include the improvement of over three acres of existing city regulated woodlands/wetland areas to allow for better access by the public.
- 12) The approval of this development will include a consistent and cohesive streetscape and signage package throughout.
- 13) The proposed development includes new public art (pocket parks) locations for placement of community art.

If you have any questions, please do not hesitate to contact me.

Sincerely,  
GreenTech Engineering, Inc.



Daniel J. LeClair, PE, PS  
President

Attachments

**LETTER OF REQUEST TO WAIVE MPZ MEETING**



June 11, 2018

Ms. Barbara McBeth, AICP  
City Planner  
City of Novi – Planning Department  
47175 10 Mile Road  
Novi, MI 48375

Subject: **Adell Center**

Dear Ms. McBeth:

Per our discussion last week I notified Kevin Adell that the July 20<sup>th</sup> Master Plan and Zoning committee meeting agenda was full. As an alternative, you mentioned that we could request a waiver from that sub-committee and move the project directly to the planning commission for the public hearing.

On behalf of Kevin Adell, for the sake of the very tight project schedule, we kindly request a waiver from attendane at the Master Plan and Zoning committee meeting.

As previously requested, we are also requesting that the city publish for a public hearing to be held at the July 11, 2018 planning commission meeting. The signs are being installed on-site today.

If you have any questions, please do not hesitate to contact me.

Sincerely,  
GreenTech Engineering, Inc.



Daniel J. LeClair, PE, PS  
President

**COMMUNITY IMPACT STATEMENT June 13,  
2018**



Community Image Builders

CIB PLANNING

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# Adell-Novi Community Impact Statement

June 13, 2018

17195 Silver Parkway, #309  
Fenton, MI 48430

Phone: 810-335-3800  
Email: [avantini@cibplanning.com](mailto:avantini@cibplanning.com)

A Community Impact Statement should address all of the following information:

1. *Expected annual number of police responses for the proposed development (can be based on statistics from similar developments);*

A survey of the operators for the proposed uses indicates that the anticipated number of police responses is negligible, since most incidents are handled by staff. This is especially true for the hotels and restaurants, while the other uses expect virtually no police calls. For the purpose of this report, we are estimating that 2 calls per month, or an annual total of 24 calls can be expected.

2. *Expected annual number of fire responses for the proposed development (can be based on statistics from similar developments);*

As with the police calls, a minimal number of fire calls can be expected, with the majority being EMS calls. The business operators confirmed that based upon calls for assistance at other locations, approximately 10-22 responses can be expected on an annual basis.

3. *Anticipated number of employees (include both permanent and construction jobs on site);*

With any of the above construction projects, there can be anywhere from 20 to 100 construction workers on-site, depending upon the phase of completion. The following is the estimated number of permanent jobs to be created for the proposed uses:

Proposed Use	Jobs
Carvana	12-15
iFly	10-15
Restaurants	40-60
Planet Fitness	10-12
Drury Hotel	15-20
Fairfield Inn	15-20

4. *Statement regarding compliance with City Performance Standards (Section 2519 of the Zoning Ordinance);*

All uses will be operated indoors and it is not anticipated that any of them will exceed the thresholds identified in the Performance Standards of *Section 5.14* of the ordinance.

5. *Estimated number of sewer and water taps and information on peak hour demand and min/max operating pressures for water system;*

The following is the estimated number of REU's for the proposed uses:

Proposed Use	REU's
Carvana	1.7
iFly	2.4
Restaurants	63 (total)
Planet Fitness	69
Drury Hotel	69
Fairfield Inn	49

6. *Relationship of the proposed development with surrounding uses;*

The proposed development provides a natural land use transition between the more intense industrial uses to the west and the retail uses in the Town Center to the east and south. With direct frontage on I-96, the site is highly visible and has the ability to create a positive impression to visitors; something that will help attract customers to retailers in the Town Center. This is especially important in light of the trend away from in-person and toward on-line shopping.

7. *Description of proposed land use;*

To date, all of the sites have committed purchasers including very unique and national companies such as Drury Hotels, I-FLY indoor sky diving, Planet Fitness, Texas Roadhouse and Carvana. Also included on the list of uses will be a second hotel and a second restaurant uses, creating an exciting destination. Mr. Adell is intending to keep the last remaining building site (Unit 4) for the time being for the purpose of hosting small seasonal events and for overflow parking if needed.

8. *Description of the environmental factors and impacts addressing the following:*

a. *Natural features on the site (e.g., unusual topography, habitat areas, wetlands, woodlands, historic trees, etc.);*

The approximate seven (7) acres of land on the south end of the property contains an existing creek, wetlands, floodplain and several trees. A walking nature pathway is proposed for this area.

b. *Temporary and permanent impacts to natural features on the site;*

The only potential impacts to this area would come from the installation of utilities, and that activity would be temporary in nature. The proposed nature pathway will meander throughout this area and will minimize the impact to the existing wetlands and woodlands.

c. *Manufacture, use or storage of any hazardous or toxic materials on the site including Environmental Protection Agency requirements and the need for a Pollution Incidence Prevention Plan (PIPP);*

Based upon the proposed uses, there is no storage of hazardous or toxic materials that would require preparation of a Pollution Incidence Prevention Plan (PIPP).

d. *Location, type, depth and contents of any existing or proposed underground storage tanks;*

Per the current owner of the site (who has extensive knowledge about the history of the site) there are no existing underground storage tanks on the property. Additionally, no new underground storage tanks are proposed as part of this development.

e. *Environmental use and/or contamination history of the site (i.e., groundwater contamination, landfill, chemical spills, etc.); and*

Per the current owner of the site, it was used for tool and die and the manufacture of auto parts for approximately 13 years, from 1965 to 1978. And he is unaware of any contamination on the property.

f. *Potential impacts to existing wildlife on site; and*

Since the seven (7) acres at the south end of the property will remain largely undisturbed, there should be no negative impacts to existing wildlife on the site.

9. *Description of the social impacts addressing the following:*

a. *Replacement or relocation of any existing uses or occupants on the site;*

There are currently no uses on the site and the only structure remaining on the property at the present time is the existing water tower. Some remnants of the former Novi Expo Center still remain, including the concrete building slab, asphalt parking lot and site entrance. As such, there is no need to replace or relocate any existing uses or occupants.

b. *Traffic impacts (information can come from any required Traffic Impact Study or statistics from other similar developments when a study is not required);*

A full traffic study is not being provided and a waiver requested, since AECOM is currently preparing a region-wide TIS and the site will be included in that study. A Traffic Generation Analysis dated 5/1/18 has been prepared by Bergmann, however, to assess the number of vehicle trips that would be generated by the proposed development. The total number of new Average Daily Site Trips is estimated at 3,988. Access to and from the site is from Crescent Boulevard, located near the southeast corner of the property. Crescent Boulevard extends to the property from Novi Road, where a signalized intersection is located. It is understood that the City of Novi will be extending Crescent Drive from the current terminus to Grand River Avenue.

c. *Proposed site amenities (i.e., sidewalks, public parks, bicycle paths, etc.); and*

Proposed site amenities include an extensive walkway system throughout the development, connecting to the abutting sidewalk system on Novi Road. There will also be a gazebo and open space on unit #4, along with a parking lot for shared and overflow parking.

d. *Increases in the permanent population of the City as a result of the proposed development (specific number should be identified and statistics from similar developments can be used).*

Since all of the uses are destination-oriented and no housing units are proposed, there should be no permanent increases in the population of the City.

**I-FLY NARRATIVE**  
**Follow-Response to Façade Comments**

## **Project Narrative**

### **iFLY Novi**

SkyGroup Investments, LLC (“iFLY”) has prepared this narrative to include in Mr. Adell’s application to provide explanations of our unique structure.

#### iFLY Project Overview:

iFLY plans to build a new indoor skydiving facility on the ring road to Liberty Center near the Northwest corner of Liberty Way and I-75. The new building will be approximately 5,000 square feet and serve over 150,000 customers annually, many of whom will travel from well outside the community to experience indoor skydiving at iFLY. Once operational, iFLY will employ and estimated 26 people (18 Full-Time, 8 Part-Time).

#### iFLY Company Overview:

iFLY is the experiential entertainment company that invented modern indoor skydiving, the simulation of true freefall conditions in a vertical wind tunnel. It’s where the dream of flight becomes a reality. We are the World’s largest designer, builder and operator of vertical wind tunnels and have been the undisputed industry leader since creating this market in 1997. We are committed to innovation, performance, safety, reliability and absolute customer satisfaction. We’re vertically integrated — designing, fabricating, installing and operating skydiving wind tunnels.

With over 65 operating facilities globally, the company has safely flown millions of customers. Our patented technology allows us to fly people of nearly all ages and abilities on a smooth, air-conditioned column of air. The airflow is completely enclosed, and is not audible from the outside of our modern facilities. There’s no parachute, no jumping, no falling and nothing attaching you to Earth. Children as young as three, to adults of any age can fly with us. It’s safe for kids, challenging for adults and realistic for skydivers. We provide all the gear and a personal instructor will guide you through your flight.

At iFLY, we draw on our extensive history of having flown over 7 million customers worldwide to help deliver an unforgettable experience. Whether you're looking for a team building activity, team outings or sales events, we have what you need to achieve your goals, including conferencing facilities and available catering. Our events are safe, challenging, thrilling, and suitable for almost everyone, regardless of gender, age or physical condition.

For more information, please visit:

- <https://www.iflyworld.com/>
- <https://www.facebook.com/iFLYAustin/>
- <https://www.linkedin.com/company/3258354/>



- <https://www.youtube.com/user/iflytunnelvision>

How iFLY Works:

The images below show an actual constructed iFLY (Woodlands, Texas), as well as the same image, overlaid to show the iFLY machine which makes flight possible. The machine becomes part of our structure, and the main occupied space (“**Building**”) becomes surrounded by wind tunnel machine parts, collectively comprising the air flow path (“**Flowpath**”). The space created between the Flowpath and the Building creates empty air space through the building, or a “**Void**”.

**Actual iFLY**



**Internal Flowpath**



**Flowpath**

**Voids**

**Building**

Building Materials Variance Request:

We understand that the Novi code specifies that the exterior of this building should include more masonry (brick and stone vs. concrete and EIFS). For our unique structure, this requirement is incredibly challenging. As described above, our occupied building is surrounded by the Flowpath, which for reasons of sound mitigation, structural soundness, and airflow quality, is constructed out of reinforced concrete. The concrete Flowpath which is exposed to the exterior is textured and painted to give the structure a professional finished look & feel. On many occasions, we have considered cladding the Flowpath with natural materials, but this causes issues relating to vibrations & weight which cause this solution to be untenable.

Sign Variance Request:

We are requesting a variance from the sign code to ensure visibility, legibility as well as to better match the scale of our use. Given the size of our building, particularly the height, the requested variances are necessary to provide an aesthetic that is in keeping with the scale of our building. Given the speed with which vehicles travel on I-96 we want to make sure our signage is clearly legible at a passing glance.

Please be advised that when iFLY opens, and likely for a considerable time thereafter this will be our only location in Michigan and within hundreds of miles of Novi, making it critical that we raise awareness of our use and location, especially given the high development costs of a use such as ours.

Setback Variance:

We are also requesting deviations for the modification of setback requirements. Our parcel (to be created via a condo plat) is part of an integrated development with shared parking with private roadway and sidewalk connections. Accordingly, strict application of setbacks from public ways, sidewalks and adjacent uses need not be required in our view for this center and the setbacks shown on the plan submitted should be deemed adequate and appropriate.

Please do not hesitate to contact us with your questions or comments. Contact information below.

Mark Lee  
Director of Development  
iFLY Holdings  
6034 W. Courtyard Drive #135  
Austin, Texas 78730  
Main: 512.647.9200 x157  
Direct: 512.201.8896  
Mobile: 262.957.6339  
Email: [mglee@iflyworld.com](mailto:mglee@iflyworld.com)

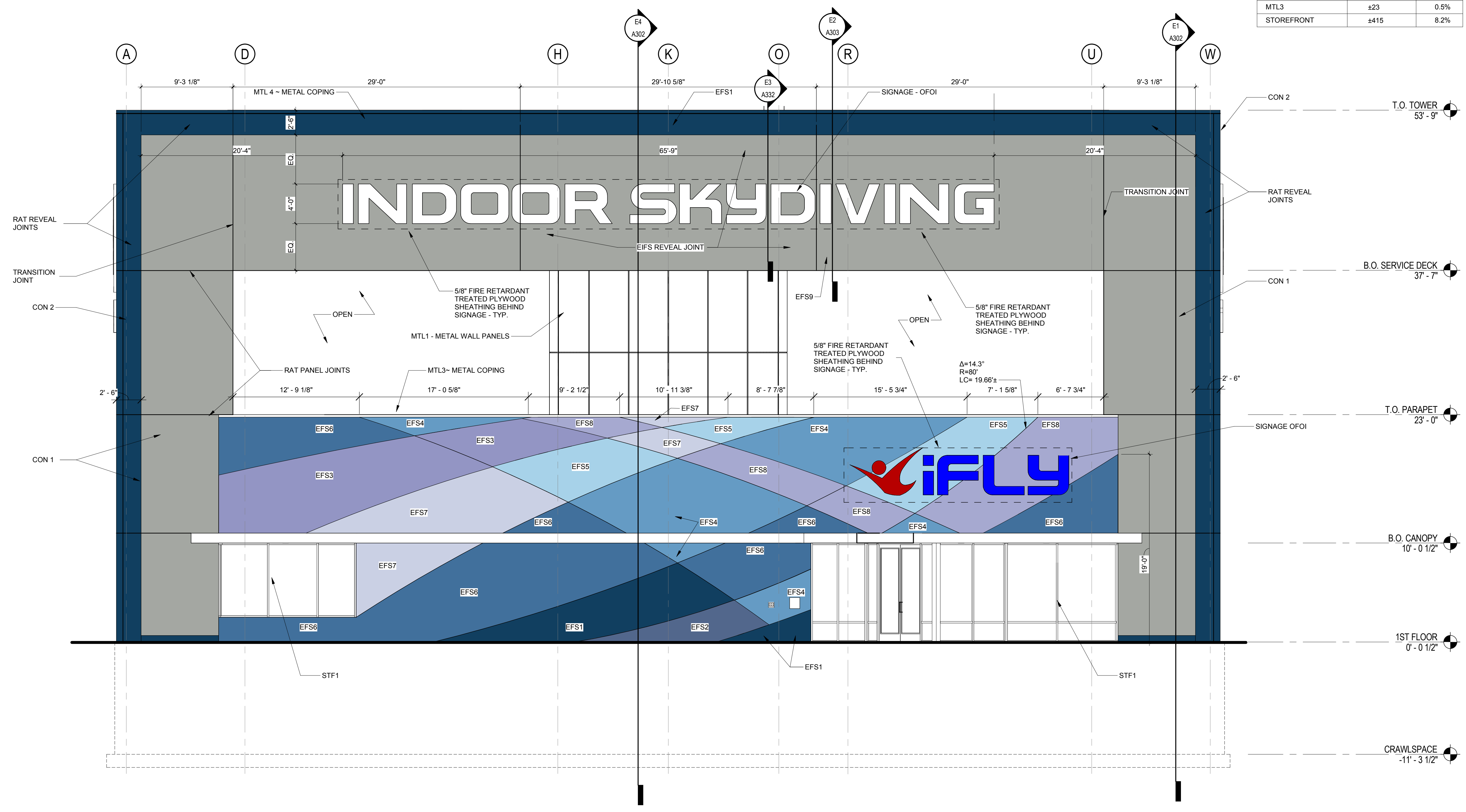
REFERENCE	COLOR	MFR/NOTES:
<b>[CON] CONCRETE</b>		
CON1 PRECAST CONCRETE PANEL	GRAY MATTERS (EFS9)	EIFS FINISH OVER CONCRETE (NO INSULATION)
CON2 PRECAST CONCRETE PANEL	LOYAL BLUE (EFS1)	EIFS FINISH OVER CONCRETE (NO INSULATION)
<b>[EFS] EXTERIOR INSULATION &amp; FINISH SYSTEM (EIFS)</b>	<b>DRYVIT</b> <b>PAREX</b>	
EFS1 EIFS	IFLY021022S    NAC74041	LOYAL BLUE (SW 6510)
EFS2 EIFS	IFLY031022S    NAC74040	REVEL BLUE (SW 6530)
EFS3 EIFS	IFLY041022S    NAC71981	DHALIA (SW 6816)
EFS4 EIFS	IFLY051022S    NAC74045	JACARANDA (SW 6802)
EFS5 EIFS	IFLY061022S    NAC72023	BLUE BELL (SW 6792)
EFS6 EIFS	IFLY071022S    NAC74042	DANUBE (SW 6803)
EFS7 EIFS	IFLY081022S    NAC74043	BREATHTAKING (SW 6814)
EFS8 EIFS	IFLY091022S    NAC74044	INDULGENT (SW 6969)
EFS9 EIFS	IFLY11022S    NAC71978	GRAY MATTERS (SW 7066)
<b>[CMU] CONCRETE MASONRY UNITS</b>		
CMU1 DARK CMU @ MECH YARD	DK19 BLACK	SPLIT SMOOTH FACE BLOCKS OR SIMILAR
CMU2 LIGHT CMU @ MECH YARD	LT17 STANDARD GREY	SMOOTH FACE BLOCKS OR SIMILAR

REFERENCE	COLOR	MFG/NOTES:
<b>[MTL] METAL</b>		
MTL1 METAL WALL PANELS	SILVER METALLIC, SMOOTH	OMEGA PANEL PRODUCTS LAMINATORS, INC.
MTL2 METAL WALL PANELS @ CANOPY	SILVER METALLIC, SMOOTH	OMEGA PANEL PRODUCTS LAMINATORS, INC.
MTL3 METAL COPING (LOW ROOF)	SILVER METALLIC, SMOOTH	OMEGA PANEL PRODUCTS LAMINATORS, INC.
MTL4 METAL COPING CAP (HIGH ROOF)	REGAL BLUE, SMOOTH	OMEGA PANEL PRODUCTS LAMINATORS, INC.
<b>[STF] STOREFRONT SYSTEM</b>		
STF1 STOREFRONT	FRAME: CLEAR ANODIZED ALUMINUM GLAZING: PPG-SOLARBLUE	

GENERAL NOTES	
A	REFER TO SHEET A201 FOR EXTERIOR FINISH SCHEDULE AND EXTERIOR GENERAL NOTES.
B	ALL EXPOSED CONDUITS, JUNCTION BOXES, PANEL BOXES, METERS, PIPES, FACE PLATES, AND EMERGENCY FIXTURES SHALL BE PAINTED TO MATCH ADJACENT SURFACE.
C	REFER TO SHEET A601 FOR DOOR/GATE SCHEDULE.
D	EXTERIOR SIGNAGE IS O.F.O.I. AND SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE BLOCKING AND ROUGH IN FOR SIGNAGE.

EXTERIOR FRONT MASS ELEVATION AREAS		
NAME	AREA	PERCENT
RETURN AIR TOWER...	+599 S.F.	11.8%
SERVICE DECK	+1423 S.F.	28.0%
RETURN AIR TOWER LEFT BUILDING	+599 S.F.	11.8%
DIFFUSER TOWER	+349 S.F.	6.9%
MATERIAL COVERAGE		
CON1	+545	10.8%
CON2	+2075	41.0%
EFS	+1557	30.8%
MTL1	+337	6.7%
MTL2	+118	2.3%
MTL3	+23	0.5%
STOREFRONT	+415	8.2%

### EXTERIOR FINISH SCHEDULE



**E1 FRONT ELEVATION**  
3/16" = 1'-0"

**Stantec**  
STANTEC ARCHITECTURE  
8211 SOUTH 48TH STREET,  
PHOENIX, AZ 85044  
T. 602.438.2200  
www.stantec.com

**STANTEC**  
801 Jones Franklin Road,  
STE. 300  
RALEIGH, NC 27606  
T. 919.532.2330  
www.stantec.com

**STANTEC**  
475 Fifth Avenue,  
12th Floor  
NEW YORK, NY 10017  
T. 212.330.6153  
www.stantec.com

**iFLY**  
CORPORATE:  
6200 Bridge Point Parkway  
Bldg #4, Suite #500  
AUSTIN, TEXAS 78730  
AOR PROJECT NO: 2007867010  
CLIENT PROJECT NO: GEN8  
**iFLY GEN8**  
ADDRESS, CITY, ETC. ETC.

Revision	
A. Design Development	07.27.17
B. Final Review Set	08.24.17
C. Issue 1	09.28.17

FRONT ELEVATION

**A201**

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ORIGINAL SHEET - ARCH D (P&S)



**iFLY**  
 CORPORATE:  
 6200 BRIDGE POINT PARKWAY  
 BLDG. #4, SUITE #500  
 AUSTIN, TEXAS 78730

AOR PROJECT NO: 2007867010  
 CLIENT PROJECT NO: GEN8

**iFLY GEN8**  
 ADDRESS, CITY, ETC. ETC.

Revision

A. Design Development	07.27.17
B. Final Review Set	08.24.17
C. Issue 1	09.28.17

REAR ELEVATION

**A202**

GENERAL NOTES

A	REFER TO SHEET A201 FOR EXTERIOR FINISH SCHEDULE AND EXTERIOR GENERAL NOTES.
B	ALL EXPOSED CONDUITS, JUNCTION BOXES, PANEL BOXES, METERS, PIPES, FACE PLATES, AND EMERGENCY FIXTURES SHALL BE PAINTED TO MATCH ADJACENT SURFACE.
C	REFER TO SHEET A601 FOR DOOR/GATE SCHEDULE.
D	EXTERIOR SIGNAGE IS O.F.O.I. AND SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE BLOCKING AND ROUGH IN FOR SIGNAGE.

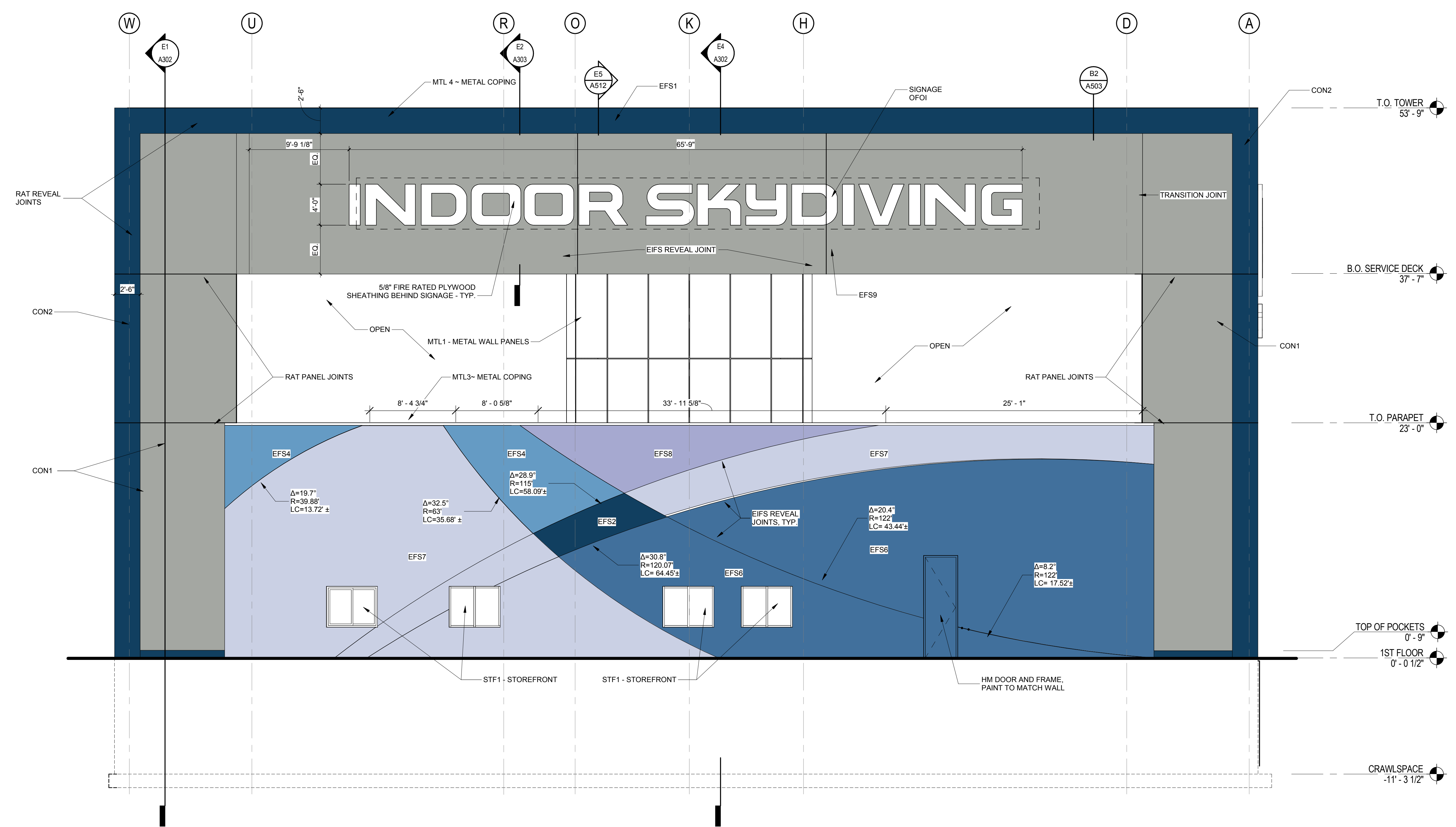
EXTERIOR REAR MASS ELEVATION AREAS

NAME	AREA	PERCENT
RETURN AIR TOWER RIGHT	±599	11.8%
SERVICE DECK	±1423	28.0%
RETURN AIR TOWER LEFT	±599	11.8%
BUILDING	±2088	41.3%
DIFFUSER TOWER	±349	6.9%

MATERIAL COVERAGE

CON1	±545	10.8%
CON2	±2075	41.0%
EFS	±1982	39.3%
MTL1	±337	6.7%
MTL3	±23	0.5%
STOREFRONT	±80	1.5%



**E1 REAR ELEVATION**  
 3/16" = 1'-0"

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**iFLY**  
CORPORATE:  
6200 Bridge Point Parkway  
Bldg #4, Suite #500  
AUSTIN, TEXAS 78730

AOR PROJECT NO: 2007867010  
CLIENT PROJECT NO: GEN8

**iFLY GEN8**  
ADDRESS, CITY, ETC. ETC.

Revision

A. Design Development	07.27.17
B. Final Review Set	08.24.17
C. Issue 1	09.28.17

LEFT SIDE  
ELEVATION

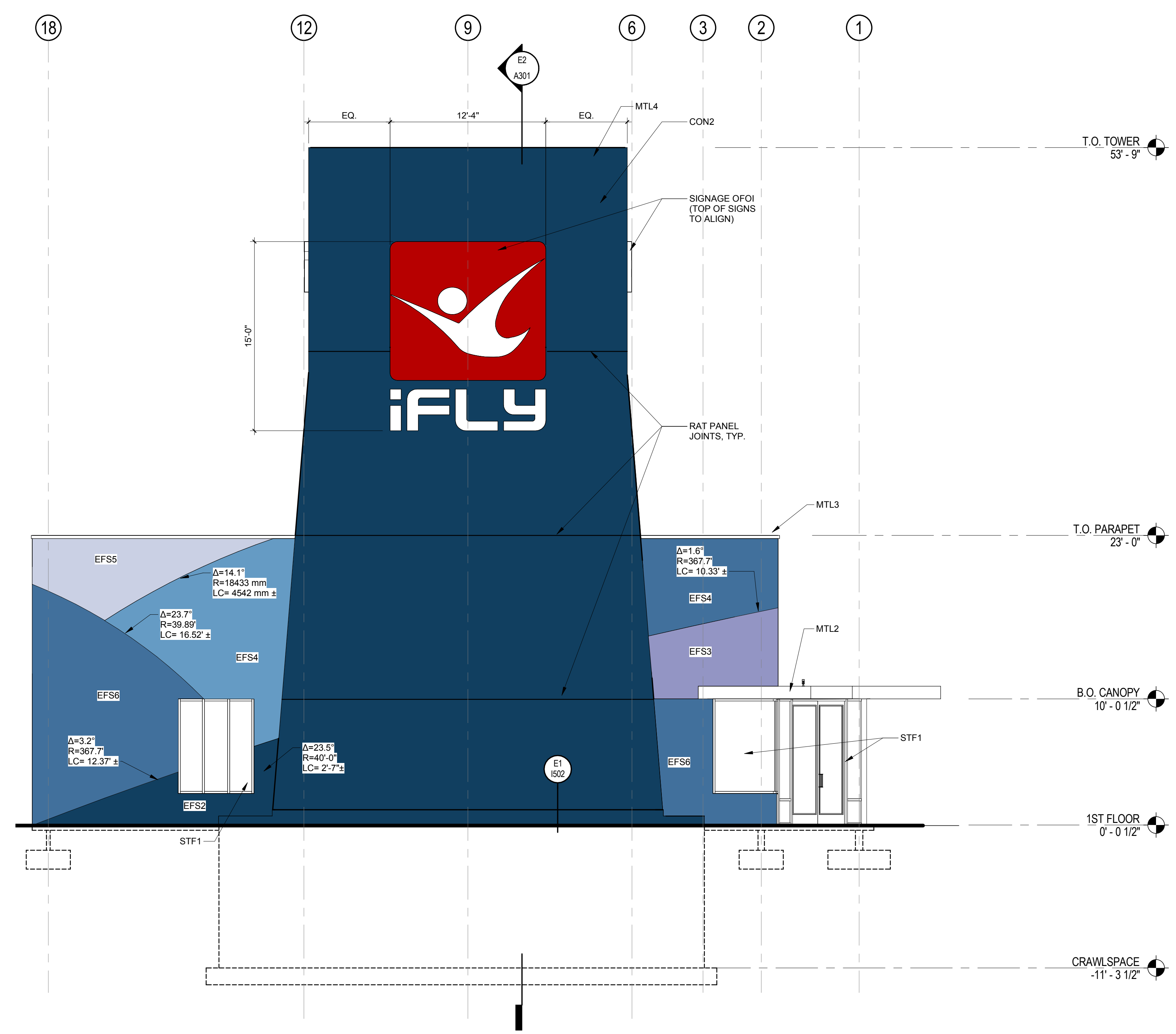
**A203**

GENERAL NOTES

A	REFER TO SHEET A201 FOR EXTERIOR FINISH SCHEDULE AND EXTERIOR GENERAL NOTES.
B	ALL EXPOSED CONDUITS, JUNCTION BOXES, PANEL BOXES, METERS, PIPES, FACE PLATES, AND EMERGENCY FIXTURES SHALL BE PAINTED TO MATCH ADJACENT SURFACE.
C	REFER TO SHEET A601 FOR DOOR/GATE SCHEDULE.
D	EXTERIOR SIGNAGE IS O.F.O.I. AND SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE BLOCKING AND ROUGH IN FOR SIGNAGE.

EXTERIOR LEFT MASS ELEVATION AREAS

NAME	AREA	PERCENT
RETURN AIR TOWER	±1468	11.8%
BUILDING	±2088	41.3%
DIFFUSER TOWER	±349	6.9%
MATERIAL COVERAGE		
CON2	±1468	68.3%
EFS	±590	27.5%
MTL1	±337	6.7%
MTL3	±8	0.4%
STOREFRONT	±80	3.8%



**E4 LEFT ELEVATION**  
A203 3/16" = 1'-0"

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**STANTEC**  
 475 FIFTH AVENUE,  
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**iFLY**  
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 BLDG. #4, SUITE #500  
 AUSTIN, TEXAS 78730

AOR PROJECT NO: 2007867010  
 CLIENT PROJECT NO: GEN8

**iFLY GEN8**

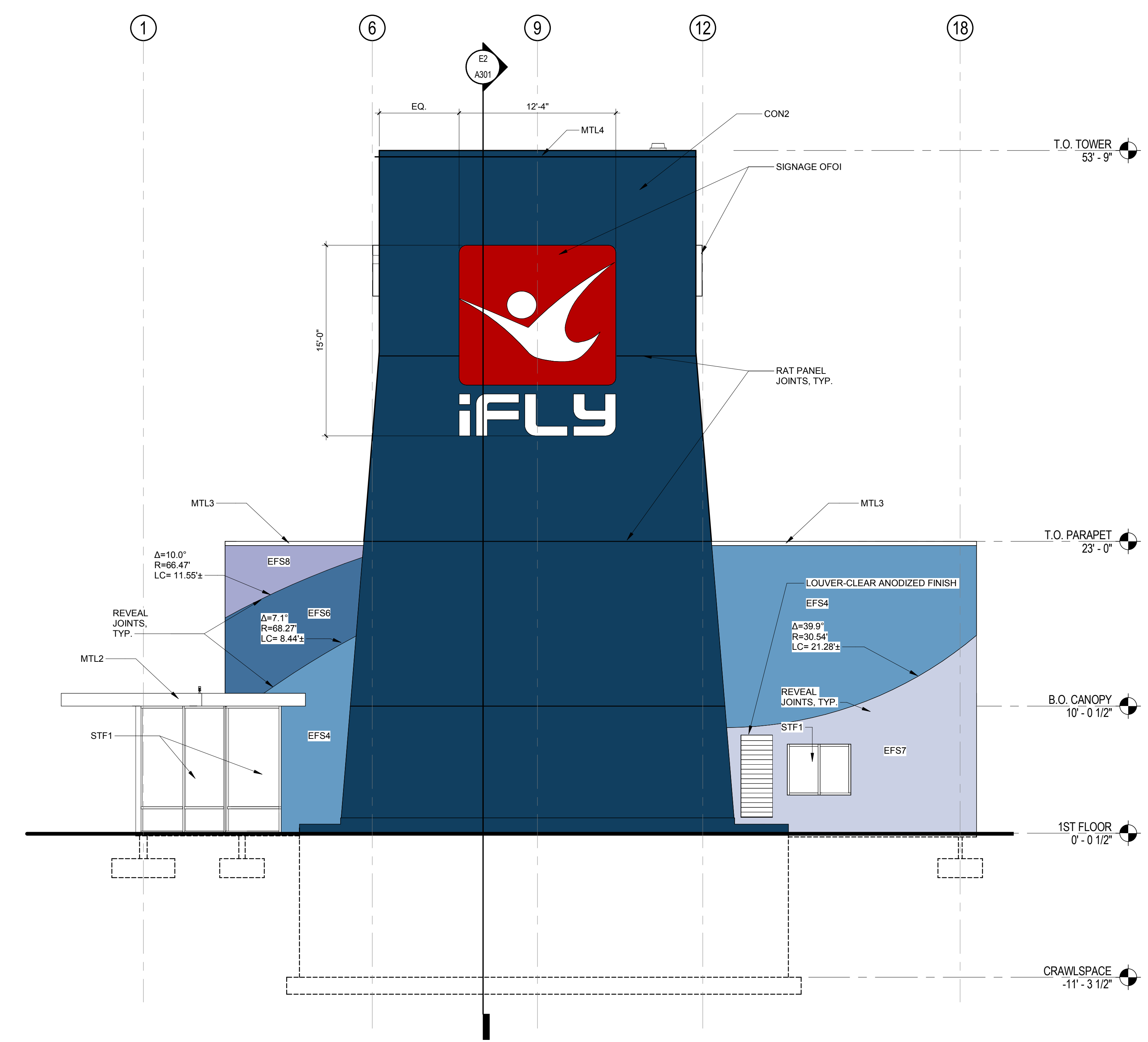
ADDRESS, CITY, ETC. ETC.

Revision

A. Design Development	07.27.17
B. Final Review Set	08.24.17
C. Issue 1	09.28.17

GENERAL NOTES	
A	REFER TO SHEET A201 FOR EXTERIOR FINISH SCHEDULE AND EXTERIOR GENERAL NOTES.
B	ALL EXPOSED CONDUITS, JUNCTION BOXES, PANEL BOXES, METERS, PIPES, FACE PLATES, AND EMERGENCY FIXTURES SHALL BE PAINTED TO MATCH ADJACENT SURFACE.
C	REFER TO SHEET A601 FOR DOOR/GATE SCHEDULE.
D	EXTERIOR SIGNAGE IS O.F.O.I. AND SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE BLOCKING AND ROUGH IN FOR SIGNAGE.

EXTERIOR LEFT MASS ELEVATION AREAS		
NAME	AREA	PERCENT
RETURN AIR TOWER	±1468	11.8%
BUILDING	±2088	41.3%
DIFFUSER TOWER	±349	6.9%
MATERIAL COVERAGE		
CON2	±1468	68.3%
EFS	±586	27.3%
MTL1	±337	6.7%
MTL3	±8	0.4%
STOREFRONT	±80	3.7%



**E4 RIGHT ELEVATION**  
 3/16" = 1'-0"

RIGHT SIDE ELEVATION

**A204**

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 5/30/2018 9:39:32 AM

# Komaragiri, Sri

**From:** Mark Lee <mglee@iflyworld.com>  
**Sent:** Wednesday, June 20, 2018 6:02 PM  
**To:** Dan LeClair; Komaragiri, Sri  
**Cc:** r.lameti  
**Subject:** RE: Adell: Facade Submittal  
**Attachments:** GEN8 ext finishes.pdf; iFLY Novi\_Narrative Summary.docx

Dan/Sri:

The Exterior Finish Schedule is at the top of Page 1 of the file attached hereto. It has a breakdown for each code. See attached & screen capture below.

REFERENCE	COLOR	MFR/NOTES
<b>CONCRETE</b>		
CON1 PRECAST CONCRETE PANEL	GRAY MATTERS (EFS9)	EFS FINISH OVER CONCRETE (NO INSULATION)
CON2 PRECAST CONCRETE PANEL	LOYAL BLUE (EFS1)	EFS FINISH OVER CONCRETE (NO INSULATION)
<b>EIFS EXTERIOR INSULATION &amp; FINISH SYSTEM (EIFS)</b>	<b>DRYVIT</b> <b>PAREX</b>	
EFS1 EIFS	#FLY0210225    NACT4041	LOYAL BLUE (SW 6518)
EFS2 EIFS	#FLY0310225    NACT4040	REVEL BLUE (SW 6536)
EFS3 EIFS	#FLY0410225    NACT3981	DHALIA (SW 6816)
EFS4 EIFS	#FLY0510225    NACT4045	JACARANDA (SW 6802)
EFS5 EIFS	#FLY0610225    NACT2023	BLUE BELL (SW 6792)
EFS6 EIFS	#FLY0710225    NACT4042	DANUBE (SW 6803)
EFS7 EIFS	#FLY0810225    NACT4043	BRIGHT/ARNO (SW 6814)
EFS8 EIFS	#FLY0910225    NACT4044	INDULGENT (SW 6969)
EFS9 EIFS	#FLY0110225    NACT3978	GRAY MATTERS (SW 7066)
<b>CMU CONCRETE MASONRY UNITS</b>		
CMU1 DARK CMU @ MECH YARD	DK19 BLACK	SPLIT SMOOTH FACE BLOCKS OR SIMILAR
CMU2 LIGHT CMU @ MECH YARD	LT17 STANDARD GREY	SMOOTH FACE BLOCKS OR SIMILAR
<b>METALS</b>		
MTL1 METAL WALL PANELS		SILVER METALLIC, SMOOTH
MTL2 METAL WALL PANELS @ CANOPY		SILVER METALLIC, SMOOTH
MTL3 METAL COPING (LOW ROOF)		SILVER METALLIC, SMOOTH
MTL4 METAL COPING CAP (HIGH ROOF)		REGAL BLUE, SMOOTH
<b>STOREFRONT SYSTEM</b>		
STF1 STOREFRONT		FRAME: CLEAR ANODIZED ALUMINUM GLAZING: PPG-SOLARBLUE

## EXTERIOR FINISH SCHEDULE

Additional clarification is as follows:

- EFS = EIFS (Trade names are Dryvit or Parex)
- Con = Precast concrete panels with a EIFS like skim coat finish, but no exterior insulation.
  - The numbers for the above (Con1-2 & EFS1-9) designate the differences in color.
- CMU = Split face block (CMU1) and Smooth face Blocks (CMU2)
  - Please be advised that the only CMU we use is when replace the metal fence around the utility yard with block walls and the attached does not show this wall/fence. Let me know if you need that to be added.
- MTL 1-4 = Metal panels and coping
- STF1 = Aluminum framed windows & doors

Please be advised that this is our standard proto as opposed to our +5' proto we are planning on constructing here. (FYI, we created the second proto to get reduce excavation costs by raising the FFE 5' above grade and ramping up to the front door.) Accordingly, my architects provided the following notes on the +5 proto.

For the proto we are using in Novi (the +5' FFE proto) we need to add EIFS for the following:

- 560 SF for the Front and Back elevation (each)
- 300 SF for each side elevation (each)

The result raises the EIFS percentage somewhat and lowers all of the others percentages.

**DRURY FOLLOW-UP RESPONSES**



**From:** [Dan LeClair](#)  
**To:** [Komaragiri, Sri](#); [McBeth, Barb](#)  
**Subject:** FW: Adell Center Application  
**Date:** Friday, June 8, 2018 11:35:13 AM  
**Attachments:** [image001.jpg](#)

---

Good morning ladies,

Please see the correspondence from Drury below. Let me know if you have any questions.

Thank You,  
Daniel J LeClair, PE, PS



51147 W. Pontiac Trail  
Wixom, Michigan 48393  
Phone: (248) 668-0700  
Fax: (248) 668-0701  
Cell: (248) 921-3942

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---

**From:** Prehm, Mike [mailto:[mike.prehm@ddcmail.com](mailto:mike.prehm@ddcmail.com)]  
**Sent:** Friday, June 8, 2018 10:50 AM  
**To:** Dan LeClair <[dan@greentechengineering.net](mailto:dan@greentechengineering.net)>  
**Subject:** Adell Center Application

Dan,

Upon receipt of the City of Novi's comments regarding our proposed 180-room Drury Inn & Suites, below we try to address questions and comments regarding signage and building materials.

Our intent is to have signage on each face of the building that is visible to the traveling public. The two most prominent signs will be located on the north and the east building elevations, both of which will identify our hotel to guests traveling on and exiting from Interstate 96. Signage on the east side of the building will also welcome guests traveling Novi Road. While not as prominent, we do believe that the height of the building will provide visibility for signage on the rear elevation to benefit guests traveling on Grand River Avenue, especially those traveling from the convention center. Further benefit will be realized when Crescent Blvd is extended south, connecting Expo Center Drive with Grand River. Signage on each facade that is visible to travel is critical to easily identify the hotel and establish Novi as a preferred lodging location when compared to Wixom, Farmington Hills, Northville and Livonia.

While the square footage of the signs on the front and rear of the building exceeds that which is defined in Novi's ordinance, we believe the signage proposed is commensurate with the size of the building. Novi's ordinance allows a wall sign to be a maximum of 250 square feet, while the maximum building height, depending on the district, is always less than our building height of 84-1/2 feet. For comparison, the 299 square feet of signage on the front facade is 2% of the facade's 16,792 square feet. Similarly, the total signage package of 786 square feet is 2% of the 48,117 square foot total of all four facades.

After the Pre-Application Meeting on May 15th, we had the opportunity to discuss the city's concerns regarding our building materials as they relate to the building facade tabulations. Following that meeting, we discussed the issue internally with architects and construction staff. While recalculation of the materials to exclude from the calculation EFIS that is applied to cornices and moldings would reduce the percentage of the material, we do not believe that our building design allows for any other materials to be used that would reduce the percentage of EFIS by an amount necessary to meet the current ordinance.

Currently, EFIS is utilized in areas that create an architectural accent by providing a radius (in the center of the front and side facades). Use of brick in these areas would produce an undesired effect as it would be impossible to create a smooth radius using the flat face of a brick. Further, the offset mortar lines will create a long-term maintenance issue.

EFIS is also utilized in areas where there is no reasonable way to structurally support the weight of the full brick used. Examples are in the front of the building, where the radius only extends from floors 2 through 7, as the front canopy is tucked under the radius to provide a weatherproof transition from canopy to building. Another example is in the rear of the building where meeting rooms and the pool area extend outward from the cylinder of the building. It is worth noting that this area will not be visible to guests or the public, as the building will back to the Rogue River. All parking and entrances to the building are located to the front and sides. Finally, we note that 50% of the facade materials are masonry (architectural cast stone and full brick).

As we have constructed this building in many different locations, we offer Grand Rapids, MI as an example of this building that is closest to Novi. We have found this building to be well received by both guests and local governing entities. The overall quality of construction - full concrete superstructure with insulated concrete form exterior walls with masonry and synthetic finish, central heating and air conditioning (no PTAC "shaker" grates on the exterior) - creates an extremely high quality and energy efficient building. While the finish materials may not meet the calculation desired by the city, we believe the quality of the overall building exceeds the intent of the ordinance and compliments the quality of existing buildings nearby.

Should you wish to discuss further or have additional questions, please do not hesitate to contact us.

Sincerely,

Michael Prehm

Development Manager

**Drury Development Corporation**

721 Emerson Road, Suite 200

St. Louis, MO 63141

Direct 314.587.2546 - Cell 314.914.3103

Main 314.423.6698

[mike.prehm@ddcmail.com](mailto:mike.prehm@ddcmail.com)

APPLICANT RESPONSE LETTER  
(REZONING REQUEST)

July 3, 2018

Barb McBeth, AICP  
City Planner  
City of Novi – Planning Department  
47175 10 Mile Road  
Novi, MI 48375

For: Adell Center  
43700 Expo Center Drive, Novi  
Parcel ID: 22-15-476-045

Dear Barb:

Please find this letter in response to the June 29 review package and our meeting with staff yesterday. The responses outlined in this letter will be included as part of all future submittals for this site as applicable.

Our responses to the review letters are as follows:

**Plan Review Center Report dated June 29, 2018:**

*General Responses:* The proposed uses listed in the table clearly identify the end users as those listed on our previous submittal. The previously submitted narrative and PRO plan list possible proposed and/or future uses for the site. Unfortunately, we currently do not have specific uses for some of the proposed units as they are not contractually committed. Based on our discussion yesterday and a follow up discussion with Mr. Adell, we kindly request that the PRO be revised to include the following language:

Unit No	Proposed End User	Proposed Height	Proposed Use Category
Unit 1	I-FLY	58'. (2 stories)	Indoor Commercial Recreation Facilities
Unit 2	Planet Fitness	40 ft to 50 ft (2 stories)	Indoor Commercial Recreation Facilities
Unit 3	Fairfield Inn & Suites	63 ft. (5 stories)	Hotels
Unit 4	Parking/Seasonal Event Spaces	Not Provided	Off-Street Parking Lot
Unit 5	Drury Inn & Suites	85 ft (7 stories)	Hotels
Unit 6	Restaurant	20ft – 30 ft (1 story)	End user to be determined
Unit 7	Restaurant	20ft – 30ft (1 story)	End user to be determined
Unit 8	Carvana	71ft (7 stories)	Automobile Dealer
Unit 9	Water Tower	120ft Existing Tower	Existing Structure

Note to the above table: Any future development or redevelopment of any of the above units may be one of the following use categories subject to City of Novi site plan approval. Future development of any units to a use category other than those listed below are subject to a PRO amendment and site plan approval from the City of Novi. The acceptable future development/redevelopment categories include:

- Retail Businesses
- Professional Services
- Off-Street Parking Lots
- Restaurants (sit down), banquet facilities or other places serving food or beverages
- Professional and Medical Offices and Office buildings
- Indoor Commercial Recreation Facilities
- Hotels and Motels
- Financial Institutions
- Wineries, Microbreweries or Brewpubs (Subject to Special Land Use Approval)
- Private Health and Fitness Facilities and Clubs
- Amusement and Entertainment Uses

*Comments:*

1. We are in concurrence that it is highly unlikely and impossible to build another exposition center on this property. Many of our proposed uses like Hotels, Recreation Facilities and Restaurants are allowed in the EXPO district, but only when associated with an exposition facility. This ordinance requirement essentially deems this property undevelopable under the current zoning. For this reason, Mr. Adell is requesting the re-zoning to TC with a PRO as one of the only alternatives for developing this property.
2. We concur that this site is ideal for redevelopment under another zoning district given it's location and proximity to the I-96/Novi Road intersection.
3. We concur, although the subject property is not within the existing Town Center District boundary.
4. We concur that the hotel and restaurant uses do not fit within the OST district.

*Design and Layout Concerns:*

Staff comments are correct in the fact that the applicant is reluctant to make major layout changes as the proposed end users are all very successful national users that are very specific in their site selections, site layouts and building requirements. The end users are expressing their confidence in this location by way of purchase prices that are much above the average for the current Novi market. As part of the site plan submittal package, the applicant will add additional benches and pedestrian lighting around the previously proposed pocket parks.

As discussed previously, we will be expanding the pedestrian connections and amenities with each individual site plan as they are submitted.

*Off-Street Parking Lots for Separate Uses:*

As indicated on the PRO plan, we are proposing the parking spaces per each of our tenants needs based on their current facilities across the country. In most instances, that number is less than those required by city ordinance. In addition, we are proposing shared access drives all of which are aimed at reducing the total pavement area on the development property. We were also proposing to reduce the width of the proposed Adell Center Drive, which was met with

Barb McBeth, AICP  
City of Novi – Planning Department  
July 3, 2018

opposition by staff. Mr Adell has now agreed to a revised site layout which includes a wider pavement width for Adell Center drive based on staff comments.

*Unlisted Use Determination:*

Carvana is providing additional information to staff separately from this response letter. It should be noted that Carvana is a fairly new national company and has over a dozen facilities across the country.

*Adell Drive:*

As a follow up to our meeting yesterday, Mr. Adell has agreed to revise the proposed Adell Center Drive road to meet the Collector Road requirements. It is expected that this concession by Mr. Adell will eliminate many of the previously requested deviations.

*Deviations:*

A revised list of requested deviations is included as an attachment to this response letter. Note that Mr. Adell has agreed to revise the proposed Adell Center Drive road to meet the Collector Road requirements. It is expected that this concession by Mr. Adell will eliminate many of the previously requested deviations.

*Future Site Plan Reviews:*

At this time, the PRO submittal is including all of the requested deviations based on each individual end users review of the city ordinance and from staff review comments. If the PRO approval is granted by city council with the deviations as requested, then we anticipate no further deviation requests. Should the need of any additional deviation requests arise, then an amendment to the PRO will be requested separately of this submittal. The applicant's attorney is including language in the PRO agreement that will allow any of the current or future Adell Center owners to request a PRO amendment should one be required.

*Engineering Deviations:*

Mr. Adell has agreed to revise the proposed Adell Center Drive road to meet the Collector Road requirements. It is expected that this concession by Mr. Adell will eliminate many of the previously requested deviations.

*Landscape Deviations:*

The conceptual layout plans shown as part of the proposed PRO submittal are conceptual in nature and are subject to preliminary and final site plan approval. It is expected that all site plans submitted will be designed to meet the City of Novi landscape requirements or by deviation either as listed on the approved list of deviations or to be applied for at a future date.

*Traffic Deviations:*

The deviation list has been revised to include a deviation for the overall length of the proposed Adell Center Drive cul-de-sac.

At this time, we are not requesting a reduction in the minimum driveway spacing (same side) requirements. We are contemplating that the speed limit within the Adell Center development will be 20 miles per hour.

Barb McBeth, AICP  
City of Novi – Planning Department  
July 3, 2018

At this time, we are not requesting a deviation to allow for painted islands in place of the curbed landscaped islands.

*Signage Deviations:*

The currently proposed signage deviations include those for IFLY (Unit 1), Drury (Unit 5), and Carvana (Unit 8). We have previously submitted an application for the development monument signage (also included as a waiver in the attached revised deviations list).

*Deviations Previously Requested:*

The deviation for “Access to Major Thoroughfare” has been removed from per staff direction. The setback deviation from I-96 has been re-worded for clarification for current and future users. We respectfully request to keep the deviation for Wetland buffer impacts on the list of deviations for clarity for current and future users. The permitted uses list has been greatly reduced and clarified.

*Applicant’s Burden under PRO Ordinance:*

Comments acknowledged

*Public Interest/Benefits to Public Under PRO Ordinance:*

Comments acknowledged

*Summary of Other Reviews:*

1. Engineering Review: As mentioned above, Mr. Adell has revised the plan to include the roadway width and 6’ wide sidewalks per the city’s standard detail for collector streets. We anticipate that this change will accommodate many of the engineering staff comments. In addition, we are currently finishing up the final site plan submittal package that will include full details of the demolition of the existing infrastructure and the proposed roadway, water main, sanitary sewer, storm sewer and underground detention infrastructure.
2. Landscape Review: Comment acknowledged.
3. Wetland Review: Comment acknowledged.
4. Woodland Review: Comment acknowledged.
5. Traffic Review: As mentioned above, Mr. Adell has revised the plan to include the roadway width and 6’ wide sidewalks per the city’s standard detail for collector streets. We anticipate that this change will accommodate many of the traffic consultant’s comments.
6. Façade Review: Each of the users for which we are request deviations are talking directly with the city’s Façade Consultant. We defer their comments to the façade consultant directly.
7. Fire Review: Comment acknowledged. We are currently working with the fire department with respect to their review requirements. In addition, fire flow tests are currently being completed as part of our final site planning and detailed engineering process.

*Next Step:*

Comments noted. Please note the discussion that we had at our meeting yesterday.

*Fire Department Review:*

1. We acknowledge the fact that there is only one point of external access to the site. The final site plan for the overall site will include a second point of access at the northwest corner of the property adjacent to the water tower. The final site plan will include a fire lane with a gate and knox box at this location.
2. The minimum width of the fire lane will be 20' wide by 14' high and will be posted as a fire lane.
3. The final site plan for the site and respective site plans for the individual sites will include fire lanes meeting the fire apparatus turning requirements.
4. Any fire access roads over 150 feet in length will include a turn around.
5. The proposed water main will be looped at two locations to the existing City of Novi water system.
6. A separate fire protection system will be included as part of the site plan for each individual unit.
7. As part of the final site plan for the development, hydrants will be spaced at 300' intervals.
8. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
9. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
10. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
11. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
12. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
13. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
14. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
15. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
16. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units. Note that Drury will be constructing their building to meet the building code standards for high rise construction.
17. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
18. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
19. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units. Note that the street name change has been presented to the street naming committee and we are awaiting approval from city council for the official change of the street name to Adell Drive.
20. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
21. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.
22. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units.



Barb McBeth, AICP  
City of Novi – Planning Department  
July 3, 2018

23. Comment noted: More detail addressing this item will be included on the Final Site Plan and on site plans for the individual units. Note that there will be security gates across the fire apparatus access road located at the northwest corner of the property.

On behalf of the applicant and based on the above description and attachments, we kindly request positive consideration by the City of Novi on this matter.

If you have any questions, please do not hesitate to contact me.

Sincerely,  
GreenTech Engineering, Inc.



Daniel J. LeClair, PE, PS  
President

Attachments

**APPLICANTS RESPONSE LETTER  
(REQUEST FOR DEVIATIONS)**

July 3, 2018

Ms. Barbara McBeth, AICP  
City Planner  
City of Novi – Planning Department  
47175 10 Mile Road  
Novi, MI 48375

Subject: **Adell Center Rezoning, EXP, Exposition District to TC, Town Center District with a Planned Rezoning Overlay (PRO)**  
43700 Expo Center Drive, Novi  
Parcel ID: 22-15-476-045

Dear Ms. McBeth:

Please find the herein our revised list of requested deviations. These revisions are based on the review letters received on Friday, June 29<sup>th</sup> and our meeting yesterday. The following deviations from the City of Novi zoning ordinances are requested as part of the proposed PRO development.

- Planning Deviation (Section 3.1.26.D) for maximum building height of structures not to exceed the greater of seven (7) stories or eighty-five feet in height which is consistent and compatible to the buildings of the proposed unit owner's facilities nationally. This deviation refers to section 3.27.2.A.i of the City of Novi code of ordinances. This deviation request would allow for the building height of the proposed Drury Hotel which is 84'-5" in height. Based on discussions with Drury, it is understood that the proposed hotel will be built to category 1 standard. Based on discussion at the pre-application meeting, we understand that the fire department has no objection to the height of the Drury building if it is constructed to category 1 standards.

The proposed Carvana Building is 75'-10" (8 tier) in height. The upper stories of the Carvana building will be used only for stationing vehicles for sale.

The proposed IFLY building is under 58' in height. The existing water tower is in excess of the maximum building height, however it is an existing non-conforming use that is not occupied.

- Planning Deviation (Section 5.12) for the requirement for frontage upon a public street to allow for the creation of a separate condominium unit site for the existing on-site water tower. It is specifically intended that the proposed unit for the water tower is tucked away from the common access drive to minimize the desire of the general public to approach the water tower. Access to the proposed unit that includes the existing water tower will be via a proposed private easement across the adjacent unit. Unit 9 will be subject to all of the rules and restrictions of the master deed, but will not be part of the common open space (general common element) so that the cost of maintaining the water tower does not become the responsibility of the condominium association. This deviation refers to section 5.12 and section 6.3.2A of the City of Novi code of ordinances.

- Planning Deviation (Section 3.1.25.D) for the exterior side yard Building Setback requirement for Unit 1 to allow for construction of a wall enclosure structure and mechanical equipment for the IFLY building. The mechanical equipment for this building is specifically located on the expressway side of the proposed IFLY building as it provides an additional sound barrier from the noise from the I-96 expressway. This benefits the City of Novi and its residents as it allows construction of the mechanical equipment within a setback that will most likely not be used for any pedestrian activities due to its close proximity to the I-96 expressway and allows for more efficient design of the remaining portion of the property. This deviation refers to section 3.1.25.D of the City of Novi code of ordinances.
- City Council variance (11-194(a)(7)) for the maximum length of the proposed cul-de-sac street length of 1450 feet from the centerline intersection of Crescent Boulevard to the center of the bulb of the Adell Center Drive cul-de-sac.
- Planning Deviation (Sec 3.6.2M) for the requirement for Wetland/Water Course Setback to allow for temporary Wetland/Water Course setback impact as required to remove the existing parking lot and storm sewer infrastructure as well as grading and construction of the new sanitary sewer line, water main line and temporary grading for the new buildings to be located on proposed Units 3, 4 and 5. This deviation refers to section 3.6.2.M of the City of Novi code of ordinances.
- Planning Deviation (Sec 3.1.25.D) for a minimum front and side yard parking setback of 20' from the proposed Adell Center Drive sidewalk, and a 20' minimum parking setback from Crescent Boulevard, I-96 Right of Way line and all other PRO Perimeter boundary lines and 0' from common lot lines between units within the Adell Center development. This deviation is being requested clarify the city's setback requirements each unit within the development and to allow the front yard parking setback to begin at the edge of the proposed Adell Center Drive sidewalk, vs. the edge of the proposed Adell Center Drive private roadway easement. This request should have no negative effect on the overall development as it provides ample room for either a proposed landscaped berm or landscaped screen wall along the frontage of each unit. The landscaped areas between the Adell Drive and adjacent parking areas shall comply with the landscape standards set forth in Section 5.5. This deviation is requested for Unit's 1 through 9 of the development. This deviation refers to section 3.1.25.D of the City of Novi code of ordinances.
- Planning Deviation for the Water Tower. The water tower is to remain on its own Unit within the development. This is not a principal permitted or accessory use of the site.
- Planning Deviation (Sec 4.19.2.F) to allow for construction of a dumpster enclosure along the I-96 side of the buildings for Units 1, 7, 8 and 9 of the development. This deviation request is to clarify the positioning of proposed dumpsters along the I-96 frontage units due to there being multiple front yards for these units.
- Planning Deviation (Sec 4.03.A) to allow for the recording of proposed site condominium units 3, 4 and 5 lying partially within the flood plain of the Walled Lake Branch of the Middle Rouge River. As part of the site plan, there will be no proposed impacts/alterations to the existing flood plain from any units 3 and 5. There will be a pedestrian bridge and walking path constructed across Unit 4 that will be within the flood plain area. This deviation refers to section 4.03.A of the City of Novi code of ordinances.

- Planning Deviation (Sec 3.27.1) for Parking, Loading, Signs, Landscaping, etc. to eliminate requirement for loading areas for the following Units.
  - i. Unit 1 – IFLY – Ifly has no regularly schedule deliveries that would require a designated loading/unloading area.
  - ii. Unit 3 – Fairfield Inn – Per Fairfield Inn representatives, their operational requirements do not necessitate the need for a designated loading space.
  - iii. Unit 5 – Drury Hotel – Drury’s deliveries are all made during the daytime non-peak parking times when the regular parking spaces are not in use.

This deviation refers to section 3.27.1 of the City of Novi code of ordinances.

- Planning Deviation (Sec 5.4.2) to allow for construction of a loading space within building and/or parking setback area adjacent to the I-96 Right of Way for Units 1, 7, 8 and 9 of the development. This deviation request is to clarify the positioning of proposed loading areas if required as part of the currently proposed users or future users of said units. Note that this deviation is subject to the waiver to eliminate the requirement of loading areas as requested herein.
- Planning Deviation to allow for Maximum allowable wall material percentages per building facade according to the following Units.
  - i. Unit 1 – IFLY – See sheet plan sheets submitted for percentages
  - ii. Unit 3 – Fairfield – See sheet plan sheets submitted for percentages
  - iii. Unit 5 – Drury – See sheet plan sheets submitted for percentages
  - iv. Unit 8 – Carvana – See sheet plan sheets submitted for percentages

This deviation refers to section 5.15 of the City of Novi code of ordinances.

- City Council Deviation from the existing sign ordinance 17-188 to allow for building signage for the following proposed units.
  - i. Unit 1 – IFLY – See sheet plan sheets submitted for percentages
  - ii. Unit 5 – Drury Hotel – See sheet plan sheets submitted for percentages
  - iii. Unit 8 – Carvana – See sheet plan sheets submitted for percentages

This deviation refers to section 3.27.1 of the City of Novi code of ordinances.

- Planning Deviation (Secton 5.2.12) for minimum number of parking spaces per unit based on each unit owners current parking requirements, or as recommended by a shared parking study as prepared by the applicant’s traffic consultant and agreed upon by the city’s traffic engineer. All of the proposed end users within the Adell Center Development are national chains with multiple locations across the United States. Based on their current facilities, they are requesting the proposed parking space numbers that reflect what they need to serve their businesses. In some instances, the proposed parking counts reflect a parking space count that is less than that required by city ordinance. The reduced parking count will benefit the City of Novi and its residents by reducing the overall parking space count, thus reducing the storm water runoff and increasing the green space area within this development. In the event that additional parking is needed for certain situations, the owners are requesting to reserve the right to share parking to accommodate peak or special event parking. This deviation request would allow the unit owners to share parking with their neighboring unit owners based on peak hour parking needs as outlined in the shared parking study as prepared by the traffic consultant and approved by the City of Novi’s traffic consultant.

- Planning Deviation for construction of two development monument signs over 200 square feet to be constructed as shown on the PRO plan and located as indicated on the PRO plan and as submitted on a separate sign application package with the City of Novi.
- Planning Deviation (Section 4.02.B) for Side Lot Lines. This section of the ordinance requires side lot lines to be at right angles or radial to the street lines. This deviation request is to allow the side unit lines for units 1, 6, 7 and 8 to be non-radial or non-perpendicular to the street lines. This deviation request is in part based on the fact that the site is irregularly shaped with only one viable access point at the southeast corner of the property. This deviation benefits the City of Novi and its residents by making the site layout more efficient and reducing the amount of un-usable area within each proposed unit, thus making the site layout more efficient.
- Planning Deviation (Section 3.271.F) to allow for a minimum of fifteen (15) percent of the gross overall site area for each site within the development to be included as part of the proposed non-developed area in the southerly portion of the overall development. There is significant benefit to the City of Novi and its residents by way of providing all of the open space in one area as opposed to providing small portions of open space sporadically throughout the site. The development plans for the proposed park area include removal of all of the low deadfall and small brush throughout the southerly portion of the site. In addition, a foot bridge is planned to the open space area to provide a connection from the northerly portion of the property to the proposed woodchip pathway that will make a connection to the proposed City of Novi loop road and sidewalk system. An additional benefit resulting from this request will be the accommodation of all of the required open space in a general common element area so that the unit owners association will be responsible for maintenance rather than relying on each unit owner to maintain the required open space areas.
- Planning Deviation (Sec 3.27.1.G) to allow for future renovations, alterations, or additions are made to the buildings within this development, the exterior building facades of the entire building shall be brought into compliance with the approved PRO agreement for this development. This deviation request is to ensure future building modifications are in compliance with approved PRO agreement.
- City Council variance to eliminate the requirement for a traffic impact study due to the city is currently undertaking a traffic study for this area.

In addition to the general site deviations requested above, the following additional site specific deviations are requested.

- Unit 3 – Fairfield Inn - Planning Deviation from the requirement for no parking stalls to be located adjacent to a parking lot entrance from a street (public or private) shall be located closer than two (2) feet from the street right-of-way (ROW) line, street easement or sidewalk, whichever is closer. This deviation request is to allow a parking stall to be located a minimum of 11.9 feet from the roadway easement. This deviation refers to section 5.3.13 of the City of Novi code of ordinances.
- Unit 3 – Fairfield Inn – Front Parking Setback from the roadway easement to allow for a 11.9' setback from the roadway easement.

- Unit 3 – Fairfield Inn – Front building setback for a proposed 53.5 foot setback. This building position offers maximum use of the property for vehicular parking and circulation. The large area between the building location and property line has been maintained that will be utilized as expansive landscaping and pedestrian use.

On behalf of the applicant and based on the above description and attachments, we kindly request positive consideration by the City of Novi on this matter.

If you have any questions, please do not hesitate to contact me.

Sincerely,  
GreenTech Engineering, Inc.



Daniel J. LeClair, PE, PS  
President

Attachments

PLANNING MEMO

UNLISTED USE DETERMINATION FOR CARVANA





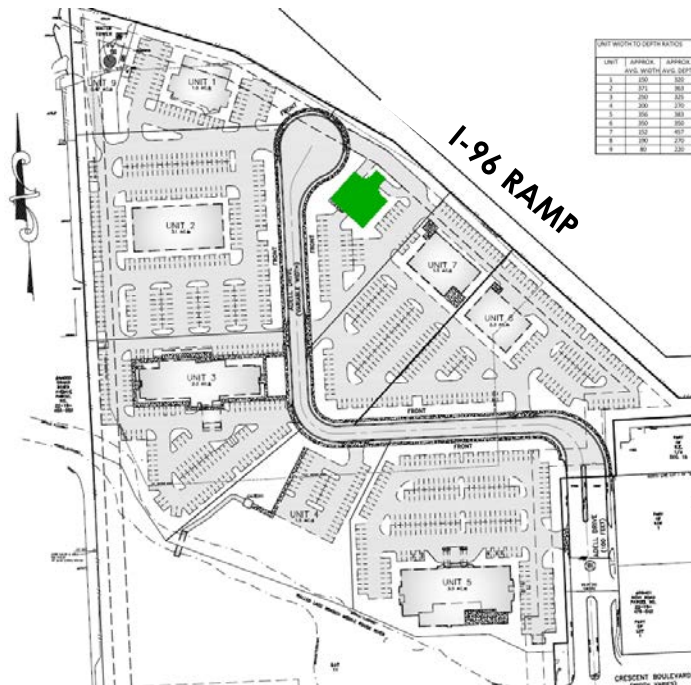
# MEMORANDUM

**TO:** PLANNING COMMISSION  
**FROM:** SRI RAVALI KOMARAGIRI, PLANNER  
**THRU:** BARBARA MCBETH, AICP, CITY PLANNER  
**SUBJECT:** UNLISTED USE DETERMINATION FOR CARVANA AS 'VENDING MACHINE FULFILLMENT CENTER'  
**DATE:** JUNE 28, 2018

Section 4.87 of the City of Novi Zoning Ordinance contains provisions for an Unlisted Use Determination, which allows the City Council, after review and recommendation by the Planning Commission, to determine the appropriate zoning district for a specific use that may not be listed in the Zoning Ordinance. A copy of the Unlisted Use ordinance language (Section 4.87) is attached to this memo.

This ordinance allows for a decision to be made on a specific use request by a specific applicant. It does not formally amend the ordinance to permit this use in the future in the district requested, although the Planning Commission and City Council may wish to pursue a Zoning Ordinance text amendment as well, after the determination is made. Once the appropriate zoning district is identified, the actual location of the proposed use will be reviewed at the time of Planned Rezoning Overlay concept plan and site plan review in conjunction with a Special Land Use request. At this point, the Planning Commission and City Council are asked to make a determination whether or not the use is appropriate for

the district in which the applicant has requested it.



The request under consideration concerns whether to allow '**Vending Machine Fulfillment Center**' as a Special Land Use within the City of Novi. Planning Staff has determined that the Zoning Ordinance does not provide for the use as it has been described. Therefore, the City Planner has forwarded the request for a decision under the Unlisted Use section of the ordinance. Staff notes are provided in **bold** throughout the memo.

The applicant has requested to allow the proposed unlisted use at the subject property; the former Exposition property located south of I-96 ramp and west of Expo Center Drive near Novi Road. The use will be located in the area indicated in green in the image

above for freeway visibility. It is proposed to be part of the development that includes other uses such as indoor recreational facilities, hotels and restaurants.

### USE SUMMARY

The applicant has provided a 'Project Development Narrative' explaining how Carvana works. Following is the summary of the narrative that pertains to the use and operational style of Carvana.

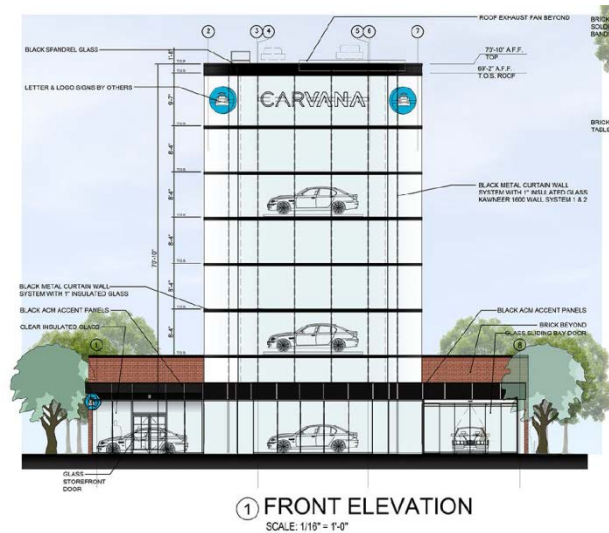
1. Carvana facilitates the sale of used car vehicles online; the sold cars are delivered to Carvana fulfillment center for customer pick up. In some events, customer advocates may deliver each car to the buyer's home on request.
2. Store hours are from 7 am to 9 pm. The customer visits the store by appointment, finishes the final paperwork to get a token. The buyer then use the token to get the car out of the 'vending machine'.
3. Carvana stores cars vertically in a 7 story tower as opposed to traditional dealership's off-street horizontal parking lots. The office building attached to the tower is only single story. Vehicles are visible through windows on all floors from all sides.
4. The tower is not occupied and is simply used as storage for pre-sold vehicles to be picked up.
5. The applicant has indicated that approximately 35-45 parking spaces are required for this use. Further clarification is required whether the surface parking vehicles.  
**Please clarify how many customers visit the site as an average daily basis.**
6. Carvana proposes to employ 10 -12 customer advocates and at least one manager at Novi location.
7. There is no servicing of vehicles or auto repair being conducted at the 'Vending Machine Fulfillment Center'. There is a designated area for a light rinse before the vehicles are loaded into the Vending machine.
8. A nine-car delivery truck will visit the site twice daily on an average. The trucks will not be parked on site. The cars will be placed in the parking spaces and the customer advocates will drive them into the tower. **The applicant is asked to dedicate nine parking spaces for temporary storage of delivered vehicles on site so that it would not conflict with required surface parking for employees and customers. Staff is unable to identify whether a dedicated loading zone is proposed.**
9. **All construction will be completed in one phase.**
10. According to a traffic report conducted by KLOA, Inc in 2017, the trips generated in and out of the site include those of the employers, customers who visit the site by appointment and the delivery trucks. It would not include customers shopping for new vehicles similar to how a traditional car dealership operates. **The applicant is asked to provide a copy of the Traffic report for further information about trip generation during peak hours and also to determine the minimum parking requirements.**

## ARCHITECTURE

The tower is made of steel and glass construction. The applicant has provided building elevations under a separate application which is under review for rezoning overlay. Here is an excerpt from the review provided by our Façade consultant.

*'The Façade Ordinance defines Display Glass as areas of vision glass that are intended through exposure to thoroughfares and pedestrian ways and lighting methods to strongly emphasize the items displayed within the building. Based on this definition the Caravan*

*tower would be considered Display Glass. Similar to the Town Center bell tower, we believe that the Carvana tower will represent an iconic element within this project and the Novi shopping district at large. We believe this justifies a deviation from strict interpretation of the Façade Ordinance with respect to Display Glass.'*



Carvana was founded in 2012 as an online automobile retailer. It is currently operating in 9 cities in United States in Texas, Florida, Tennessee and North Carolina. If the unlisted use, site plan and special land use are approved in Novi, it will be the first store in Michigan. **It is an experimental concept, which is becoming popular. However there is no guarantee for the long-term viability of the use. Staff is concerned that there is no alternate use for the building if the proposed use of 'Vending Machine Fulfillment Center' eventually becomes outdated, primarily because the glass tower is built to store vehicles and not for human occupancy. The options to repurpose the building for another use is unlikely.**

## ORDINANCE REQUIREMENTS

1. The proposed zoning category only allows for a maximum of 5 stories or 65 feet building. The applicant is requesting a deviation for 7 stories and 75 feet height.
2. The applicant is also requesting multiple deviations from our sign Ordinance, which are yet to be reviewed by staff.
3. The current Zoning Ordinance does not provide guidance to determine required parking for such a use. The applicant is proposing up to 40 spaces. It appears that there will be 10-12 employees on site and 9 parking spaces are required to temporarily park delivered vehicles. **Please clarify how many customers visit the site on an average daily basis. The applicant is asked to provide a copy of the Traffic report for further information about trip generation during peak hours and also to determine the minimum parking requirements.**

- The tower appears to be lit 24 hours a day. The applicant is asked to provide additional information about lighting and its impact on the surroundings.

**IDENTIFIED USE AT OTHER COMMUNITIES**

At the time of Pre-application meeting, staff requested the applicant to include information about their existing locations and how other local governments defined their use. A list of existing locations is included, but the approved use categories are not listed. **The applicant is asked to provide the missing information in the table below so that that staff can make a recommendation for an appropriate zoning district.**

Existing Operating Location	Use Type	Zoning District
Nashville, TN		
Dallas, TX		
Austin, TX		
Houston, TX		
San Antonio, TX		
Jacksonville, FL		
Tampa, FL		
Charlotte, NC		
Raleigh, NC		

The applicant in the Planned Rezoning Overlay narrative which is under review currently as a separate application has stated that the use would fall under the phrases “retail uses” and “including but not limited to”. **This may not be applicable, as all sales happen online and only delivery happens on site. There are no walk-ins allowed for customers looking to buy a car. There are no vehicles on display for sale.**

The intent of Town Center Ordinance states that uses which have as their principal function of used motor vehicle sales would have a disruptive effect on the intended pedestrian orientation of the districts. The applicant states that all sales are facilitated online and this is not defined by traditional used car sales vehicle facility.

**As discussed above, the proposed use has similar characteristics such as retail allowed in TC, B districts etc. or car sales allowed in B-3 district, warehousing facilities allowed in I-1, OST districts or Vehicular parking districts such as P-1 district. However, it does not entirely meet the intent of any of those districts.** The applicant has indicated that Carvana facilities are always typically located with freeway frontage. Section 3.1.25 which lists the permitted uses is attached to this memo.

## **USED CAR DEALERSHIP: STATE OF MICHIGAN**

License for used car dealer is classified as 'Class B' License under State of Michigan licensing regulations. Per the Original Vehicle Dealer License Application Instruction Booklet, '*used or second hand vehicle dealer allows dealer to buy and sell used vehicles to and from the general public and other licensed vehicle dealers*'.

Per the booklet, '*new and used vehicle dealers are required to provide proper servicing facilities. The servicing facility must be located within 10 miles of the dealership's established place of business*'. **The applicant indicated that there is no servicing provided on site, but information about off-site service facility, if any is not indicated.**

**Additional requirements about what an established plan of business must include are listed in the excerpts of the booklet attached. The current location meets all those requirements, except for on-site service and repair or alternate options. The applicant is asked to clarify if a local Michigan dealership license is required to establish Caravan facility at this location.**

## **UNLISTED USE DETERMINATION**

The Planning Commission is asked to review the applicant's narrative, elevations, signage and floor plans, which are attached, along with staffs comments and determine whether or not the use would be appropriate as a Special Land Use in all determination on the use and if it would be most appropriate for the current proposed zoning category of Town Center in the current proposed location.

**Note: If approved, the applicant will still need to obtain Special Land Use approval through the Planning Commission during Preliminary Site Plan review.**

---

### **Attachments:**

1. Carvana: Project Development Narrative
2. Carvana: Building Elevations
3. Carvana: Floor Plan
4. Carvana: Signage Plans
5. Section 3.1.25: TC Permitted Uses
6. 4.87 Unlisted Use Determinations
7. Excerpt from Original Vehicle Dealer License Application Instruction Booklet for Class B License

## Carvana: Project Development Narrative



**CARVANA**

## **Proposed Development Project Narrative**

**Submitted by:  
Arwa Lulu  
Carvana, LLC  
1930 W. Rio Salado Pkwy  
Tempe, AZ 85271**

**Submitted to:  
The City of Novi, Detroit  
45175 Ten Mile Road  
Novi, Michigan 48375**

**June 5, 2018**



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## Development Team

Applicant:

Carvana, LLC.  
Arwa Lulu, Entitlements Associate  
1930 West Rio Salado Parkway  
Tempe, AZ 85281  
480-244-3900

Architect:

WHN+ Architects  
Jim Hodges  
330 West 10<sup>th</sup> Street  
Charlotte, NC 28202  
703-333-9952

Engineer:

GreenTech Engineering, INC.  
Daniel LeClair, PE, PS  
51147 W. Pontiac Trail  
Wixom, Michigan 48393  
248-668-0700



## **Section 1. Introduction**

### **Introduction**

Carvana, LLC is pleased to submit this project narrative to the City of Novi in support of the overall Planned Rezone Overlay (“PRO”) plans. This request will allow for the development of a Carvana Vending Machine Fulfillment Center at 43700 Expo Center Drive as part of the overall Adell Center Multi-Use Development proposed in Novi, Michigan.

As described herein, Carvana’s proposed use differs significantly from a traditional automobile sales facility. Rather than utilizing large surface parking areas to store and display vehicle inventory on-site, Carvana facilitates the sale of used vehicles online at Carvana.com. The cars are then transported to the Carvana Fulfillment Center nearest the customer and is then made available for customer pickup via a state of the art Vending Machine structure. Through the use of the Vending Machine Fulfillment Center, Carvana has transformed the stress-filled car buying experience into a user-friendly and exciting customer experience.

### **About Carvana**

Carvana is an online automotive retailer that is disrupting the used car industry. Through a robust, user-friendly website, Carvana offers over 7,500 pre-owned vehicles for sale and provides buyers the opportunity for a 360° view of the interior and exterior of the vehicle prior to purchase. Customers are given a 7-day test drive and delivery of the vehicle can be made in as little as 24 hours.

- Carvana was founded in 2012 as an online automotive retailer. Carvana has been described as an “Amazon for cars” and provides the ability for customers to buy a car in as less than 10 minutes.
- In 2018 Carvana made the Inc. 5000 list as one of the fastest growing companies in the nation coming in at #34.
- Carvana is a publicly-traded company and has current revenues in excess of \$350,000,000 per year.

- Carvana is now the 2<sup>nd</sup> largest pre-owned automotive retailer in the state of Georgia.
- Through the use of a coin-operated Vending Machine Carvana delivers cars to buyers in an entertaining and memorable way.
- Carvana currently has Vending Machine stores operating in:
  - Nashville, TN
  - Dallas, TX
  - Austin, TX
  - Houston, TX
  - San Antonio, TX
  - Jacksonville, FL
  - Tampa, FL
  - Charlotte, NC
  - Raleigh, NC

With the development of its Vending Machine properties, Carvana provides a non-traditional bricks-and-mortar identity for those consumers who want to establish an in-person relationship with their car retailer. Visitors to the Vending Machine are welcomed by appointment-only and standard hours of operation are daily from 7am to 9pm.

### The Carvana Difference

The Carvana Difference	
Traditional Automobile Dealers	Carvana
10 to 20 acres	1 to 3 acres
30,000+ building square footage	5,700+/- building square footage
Over 650 parking spaces	Approx. 35-45 parking spaces
Vehicles are displayed for sale	Vehicles are pre-sold, awaiting customer pickup
Walk-ins are welcome and encouraged	Store visit by appointment-only
Large areas of impervious surfaces	Small areas of impervious surfaces
Fuel pumps	No fuel pumps
Balloons, loud signs, marketing gimmicks	No marketing gimmicks, loud music or large banners leading to site

### Site size

Carvana packs a lot of excitement into a small site. While most traditional car dealers require 10 to 20 acres to house their showrooms and inventory, Carvana requires on average only 1 to 3 acres for their Vending Machine store locations and only 1.3 acres

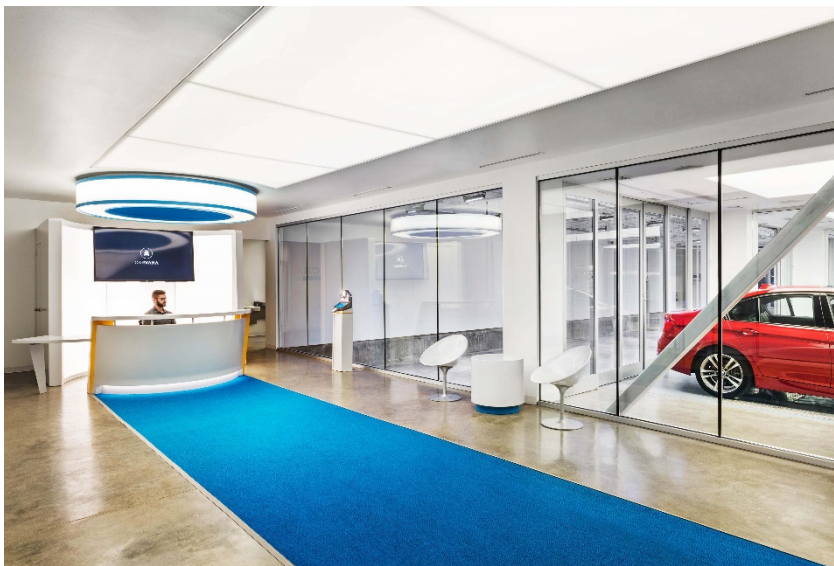
specifically for Novi, MI. Carvana is able to comfortably place the tower, welcome center, customer parking, landscaping and storm water retention on the site.

## Architecture and Engineering



The Carvana fulfillment center features an award-winning, contemporary, single-story building connected to a 75-foot glass and steel octagonal tower (pictured left). The building center includes: The Customer Center, employee office space and the Vending Machine Car Tower, which is attached to the building. Inside the

tower are pre-purchased vehicles awaiting customer pick-up. It is important to note that the tower is not occupied and acts simply as temporary storage of vehicles.



After completing the final sales paperwork in the Carvana Customer Center (pictured left), customers place their token in the coin machine and that is what initiates the car to vend from the tower. The customer's car is brought down from the tower via a proprietary automated delivery system, and brought out to the customer through two

glass doors and into a drive aisle closest to the Customer Center entrance.

## Project Location

Carvana is under contract to purchase the approximately 1.3-acre parcel with direct visibility off from I-96. The location is shown in the aerial on the next page, and is also known as parcel ID 22-15-476-045.



## Plan of Operation

Outlined below is Carvana's operational detail, which includes a thorough understanding of the unique nature of the typical operations.

**Workforce:** Carvana projects to employ 10-12 Customer Advocates and at least one Manager at this location. These employees will work in shifts. Their activities include:

- Setting appointments with customers for pick-up of purchased vehicle
- Welcoming customers to facility
- Finalizing purchase paperwork, obtaining final signatures
- Assist in off-loading of vehicles from Carvana haulers/delivery trucks
- Loading of vehicles into Vending Machine

**Clients and Customers:** Before a customer completes their online vehicle purchase, they are prompted to schedule their pickup time before arriving to the Carvana Vending Machine. Customer visits to the site are by appointment only.

**Processes Conducted on the Site:** As previously mentioned, activities on the site include setting up appointments with customers to schedule pick-up of their cars;

off-loading of vehicles from the delivery trucks; loading of vehicles into the Vending Machine; finalization of purchase paperwork and presentation of cars to their owners. Standard hours of operation are from 7:00 a.m. to 9:00 p.m. There will be no servicing of vehicles or auto repair being conducted at the Vending Machine facility, although there is a designated area in the building where the vehicle is placed for a light rinse before being loaded into the Vending Machine for customer pick-up. Additionally, there are no outdoor speakers or any promotional signs.

**Materials Used:** There are no hazardous materials used at the Carvana Vending Machine Fulfillment Center.

**Receiving and Deliveries:** There are deliveries of cars to the site via a nine-car delivery truck. Carvana owns and operates their own fleet of delivery vehicles and the drivers are Carvana employees. Accordingly, Carvana controls the days and times of deliveries to the site. It is anticipated that there will be on average two deliveries of vehicles to the site per day. Additionally, for those customers who choose the at-home delivery option, single car haulers are available on site and Customer Advocates will deliver the vehicle to the customer's home.

**Waste:** The waste produced at a Carvana Vending Machine site is the same as an office building, largely dry paper and cardboard. The site will have an enclosed trash receptacle.

## **Phasing**

All of the construction and development proposed in this application will be undertaken in a single phase.

## **Circulation**

General customer and employee parking is provided on site. Drivers operating the nine car haulers will be instructed to access the site into the loading/unloading area and vehicles will be off-loaded into the empty parking spaces temporarily. Once the delivery is complete, the hauler will be directed to exit the site since these multi car haulers are not placed on site Customer Advocates will then immediately load the vehicles into the tower.

## Traffic

Carvana has conducted a traffic evaluation for a different Vending Machine site with the same use proposed in Novi, MI. The traffic report was conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) in 2017. Carvana's Vending Machine facility is limited to customer pick-ups of their already purchased vehicles, which are made by appointment only. No vehicle repair, customization, auto-body services or auto parts sales will be available at the proposed center. As such, the vehicle trips accessing Carvana during peak hours may be further reduced from what is estimated to be generated.

The Vending Machine facility does not house "for sale" vehicles in the tower or in the available parking spaces, therefore Carvana would not generate traffic for customers shopping for a new vehicle similar to how a traditional car dealership operates.

## Impact of Proposed Development and Requested Deviations

The innovative site plan and unique nature of this land use allow for what many could consider a vacant, remnant parcel, to instead be redeveloped into a vibrant use and strong contributor to economic development in the City of Novi. The proposed Carvana use represents an improvement over the existing site uses and conditions. The proposed development uses a smaller footprint than a traditional car dealership, and by reducing the amount of parking on the site, more green area is provided. Additionally, the overall traffic impact to the site will be reduced.

The proposed development will also have a positive aesthetic impact on the surrounding area. The new Carvana tower will complement the Multi-Use Development and will become a destination location for consumers in the City of Novi and the larger Detroit metropolitan area. The Carvana facility will join the existing development and inject some well-needed vitality to the area. The operational impact on the surrounding area will be minimal.



## Section 2. Deviation Requests

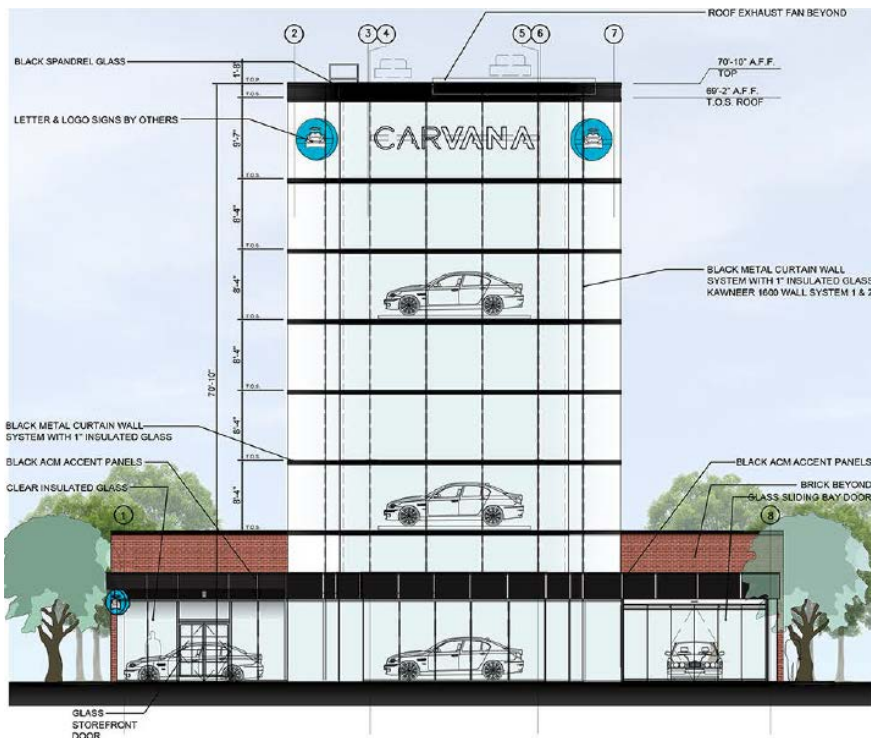
### Deviation Requests

Carvana's site specific deviations include requests for: a 75-foot glass tower; a total of eight signs on the tower and one on the building; a combination of spandrel glass, brick, ACM panels and clear glass for building materials; and approximately 35 parking spaces to accommodate for business needs.

### Height

The proposed Carvana Vending Machine Fulfillment Center will feature a contemporary, single-story building connected to an 8-tier/75-foot clear glass and steel tower. As shown on the Site Plan, **Exhibit A** of this narrative, the building is placed on the site parallel to Interstate 96. The Vending Machine tower component will be visible from the interstate. Zoning District "TC" allows for 5 stories or 65 feet as the maximum building height and Carvana requests a deviation to allow for an additional 10 feet.

### Signage



Signage is used as proper way finding and an essential component of any businesses' overall marketing strategy, and that is no different for Carvana. The Vending Machine tower is where most of the signage is placed (pictured left). Since Carvana's tower will be visible from Interstate 96, signage is placed at the top of the tower to make our building noticeable for customers. This type of signage not



only provides wayfinding to the Carvana facility, but also provides emphasis to the buildings architecture.

Carvana is requesting a variance from Chapter 28: Signs Sec. 28-5. from the City of Novi Zoning Ordinance. The Town Center District (“TC”) allows wall signage for a maximum of 250 square feet or a canopy sign up to 24 square feet.

**Permanent Signs Permitted According to District:**

District	Wall Sign (per tenant, multiple)	Wall Sign (single tenant)		Canopy Sign
Business (B1-3, FS, RC, NCC, C, TC, EXPO)	30-65 square foot maximum based on frontage, (1) sign per business, footnote (7)	Permitted nonresidential only 250 square foot maximum, (1) sign footnote (7)	or	(1) sign of 24 square foot maximum

Item	Description	Qty	Sign Area
A1	Wall Sign Logo	1	23.77
A2	Wall Sign Letters	1	55.00
A1	Wall Sign Logo	1	23.77
A2	Wall Sign Letters	1	55.00
A1	Wall Sign Logo	1	23.77
A2	Wall Sign Letters	1	55.00
A1	Wall Sign Logo	1	23.77
A2	Wall Sign Letters	1	55.00
B	Blade Sign	1	9.00
Aggregate Total			324.08

Carvana requires a total of nine signs for the development: four letter signs and four logo signs on the tower and one canopy sign at the front entrance. The total sign area is 324.09 square feet: 315.08 sq. ft. for the wall signs and 9 sq. ft. for the canopy sign. Carvana is requesting an additional 65.08 square feet of sign area for the wall signs as well as a canopy sign for the front entrance leading customers into the Customer Center area. The chart pictured left summarizes the total square

footage utilized.

## Building Material



Carvana proposes the store concept as pictured left in the artist's rendering.

The occupied building structure material is comprised of brick, ACM panels and clear glass. The material proposed for the occupied building space is listed below:

### OCCUPIED SPACES

CARVANA	FRONT
Brick	30%
Clear Glass	53%
Spandrel Glass	0%
ACM Panels	17%
HM Doors	0%

Carvana is requesting a variance from the façade requirements as listed below. Carvana requests to have the occupied building portion made primarily of clear glass, brick and ACM Panels. The tower attached to the building will include glass and steel.

Carvana	Front	Right	Left	Rear	Ordinance Maximum (Minimum)
Brick	0%				100% (30% Min.)
Brick and Stone Combined	0%				50% Min (TC Ord.)
Spandrel Glass	20%				50%
LED Accent Light Band (2 rows)	N.P.				0%
EIFS	80%				25%

## Parking

Carvana proposes up to 40 parking spaces for the site to accommodate for the parking needs of employees, visitors picking up their newly purchased vehicle, and cars which have been delivered to the site but have not yet been loaded into the Vending Machine. Carvana haulers will enter the site and unload into the empty parking spaces. Once

vehicles are unloaded and the hauler has left the site, the pre-purchased vehicles will be loaded into the tower. On average, our Vending Machine facilities have anywhere from 35-45 surface parking spaces, which is significantly less than a traditional car dealership that may house over 600 parking spaces.

Carvana requests a total of at least 40 parking spaces for the site. The "TC" district allows for 30 spaces.



### **Section 3. Conclusion**

This proposed development will bring a new and exciting company, whose values, mission and goals align closely with the goals espoused in the City of Novi's Master Plan for Land Use Review, most recently updated in 2016.

Some of these shared visions are:

- Embracing new development of land that is of high quality design and materials
- Promoting environmental stewardship through the preservation of open space and utilization of low-impact development techniques
- Placing a high value on economic development through maintaining the balance between the economy, environment and community to create sustainable developments that meets the needs of the community today and in the future

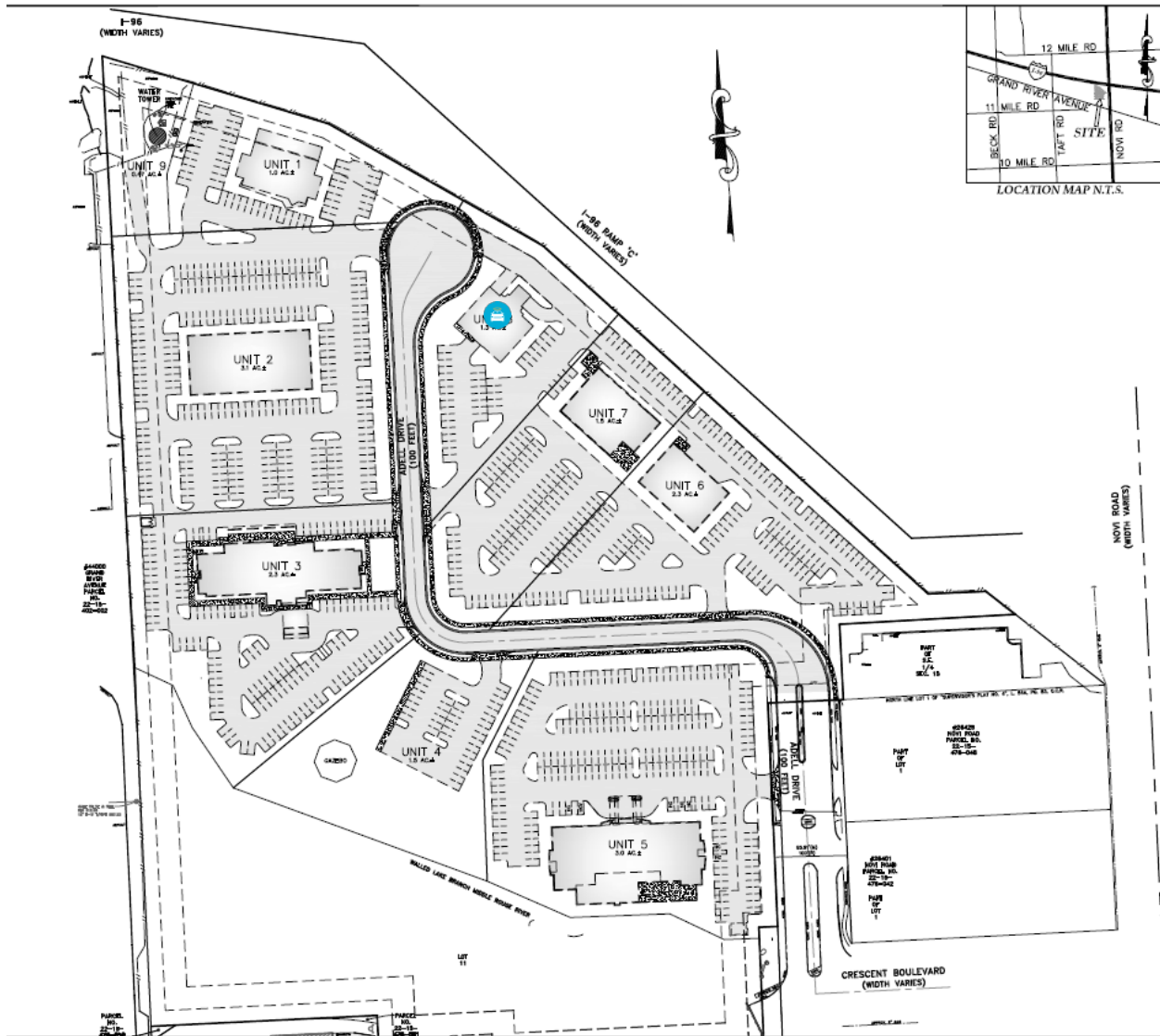
Carvana looks forward to working with City of Novi to develop a Carvana Vending Machine Facility, which will provide residents of Novi, and entire Detroit metropolitan area, with a new retail car buying experience. The development team looks forward to working with the City of Novi to make the vision for this parcel a reality and respectfully request support of this development.



CARVANA

Exhibit A

Overall P.R.O. Plan



## Carvana: Building Elevations

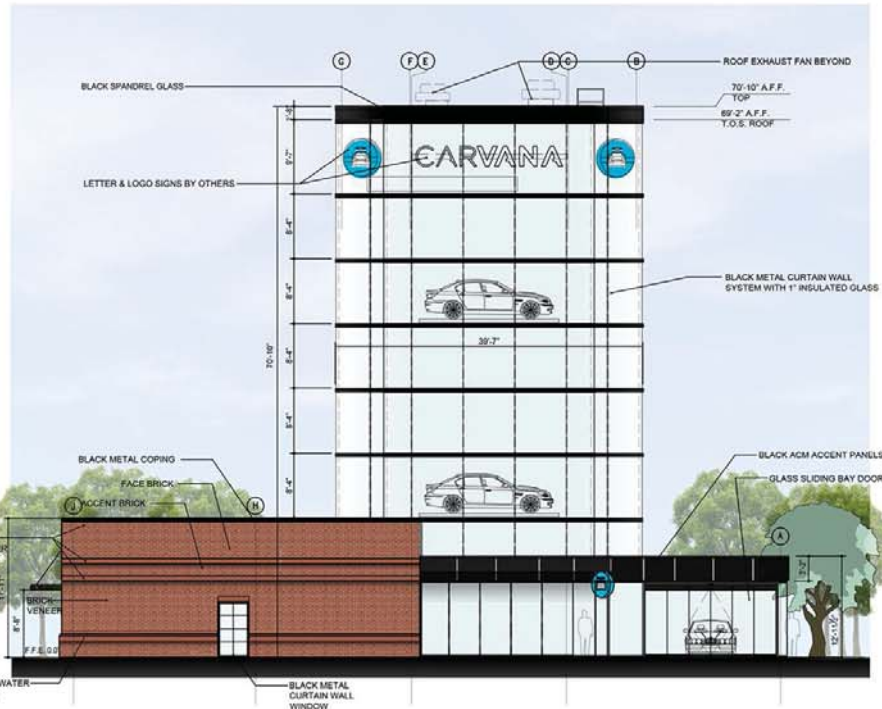
OCCUPIED SPACES

CARVANA	FRONT
Brick	30%
Clear Glass	53%
Spandrel Glass	0%
ACM Panels	17%
HM Doors	0%



① FRONT ELEVATION

SCALE: 1/16" = 1'-0"



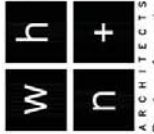
② LEFT ELEVATION

SCALE: 1/16" = 1'-0"

OCCUPIED SPACES

CARVANA	LEFT
Brick	70%
Clear Glass	26%
Spandrel Glass	0%
ACM Panels	4%
HM Doors	0%

330 W. 19th Street  
Charlotte, NC 28202  
704.333.8962  
www.ahw.com



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CARVANA



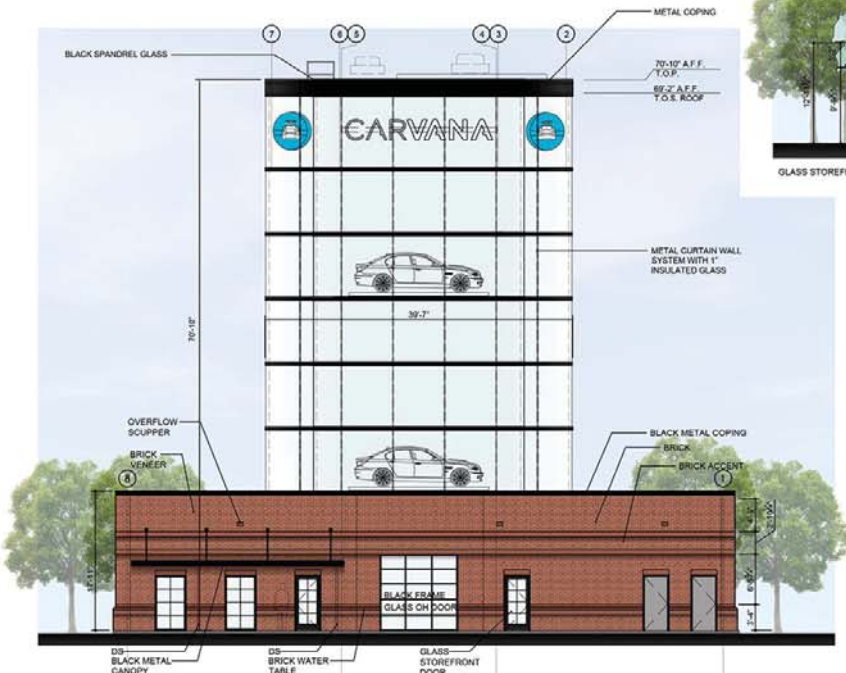
1. MAY 30, 2018

Date Issued: MAY 25, 2018  
43700 EXPO CENTER DRIVE

SD-2  
DETROIT, MI (NOVI)

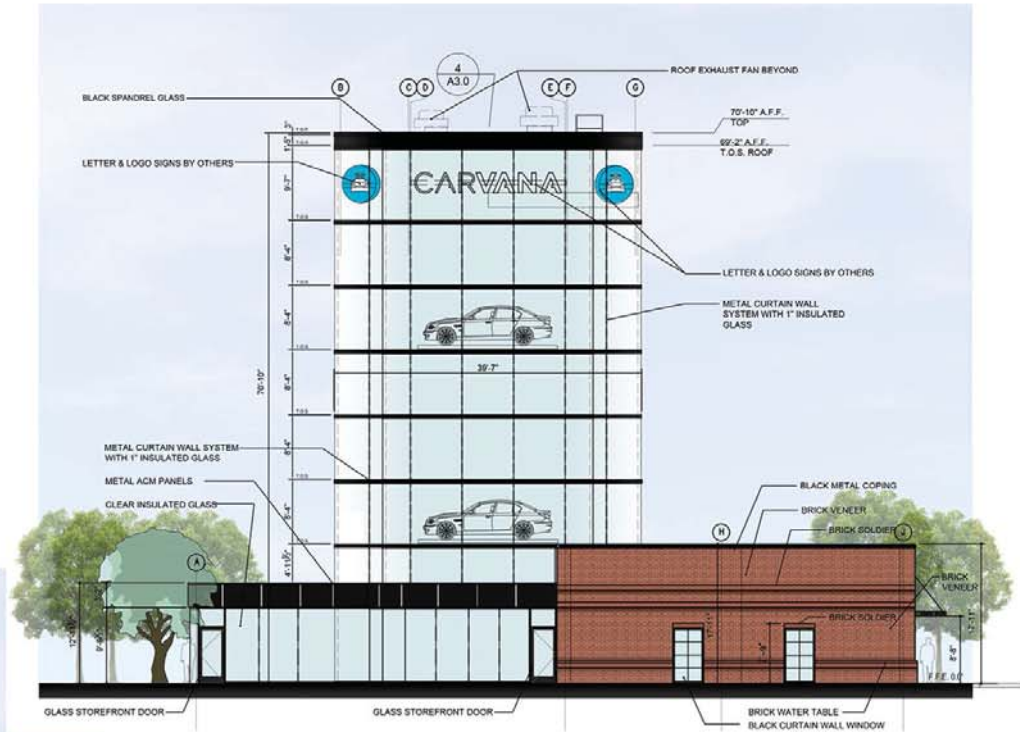
OCCUPIED SPACES

CARVANA	REAR
Brick	82%
Clear Glass	14%
Spandrel Glass	0%
ACM Panels	0%
HM Doors	4%



③ REAR ELEVATION

SCALE: 1/16" = 1'-0"



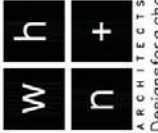
④ RIGHT ELEVATION

SCALE: 1/16" = 1'-0"

OCCUPIED SPACES

CARVANA	RIGHT
Brick	93%
Clear Glass	7%
Spandrel Glass	0%
ACM Panels	0%
HM Doors	0%

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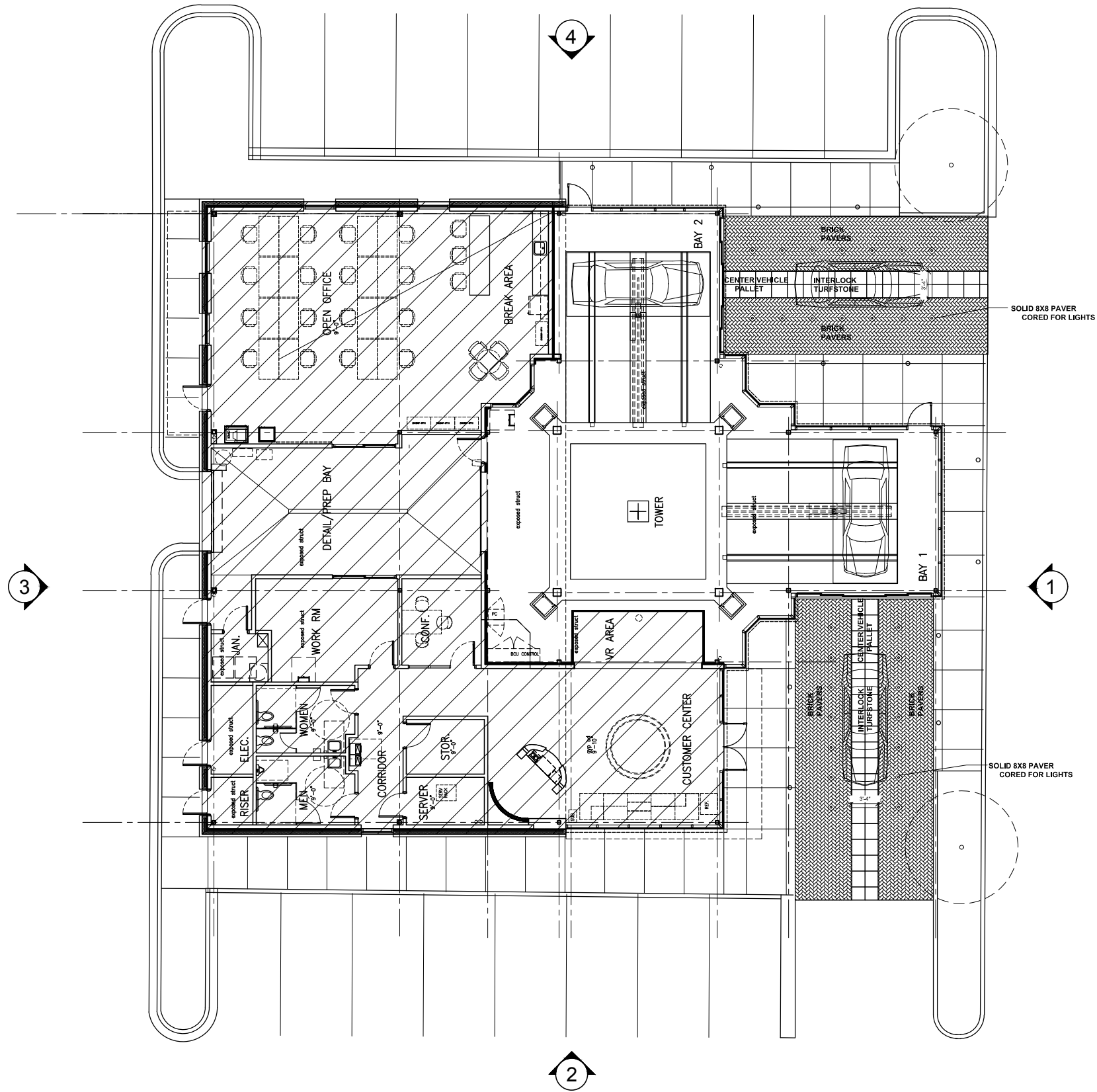
1. MAY 30, 2018

Date Issued: MAY 25, 2018  
43700 EXPO CENTER DRIVE

SD-3  
DETROIT, MI (NOVI)



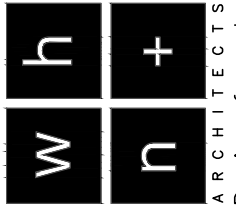
**Carvana: Floor Plan**



1 SCHEMATIC FLOOR PLAN  
SD1 SCALE: 1/16"=1'-0"

OCCUPIED SPACES

330 W. 10th Street  
Charlotte, NC 28202  
704.333.9952 phone  
704.333.9962 fax  
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CARVANA

A	B	C	D
Purchase		Annual Rent,	
Access Rating		Parking Cour	
Height by Right		Height Desir	
Zoning / Use		Variance Ne	
Launch Projection Q/Y			

Date Issued: MAY 30, 2018

43700 EXPO CENTER DRIVE

**SD-1**  
DETROIT, MI (NOVI)

## Carvana: Signage Plans



# CARVANA

**43700 EXPO CENTER DRIVE  
NOVI, MI 43375**

DRAWING NO.

026869

**REVISIONS**

**DATE: REVISION # & DESCRIPTION**

**DESIGNER**



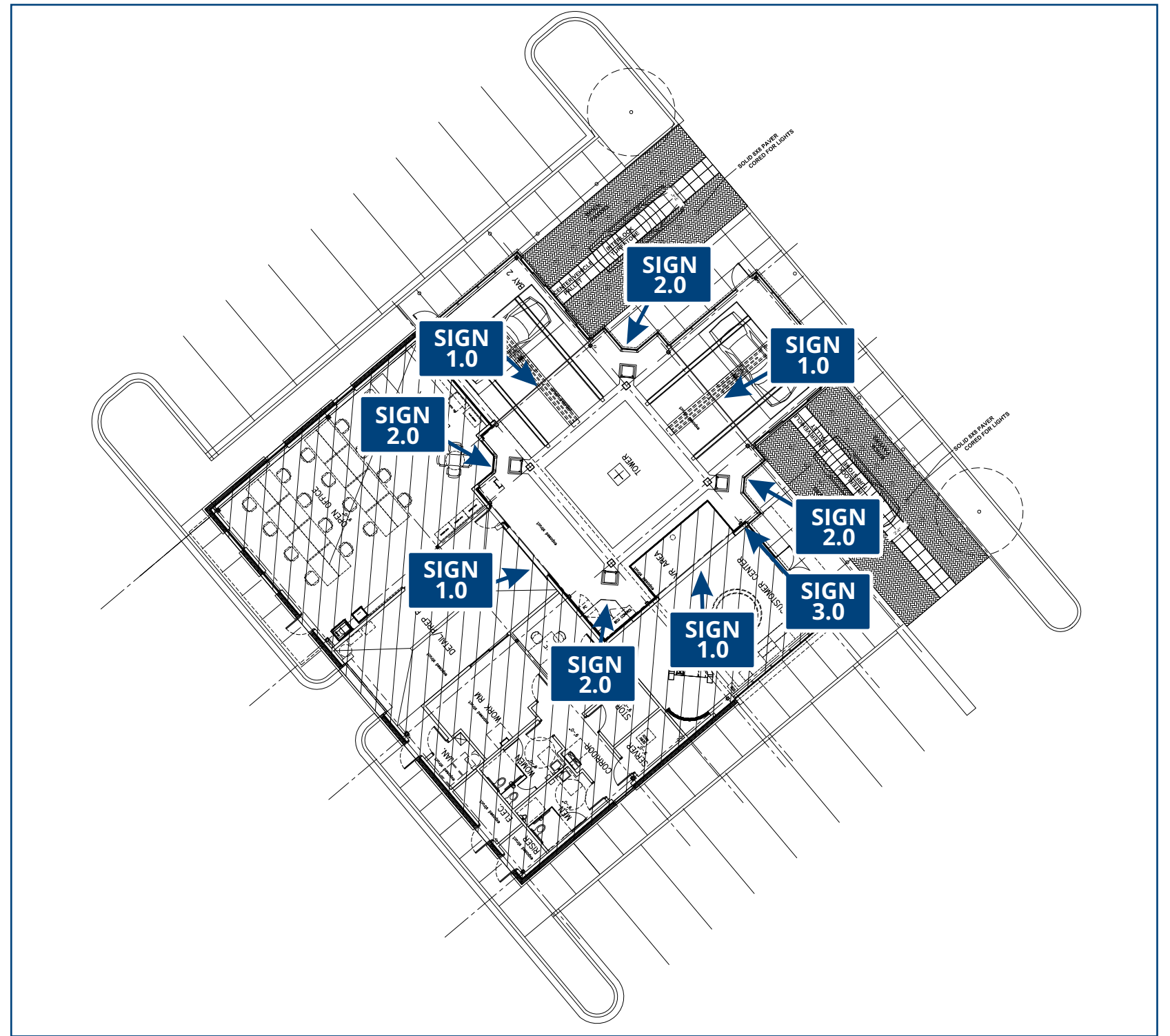
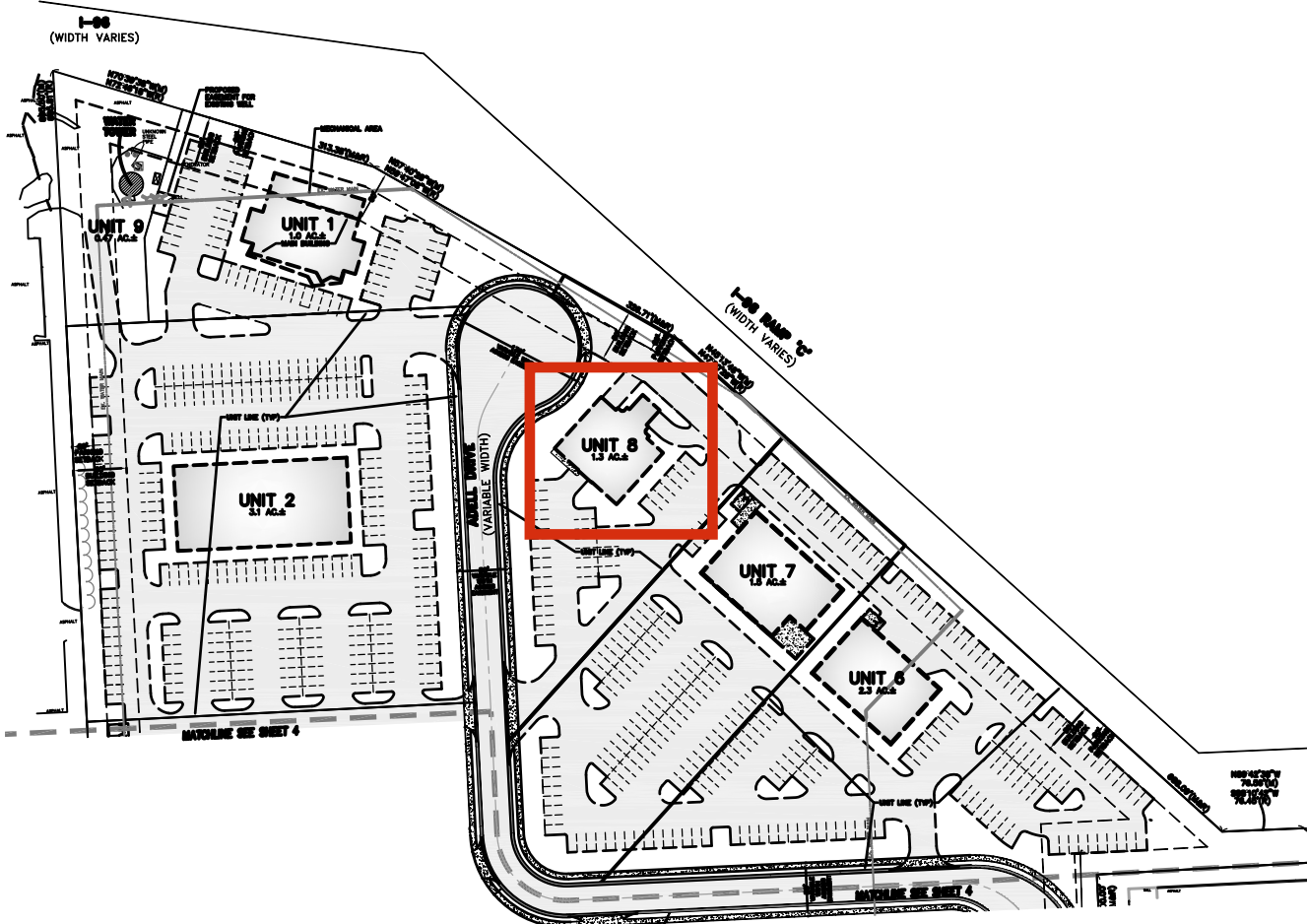
800.861.8006  
ADVANCESIGNGROUP.COM



**SUMMARY OF SIGNAGE**

SIGN	GRAPHIC	DESCRIPTION	SQ FT
1.0		- ILLUMINATED CHANNEL LETTERS - RACEWAY MOUNTED - 4 SETS REQUIRED	57.33 each x4 229.32
2.0		- ILLUMINATED CHANNEL LOGOS - CUSTOM BRACKET MOUNTED - 4 REQUIRED	24.17 each x4 96.68
3.0		- UNDER CANOPY SIGN - 1 REQUIRED	9.0

335  
AGGREGATE  
TOTAL



NORTH

SITE MAP

NOT TO SCALE



**CARVANA**

43700 EXPO CENTER DRIVE  
NOVI, MI 43375

DRAWING NO.

026869

DATE OF  
LAST CHANGE:

6/19/18

REVISION NO.

-

**SITE**

**APPROVALS**

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CONSULTANT:

AW

PROJECT MANAGER:

A. SCHEIBER

DESIGNER:

THW

FILE LOC:

NOVI, MI  
026869



ADVANCE SIGN GROUP

800.861.8006  
ADVANCESIGNGROUP.COM



**FIELD SURVEY REQUIRED**



- = wiring location
- each raceway has wiring setup to exit the left "C" end, puck logo signs have wiring exiting along the top left support tube, the under canopy sign has wiring exiting out of the vertical support tube/mounting plate
- LED border tubing wiring locations to be determined in the field.

**FRONT ELEVATION**

**SCALE: 1/16" = 1'-0"**



**CARVANA**  
 43700 EXPO CENTER DRIVE  
 NOVI, MI 43375

**DRAWING NO.**  
 026869  
**FRONT ELEV**

**DATE OF LAST CHANGE:**  
 6/19/18  
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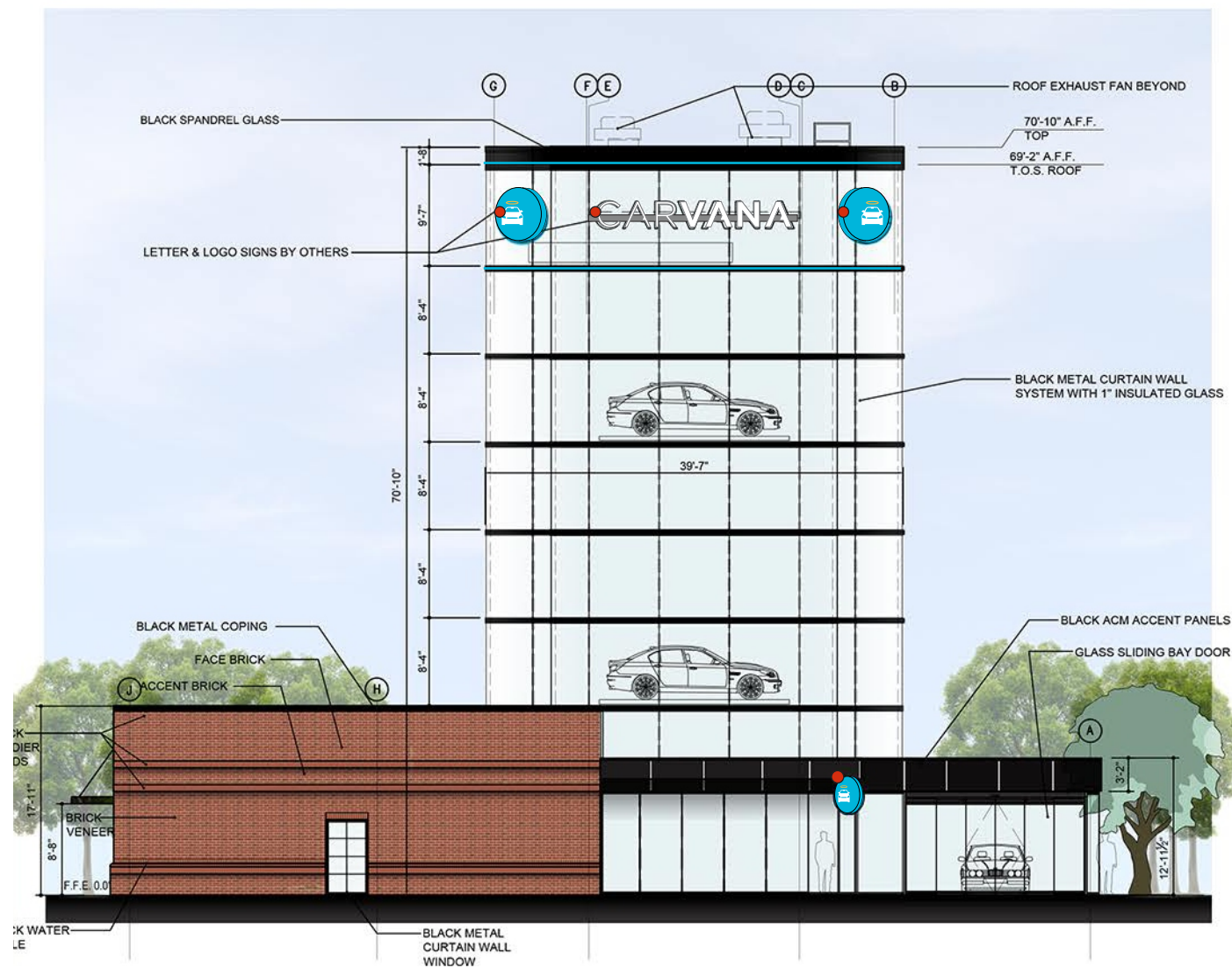
**CONSULTANT:**  
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**PROJECT MANAGER:**  
 A. SCHEIBER  
**DESIGNER:**  
 THW  
**FILE LOC:**  
 NOVI, MI  
 \026869



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LEFT ELEVATION

SCALE: 1/16" = 1'-0"



**CARVANA**

43700 EXPO CENTER DRIVE  
NOVI, MI 43375

DRAWING NO.  
**026869**

DATE OF  
LAST CHANGE:  
6/19/18

LEFT  
ELEV

REVISION NO.

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FILE LOC:  
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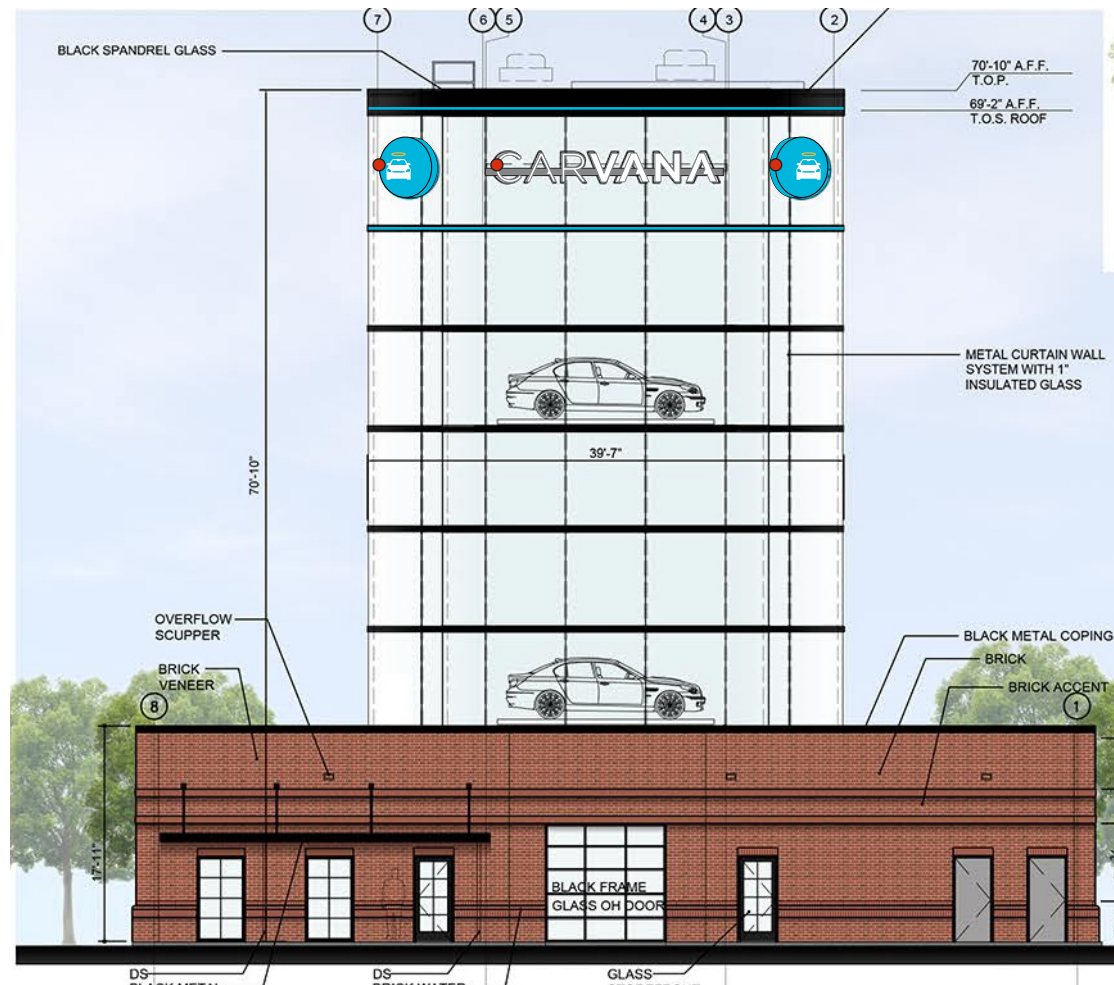


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**FIELD SURVEY REQUIRED**



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REAR ELEVATION

SCALE: 1/16" = 1'-0"



**CARVANA**  
43700 EXPO CENTER DRIVE  
NOVI, MI 43375

DRAWING NO.  
**026869**  
REAR ELEV

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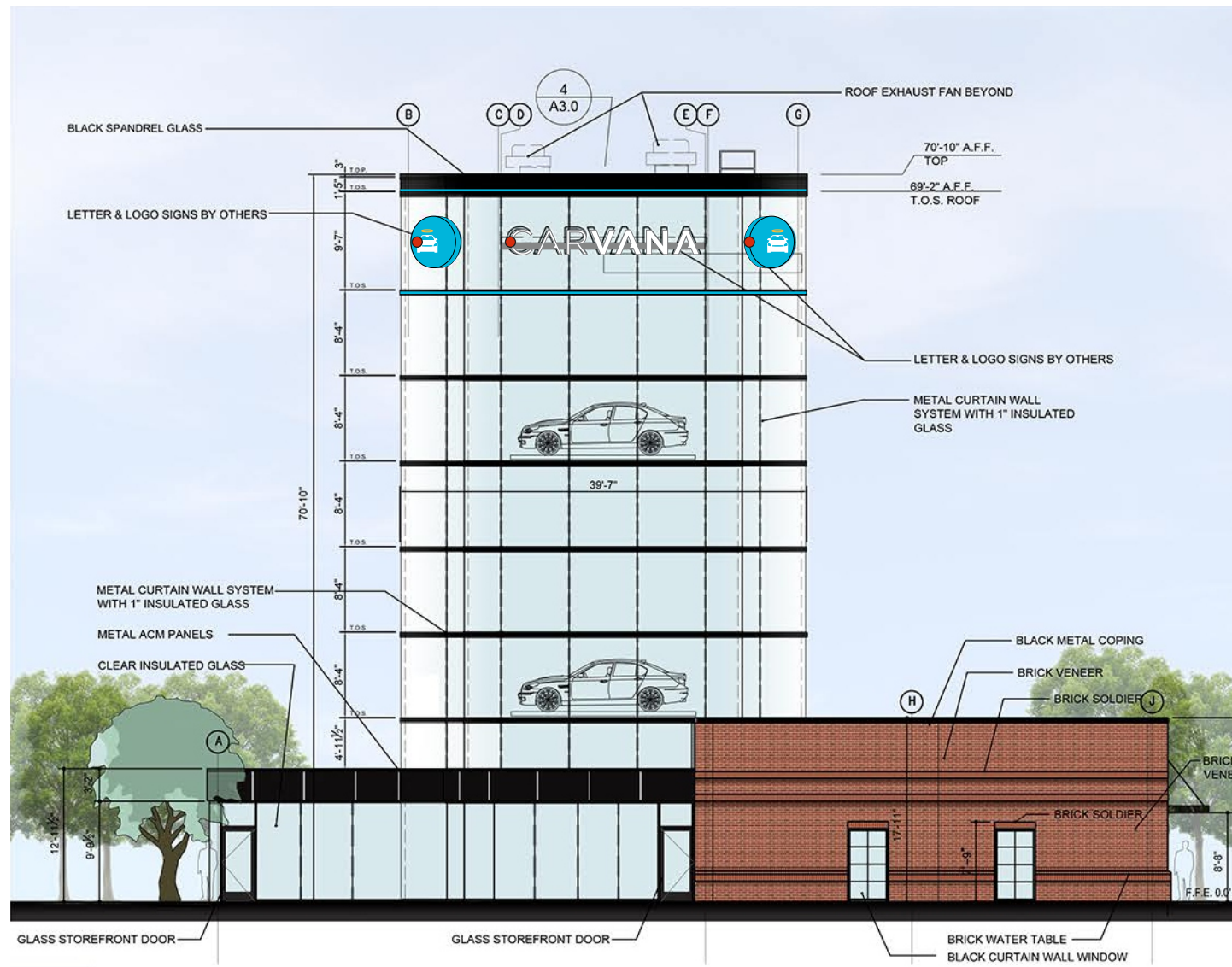


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**RIGHT ELEVATION**

**SCALE: 1/16" = 1'-0"**



**CARVANA**  
 43700 EXPO CENTER DRIVE  
 NOVI, MI 43375

**DRAWING NO.**  
 026869  
**RIGHT ELEV**

**DATE OF LAST CHANGE:**  
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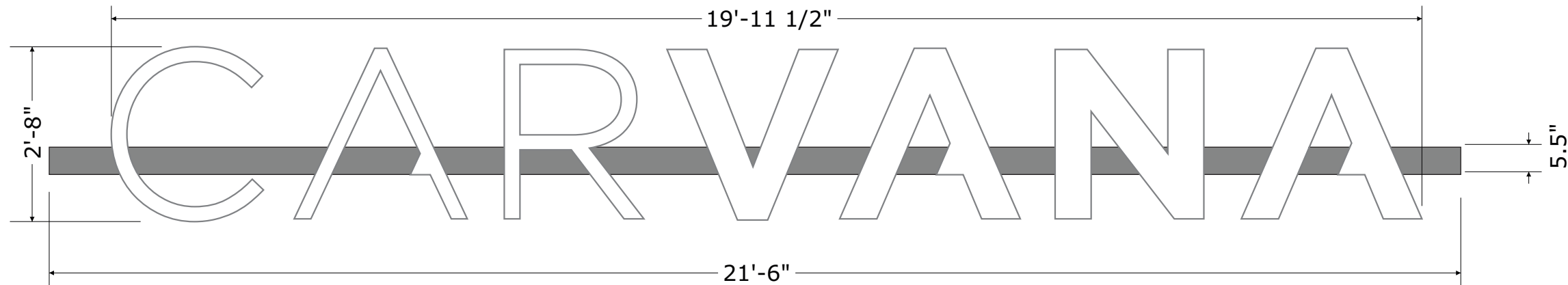
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 A. SCHEIBER  
**DESIGNER:**  
 THW  
**FILE LOC:**  
 NOVI, MI  
 \026869



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SIGN WEIGHT = 148lbs

**RACEWAY MOUNTED ILLUMINATED FACE LIT CHANNEL LETTERS**

(4) FOUR REQUIRED

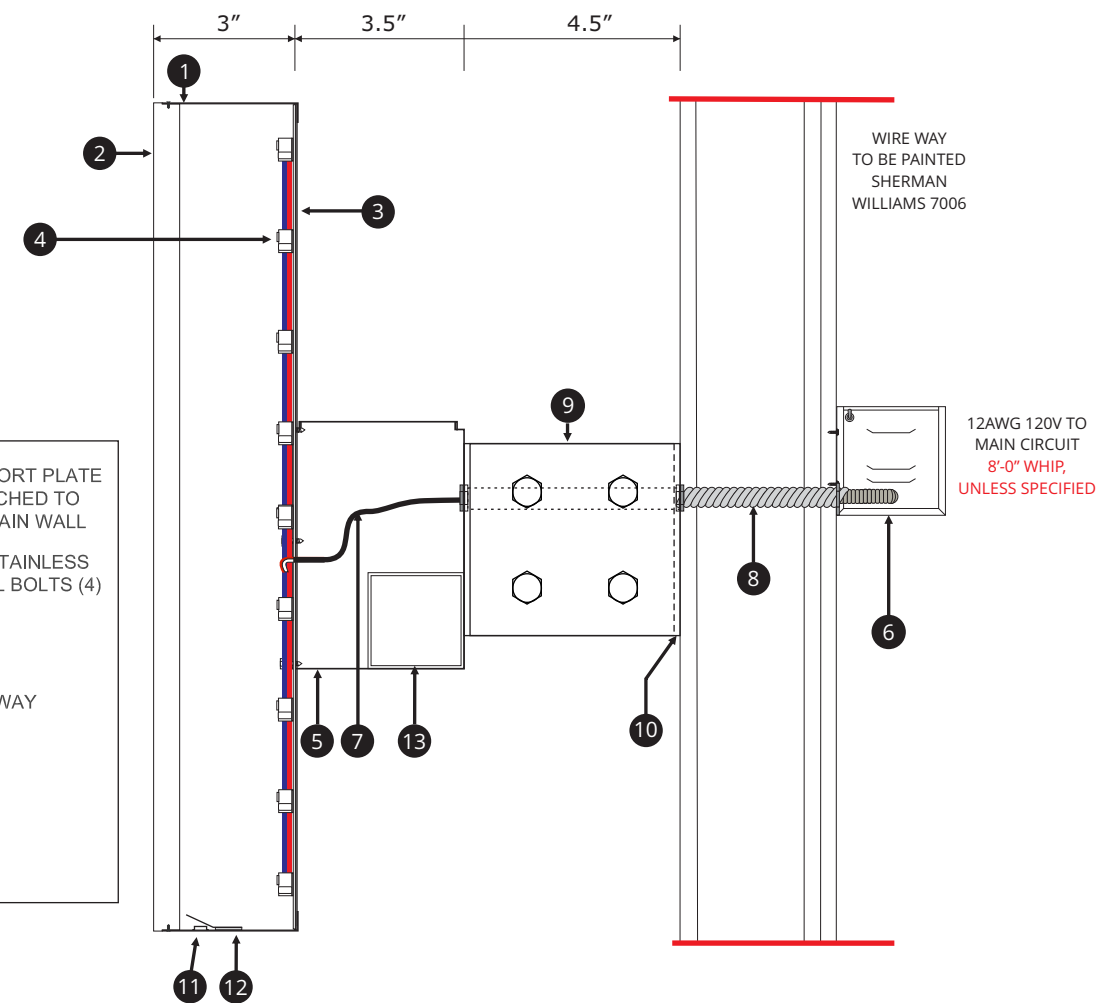
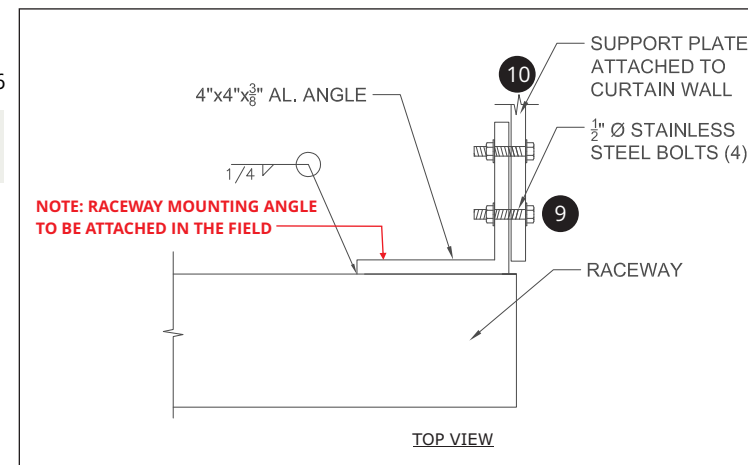
SCALE 1/2" = 1'-0"

57.33 SQUARE FEET

**SIGN DETAILS**

- 1 3" DEEP - .040 ALUMINUM RETURNS PAINTED TO MATCH PMS COOL GRAY 9 OUTSIDE / WHITE INSIDE
- 2 1/2" THICK CLEAR ACRYLIC WITH 1/8" THICK ROUTED LIP 3635-70 DIFFUSER ON SECOND SURFACE
- 3 .080 ALUMINUM BACKS PAINTED OUTSIDE PMS COOL GRAY 9/INSIDE WHITE
- 4 WHITE LED ILLUMINATION (QTY & PLACEMENT DETERMINED BY SIGN SIZE)
- 5 3.5" x 5.5" SIGNCOMP 1970N EXTRUDED ALUMINUM RACEWAY PAINT OUTSIDE TO MATCH PMS COOL GRAY 9
- 6 REMOTE 120V POWER SUPPLIES MOUNTED INSIDE UL LISTED/RECOGNIZED BOXES PAINT TO MATCH SHERMAN WILLIAMS 7006
- 7 UL LISTED/RECOGNIZED 18 AWG/2PLTC WIRING AND FIXTURES GOING TO POWER SUPPLIES
- 8 SIGN TO HAVE AN 20'-0" WHIP
- 9 MOUNTING HARDWARE TO BE 4"x4"x3/8" ALUMINUM ANGLE ATTACHED WITH FOUR 1/2" STAINLESS STEEL BOLTS
- 10 WALL CONSTRUCTION TO HAVE TWO SUPPORT PLATES ATTACHED TO CURTAIN WALL
- 11 WEEP HOLES
- 12 WEEP HOLE COVERS - .040" ALUMINUM SHAPED TO COVER WEEP HOLES INTERNALLY, AFFIXED WITH VHB TAPE
- 13 2"x 2" ALUMINUM SQUARE TUBE WELDED TO THE INSIDE OF THE RACEWAY FOR ADDED SUPPORT.

SW 7006  
Extra White



DRAWING NO.  
**026869**

SIGN NO.  
**1**

DATE OF LAST CHANGE:  
6/19/18

REVISION NO.  
-

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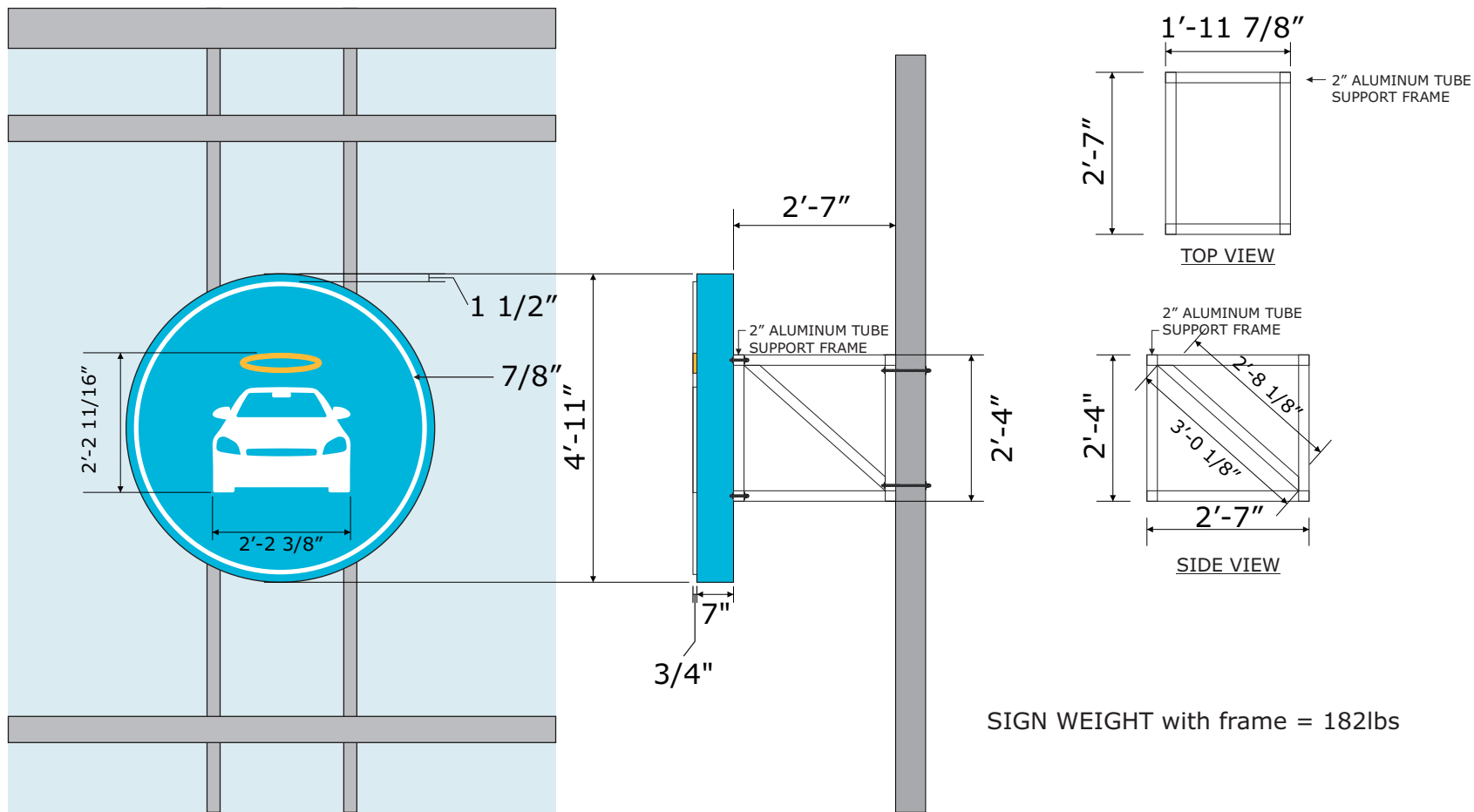
**CONSULTANT:**  
AW

**PROJECT MANAGER:**  
A. SCHEIBER

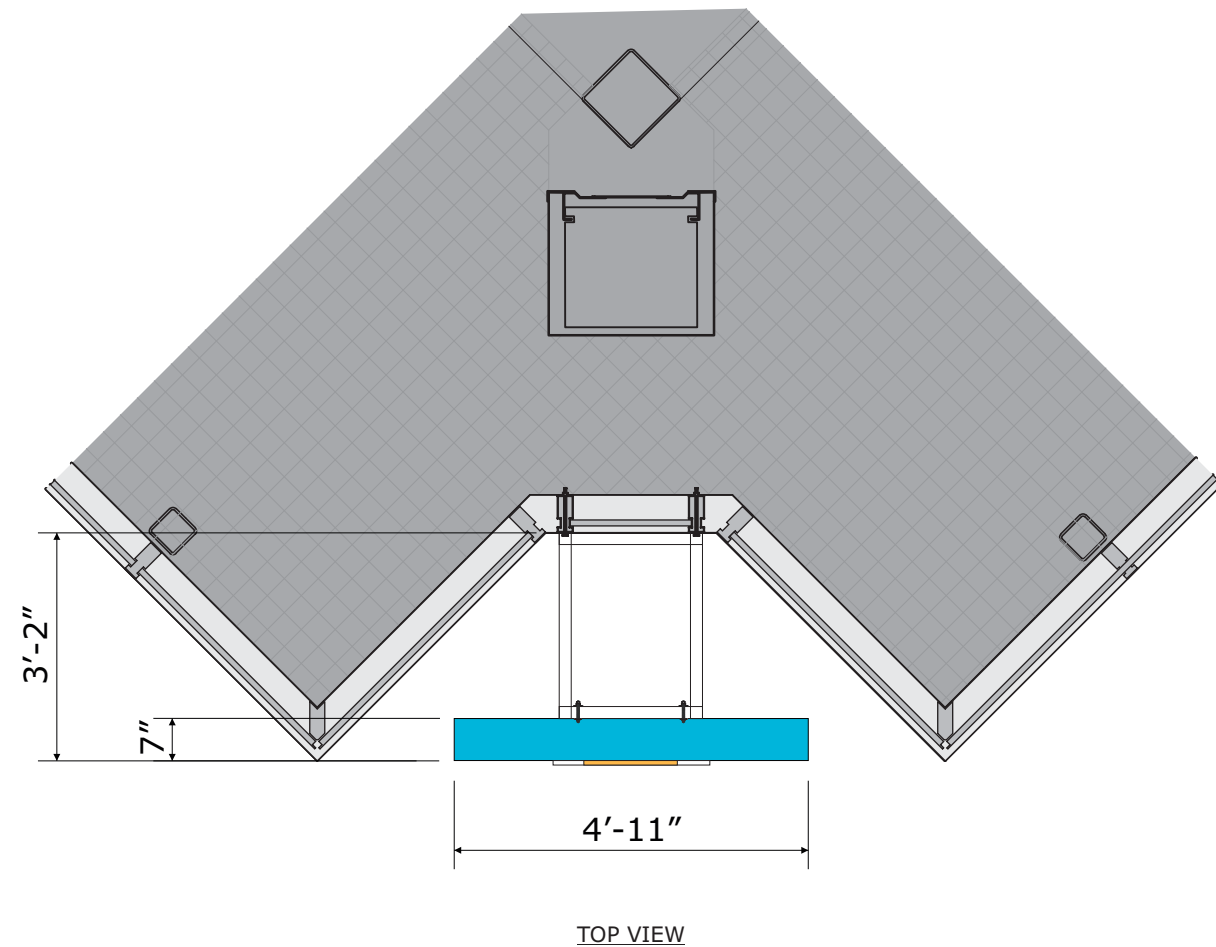
**DESIGNER:**  
THW

**FILE LOC:**  
NOVI, MI  
026869





SIGN WEIGHT with frame = 182lbs



**ILLUMINATED SIGN CABINET WITH ALUMINUM SQUARE TUBE FRAME.**

(4) FOUR REQUIRED

**SCALE: 3/8" = 1'-0"**

24.17 SQ. FT.

**SIGN DETAILS**

- 1 FACES TO BE ROUTED .090" ALUMINUM PAINTED TO MATCH PMS 638 C WITH 1" PUSH-THRU CLEAR ACRYLIC (1" W/.75" EXPOSED) GRAPHICS
- 2 3635-70 DIFFUSER ON SECOND SURFACE FOR WHITE RING & CAR GRAPHICS. "HALO" GRAPHIC TO HAVE SECOND SURFACE 3M 3630-075 MARIGOLD YELLOW VINYL
- 3 .090 ALUMINUM BACKS PAINTED TO MATCH PMS COOL GRAY 9 /INSIDE WHITE
- 4 .090" ALUMINUM SIGN CABINET WITH 1 1/4" RETAINER PAINTED TO MATCH PMS 638 C . 1/8" ALUMINUM MATCHPLATES
- 5 WHITE LEDS
- 6 2" x 2" ALUMINUM TUBE MOUNTING FRAME PAINT OUTSIDE TO MATCH COOL GRAY 9
- 7 REMOTE 120V POWER SUPPLIES MOUNTED INSIDE UL LISTED/RECOGNIZED BOXES
- 8 UL LISTED/RECOGNIZED 18 AWG/2PLTC WIRING AND FIXTURES GOING TO POWER SUPPLIES
- 9 SIGN TO HAVE AN 20'-0" WHIP
- 10 MOUNTING HARDWARE TO BE 3/8" STAINLESS STEEL THRU-BOLTS, NUTS, AND WASHERS TO MOUNT FRAME TO ALUMINUM CURTAINWALL FRAME MEMBERS  
3/8" STAINLESS STEEL THRU-BOLTS, NUTS, AND WASHERS TO MOUNT SIGN TO ALUMINUM FRAME
- 11 WALL CONSTRUCTION IS TO CURTAINWALL FRAMING



**CARVANA**  
43700 EXPO CENTER DRIVE  
NOVI, MI 43375

**DRAWING NO.**

**026869**

**SIGN NO.**

**2.0**

**DATE OF LAST CHANGE:**  
6/19/18

**REVISION NO.**

**-**

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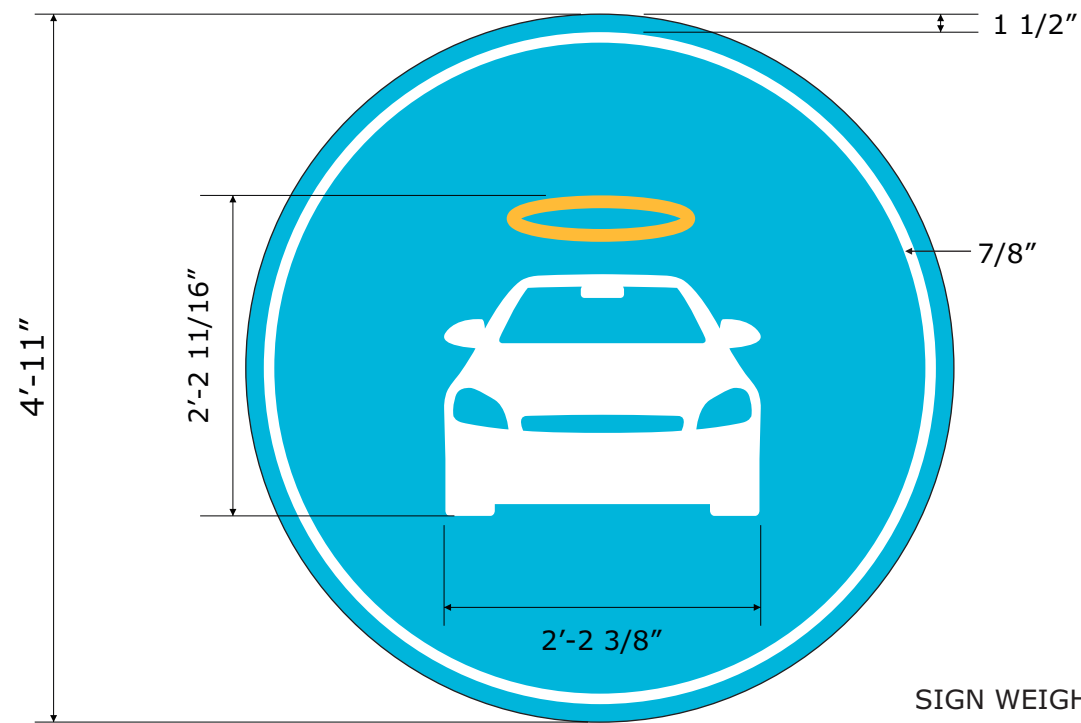
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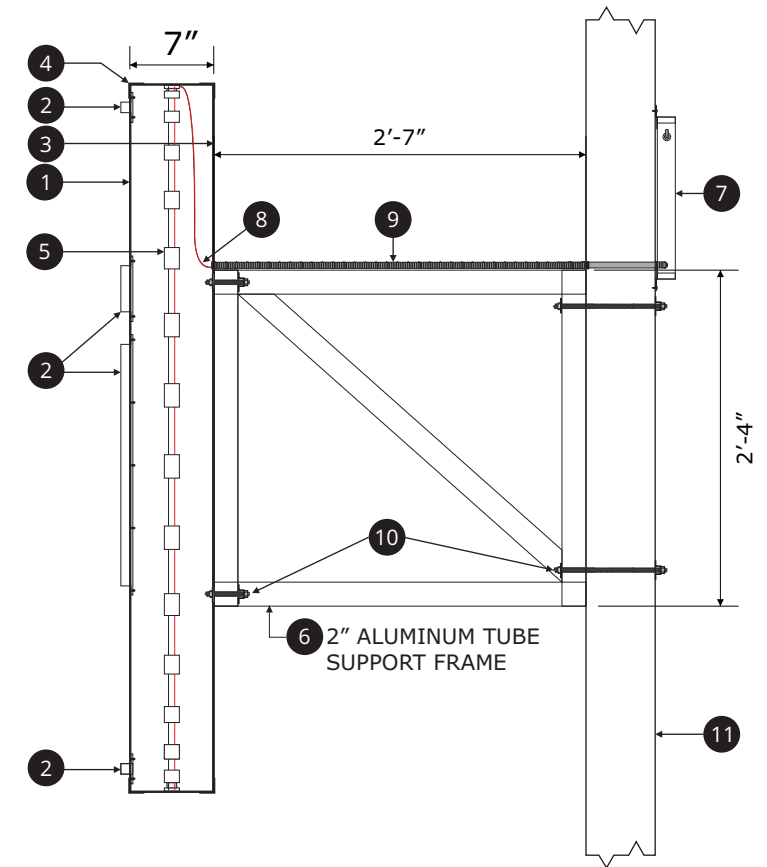
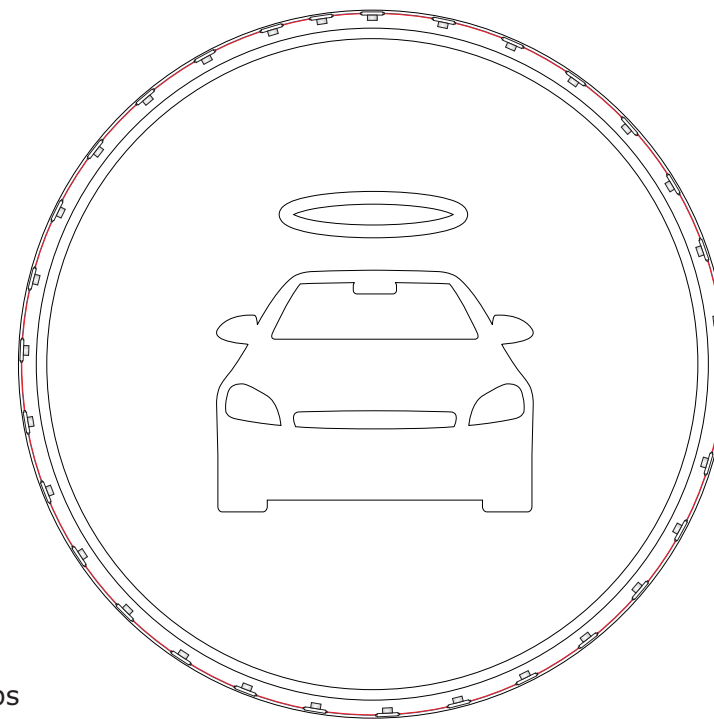
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SIGN WEIGHT with frame = 182lbs



**ILLUMINATED SIGN CABINET WITH ALUMINUM SQUARE TUBE FRAME.**

**SCALE: 3/4" = 1'-0"**

24.17 SQ. FT

**SIGN DETAILS**

- 1 FACES TO BE ROUTED .090" ALUMINUM PAINTED TO MATCH PMS 638 C WITH 1" PUSH-THRU CLEAR ACRYLIC (1" W/.75" EXPOSED) GRAPHICS
- 2 3635-70 DIFFUSER ON SECOND SURFACE FOR WHITE RING & CAR GRAPHICS. "HALO" GRAPHIC TO HAVE SECOND SURFACE 3M 3630-075 MARIGOLD YELLOW VINYL
- 3 .090 ALUMINUM BACKS PAINTED TO MATCH PMS COOL GRAY 9 /INSIDE WHITE
- 4 .090" ALUMINUM SIGN CABINET WITH 1 1/4" RETAINER PAINTED TO MATCH PMS 638 C . 1/8" ALUMINUM MATCHPLATES
- 5 WHITE LEDS
- 6 2" x 2" ALUMINUM TUBE MOUNTING FRAME PAINT OUTSIDE TO MATCH COOL GRAY 9
- 7 REMOTE 120V POWER SUPPLIES MOUNTED INSIDE UL LISTED/RECOGNIZED BOXES
- 8 UL LISTED/RECOGNIZED 18 AWG/2PLTC WIRING AND FIXTURES GOING TO POWER SUPPLIES
- 9 SIGN TO HAVE AN 20'-0" WHIP
- 10 MOUNTING HARDWARE TO BE 3/8" STAINLESS STEEL THRU-BOLTS, NUTS, AND WASHERS TO MOUNT FRAME TO ALUMINUM CURTAINWALL FRAME MEMBERS  
3/8" STAINLESS STEEL THRU-BOLTS, NUTS, AND WASHERS TO MOUNT SIGN TO ALUMINUM FRAME
- 11 WALL CONSTRUCTION IS TO CURTAINWALL FRAMING



**CARVANA**  
43700 EXPO CENTER DRIVE  
NOVI, MI 43375

**DRAWING NO.**  
026869  
**SIGN NO.**  
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**DATE OF LAST CHANGE:**  
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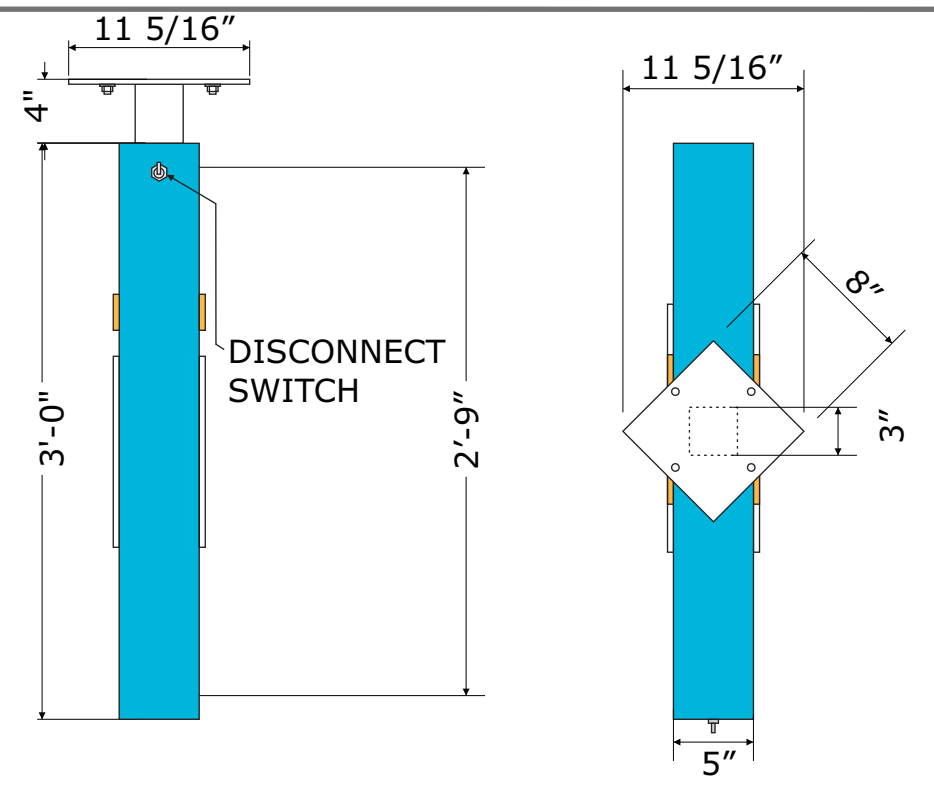
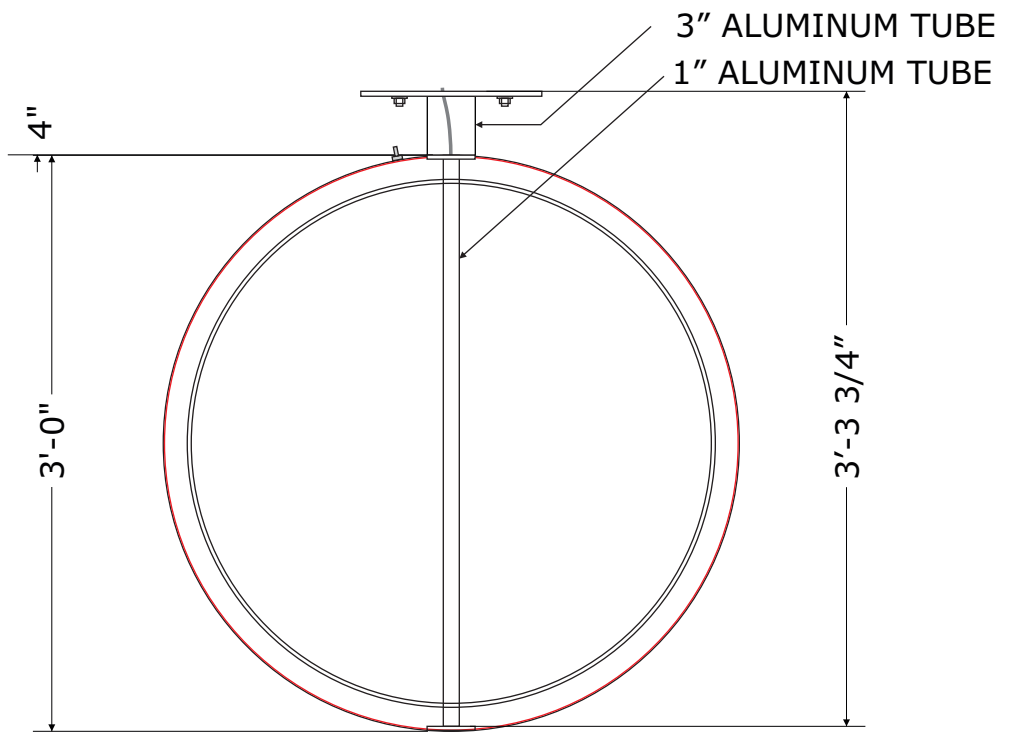
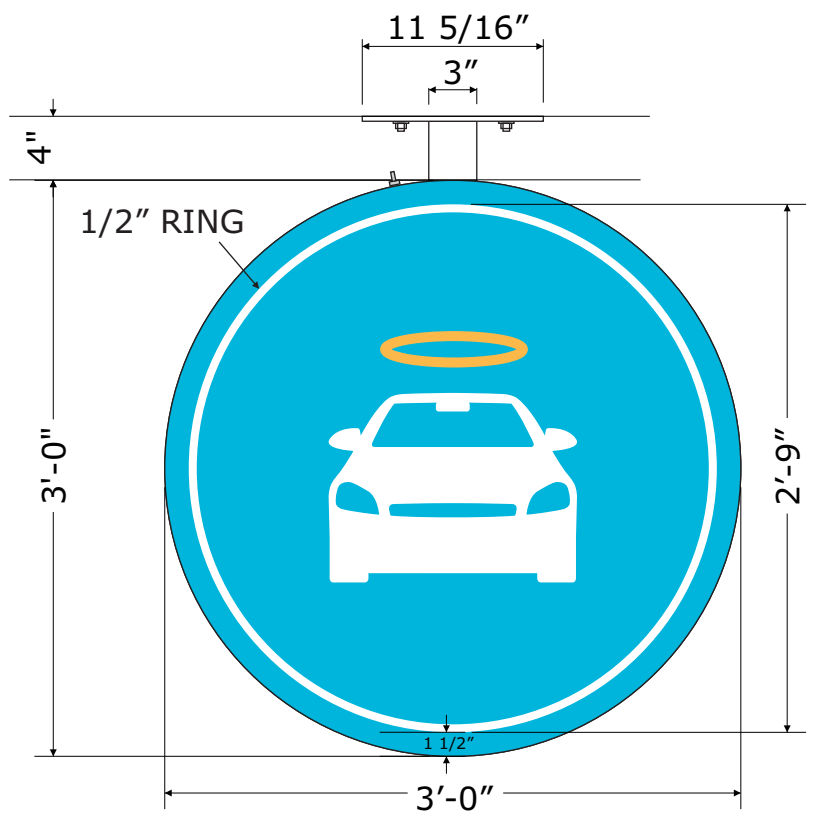
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**PROJECT MANAGER:**  
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**DESIGNER:**  
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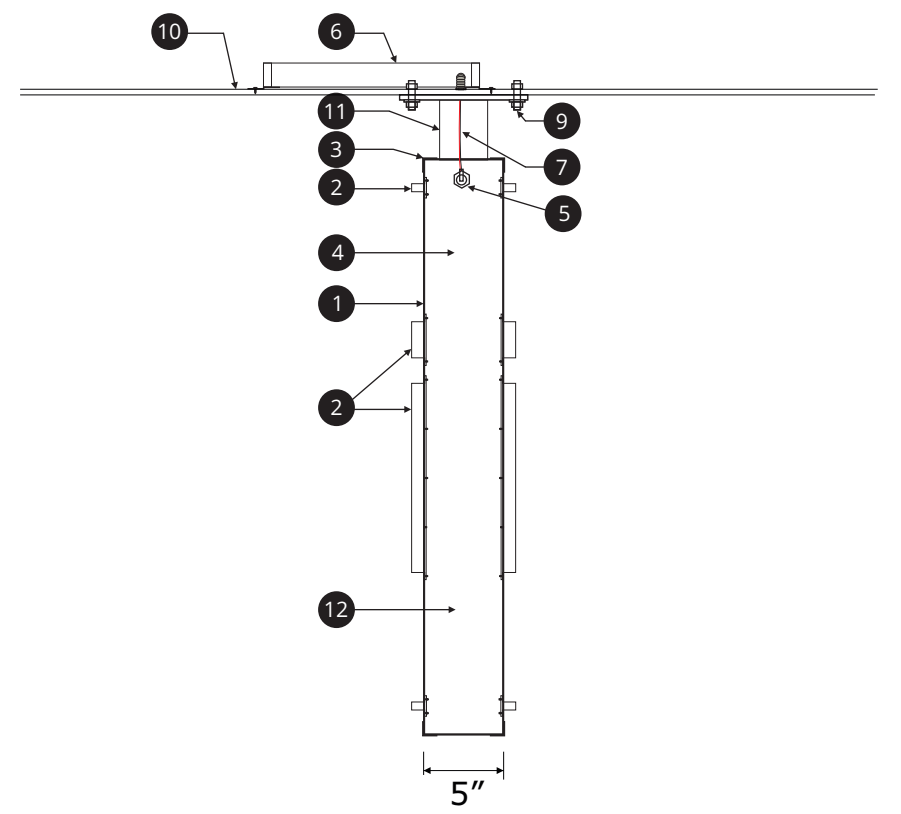


**UNDER CANOPY SIGN**

**SCALE: 1" = 1'-0"**  
9 SQUARE FEET

**SIGN DETAILS**

- 1 FACES TO BE ROUTED .090" ALUMINUM PAINTED TO MATCH PMS 638 C WITH 1" PUSH-THRU CLEAR ACRYLIC (1" W/.75" EXPOSED) GRAPHICS
- 2 3635-70 DIFFUSER ON SECOND SURFACE FOR WHITE RING & CAR GRAPHICS. "HALO" GRAPHIC TO HAVE SECOND SURFACE 3M 3630-075 MARIGOLD YELLOW VINYL
- 3 .090" ALUMINUM SIGN CABINET WITH 1 1/4" RETAINER PAINTED TO MATCH PMS 638 C .
- 4 WHITE LEDS
- 5 DISCONNECT SWITCH MOUNTED TO TOP OF SIGN
- 6 REMOTE 120V POWER SUPPLIES MOUNTED INSIDE UL LISTED/RECOGNIZED BOXES
- 7 UL LISTED/RECOGNIZED 18 AWG/2PLTC WIRING AND FIXTURES GOING TO POWER SUPPLIES
- 8 SIGN TO HAVE AN 8'-0" WHIP, **INSTALLER TO SPECIFY IF SPECIFIC LENGTH IS REQUIRED**
- 9 MOUNTING HARDWARE TO BE 3/8" STAINLESS STEEL THRU-BOLTS, NUTS, AND WASHERS TO MOUNT TO UNDERCANOPY
- 10 CEILING CONSTRUCTION IS TO BE DETERMINED BY SURVEY
- 11 3" SQUARE ALUMINUM TUBE
- 12 1" SQUARE ALUMINUM TUBE



**CARVANA**  
43700 EXPO CENTER DRIVE  
NOVI, MI 43375

**DRAWING NO.**  
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**SIGN NO.**  
3.0

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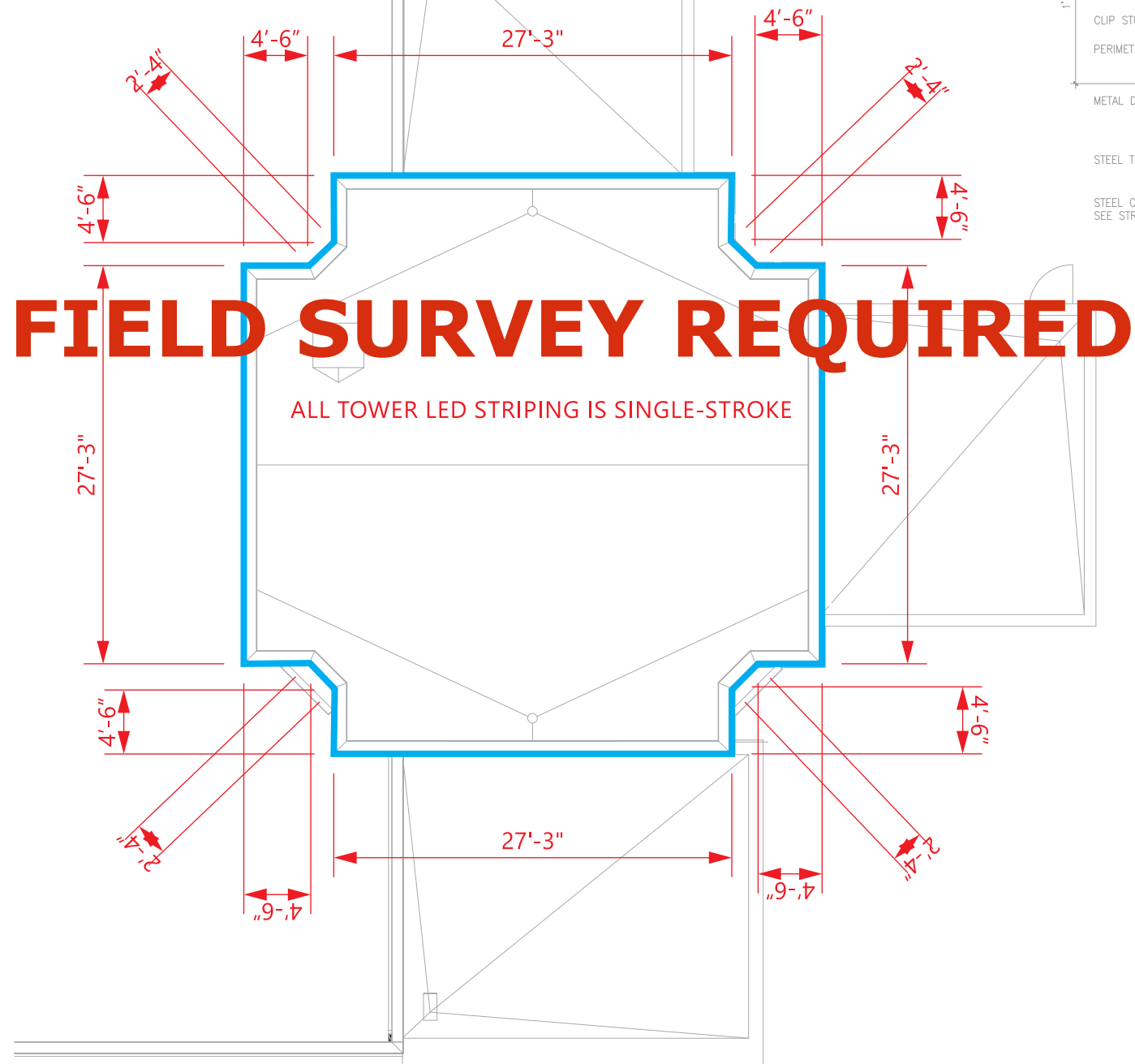
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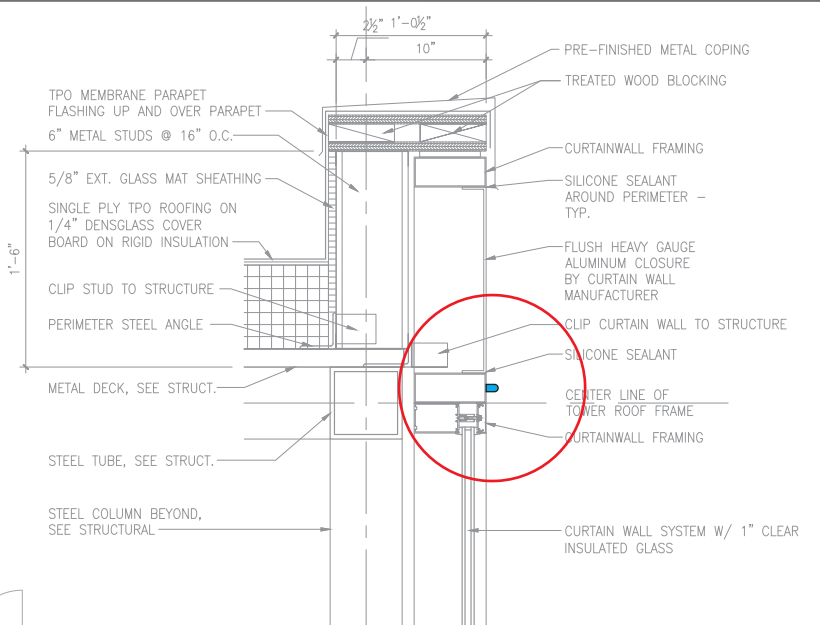
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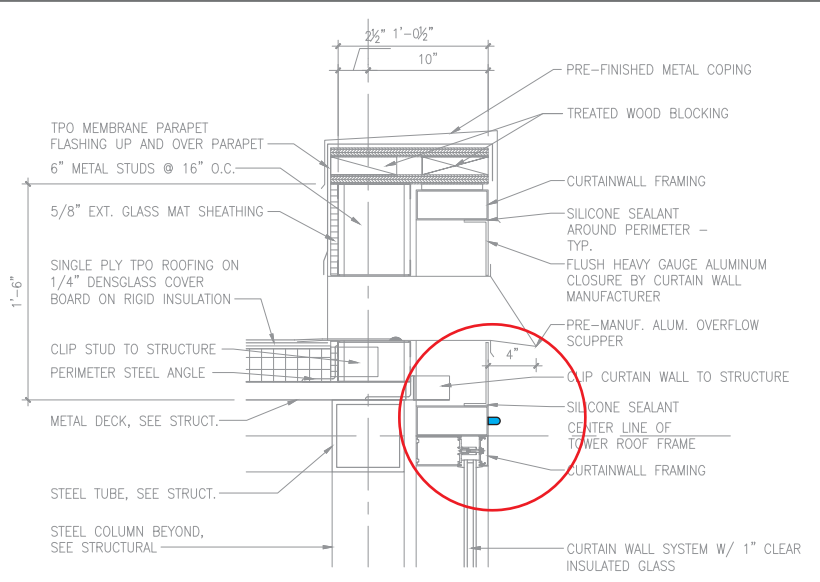


**FIELD SURVEY REQUIRED**

ALL TOWER LED STRIPING IS SINGLE-STROKE



**3**  
A1.6 TOWER\ PARAPET\ DETA  
SCALE - 3/4" = 1'-0"



**4**  
A1.6 TOWER\ SCUPPER\ DETA  
SCALE - 3/4" = 1'-0"

**VERIFY ALL MEASUREMENTS  
IN THE FIELD TO ASSURE ADEQUATE  
FOOTAGE TO COMPLETE WORK**

**BUILDING OUTLINE**

- LED-ILLUMINATED ARCHITECTURAL TUBING
- SPECIFIC BRAND TO BE DETERMINED

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43700 EXPO CENTER DRIVE  
NOVI, MI 43375

<b>DRAWING NO.</b> 026869	<b>DATE OF LAST CHANGE:</b> 6/19/18
<b>SIGN NO.</b> 4.0	<b>REVISION NO.</b> -

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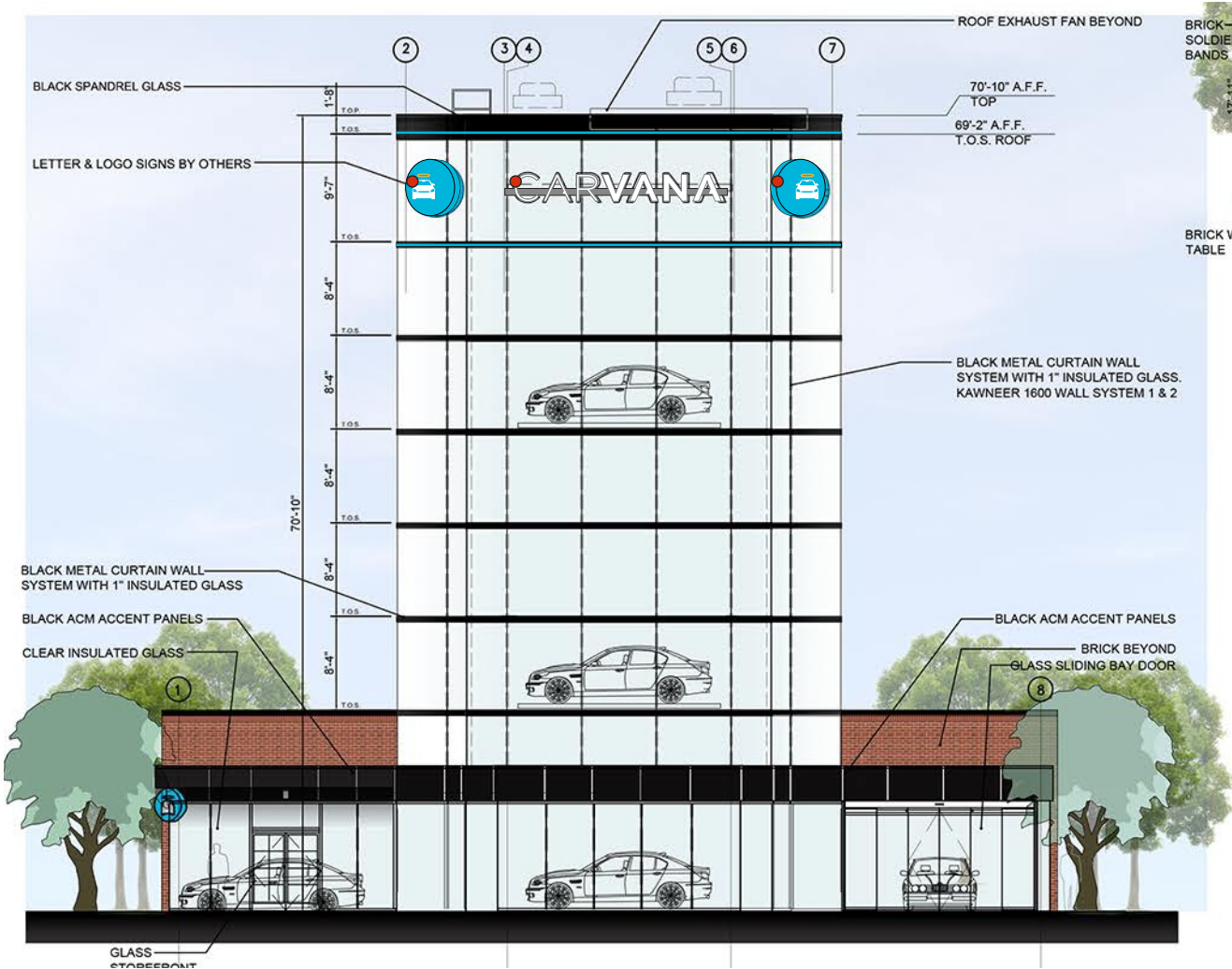
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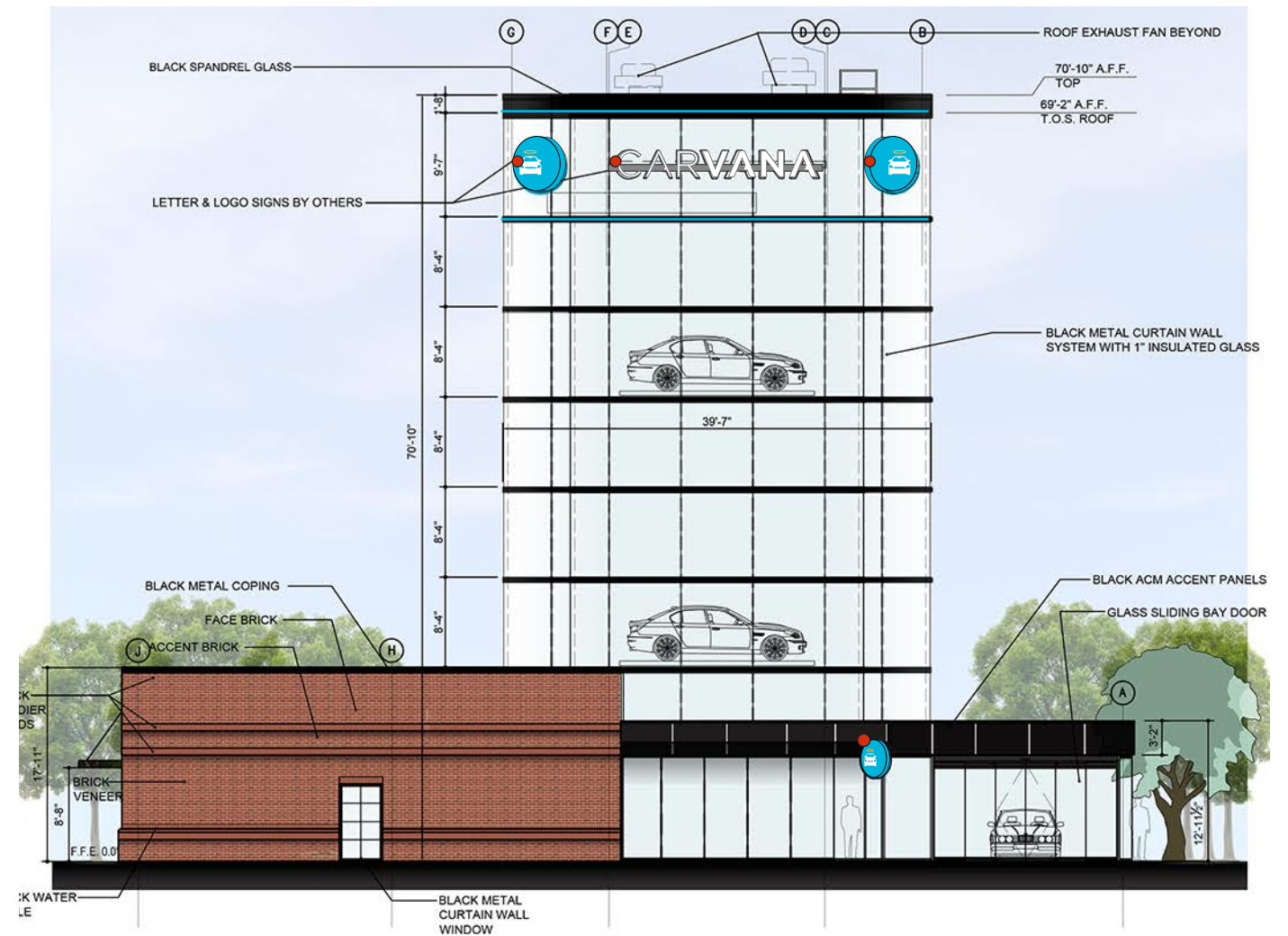
**FIELD SURVEY REQUIRED**



**FRONT ELEVATION**

SCALE: 1/16" = 1'-0"

**FIELD SURVEY REQUIRED**



**LEFT ELEVATION**

SCALE: 1/16" = 1'-0"

**BUILDING OUTLINE**

SCALE: 1/16" = 1'-0"

- LED-ILLUMINATED ARCHITECTURAL TUBING
- SPECIFIC BRAND TO BE DETERMINED



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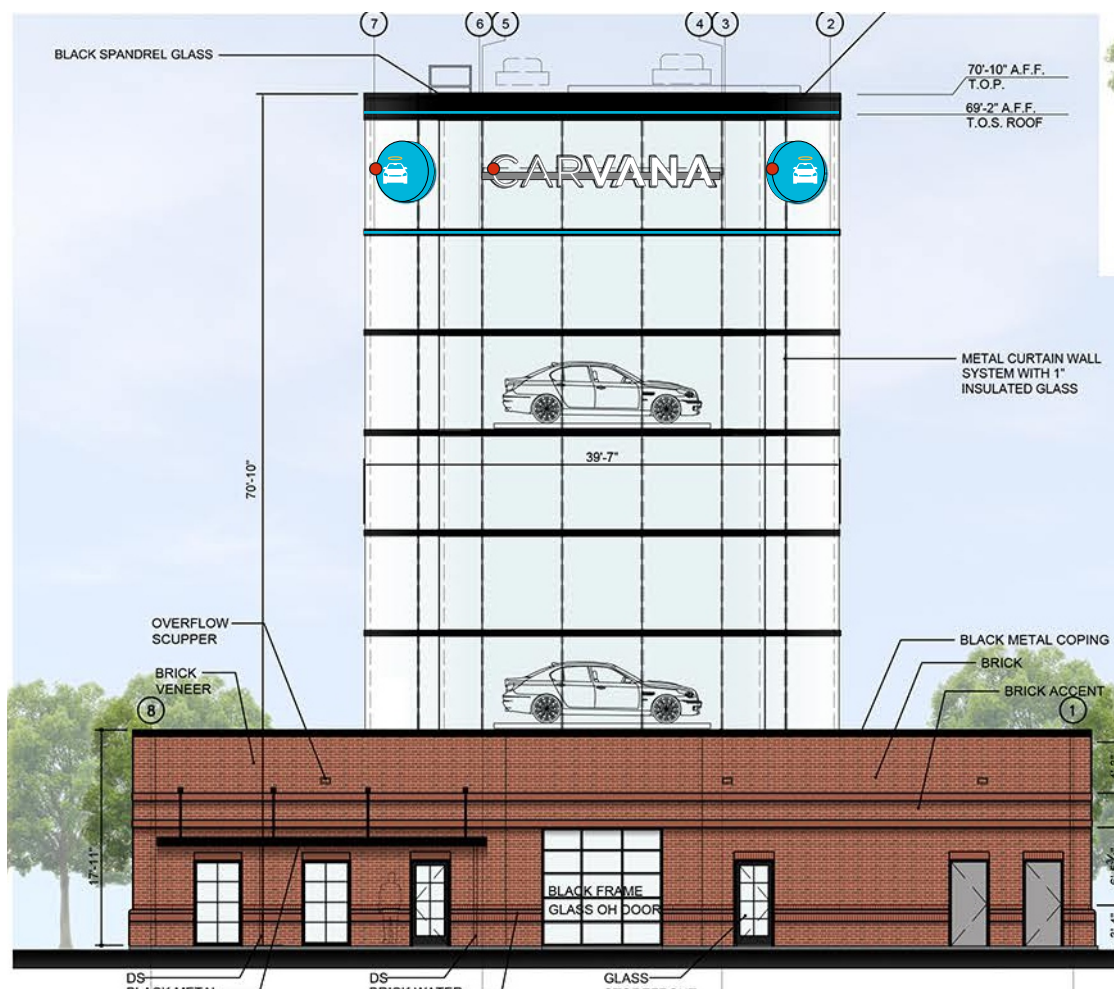
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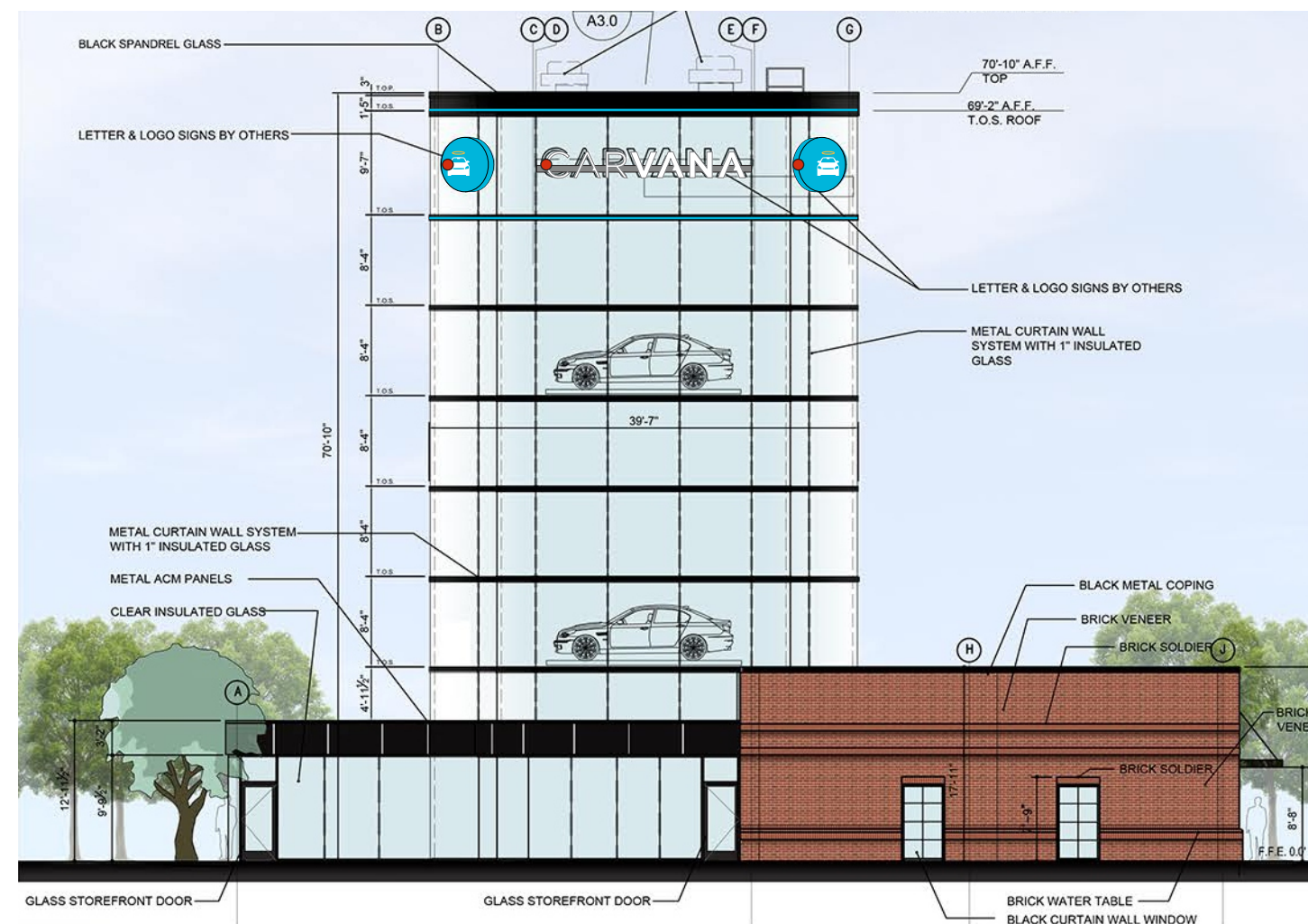
**FIELD SURVEY REQUIRED**



**REAR ELEVATION**

**SCALE: 1/16" = 1'-0"**

**FIELD SURVEY REQUIRED**



**RIGHT ELEVATION**

**SCALE: 1/16" = 1'-0"**

**BUILDING OUTLINE**

**SCALE: 1/16" = 1'-0"**

- LED-ILLUMINATED ARCHITECTURAL TUBING
- SPECIFIC BRAND TO BE DETERMINED



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**Section 3.1.25: Town Center Permitted Uses**

1 Purpose and Introduction

2 Definitions

3 Zoning Districts

4 Use Standards

5 Site Standards

6 Development Procedures

7 Admin and Enforcement

### A. INTENT

The TC, Town Center district is designed and intended to promote the development of a pedestrian accessible, commercial service district in which a variety of retail, commercial, office, civic and residential uses are permitted. Each use shall be complementary to the stated function and purpose of the district and shall not have adverse impact upon adjacent street capacity and safety, utilities, and other City services.

The TC Town Center district is further designed and intended to discourage the development of separate off-street parking facilities for each individual use, and to encourage the development of off-street parking facilities designed to accommodate the needs of several individual uses. Furthermore, it is recognized that uses which have as their principal function the sale or servicing of motor vehicles, such as automobile service establishments, car washes, or new and used motor vehicle sales or service establishments, and drive-in restaurants and restaurants with drive-through facilities, have a disruptive effect on the intended pedestrian orientation of the districts

**i** **User Note:** For uses listed in **bold blue**, refer to Article 4, or click on use, for use-specific standards

### B. PRINCIPAL PERMITTED USES

- i. **Retail businesses use** §4.78.3
- ii. Retail business service uses
- iii. **Dry cleaning establishments, or pick-up stations, dealing directly with the consumer** §4.24
- iv. Business establishments which perform services on the premises
- v. Professional services
- vi. Post office and similar governmental office buildings, serving persons living in the adjacent residential area
- vii. Off-street parking lots
- viii. Private clubs , fraternal organizations and lodge halls
- ix. **Places of worship** §4.10
- x. **Retail business** §4.27
- xi. **Service establishments of and office showroom or workshop nature** §4.27
- xii. **Restaurants (sit-down), banquet facilities or other places serving food or beverage** §4.27
- xiii. **Theaters, assembly halls, concert halls, museums or similar places of assembly** §4.27
- xiv. **Business schools and colleges or private schools operated for profit** §4.27
- xv. Offices and office buildings
- xvi. Municipal uses
- xvii. Indoor commercial recreation facilities
- xviii. Outdoor theaters, plazas, parks, public gathering places, including those along a river walk, and like public facilities

### B. PRINCIPAL PERMITTED USES (continued)

- xix. Hotels
- xx. **Financial institutions** §4.81
- xxi. **Residential dwellings** §4.82
- xxii. **Day care centers** and **adult day care centers** §4.12.2
- xxiii. Instructional centers
- xxiv. Other uses similar to the above uses subject to conditions noted
- xxv. **Accessory structures and uses** §4.19 customarily incidental to the above permitted uses

### C. SPECIAL LAND USES

The following uses shall be permitted by the City Council, following review and recommendation of the Planning Commission.

- i. **Open air business uses** §4.80.1
- ii. **Sale of produce and seasonal plant materials outdoors** §4.30
- iii. **Veterinary hospitals** or **clinics** §4.31
- iv. **Microbreweries** §4.35
- v. **Brewpubs** §4.35



4.74 PARKING FOR SALE OF NEW, UNLICENSED MOTOR VEHICLES AND PARKING OF LICENSED RENTAL AND LOANER MOTOR VEHICLES

In the P-1 district, parking for sale of new, unlicensed motor vehicles and parking of licensed rental and loaner motor vehicles, but not including junk or inoperable motor vehicles, partially dismantled or damaged motor vehicles, are permitted as a special land use without time limitation subject to the following conditions:

1. Motor vehicles parked pursuant to this section shall be limited to passenger vehicles (cars, vans, pick-up trucks and sports utility vehicles), and shall not include semi-trucks or trailers, step-vans or other commercial vehicles.
2. No car hauler delivery operations shall be allowed.
3. A landscaped berm shall be provided around all sides of a P-1 zoned site abutting a residential zoned district which shall comply with standards at [Section 3.14.5.E](#).
4. No dumpster or trash storage facility shall be permitted.
5. Night lighting shall be shielded from all adjacent residential zoned districts.
6. A noise impact statement is required subject to the standards of [Section 5.14.10.B](#).

4.75 CONFERENCE CENTERS

In the C district, conference centers, including meeting rooms and halls, conference rooms, banquet rooms, pre-function space, and catering uses comprising contiguous space are permitted as a special land use.

4.76 RETAIL SALE OF PRODUCTS OR SERVICES OCCURRING AS PART OF A SCHEDULED EXPOSITION FUNCTION

1. In the EXPO district, retail sale of products or services occurring as part of a scheduled exposition function are a permitted use, provided, a scheduled exposition function that involves the sale of products shall not be repeated more often than six (6) times within a calendar year.
2. In the EXO district, retail sale of products or services are permitted as a special land use when occurring as part of a scheduled exposition function occurring in an exposition facility .

4.77 I-1 USES IN THE EXPO DISTRICT

The listed uses in Section 3.1.14.B.xvii through xxvii are permitted uses in the EXPO district subject to the following:

1. All usage as an exposition facility ceases in the district;
2. Subject to the conditions required in [Section 3.14.1 through Section 3.14.5](#).

4.78 RETAIL USES, SERVICE USES AND RESTAURANT USES

1. In order to provide a limited amount of retail and personal service establishments to serve the employees of and visitors to the nearby office use areas, the following additional uses shall be permitted by the Planning Commission on a limited number of properties located in the OST district and EXO Overlay district as a Retail Service Overlay.
  - A. Retail uses, service uses and restaurant uses are permitted as a special land use subject to the following:
    - i. Generally recognized retail businesses and personal service establishments, as permitted in the B-1 district, [Section 3.1.10.B.i](#) and [3.1.10.B.ii](#), and other uses similar to the identified uses, subject to the limitations of [Section 3.19.5 and 3.19.7](#).
    - ii. Restaurants, including sit down, banquet facilities or other places serving food or beverage, except those having the character of a drive-in or having a drive-through window, subject to the limitations of [Section 3.19.7](#).
    - iii. Fast food drive-through restaurants shall be permitted only on properties meeting the requirements of subsection B, below subject to the limitations of subsection B.ii.d and [Section 3.19.7](#).

B. These above uses shall only be permitted on properties:

- i. Located within the areas designated "Office, Research, Development & Technology with Retail Service Overlay" on the Future Land Use Map in the City of Novi's Master Plan for Land Use; or



## 4.87 Unlisted Use Determinations

- 1 Purpose and Introduction
- 2 Definitions
- 3 Zoning Districts
- 4 Use Standards
- 5 Site Standards
- 6 Development Procedures
- 7 Admin and Enforcement

- e. Documented compliance with applicable local, state and national regulations including, but not limited to, all applicable safety, construction, environmental, electrical and communications. The MWET shall comply with Federal Aviation Administration (FAA) requirements, Michigan Airport Zoning Act, Michigan Tall Structures Act and any applicable airport overlay zone regulations.
- f. Proof of applicant's liability insurance.
- g. Evidence that the utility company has been informed of the customer's intent to install an interconnected, customer-owned generator and that such connection has been approved. Off-grid systems shall be exempt from this requirement.
- h. A written description of the anticipated life of each MWET; the estimated cost of decommissioning; the method of ensuring that funds will be available for decommissioning and site restoration; and removal and restoration procedures and schedules that will be employed if the MWET(s) become inoperative or non-functional.
- i. The applicant shall submit a decommissioning plan that will be carried out at the end of the MWET's useful life, and shall describe any agreement with the landowner(s) regarding equipment removal upon termination of the lease.
- j. The proposed plan shall conform to the requirements of [Section 6.1](#) of the Zoning Ordinance: Site Plan Review (All Districts).
- vi. Certification and Compliance. The City must be notified of a change in ownership of a MWET or a change in ownership of the property on which the MWET is located.
- E. Temporary Uses Related to Wind Energy Turbines. The following is permitted in all zoning districts as a temporary use, in compliance with the provisions contained herein, and the applicable WET regulations.

- i. Anemometers
  - a. The construction, installation or modification of an anemometer tower shall require a building permit and shall conform to all applicable local, state and federal safety, construction, environmental, electrical, communications and FAA requirements.
  - b. An anemometer shall be subject to the minimum requirements for height, setback, separation, location, safety requirements and decommissioning that correspond to the size of the WET that is proposed to be constructed on the site.
  - c. An anemometer shall be permitted for no more than thirteen (13) months for a SSMWET, STMWET or MWET.

**4.87 UNLISTED USE DETERMINATIONS**

Where a proposed use of land or use of a building is not contemplated or specified by this Ordinance, or where the Planning Division has a question as to the appropriateness of a use that involves other features that were not contemplated or specified by this Ordinance, the Planning Division shall request a determination from the City Council, after review and recommendation from the Planning Commission, as to what district or districts, if any, in which the proposed use may be appropriate as a special land use.

In acting upon the request, the City Council shall take into consideration the spirit, purpose and intent of the Ordinance and the Master Plan for Land Use. If the City Council determines that:

1. Such use does not appear to be expressly authorized in the zoning ordinance as a principal permitted use or a principal use permitted subject to special conditions,
2. Such use does not appear to have been contemplated by this Ordinance as a principal permitted use or a principal permitted use subject to special conditions, or



- 1 Purpose and Introduction
- 2 Definitions
- 3 Zoning Districts
- 4 Use Standards
- 5 Site Standards
- 6 Development Procedures
- 7 Admin and Enforcement

- e. Documented compliance with applicable local, state and national regulations including, but not limited to, all applicable safety, construction, environmental, electrical and communications. The MWET shall comply with Federal Aviation Administration (FAA) requirements, Michigan Airport Zoning Act, Michigan Tall Structures Act and any applicable airport overlay zone regulations.
- f. Proof of applicant's liability insurance.
- g. Evidence that the utility company has been informed of the customer's intent to install an interconnected, customer-owned generator and that such connection has been approved. Off-grid systems shall be exempt from this requirement.
- h. A written description of the anticipated life of each MWET; the estimated cost of decommissioning; the method of ensuring that funds will be available for decommissioning and site restoration; and removal and restoration procedures and schedules that will be employed if the MWET(s) become inoperative or non-functional.
- i. The applicant shall submit a decommissioning plan that will be carried out at the end of the MWET's useful life, and shall describe any agreement with the landowner(s) regarding equipment removal upon termination of the lease.
- j. The proposed plan shall conform to the requirements of [Section 6.1](#) of the Zoning Ordinance: Site Plan Review (All Districts).
- vi. Certification and Compliance. The City must be notified of a change in ownership of a MWET or a change in ownership of the property on which the MWET is located.
- E. Temporary Uses Related to Wind Energy Turbines. The following is permitted in all zoning districts as a temporary use, in compliance with the provisions contained herein, and the applicable WET regulations.

- i. Anemometers
  - a. The construction, installation or modification of an anemometer tower shall require a building permit and shall conform to all applicable local, state and federal safety, construction, environmental, electrical, communications and FAA requirements.
  - b. An anemometer shall be subject to the minimum requirements for height, setback, separation, location, safety requirements and decommissioning that correspond to the size of the WET that is proposed to be constructed on the site.
  - c. An anemometer shall be permitted for no more than thirteen (13) months for a SSMWET, STMWET or MWET.

**4.87 UNLISTED USE DETERMINATIONS**

Where a proposed use of land or use of a building is not contemplated or specified by this Ordinance, or where the Planning Division has a question as to the appropriateness of a use that involves other features that were not contemplated or specified by this Ordinance, the Planning Division shall request a determination from the City Council, after review and recommendation from the Planning Commission, as to what district or districts, if any, in which the proposed use may be appropriate as a special land use.

In acting upon the request, the City Council shall take into consideration the spirit, purpose and intent of the Ordinance and the Master Plan for Land Use. If the City Council determines that:

1. Such use does not appear to be expressly authorized in the zoning ordinance as a principal permitted use or a principal use permitted subject to special conditions,
2. Such use does not appear to have been contemplated by this Ordinance as a principal permitted use or a principal permitted use subject to special conditions, or



**Excerpt from Original Vehicle Dealer License Application Instruction  
Booklet for Class B License**

# READ THIS FIRST!

## *ORIGINAL VEHICLE DEALER LICENSE APPLICATION*

## INSTRUCTION BOOKLET

### CONTACT US:

Michigan Department of State  
Traffic Safety Programs Division  
Business Licensing Section  
Lansing, MI 48918

Telephone: (888) SOS – MICH  
(888) 767 – 6424

Fax: (517) 335 – 2810

E-Mail: [licensing@michigan.gov](mailto:licensing@michigan.gov)



## 5. LICENSE CLASSIFICATIONS

Indicate the license classification(s) for which you are applying.

A dealer license may be issued in one or more classifications (classes), depending on the business activity performed.

The Michigan Vehicle Code has different requirements for each class of dealer. The following descriptions explain what each class of dealer is permitted to do, and which classes are compatible.

### **Class A (New Vehicle Dealer)**

Allows dealer to buy and sell new vehicles under a franchise agreement or bonafide contract with a new vehicle manufacturer.

Class A is compatible with B, C or R, and E classes.

### **Class B (Used or Secondhand Vehicle Dealer)**

Allows dealer to buy and sell used vehicles to and from the general public and other licensed vehicle dealers.

Class B is compatible with A, C or R, and E.

### **Class C (Used Vehicle Parts Dealer) and Class R (Automotive Recycler)**

Allows dealer to buy or otherwise acquire late model major component parts for resale, either at wholesale or at retail, and/or acquire vehicles to dismantle for the resale of their parts, selling the remains as scrap. These are the only classes which can legally buy late model distressed vehicles (salvage or scrap vehicles), or late model major component parts from insurance companies, or through auctions, brokers, or salvage pools in Michigan.

Note: A Class C or Class R dealer may only be represented at an auction, broker, or salvage pool by its own **Licensed Salvage Vehicle Agent**. Contact the Business Licensing Section to obtain a Salvage Vehicle Agent application.

Class C and Class R are compatible with A, B, and E; or E and F.

Class C and Class R are NOT COMPATIBLE with one another, or with Class D or Class G.

## 7. BUSINESS DAYS AND HOURS

List the specific days and hours the business will be open. Class A and Class B dealers are required to maintain a minimum of 30 business hours per week.

All other classifications of dealers must maintain a minimum of 4 consecutive business hours per week.

If you change your business hours, you must notify the Business Licensing Section in writing, by fax at (517) 335-2810, or by e-mail at [licensing@michigan.gov](mailto:licensing@michigan.gov).

## 8. OWNERS, PARTNERS, CORPORATE OFFICERS, DIRECTORS

List the full name, home address, social security number and birthdate for all owners, partners, corporate officers, members and directors.

For corporations, “owners” also includes any stockholder holding 25% or more of the stock issued.

Limited Liability Companies must include information for all managers.

If the owner, partner or manager listed on the application form is itself a corporation, partnership or limited liability company, the individuals who are the owners, partners, corporate officers, directors, managers, or stockholders or members holding 25% or more of the stock issued for the company listed in Item 8 must be disclosed. Use a separate sheet to provide the full name, home address, home telephone and birthdate for each individual.

Each individual listed in Item 8 must have fingerprints on file with this department. Individuals disclosed under a corporation, partnership or limited liability company listed as a licensee who will have direct impact on the operation of the business must also have fingerprints on file. Please refer to the instructions for completing fingerprint cards for more details.

## 9. SERVICING FACILITY REQUIREMENT (Classes A and B only)

New and used vehicle dealers are required to provide proper servicing facilities. The servicing facility must be located within 10 miles of the dealership’s established place of business. **This requirement applies even if vehicles are sold “AS IS” or wholesale.**

This requirement may be met in one of two ways:

1. Register as a motor vehicle repair facility. A Motor Vehicle Repair Facility Registration Application is enclosed. It is not necessary to register again if the business is currently registered and the ownership has not changed.
2. Enter into an agreement with a currently registered motor vehicle repair facility.

An agreement form is enclosed. Make sure the agreement shows the repair facility registration number and the repair facility **owner's** signature.

## 10. BUSINESS LOCATION DESCRIPTION

Check the appropriate boxes.

A dealer's established place of business must meet all applicable zoning and municipal requirements.

**For a Class A and Class B dealer**, an established place of business must include the following:

- The premises must contain a permanently enclosed building or structure either owned, leased, or rented by a dealer, which is not a residence, tent, temporary stand, or any temporary quarters;
- The building or structure is required to be continuously occupied in good faith for the purpose of selling, buying, trading, leasing, or otherwise dealing in motor vehicles;
- All books, records, and files necessary to conduct the business of a Class A or class B dealer must be maintained in the building or structure;
- A building or structure housing an office of at least 150 square feet in size, equipped with standard office furniture, working utilities, a working restroom, and a working telephone listed in the name of the business on the dealer's license;
- Land space of no less than 1,300 square feet to accommodate the display of a minimum of 10 vehicles of the kind and type that the dealer is licensed to sell and an additional 650 square feet for customer parking. The display and customer parking areas must be adequately surfaced and well lit during business hours;

- An exterior sign displaying the name of the dealership that is permanently affixed to the building or land with letters clearly visible from a highway identifies the premises;
- Conspicuous posting of the dealer's regular hours of operation. The posted hours must be not less than 30 hours per week;
- The premises must contain a registered repair facility on site for the repair and servicing of motor vehicles of a type sold at the established place of business, unless the dealer has entered into a written servicing agreement with a registered repair facility at a location not to exceed 10 miles' distance from the established place of business. If repairs are conducted pursuant to a servicing agreement, the servicing agreement must be conspicuously posted in the office;

A dealer may not combine space and resources with another dealer to meet the minimum established place of business requirements for a new or used vehicle dealer. Each licensed new or used dealer must meet the established place of business requirements separately.

**For a Class W dealer**, an established place of business must include the following:

- The premises shall contain a permanently enclosed building or structure that is either owned, leased, or rented by a wholesaler, which is not a commercial mailbox, tent, temporary stand, or other temporary quarters.
- All books, records, and files necessary to conduct the business of the wholesaler shall be maintained in the building or structure described in the above paragraph.
- The premises shall not be used for the display of vehicles. However, the premises may be used for the storage of vehicles purchased by the wholesaler prior to sale to a licensed vehicle dealer.
- The premises shall be identified by an exterior sign displaying the name of the wholesaler that is permanently affixed to the building or land with letters clearly visible from the roadway.

**For other dealer classes** an established place of business means the place actually occupied either continuously or at regular periods where books and records are kept and a large share of business is transacted.

Estimate the greatest number of vehicles you will have in inventory at one time.

APPLICANT RESPONSE LETTER  
(UNLISTED USE DETERMINATION)



CARVANA

July 3, 2018

Sri Ravali Komaragiri, Planner  
City of Novi, Planning Department  
45175 Ten Mile Road  
Novi, MI 48375

RE: Unlisted Use Determination for Carvana as a 'Vending Machine Fulfillment Center'

Dear Ms. Komaragiri,

Carvana is excited to be working with the City of Novi in Michigan as part of the Adell Center Project. This letter is to provide you and City Staff with supplemental information as requested regarding use classification, operational use, and customer traffic to the site.

#### USE SUMMARY

1. Addressing item #5. Please clarify how many customers visit the site as an average daily basis. **Depending on metro size and market maturity, customer visits to the site can vary from 6 to 15 customer trips per day.**
2. Addressing #8. Staff is unable to determine whether a dedicated loading zone is proposed. **A loading/unloading zone will be proposed north of our building and south east of the cul-de-sac. The loading/unloading zone will be displayed on our site plan application that is forth coming.**
3. Addressing #10. The application is asked to provide a copy of the traffic report for further information about trip generation during peak hours and also to determine the minimum parking requirements. **A traffic statement has not been prepared for this specific site in Novi, MI at this time. Using a traffic impact study that was prepared for a different location as an example for this site would not be appropriate. Each market varies in size and therefore a previously conducted traffic study would not accurately represent the amount of traffic we expect to receive at this site in Novi, MI.**

#### ARCHITECTURE

Carvana has designed this building to fit more in line with the façade requirements provided by City of Novi. The occupied building structure material would be comprised of brick, ACM panels and clear glass. The proposed building with brick material is being constructed in three other markets at this time. We developed our first Vending Machine Fulfillment Center in 2013 in Nashville, TN and the Carvana Fulfillment Center is up and running in 12 locations and continuously expanding Fulfillment Center numbers in 2018 and 2019. Carvana is ranked number 34 in Inc. 5000's list of America's fastest growing companies.



CARVANA

### **ORDINANCE REQUIREMENTS**

1. Carvana is requesting a height deviation from the ordinance. The tower attached to the building is an 8 tier tower that is up to 75 feet.
2. A sign plan and sign permit application have been provided for review.
3. Proposed parking for the Carvana Vending Machine Fulfillment Center is 35 to 40 parking spaces. On average from our existing sites, 35 to 40 spaces accommodate for our operational needs at the Fulfillment Center. These parking spaces accommodate for the parking needs of employees, visitors picking up their pre-purchased vehicle, and cars which have been delivered to the site but have not yet been loaded into the car tower.

A number of 3-5 employees may be on site during work hours to coordinate customer pick-ups, manage hauler delivery to the site and assist all visitors on site during a customer pick-up. Visitors are welcomed to the site by appointment only to pick-up their pre-purchased vehicle.

An outdoor display area will not be provided on site since Carvana does not advertise for sale vehicles on the Property. Only vehicles purchased for pick-up will be transported into the vending machine. Cars that are brought back to the facility from a customer return may be placed temporarily in an open parking space until a hauler is available for a pick-up.

While all parking spaces may not be used initially, Carvana proposes 35-40 parking spots on site to accommodate for future business growth.

4. Lighting on the property will include interior and exterior lighting. The tower attached to the building will also include external lights for proper display. It is worth noting that the lights used are not intended to produce light pollution and are pointed inward toward the cars stored in the tower. A copy of a photometric plan is provided for reference of the foot candle measurements.

### **IDENTIFIED USE AT OTHER COMMUNITIES**

Each jurisdiction Carvana is in either permitted our use or helped us obtain approval for our use. Due to the uniqueness of each municipalities zoning code, city staff has always given us a clear path toward development in order to receive proper entitlements for the Carvana Vending Machine Fulfillment Center.



CARVANA

#### ADDITIONAL INFORMATION

1. Pre-purchased cars are delivered to the Carvana facility. The company has an inventory of over 7,500 vehicles. Where are they located?

**Cars are stored across the nation in our Inspection Centers. We currently have four locations in:**

1. Tolleson, AZ
2. Blue Mound, TX
3. Winder, GA
4. Delanco, NJ

2. Is there a copy of the floor plan available?

**A copy of the floor plan is included as part of this response letter.**

3. Are service/oil changes for customers offered on site?

**There are no auto service repair and oil changes available on site. Carvana does not participate in repair services at the Vending Machine Fulfillment Center. Additionally, there are no certified mechanics employee on site or any equipment for auto repair.**

4. Is there a copy of the sign permit application?

**A copy of the sign permit application is included as part of this response letter.**

#### CONCLUSION

Please consider this as our letter of explanation for the requested information that City of Novi is seeking. Please don't hesitate to reach out to me regarding any additional information you may need. Thank you for your time and consideration regarding Carvana's Project in the City of Novi.

Thank You,

Arwa Lulu, Entitlements Associate

Carvana, LLC

[Arwa.Lulu@carvana.com](mailto:Arwa.Lulu@carvana.com)

480-244-3900